



PLANNING COMMISSION

Agenda

Regular Meeting

June 5, 2024, 5 PM

Council Chamber
1200 Carlsbad Village Dr.
Carlsbad, CA 92008
carlsbadca.gov

Welcome to the Planning Commission Meeting

We welcome your interest and involvement in the city's legislative process. This agenda includes information about topics coming before the Planning Commission and the action recommended by city staff. You can read about each topic in the staff reports, which are available on the city website.

How to watch

In Person



City Council Chamber
1200 Carlsbad Village Drive

Online



Watch the livestream at
carlsbadca.gov/watch

How to participate

If you would like to provide comments to the Commission, please:

- Fill out a speaker request form, located in the foyer.
 - Submit the form to the Clerk before the item begins.
 - When it's your turn, the Clerk will call your name and invite you to the podium.
 - Speakers have three minutes, unless the presiding officer (usually the chair) changes that time.
 - You may not give your time to another person, but can create a group. A group must select a single speaker as long as three other members of your group are present. All forms must be submitted to the City Clerk before the item begins and will only be accepted for items listed on the agenda (not for general public comment at the beginning of the meeting). Group representatives have 10 minutes unless that time is changed by the presiding officer or the Commission.
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- **In writing:** Email comments to planning@carlsbadca.gov. Comments received by 2 p.m. Wednesday, will be shared with the Commission prior to the meeting. When e-mailing comments, please identify in the subject line the agenda item to which your comments relate. All comments received will be included as part of the official record.

Reasonable accommodations

Reasonable Accommodations Persons with a disability may request an agenda packet in appropriate alternative formats as required by the Americans with Disabilities Act of 1990. Reasonable accommodations and auxiliary aids will be provided to effectively allow participation in the meeting. Please contact the City Manager's Office at 442-339-2821 (voice), 711 (free relay service for TTY users), 760-720-9461 (fax) or manager@carlsbadca.gov by noon on the Tuesday before the meeting to make arrangements. City staff will respond to requests by noon on Wednesday, the day of the meeting and will seek to resolve requests before the start of the meeting in order to maximize accessibility.

CALL TO ORDER:

ROLL CALL:

APPROVAL OF MINUTES:

Minutes of the Regular Meeting held on May 15, 2024

PRESENTATIONS: None.

PUBLIC COMMENT: *The Brown Act allows any member of the public to comment on items not on the agenda. Please treat others with courtesy, civility, and respect. Members of the public may participate in the meeting by submitting comments as provided on the front page of this agenda. The Commission will receive comments in the beginning of the meeting. In conformance with the Brown Act, no action can occur on these items.*

CONSENT CALENDAR: *The items listed under Consent Calendar are considered routine and will be enacted by one motion as listed below. There will be no separate discussion on these items prior to the time the Commission, votes on the motion unless members of the Commission, staff, or the public request specific items be discussed and/or removed from the Consent Calendar for separate action.*

PUBLIC HEARINGS:

1. **SDP 2023-0014 (DEV2023-0078) CARLSBAD VILLAGE MIXED USE:** Adoption of a resolution recommending approval of a Site Development Plan, SDP 2023-0014, to demolish five existing commercial structures and consolidate four parcels of land into two parcels; and construct a mixed-use development consisting of 13,800 square feet of neighborhood-serving commercial within two one-story buildings, 218 multiple-family residential apartment units within two five-story buildings, and a five-story above grade parking structure on a 4.12-acre property located at 945-1065 Carlsbad Village Drive in the northwest quadrant of the city, the Village & Barrio Master Plan, and Local Facilities Management zone.

ACTION TYPE: Quasi-Judicial

STAFF RECOMMENDATION: Take public input, close the public hearing, and adopt the resolution.

PLANNER: Jason Goff **ENGINEER:** Nichole Fine

DEPARTMENTAL REPORTS: None.

PUBLIC COMMENT: *Continuation of the Public Comments This portion of the agenda is set aside for continuation of public comments, if necessary, due to exceeding the total time allotted in the first public comments section. In conformance with the Brown Act, no Council action can occur on these items.*

PLANNING COMMISSIONER REPORT:

CITY PLANNER REPORT:

CITY ATTORNEY REPORT:

ADJOURNMENT:



PLANNING COMMISSION

Minutes

May 15, 2024, 5 p.m.

Council Chamber
1200 Carlsbad Village Drive
Carlsbad, CA 92008

CALL TO ORDER: 5 p.m.

ROLL CALL: Kamenjarin, Hubinger, Merz, Meenes, Stine (Lafferty – Absent)

PLEDGE OF ALLEGIANCE: Commissioner Merz led the Pledge of Allegiance.

APPROVAL OF MINUTES: Minutes of the Regular Meeting held on May 1, 2024, were approved as presented. 4/0/1/1 (Lafferty – Absent; Stine - Abstain).

PRESENTATIONS: None.

PUBLIC COMMENT: Resident, Tim Morgan spoke in opposition of the netting proposed at Valley Middle School field.

PUBLIC HEARINGS:

1. **ZCA 2024-0001, LCPA 2024-0020 (PUB 2024-0005) 2024 ZONE CLEAN UP** Adoption of a resolution recommending approval of a Zone Code amendment and a Local Coastal Program amendment to the Zoning Ordinance (Title 21) of the Carlsbad Municipal Code to complete various miscellaneous clean up changes.

ACTION TYPE: Legislative

STAFF RECOMMENDATION: Take public input, close the public hearing, and adopt the resolution.

PLANNER: Nicole Morrow **ENGINEER:** N/A

Chair Kamenjarin opened the duly noticed public hearing at 5:08 p.m.

Chair Kamenjarin noted that none of the commissioners had any disclosures to make regarding this item.

City Planner, Eric Lardy introduced Associate Planner, Nicole Morrow, and Senior Planner, Scott Donnell who reviewed a PowerPoint presentation on the item. (on file in the Office of the City Clerk).

In response to Commissioner Stine's inquiry about proposed restrictions for inclusionary housing, Senior Planner Scott Donnell explained that for projects with both market rate and inclusionary units, the inclusionary units must be the same tenure as the market rate units, consistent with the state Civil Code.

Chair Kamenjarin opened the public testimony at 5:32 p.m. and asked if there were any members of the public who wished to speak on the project.

Resident Joe Collins spoke on behalf himself and seven homeowners in his neighborhood. He remarked that they have made complaints recently about a very large chicken coop that was built adjacent to their neighborhood. He expressed that 25 chickens near homes are too many, it is too loud, promotes rodents and there are unpleasant odors associated with the coop.

Resident David McFeaters repeated the sentiments noted by his neighbor Joe Collins. He added this has been an issue for years and it is affecting their property value.

Seeing no one else wishing to speak, Chair Kamenjarin closed the public testimony at 5:38 p.m.

In response to the residents' comments, Associate Planner, Nicole Morrow explained that the updates reviewed this evening with regards to poultry are for the Residential One Zone and the public's comments were regarding a Residential Agricultural Zone (RA Zone) which allows for much more agricultural uses like chickens.

City Planner Lardy added that this item did not come to planning's attention until recently and could not be addressed in this report; but could possibly be referred to staff as future Clean Up item.

Commissioner Stine reiterated that the Commission cannot take action on the number of Chickens in the RA Zone because that is not part of the Agenda this evening. But Mr. Stine added that he would like to see staff look into this topic and make recommendations regarding the number of animals allowed in the RA Zone.

Motion by Commissioner Meenes, seconded by Commissioner Stine, to adopt Resolution No.7514. Motion carried, 5/0/1 (Lafferty - Absent).

Chair Kamenjarin closed the duly noticed public hearing at 5:49 p.m.

DEPARTMENTAL REPORTS:

2. **ELECTION OF VICE CHAIR** – The Planning Commission will elect a Vice-Chair for the remainder of the 2024 calendar year.

ACTION TYPE: Administrative

STAFF RECOMMENDATION: Appoint a Vice-Chair

PLANNER: Eric Lardy **ENGINEER:** N/A

Motion by Commissioner Stine, seconded by Commissioner Hubinger, to appoint Commissioner Meenes serve as Vice Chair 5/0/1 (Lafferty - Absent).

PUBLIC COMMENT: *Continuation of the Public Comments This portion of the agenda is set aside for continuation of public comments, if necessary, due to exceeding the total time allotted in the first public comments section. In conformance with the Brown Act, no Council action can occur on these items. None.*

PLANNING COMMISSION MEMBER REPORTS: None

CITY PLANNER REPORT: Historic Preservation Commission and Planning Commission Work Programs will be presented to the City Council next week providing an opportunity to the Chair or Vice Chair to present on this topic if interested. Mr. Lardy also reviewed the calendar of upcoming Planning Commission projects and highlighted some going to City Council. Mr. Lardy added that the July 3, 2024 meeting will likely be cancelled due to the holiday.

CITY ATTORNEY REPORT: None.

ADJOURNMENT: 5:50 p.m.

Cynthia Vigeland
Administrative Secretary



Staff Report

1

- Meeting Date:** June 5, 2024
- To:** Planning Commission
- Staff Contact:** Jason Goff, Senior Planner; 442-339-2643; jason.goff@carlsbadca.gov
- Subject:** Carlsbad Village Mixed-Use. Consideration of a land use request to consolidate land into two lots, demolish a 58,735 square-foot retail center, and construct a mixed-use development consisting of 13,800 square feet of neighborhood-serving commercial within two one-story buildings, 218 multiple-family residential apartment units within two five-story buildings, and a five-story above grade parking structure.
- Location:** 945-1065 Carlsbad Village Drive / 203-320-53-00, 203-320-54-00, 203-320-55-00, and 203-320-56-00 / District 1
- Case Numbers:** SDP 2023-0014 (DEV2023-0078)
- Applicant/Representative:** Jonathan Frankel, 925-708-3638, jfrankel@landusepro.com
- CEQA Determination:** Not a Project Exempt IS/ND or IS/MND EIR
 Other:
- Permit Type(s):** SDP CUP CDP TM/TPM GPA REZ LCPA
 Other:
- CEQA Status:** The environmental assessment IS on the Agenda for discussion
 A CEQA determination was already issued. That decision is final and IS NOT on the Agenda
- Commission Action:** Decision Recommendation to City Council Informational (No Action)

Recommended Actions

That the Planning Commission **ADOPT** the Planning Commission Resolution (Exhibit 1) **RECOMMENDING APPROVAL** of Site Development Plan SDP 2023-0014, based on the findings and subject to the conditions contained therein.

Existing Conditions & Project Description

Existing Setting

The subject site consists of four parcels totaling 4.12 acres located at 945-1065 Carlsbad Village Drive (Exhibit 2) and within Local Facilities Management Zone 1. The properties are currently developed with existing retail consisting of 58,735 square feet. Topographically, the site descends gently in elevation from approximately 73 feet (northeast) above mean sea level (MSL) to 63 feet MSL (southwest). The site is bounded by Carlsbad Village Drive to the north, Oak Avenue to the south, Interstate-5 to the east, Hope Avenue Alley to the west; and is also located approximately 0.4-miles from the Carlsbad Village center and 0.6 miles from local beaches.

Site Map



Table “A” below includes the General Plan and Zoning designations, Village & Barrio Master Plan (VBMP) land use district(s), and current land uses of the subject site and surrounding properties.

TABLE A – SITE AND SURROUNDING LAND USE

Location	General Plan Designation	Zoning Designation	VBMP Land Use District	Current Land Use
Site	Village Barrio (V-B)	Village Barrio (V-B)	Freeway Commercial (FC)	Retail shopping center with five buildings totaling 58,735 square feet anchored by a 24,800-square-foot “Smart & Final” grocery store. Other uses in the center include a hardware store, laundromat, pharmacy, bar, liquor store, thrift stores, antique store, restaurants, etc.
North	V-B	V-B	FC	Carlsbad Village Drive, gas station, a Carl’s Jr fast food restaurant, commercial uses, and a four-story multiple-family apartment building.
South	V-B	V-B	Barrio Perimeter (BP)	Oak Avenue, mix of residential uses from single-family to multiple-family, one, two, and three-story construction.
East	V-B; and Transportation Corridor (TC)	V-B; TC	V-B; T-C	Gas Station, Interstate-5
West	V-B	V-B	Village Center (VC)	Hope Avenue Alley, various public and commercial uses, including a Jack in the Box fast food restaurant.

General Plan Designation(s)



Zoning Designation(s)



Proposed Project

The project developer, Tooley Interests LLC, herein referred to as “Developer,” has submitted application for the development of a mixed-use development consisting of 13,800 square feet of neighborhood-serving commercial within two (2) stand-alone one-story buildings (Building A = 8,000 square feet / Building B = 5,800 square feet), 218 multiple-family residential apartment units within two (2) stand-alone five-story buildings with a bridge connecting the fourth levels, and a five-story above-grade parking garage. The proposed project includes demolition of an existing retail shopping center with five (5) buildings totaling 58,735 square feet. To facilitate the development of the project site and to achieve the desired density yield of 218 units at 53 dwelling units per acre, the project is conditioned to submit for a ministerial approval of a Tentative Parcel Map to consolidate the four (4) existing parcels into two (2) lots, which also includes recordation of a covenant of easements for private reciprocal access, parking and drainage purposes; dedication of easements for right-of-way along Carlsbad Village Drive and Oak Avenue; dedication of easements for Fire Department access along the Hope Avenue Alley; and dedication of a public drainage easement and a water easement for public water and access purposes. According to the application, the two (2) single-story commercial buildings fronting Carlsbad Village Drive are anticipated to contain a combination of retail and restaurant uses, including a neighborhood market.

The Developer is seeking a density bonus under State law, which would entitle the developer to increase the density of the project by 50% in return for designating at least 15% of the proposed base dwelling units (145 units) affordable to very low-income households. The Freeway Commercial (FC) district in the Village and Barrio Master Plan allows for 35 dwelling units per acre. The density bonus that is authorized by State law (Gov. Code §65915) is set on a sliding scale based upon the percentage of affordable units in the project, ranging from 5% to 50% additional units over the base number of units permitted pursuant to the Village & Barrio Master Plan (VBMP). With the 50% density bonus, the maximum number of residential units allowed on the site is 218 units, with 22 of those required to be restricted as affordable to very low-income households (per State Density Bonus Law). In addition, the developer is required to comply with Carlsbad’s Inclusionary Housing Ordinance. In this case, since very low-income units are proposed, 12.5% of the total number of units proposed or 27 units are required. The Developer is proposing to provide 27 very low-income units (50% of the area median income), which satisfies both the State Density Bonus Law and Inclusionary Housing Ordinance requirements. The project is proposing five density bonus waivers that are discussed in Exhibits 4 and 6.

Vehicular access to the site will occur via three access points from two surrounding streets and the Hope Avenue Alley. One vehicular driveway is provided on the northern boundary of the site along Carlsbad Village Drive

which provides access via an existing driveway and left turn pocket. A second access will be provided from the midpoint of the Hope Avenue Alley. Both the at-grade parking stalls and parking structure will be accessed via these driveways. The third access will be provided to the parking garage from Oak Avenue for residential tenants only, which is restricted by a gate.

Along Carlsbad Village Drive, the project is proposing an 8-foot right-of-way dedication and construction of a new 16-foot-wide parkway with 10-foot-wide sidewalks and 6-foot-wide landscape planters with street trees; an existing six-inch diameter water line located within the right-of-way is being replaced with an eight-inch diameter water line; and an existing North County Transit bus stop will be upgraded to include a bus shelter and bench. On Oak Avenue, the project will maintain the existing 10-foot right-of-way along the project’s frontage, but will replace existing curb, gutter, and sidewalk with a new 10-foot-wide sidewalk reduced down to 5.5 feet at each tree well planter location. Along the north side of Oak Avenue, the project will improve the dead-end street design consistent with city standard cul-de-sac design.

Pursuant to the FC Supplemental District Standard 2.7.4(G)(1), a maximum height limit of 45 feet and four (4) stories applies to the site. The project will utilize a density bonus waiver to allow a maximum building height limit of five (5) stories measuring 56 feet in height. Parapets of 4.5 feet tall are proposed around the edge of the roof structures. Stair and elevator towers of various heights would extend up to 10 feet above the building height except for the elevator towers on and adjacent to the parking structure that will extend 14 feet above the maximum building height in order to provide elevator access to the rooftop parking level. No roof structures or any other space projecting above the roof top provides additional usable floor space beyond its purpose.

As it relates to height specifically, the two (2) proposed one-story commercial buildings fronting Carlsbad Village Drive and the Hope Avenue Alley each range from 20 feet to 24.5 feet in height. The two (2) proposed five-story apartment buildings fronting Oak Avenue and the Hope Avenue Alley are separated by a grade break of an approximately 2.2 feet. The most westerly oriented apartment building (Building C) is the tallest of the two (2) proposed buildings situated at 56 feet in height as measured at the top of roof, and includes projections, such as parapets extending up to 60.5 feet, and stair and elevator towers extending up to 66 feet. The easterly situated apartment building (Building D), which is connected to the proposed five-story parking garage, is 53.8 feet in height as measured at the top of roof, and includes similar projections, such as parapets extending up to 58.3 feet in height, stair towers extending up to 63.8 feet and elevator towers extending up to 67.8 feet in height. Bollard type lighting, consisting of 4-foot-tall bollards, are proposed on the interior parapet walls of the fifth level of the parking garage to provide roof top lighting, but also avoid glare from overspilling the top deck.

The residential building design incorporates stucco as the primary building material with horizontal and vertical composite wood siding, metal guardrails and composite shutters and trellis features. The commercial building designs incorporate horizontal and vertical composite wood siding as the primary building material with a stucco base, pitched metal roofs and glass storefronts. Accents include metal canopies, metal awnings, and composite wood trellises.

A general breakdown of the apartment unit types is summarized in Table “B” below.

TABLE B – DETAILS FOR UNIT TYPE

Unit Type	Unit Size Range	Deck Area (Minimum)	Market Rate Quantity	Affordable Quantity	Total
Studio	436 SF	80 SF	13	2	15
One-Bedroom	637 SF to 1,132 SF	80 SF	76	11	87
Two-bedroom	879 SF to 1,221 SF	80 SF	79	11	90
Three-bedroom	1,203 SF to 1,365 SF	80 SF	23	3	26
			191 units	27 units	218

To address open space requirements and provide amenities for the residential portion of the project, a total of 23,110 square feet of common open space is proposed and 17,760 square feet of private open space is proposed. Common open space includes three (3) interior courtyard areas on the ground level and a “sky deck” on the fifth level. The central courtyard areas include a pool and spa with enhanced paving, raised planters and landscaping, cabanas with fireplaces, and outdoor barbeques and social cooking/dining areas. Also included are two (2) large turf areas for lawn games. Additional outdoor barbeques and social cooking/dining areas are oriented around these turf areas. The “sky deck” on the fifth level is accessed from an undescribed amenity area. The private open space includes decks for each unit meeting the minimum 80-square-foot area and minimum 6-foot dimensional requirements.

Pursuant to State Density Bonus Law (Gov. Code §65915(p)(2)), the residential portion of the project is required to provide 0.5 parking spaces per unit, resulting in a minimum requirement of 109 parking stalls. Further, per Assembly Bill (AB) 2097, no parking is required for the project due to the site’s one-half mile proximity to a major transit stop. However, the project will provide a total of 340 parking stalls at grade surrounding the residential and commercial buildings and within a five-story above grade parking structure¹.

Grading for the proposed project includes 1,200 cubic yards of fill and 10,000 cubic yards of cut. As a result, a total of 7,520 cubic yards of material is proposed to be exported from the site. Plans are attached to the staff report (Exhibits 14 and 15).

Public Outreach & Comment

The Developer has completed the Early Public Notice procedures pursuant to City Council Policy No. 84 (Development Project Public Involvement Policy). A notice of project application was mailed on June 15, 2023 to all owners of property located within a 600-foot radius of the project site. A two-foot-tall by three-foot-wide yellow sign was also posted at the project site on June 15, 2023, notifying all pass-by traffic of the project, which provides project name, application numbers, description, as well as both Developer and city staff contact information. A total of 132 property owners were notified through the notice of project application.

In addition to the above, the Developer also completed the Enhanced Stakeholder Outreach Program pursuant to City Council Policy No. 84 (Development Project Public Involvement Policy). In this case, the Developer created a project website. The above notice that was mailed on June 15, 2023 included a URL to a project website (www.carlsbadvillagemixeduse.com) for those interested in learning more about the project and/or providing feedback. The Project website included information about the project, conceptual project images, and a fillable form to provide feedback.

A total of seven comments were received at the time of the filing of the Enhanced Stakeholder Outreach report on August 10, 2023 to the city. A copy of the report is included as Exhibit 10. Additional public comments received during the review process of this application have all been added to the Public Comments Exhibit 11. Additional public comments will be batched and distributed to the Planning Commission as they are received.

Response to Public Comment & Project Issues

Public comments received were responded to in a timely manner with updates on City processes and confirming that all comments would be attached to this staff report. Public comments were also forwarded on to the applicant. Additionally, all commentators were added to an interested parties/stakeholders list to be notified

¹ Refer to Exhibit 5 for an info-bulletin that provides more information on restrictions imposed by the state legislature under AB 2097 that limit the city’s ability to require minimum parking standards on certain private development projects, including residential density bonus projects. In Carlsbad there are currently only two major public transit stops (i.e. Carlsbad Village Station and Poinsettia Station).

separately of the public hearing dates in addition to the city's public noticing procedures. Main project issues identified in the public comments are:

- **Building Height:** The Village & Barrio Master Plan allows for four stories and 45 feet in the Freeway Commercial (FC) District. However, the project is implementing State Density Bonus Law as discussed throughout staff report, which includes requests for waivers where such deviations are necessary to achieve the density allowed under State law.
- **Loss of retail grocery store:** The city cannot mandate that the grocery store, or any private business, remain. Landlord and tenant arrangements are outside of the purview of the city. However, the proposed project will result in the removal of the current tenants. The CEQA analysis looked at vehicle miles travelled, the measure to look at if there are transportation impacts of a project and did not identify impacts. The same is true for other uses such as the pharmacy and the hardware store. These are retail uses that can still be provided within the Village & Barrio Master Plan in the future and are allowed within several of the nearby districts, such as the:
 - Village Center (VC) District
 - Village General (VG) District
 - Hospitality (H) District
 - Freeway Commercial (FC) District
 - Pine-Tyler Mixed-Use (PT) District
- **Project design and neighborhood character:** California Senate Bill 330 (SB-330), commonly referred to as the Housing Crisis Act, dictates that only objective design standards can be applied to eligible housing development projects. Development standards that are subjective in nature cannot be applied to the project. This application satisfies the objective standards within the Village and Barrio Master Plan, or proposes to use the provisions allowed by density bonus under state law. Additionally, the project is protected by SB-330 and was deemed complete prior to the city's Objective Design Standards being adopted in Fall 2023.

Project Analysis

General Plan Consistency

The City of Carlsbad General Plan includes several goals and policies that guide development and land use within the city. A discussion of how the project is consistent with the applicable General Plan policies is summarized in Exhibit 4.

Village and Barrio Master Plan and Municipal Code Consistency

The Village & Barrio Master Plan as well as certain requirements and provisions in Title 21 of the Municipal Code (Zoning Ordinance) guide development and land use within the city, consistent with the General Plan. Specific compliance with these relevant requirements is described in Exhibit 4.

State Density Bonus

The Developer is seeking an increase in the allowable density of the project under Gov. Code §65915, the State Density Bonus Law. This law allows a developer to increase the density on a property above the maximum limit set by a city's general plan in exchange for reserving a certain number of the new dwelling units as affordable or other qualifying housing for at least 55 years. Developers can also have development standards waived or revised when such deviations are needed to achieve the density allowed under State law.² The 4.12 acre site is located in the Freeway Commercial (FC) District of the Village & Barrio Master Plan which permits a density range of 28 to 35 dwelling units per acre (stated as a minimum to maximum density range). This results in a

² Refer to Exhibit 7 for an info-bulletin that provides more information on how State Density Bonus Law works.

maximum base density of 144.2 units (4.12 acres multiplied by 35 units per acre), which rounds up to 145 units for this site (pursuant to Density Bonus Law, all density calculations are rounded up.) The density bonus that is authorized by State law is set on a sliding scale based upon the percentage of affordable units in the project, ranging from 5% to 50% additional units over the number ordinarily permitted. The Developer is requesting a 50% density bonus allowed under State law, for a total of 73 units (145 units multiplied by 50%). In exchange for the 50% density bonus, the Developer is required to designate 15% of the 145 base maximum density units, or 22 units, as affordable density bonus units. The affordable density bonus units are required to be affordable to “very low-income households” for a period of 55 years. At the maximum 50% density bonus, a total of 218 units could be constructed, and the developer is proposing to construct the maximum 218 units at a density of 52.9 dwelling units per acre. The project is requesting incentives, concessions or waivers as allowed under State Density Bonus Law. Pursuant to Carlsbad Municipal Code (CMC) Section 21.86.060 of the Density Bonus Ordinance, a project can request incentives, concessions and waivers as defined in State Density Bonus Law, based on the percentage of affordable units.

Specifically, the Developer is requesting five waivers from development standards of the Village & Barrio Master Plan as listed below and included in Exhibit 6. Pursuant to State Density Bonus Law, there is no limit to the number of waivers an applicant can request as long as the waiver does not cause a specific adverse impact on public health or safety, an adverse impact on property listed on the California historical register or would violate state or federal law.

Waivers

- *Waiver of VBMP FC Supplemental District Standard 2.7.4(G)(1) limiting maximum building height to 45 feet and 4 stories.*
- *Waiver of VBMP Area-Wide Standard 2.6.2(C)(4)(c) to limit architectural features up to 10 feet above maximum building height.*
- *Waiver of VBMP Area-Wide Standard 2.6.2(C)(4)(b) to limit roof structures above the building height maximum: (i) guardrails, roof deck amenities, trellises, parapets up to 42 inches above maximum height and (ii) roof structures specifically for the housing of elevators and stairways up to 10 feet above maximum height.*
- *Waiver of VBMP FC Supplemental District Standard 2.7.4(G)(2)(b) to limit the total square footage of enclosed fourth floor space not to exceed 80% of the largest floor space below.*
- *Waiver of VBMP Area-Wide Standard 2.6.1(A)(3) to require vehicle access be taken from an alley and limit driveway apron to 20 feet.*

Inclusionary Housing Ordinance

In addition to the mandatory affordable housing provisions of State Density Bonus Law, the project is also required to comply with the city’s Inclusionary Housing Ordinance, which is intended to ensure that all residential development provides a range of housing opportunities for all economic segments of the population, including households of lower and moderate income.³ Pursuant to the Inclusionary Housing Ordinance, projects are typically required to designate 15% of their total units as affordable to lower income households. However, pursuant to City Council Policy Statement No. 57, Inclusionary Housing Ordinance – Alternative Means of

³ Refer to Exhibit 8 for an info-bulletin that provides more information on how the Inclusionary Housing Ordinance works and Exhibit 9 for City Council Policy Statement Policy No. 57 which provides additional information on options to satisfy the intent of the Inclusionary Housing Ordinance

Compliance, the percentage of units designated as affordable to lower income households can be reduced to 12.5% if:

1. All of the affordable units are made available to very low or extremely low-income households, or combination thereof;
2. The units are located on the same site as the market-rate units; and
3. No financial assistance from the city is required.

For this project, the applicant is proposing to satisfy the affordable housing requirements by providing 12.5% or a total of 27 units (12.5% multiplied by 218 total units = 27.25, rounded down to 27 units) as affordable to very low-income households. The inclusionary units will be located on the same site as the market-rate units and the Developer is not requesting financial assistance from the city.

The 22 very low-income units required under State density bonus can also be used to satisfy the affordable housing requirements of the city’s Inclusionary Housing Ordinance under the terms of Carlsbad Municipal Code Section 21.85.040 – Affordable housing standards and City Council Policy Statement No. 57, Inclusionary Housing Ordinance – Alternative Means of Compliance. Therefore, for the project to demonstrate consistency with the Inclusionary Housing Ordinance and City Council Policy Statement No. 57, five additional inclusionary affordable units are required (in addition to those required by Density Bonus Law). The total number of required affordable units for the project is 27 units.

Summary of density bonus request and calculations

Base Units	(4.12 acres x 35 du/acre)	145 units (144.2 rounded up)
Density Bonus Units	(145 base units x 50%)	73 units (72.5 rounded up)
Maximum No. of Units (Allowed)	(base units + density bonus)	218 units
Total No. of Units (Requested/Proposed)		218 units
Density Bonus Affordable Units (Very Low)	(145 base units x 15%)	22 (21.8 rounded up)
Inclusionary Affordable Units (Very Low)	(218 total units x 12.5%)	27 (27.25 rounded down)
<hr/>		
Total Affordable Units*		27 units

*Inclusionary units also count as Density Bonus Affordable Units

Housing Crisis Act of 2019 (HCA)

This project is subject to California Gov. Code §65589.5(j)(1), which states when a proposed housing development project complies with the applicable, objective general plan, zoning, and subdivision standards and criteria that were in effect at the time that the application was deemed complete, cities shall not disapprove the project or impose a condition requiring lower density unless the city finds based on a preponderance of evidence that the project would have a specific, adverse impact on public health and safety, or there is no feasible method to satisfactorily mitigate or avoid such adverse impact. A “specific, adverse impact” means a significant, quantifiable, direct, and unavoidable impact, based on objective, identified written public health or safety standards, policies, or conditions as they existed on the date the application was deemed complete; and there is no feasible method to satisfactorily mitigate or avoid the adverse impact, other than the disapproval of the project or the approval of the project upon the condition that it be developed at a lower density.

There is no preponderance of evidence that the project would have a specific, adverse impact on public health and safety. The design of the site development and the types of improvement would not cause serious public health or safety problems since the project would not degrade the levels of service on the adjoining streets, drainage system, public facilities, and city sewer and water is available to the site or can be provided. The project’s proposed street alignments, grades, and widths; drainage and sanitary facilities and utilities, including

alignments and grades thereof; location and size of all required easements and right-of-way; lot size and configuration; and traffic and emergency response access were all reviewed for compliance to relevant city policies and codes. The proposed deviations (i.e., waivers requested as part of the proposed density bonus, which are permitted pursuant to State Density Bonus Law), supplement the Developer's land use application to avoid restrictive zoning limitations that would preclude the project as it has been designed and submitted to the city.

Discretionary Actions & Findings

The proposed project requires the approval of a Site Development Plan, which is discussed below.

Site Development Plan, SDP 2023-0014

Pursuant to Section 6.3.3 of the Village & Barrio Master Plan, approval of a Site Development Plan is required for the construction of buildings over 5,000 square feet or projects with more than four dwelling units. Staff finds that the required findings for this application can be met (Exhibit 1).

Environmental Review

The California Environmental Quality Act ("CEQA"), and its implementing regulations ("CEQA Guidelines") adopted by the Secretary of the California Natural Resources Agency, list classes of projects that have been determined not to have a significant effect on the environment and as a result are exempt from further environmental review under CEQA. The City Planner, through the process outlined in CMC Section 19.04.060, completed a review of the project and potential environmental impacts associated with the project pursuant to CEQA and determined that the project qualified for an exemption pursuant to CEQA Guidelines Section 15332 – In-Fill Development. CEQA Guidelines Section 15332 is a Class 32 exemption for projects under five acres, located within urbanized areas, and consistent with the General Plan and zoning regulations for the site.

This notice was posted on Feb. 29, 2024 for a period of 10-days. No appeals from the public were filed and no letters were received from the public on the CEQA determination. Therefore, the determination that the project is exempt from CEQA Guidelines Section 15332 – In-Fill Development, is final and is not subject to consideration by the public or the Planning Commission. Please see Exhibit 12 for reference.

Conclusion

Considering the information above and in the referenced Exhibits, staff has found that the proposed project is consistent with all applicable policies of the General Plan, Village & Barrio Master Plan, provisions of the Municipal Code and Local Facilities Management Zone 1. All required public improvement and utilities are available to serve the proposed development. In addition, there are no environmental issues associated with the project.

The project is conditioned to ensure the proposed project's compatibility with the surrounding properties and that the public health, safety, and welfare of the community are maintained. The project would be required to comply with all applicable California Building Standards Codes and engineering standards through the standard building permit and civil improvement plan checking process. Staff recommends the Planning Commission adopt the resolution, recommending approval of the proposed project to the City Council as described in this staff report.

Exhibits

1. Planning Commission Resolution
2. Location Map
3. Disclosure Statement
4. Project Analysis (General Plan, Zoning Ordinance, Village & Barrio Master Plan)
5. Informational Bulletin IB-131 – AB 2097 Parking Requirements
6. Supplemental Application - Density Bonus Checklist
7. Informational Bulletin IB-112 – State Density Bonus Law
8. Informational Bulletin IB-157 – Inclusionary Housing Program
9. City Council Policy Statement No. 57 – Inclusionary Housing Ordinance - Alternative Means of Compliance
10. Enhanced Stakeholder Public Outreach Report
11. Public Comments
12. Notice of CEQA Determination of Exemption, Feb. 29, 2024
13. List of Acronyms and Abbreviations
14. Reduced Exhibits
15. Full Size Exhibits “A” – “UU” dated June 5, 2024 (on file in the Planning Department)

PLANNING COMMISSION RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF CARLSBAD, CALIFORNIA, RECOMMENDING APPROVAL OF SITE DEVELOPMENT PLAN, SDP 2023-0014, TO DEMOLISH FIVE EXISTING COMMERCIAL STRUCTURES AND CONSOLIDATE FOUR PARCELS OF LAND INTO TWO PARCELS; AND CONSTRUCT A MIXED-USE DEVELOPMENT CONSISTING OF 13,800 SQUARE FEET OF NEIGHBORHOOD-SERVING COMMERCIAL WITHIN TWO ONE-STORY BUILDINGS, 218 MULTIPLE-FAMILY RESIDENTIAL APARTMENT UNITS WITHIN TWO FIVE-STORY BUILDINGS, AND A FIVE-STORY ABOVE GRADE PARKING STRUCTURE ON A 4.12-ACRE PROPERTY LOCATED AT 945-1065 CARLSBAD VILLAGE DRIVE IN THE NORTHWEST QUADRANT OF THE CITY, THE VILLAGE & BARRIO MASTER PLAN, AND LOCAL FACILITIES MANAGEMENT ZONE 1.

CASE NAME: CARLSBAD VILLAGE MIXED USE

CASE NO.: SDP 2023-0014 (DEV2023-0078)

WHEREAS, **Tooley Interests, LLC**, “Developer,” has filed a verified application with the City of Carlsbad regarding property owned by **GRT Carlsbad Village, LLC**, “Owner,” described as

PARCELS A THROUGH D AS SHOWN ON "CITY OF CARLSBAD M.S. 2018-0014", IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO PARCEL MAP THEREOF NO. 21699 FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, JULY 16, 2019.

(“the Property”); and

WHEREAS, said verified application constitutes a request for a Site Development Plan as shown on Exhibit(s) “**A**” - “**UU**” dated **June 5, 2024**, on file in the Planning Division, **SDP 2023-0014 – CARLSBAD VILLAGE MIXED USE** as provided by Chapters 21.06, 21.85 and 21.86 of the Carlsbad Municipal Code and the Village & Barrio Master Plan; and

WHEREAS, the Planning Division studied the **Site Development Plan** application and performed the necessary investigations to determine if the project qualified for an exemption from further environmental review pursuant to the California Environmental Quality Act, (CEQA, Public Resources Code section 21000 et. seq.), and its implementing regulations (the State CEQA Guidelines), Article 14 of the California Code of Regulations section 15000 et. seq. After consideration of all evidence presented, and studies and investigations made by the City Planner and on its behalf, the City Planner

determined that the project was exempt from further environmental review pursuant to State CEQA Guidelines section 15332 – In-fill Development Projects. CEQA Guidelines Section 15332 is a Class 32 exemption for projects under five acres, located within urbanized areas, and consistent with the General Plan and zoning regulations for the site. The project will not have a significant effect on the environment and all of the requirements of CEQA have been met; and

WHEREAS, on Feb. 29, 2024, the city distributed a notice of intended decision to adopt the “In-fill Development Projects” exemption. The notice was posted for a 10-day period, which began on Feb. 29, 2024 and ended on March 11, 2024. The city did not receive any comment letters or an appeal on the CEQA findings and determination. The effective date and order of the City Planner CEQA determination was March 11, 2024.

WHEREAS, the Planning Commission did, on **June 5, 2024**, hold a duly noticed public hearing as prescribed by law to consider said request relative to the **Site Development Plan** land use application; and

WHEREAS, at said public hearing, upon hearing and considering all testimony and arguments, if any, of all persons desiring to be heard, said Commission considered all factors relating to the **Site Development Plan**.

NOW, THEREFORE, BE IT HEREBY RESOLVED by the Planning Commission of the City of Carlsbad as follows:

- A) That the foregoing recitations are true and correct.
- B) That based on the evidence presented at the public hearing, the Planning Commission **RECOMMENDS APPROVAL OF SDP 2023-0014 – CARLSBAD VILLAGE MIXED USE**, based on the following findings and subject to the following conditions:

Findings:

Site Development Plan, SDP 2023-0014

1. That the proposed development or use is consistent with the General Plan and any applicable master plan or specific plan, complies with all applicable provisions of Chapter 21.06 of the Carlsbad Municipal Code, and all other applicable provisions of this code, in that **the project**

proposes the demolition of an existing shopping center with approximately 58,735 square feet of commercial uses, and in its place the construction of a new mixed-use development consisting of 13,800 square feet of neighborhood-serving commercial within two one-story buildings, 218 multiple-family residential apartment units (191 market rate and 27 affordable) within two five-story buildings, 289 garage parking spaces within a five-story parking structure, and 51 surface parking spaces, all on a 4.12-acre infill site located within the Freeway Commercial (FC) District of the Village & Barrio Master Plan. Mixed-use, consisting of multiple-family residential and commercial (i.e., restaurants, retail, markets, etc.), is permitted by right within the FC District. The project is consistent with the various elements and objectives of the Village (V) General Plan Land Use designation and the Village & Barrio Master Plan as discussed in the findings below and in the project staff report dated June 5, 2024 incorporated by reference. Although the project's residential density of 53 dwelling units per acre is above the 35 du/ac maximum that is allowed by the FC District, the 218-unit residential project can be found consistent with the General Plan Land Use Policies as discussed in the findings below and in the project staff report dated June 5, 2024 through use of a Density Bonus as allowed under California Law and the Carlsbad Municipal Code. State law encourages cities to provide affordable housing through incentives to developers (i.e., State Density Bonus Law, Government Code §65915). State Density Bonus Law allows a developer to increase density on a property above the maximum density, set under the land use policies of the General Plan, zoning district, and Specific Plan/Master Plan. In this instance, based on the adopted land use density of the Freeway Commercial (FC) District of the Village and Barrio Master Plan, the base number of units allowed for a 4.12-acre parcel at 35 du/ac is 145 dwelling units. The applicant is requesting a 50% increase in density to yield 218 total units. Density Bonus Law stipulates that a request for a density bonus does not constitute a valid basis on which to find a proposed housing development project is not compliant with a general plan. In addition, State Density Bonus Law explicitly requires the city to consider "the density allowed under the land use element of the general plan" in determining maximum allowable residential density and General Plan conformance. Therefore, the proposed density of the project is deemed compliant with the land use density designation of the General Plan and Village and Barrio Master Plan.

2. That the requested development or use is properly related to the site, surroundings and environmental settings, will not be detrimental to existing development or uses or to development or uses specifically permitted in the area in which the proposed development or use is to be located, and will not adversely impact the site, surroundings or traffic circulation, **in that the proposed mixed-use development is located within the Freeway Commercial (FC) District of the Village & Barrio Master Plan. The project will not be detrimental to existing uses or to uses specifically permitted in the area in which the use is located in that mixed-use, consisting of multiple-family residential and commercial (i.e., restaurants, retail, markets, etc.), is permitted by right in the FC District. All the properties surrounding the site are designated for FC uses under the VBMP, except for properties located to the south of the site, across Oak Avenue, which are designated as Barrio Perimeter (BP) and those along Harding Avenue to the west, which are designated as Village Center (VC). The project site is surrounded by a variety of urban uses, including an adjacent gas station, a four-story mixed-use building across Carlsbad Village Drive to the north, one-to-three-story residential (multiple-family and single-family) buildings to the south across Oak Avenue, and various one-story public and commercial uses to the west across the Hope Avenue Alley along Harding Street. The proposed site design places residential adjacent to residential to the south and commercial development adjacent to existing commercial development to the north and west. The proposed mixed-use development will not adversely impact the site, surroundings, or traffic circulation. The mixed-use project is estimated**

to generate fewer trips (2,242 ADT) than the existing commercial land uses (7,044 ADT) it intends to replace, resulting in a net decrease of 4,802 Average Daily Trips (ADT). Both Carlsbad Village Drive and Oak Avenue are designed to adequately handle the traffic generated by the project, and the project is adequately parked on-site in accordance with State Density Bonus Law and the Village & Barrio Master Plan.

3. That the site for the intended development or use is adequate in size and shape to accommodate the use, in that **pursuant to State Density Bonus Law, the Developer has requested waivers to standards in the Village & Barrio Master Plan related to maximum building height and the number of floor levels, height limits on roof structures, height limits on architectural features, limitations on total square footage of the fourth floor, and restricting vehicular access to an alley. The project is entitled to the requested number of units and the density bonus waivers are necessary for the project to be developed at the density. The waivers are permitted pursuant to CMC Chapter 21.86 and State Density Bonus Law.**
4. That all yards, setbacks, walls, fences, landscaping, and other features necessary to adjust the requested development or use to existing or permitted future development or use in the neighborhood will be provided and maintained, in that **the project complies with all applicable development standards for mixed-use projects within the FC District of the Village & Barrio Master Plan, including lot coverage, open space, height, setbacks, and parking, etc., subject to density bonus provisions for concessions and waivers, including reductions of standards. The architecture of the building is compatible with the surrounding residential and commercial development. Landscaping along the street frontages and throughout the project will be provided consistent with the requirements of the city's Landscape Manual and the Village & Barrio Master Plan.**
5. That the street systems serving the proposed development or use is adequate to properly handle all traffic generated by the proposed use, in that **the proposed project is considered a through lot since it has two primary street frontages, Carlsbad Village Drive to the north, and Oak Avenue to the south, both of which are connected by the existing Hope Avenue Alley bordering the entire westerly property line of the subject property. Along Carlsbad Village Drive, the project is proposing 8-feet of right-of-way dedication. With a total of 16-feet from curb to property line, the project proposes a 10-foot-wide sidewalk buffered by a 6-foot-wide landscape planter with street trees. In addition to the pedestrian improvements described, the project will also be updating an existing bus stop on Carlsbad Village Drive with a new Americans with Disabilities Act (ADA) compliant bus stop pad with shelter, bench, trash receptacles and shelter lighting, etc., pursuant to NCTD standards. On Oak Avenue, the project will maintain the existing 10-foot right-of-way along the project frontage and will replace the existing curb, gutter, and sidewalk. Along this frontage, the project is proposing a 10-foot-wide sidewalk reduced down to 5.5 feet at each tree well planter location. Along the north side of Oak Avenue, the project also proposes to improve the dead-end street design to a cul-de-sac pursuant to City of Carlsbad standards. Primary vehicular access into the project site will be provided via two existing driveways on Carlsbad Village Drive – one (1) unsignalized right-turn only driveway to the Hope Avenue Alley along the western boundary of the site and one (1) unsignalized private driveway permitting right turns and inbound left turns only. The existing median in Carlsbad Village Drive prohibits left turns out from this driveway. Secondary vehicular access will be provided on Oak Avenue via a full-access driveway located at the end of the cul-de-sac where Oak Avenue terminates west of Interstate-5. The proposed project is estimated to generate fewer trips (2,242 ADT) than the existing land uses (7,044 ADT) it will**

replace, resulting in a net decrease of 4,802 Average Daily Trips (ADT). Both Carlsbad Village Drive and Oak Avenue are designed to adequately handle the traffic generated by the project. The project site is also located within 0.4 miles of the Carlsbad Village mass transit station which provides bus, train, and Coaster service on a daily basis. Bike lanes are provided on Carlsbad Village Drive. In addition, the Developer will be required to pay traffic impact fees in accordance with Carlsbad Municipal Code Section 18.42 prior to issuance of building permits that will go towards future road improvements.

Inclusionary Housing and Density Bonus

6. Project has been granted additional units, incentives, and waivers in accordance with density bonus provisions found in Government Code §65915 and Carlsbad Municipal Code Chapter 21.86. Per density bonus definitions, the project had a calculation of 145 Base Units. Applicant requested a 50% increase, which allows up to an additional 73 units for a total maximum project size of 218 units. Based on the requested density bonus, Applicant must provide 15% of the Base Units to very-low-income units as defined by California Health and Safety Code §50053. The project is conditioned to provide 22 units ($145 \times 0.15 = 21.8$ rounded up to 22) density bonus affordable units based on calculations in Government Code §65915 and enforced by the California Department of Housing and Community Development.
7. Project is also subject to Carlsbad Municipal Code Chapter 21.85, requiring 15% of the total residential units be affordable for lower income households. Carlsbad Municipal Code (CMC) Section 21.85.070 provides alternatives to construction of inclusionary units, including reducing the required number of inclusionary units to twelve and a half (12.5) percent for projects that restrict all affordable units to extremely low or very-low-income households. Under this alternative, this project is providing very-low-income units pursuant to Government Code §65915, and therefore, the very-low-income inclusionary units shall be twelve and a half (12.5) percent of the total residential units approved by the final decision-making authority, including density bonus units. Therefore, the affordable units required by this code section is 27 units ($218 \times 0.125 = 27.25$ rounded down to 27). The project is conditioned to provide 27 inclusionary units based on Carlsbad Municipal Code Chapter 21.85.
8. Per Carlsbad Municipal Code Section 21.86.130, those affordable dwelling units provided to meet the inclusionary requirement established pursuant to Chapter 21.85 of this title, shall be counted toward satisfying the density bonus requirements of this chapter. Twenty-two (22) of the 27 units count toward the density bonus affordable requirement, as long as they meet the affordability requirement identified in Government Code §65915. Therefore, the conditions identified in the two findings above are cumulative and will be satisfied through one Affordable Housing Agreement to provide 27 total affordable units based on Carlsbad Municipal Code Chapter 21.85, all of which will be at the very-low-income level to satisfy Carlsbad Municipal Code Chapter 21.86.

General

9. The **Planning Commission** finds that the project, as conditioned herein, is in conformance with the Elements of the city's General Plan, based on the facts set forth in the staff report dated **June 5, 2024** including, but not limited to the following:

- a. **Land Use & Community Design, Goal 2-G.2 – The mixed-use project provides 13,800 square feet of new neighborhood-serving commercial development that is oriented towards Carlsbad Village Drive in accordance with the Village & Barrio Master Plan, 218 multiple-family residential apartment units (191 market rate and 27 affordable) and 340 parking spaces all located near jobs, schools, beaches, lagoons, and other convenient neighborhood-serving commercial services. The project provides 340 parking spaces onsite and also has access to public transit via the Carlsbad Village Station providing bus, train, and Coaster services, as well as NCTD bus route 315 providing service between Camp Pendelton, Sprinter, and the Carlsbad Village Station.**
- b. **Land Use & Community Design, Goal 2-G.3 – The mixed-use project is proposed on a developed site surrounded by urban development. In addition to 13,800 square feet of new neighborhood-serving commercial development oriented towards Carlsbad Village Drive, the project will provide future residents with access to convenient neighborhood-serving commercial uses onsite and within the area, as well as providing new housing to support nearby employment centers.**
- c. **Land Use & Community Design, Goal 2-G.4 – The multiple-family residential portion of the mixed-use project includes 218 apartments units (191 market rate and 27 affordable) in a variety of bedroom counts and provides much-needed higher density housing located in close proximity to neighborhood-serving commercial uses, nearby employment centers and the mass transit station located in the core of Carlsbad Village.**
- d. **Land Use & Community Design, Goal 2-G.30 and Policy 2-P.70 – The proposal to construct a mixed-use development with 13,800 square feet of commercial and 218 multiple-family residential apartments would enhance the vitality of the Village by providing new residential and updated commercial land uses near the downtown core area. The project reinforces the pedestrian orientation desired for the downtown area by providing residents an opportunity to walk to shopping, restaurants, recreation, and mass transit functions. The project’s proximity to existing bus routes and other mass transit helps to further the goal of providing new economic development near transportation corridors.**
- e. **Mobility, Goal 3-G.3 and Policy 3-P.5 – Along Carlsbad Village Drive, the project is proposing an 8-foot right-of-way dedication and construction of a new 16-foot-wide parkway with 10-foot-wide sidewalks and 6-foot-wide landscape planters with street trees. On Oak Avenue, the project will maintain the existing 10-foot right-of-way along the project frontage, but will replace the existing curb, gutter, and sidewalk with a 10-foot-wide sidewalk reduced down to 5.5 feet at each tree well planter location. Along the north side of Oak Avenue, the project also proposes to improve the dead-end street design to a cul-de-sac pursuant to City of Carlsbad standards. In addition, the proposed project is located approximately 0.4 miles from the Carlsbad Village station, which provides rail and bus service throughout the day. The project’s proximity to the transit station would provide visitors with new neighborhood-serving commercial and residents with the opportunity to commute to major job centers, thereby reducing vehicle miles traveled (VMTs) and the carbon footprint. Furthermore, the project supports walkability and mobility by locating the project near other existing goods and services within the Village.**
- f. **Noise, Goals 5-G.1 and 5-G.2 – Dudek Environmental Consulting conducted a noise study (Dec. 2023) for the proposed project. The principal noise source associated with the**

proposed project is traffic generated from the Interstate-5 freeway and the surrounding street system. As it relates to exterior use areas, the project will include outdoor usable space within multiple interior courtyards. Due to sound-blocking acoustical shielding afforded by the onsite positions and multi-floor structural arrangement of the proposed buildings, these exterior use areas will not be subject to noise exposure levels exceeding the City's 65 dBA CNEL standard. As it relates to interior spaces, pursuant to Chapter 18.04.080 of the Carlsbad Municipal Code (CMC), any new residence or addition of one or more habitable rooms to an existing residence located within the noise impact boundary of an airport or freeway must be designed to ensure that internal noise levels due to airport or freeway operations do not exceed 45 dB. The project will not be built without adequately demonstrating compliance with the California Building Code (CBC), as locally amended. Modeling was performed to determine interior noise levels from street-facing units of the project. An assessment was conducted on street-facing units intended for residential occupation since the facade noise levels were modeled above 60 dBA CNEL. As a project design feature, glass assemblies would require a closed window condition to reduce the interior noise levels below the City's 45 dBA CNEL threshold. Therefore, mechanical ventilation (e.g., air conditioning) will be installed in impacted street-facing units to move air within the structure and control temperature when windows are closed. Pursuant to standard building permit procedures, a final interior noise assessment will be conducted to ensure interior noise reductions are met. As it relates to airport noise, the nearest airport is McClellan-Palomar Airport, which is approximately 4.0 miles southeast of the project site and is located outside of all CNEL noise contours outlined in the McClellan-Palomar Airport Land Use Compatibility Plan (ALUCP) and included in the City's General Plan.

- g. Public Safety, Goal 6-G.1 – The proposed project is not located in any Very High Fire Severity Zone, Flood Zones, or Earthquake Fault Zones. The proposed structural improvements would be required to meet all seismic design standards at time of building permit in accordance with the California Building Code. The Fire & Life Safety Division has approved the proposed conceptual building design with fire sprinklers included throughout per the National Fire Protection Association (NFPA) standards. In addition, an Alternative Means and Method Request (AM&M) plan has been reviewed and approved by the Fire & Life Safety Division (June 23, 2023) and will be implemented at the construction stage of development.**
- h. Public Safety, Goal 6-G.4 – The proposed project will provide all supporting water infrastructure in accordance with Fire & Life Safety requirements; fire sprinklers are required and provided for all residential structures; and the dwelling units proposed by this project are all within a five minute emergency response time. In addition, the existing six-inch (6") diameter water line located within the Carlsbad Village Drive project frontage right-of-way is proposed to be replaced with a new eight-inch (8") diameter water line.**
- i. Public Safety, Policy 6-P.6 – The project has been conditioned to develop and implement a program of "best management practices" for the elimination and reduction of pollutants which enter and/or are transported within storm drainage facilities.**
- j. Public Safety, Policy 6-P.34 – The project is required to comply with all Building and Fire codes to ensure that fire protection standards are met by the proposed structures. The Fire & Life Safety Division has approved conceptual building designs with fire sprinklers included throughout per the National Fire Protection Association (NFPA) standards. In addition, an**

Alternative Means and Method Request (AM&M) plan has been reviewed and approved by the Fire & Life Safety Division dated June 23, 2023 and shall be implemented at the construction stage of development. Therefore, the proposed project is consistent with the applicable fire safety requirements.

- k. **Public Safety, Policy 6-P.39** – The project is required to construct or pay applicable fees for necessary improvements, public utilities, and facilities in accordance with Growth Management requirements; Fire Station No. 1, located less than 0.25-miles of the project site, is within emergency response timeframes; and the project design will not affect the city’s ability to implement its Emergency Operations Plan. Improvements to existing utilities includes the replacement of an existing six-inch (6”) diameter water line located within the Carlsbad Village Drive project frontage right-of-way with a new eight-inch (8”) diameter water line.

- l. **Sustainability, Policy 9-P.1** – The project implements and is consistent with measures identified in the Climate Action Plan (CAP) through the provision of renewable energy generation (photovoltaic systems), energy conservation (Green Building Code), and by accommodating Zero-Emission vehicles and other greenhouse gas reduction measures and features. At building permit, the project will also be reviewed for compliance with the CAP ordinances.

- m. **Housing, Goal 10-G.3** – Per CMC Chapter 21.85, a project is required to provide 15% of the total residential units as affordable for lower income households. Pursuant to City Council Policy No. 57, the percentage can be reduced to 12.5% if the inclusionary units are very low or extremely low-income, the units are located on the same site as the market-rate units and the Developer is not requesting financial assistance from the city. The project proposes 27 very low-income units onsite, and the Developer is not requesting financial assistance from the city. In addition, the project is conditioned to enter into an Affordable Housing Agreement with the city to provide and deed restrict 27 dwelling units as affordable to very low-income households for 55 years, earning up to 50% of the area median income (AMI).

- n. **Housing, Policy 10-G.2 and 10-P.15** – The proposed project will increase the diversity of housing in Carlsbad by adding 218 multiple-family apartment units to the city’s housing inventory. In addition, the project will increase housing diversity by providing housing offered at a price affordable to very low-income households. The project is conditioned to enter into an Affordable Housing Agreement with the city to provide and deed restrict 27 dwelling units (12.5%) as affordable to very low-income households for 55 years, earning up to 50% of the area median income (AMI). Additionally, of those 27 affordable units, a minimum of three (3) units will have three-bedrooms, consistent with requirements of the city’s Inclusionary Housing Ordinance.

- a. **Housing, Policy 10-P.19** – The city currently has a jobs/housing ratio where there are more jobs than housing. The project replaces an existing commercial shopping center with a new mixed-use development consisting of 13,800 square feet of neighborhood-serving commercial and 218 multiple-family apartments (191 market rate and 27 affordable). The residential portion of this mixed-use project expands the city’s affordable housing stock to assist in meeting its Regional Housing Needs Assessment and provides an adequate number of housing units to meet the needs of very low-income households.

10. The project is consistent with the Citywide Facilities and Improvements Plan, the Local Facilities Management Plan for Zone 1 and all city public facility policies and ordinances. The project includes elements or has been conditioned to construct or provide funding to ensure that all facilities and improvements regarding sewer collection and treatment; water; drainage; circulation; fire; schools; parks and other recreational facilities; libraries; government administrative facilities; and open space, related to the project will be installed to serve new development prior to or concurrent with need. Specifically,
 - a. The project has been conditioned to provide proof from the **Carlsbad Unified** School District that the project has satisfied its obligation for school facilities.
 - b. The Public Facility fee is required to be paid by Council Policy No. 17 and will be collected prior to the issuance of building permit.
 - c. The Local Facilities Management fee for Zone 1 is required by Carlsbad Municipal Code Section 21.90.050 and will be collected prior to issuance of building permit.
11. The project has been conditioned to pay any increase in public facility fee, or new construction tax, or development fees, and has agreed to abide by any additional requirements established by a Local Facilities Management Plan prepared pursuant to Chapter 21.90 of the Carlsbad Municipal Code. This will ensure continued availability of public facilities and will mitigate any cumulative impacts created by the project.
12. This project has been conditioned to comply with any requirement approved as part of the Local Facilities Management Plan for Zone 1.
13. That all necessary public facilities required by the Growth Management Ordinance will be constructed or are guaranteed to be constructed concurrently with the need for them created by this project and in compliance with adopted city standards.
14. That the project is consistent with the city's Landscape Manual and Water Efficient Landscape Ordinance (Carlsbad Municipal Code Chapter 18.50).
15. The City Planner has determined that the project belongs to a class of projects that the State Secretary for Resources has found do not have a significant impact on the environment, and it is therefore categorically exempt from the requirement for preparation of environmental documents pursuant to **Section 15332 – In-fill Development Projects** of the State CEQA Guidelines as an in-fill development project. In making this determination, the City Planner has found that the exceptions listed in Section 15300.2 of the State CEQA Guidelines do not apply to this project. No appeals were made to this determination; therefore, it is final.
16. The Planning Commission has reviewed each of the exactions imposed on the Developer contained in this resolution, and hereby finds, in this case, that the exactions are imposed to mitigate impacts caused by or reasonably related to the project, and the extent and the degree of the exaction is in rough proportionality to the impact caused by the project.

Conditions:

NOTE: Unless specifically stated in the condition, all of the following conditions, upon the approval of

this proposed development, must be met **prior to approval of a parcel map, building or grading permit whichever occurs first. All references to the "Site Plan" reference the project plans dated Nov. 13, 2023 attached as Exhibits 14 and 15 of the Planning Commission Staff Report dated June 5, 2024.**

1. If any of the following conditions fail to occur, or if they are, by their terms, to be implemented and maintained over time, if any of such conditions fail to be so implemented and maintained according to their terms, the city shall have the right to revoke or modify all approvals herein granted; deny or further condition issuance of all future building permits; deny, revoke, or further condition all certificates of occupancy issued under the authority of approvals herein granted; record a notice of violation on the property title; institute and prosecute litigation to compel their compliance with said conditions or seek damages for their violation. No vested rights are gained by Developer or a successor in interest by the city's approval of this **Site Development Plan**.
2. Staff is authorized and directed to make, or require the Developer to make, all corrections and modifications to the **Site Development Plan** documents, as necessary to make them internally consistent and in conformity with the final action on the project. Development shall occur substantially as shown on the approved Exhibits. Any proposed development, different from this approval, shall require an amendment to this approval.
3. Developer shall comply with all applicable provisions of federal, state, and local laws and regulations in effect at the time of building permit issuance.
4. If any condition for construction of any public improvements or facilities, or the payment of any fees in-lieu thereof, imposed by this approval or imposed by law on this project are challenged, this approval shall be suspended as provided in Government Code §66020. If any such condition is determined to be invalid, this approval shall be invalid unless the City Council determines that the project without the condition complies with all requirements of law.
5. Developer/Operator shall and does hereby agree to indemnify, protect, defend, and hold harmless the City of Carlsbad, its Council members, officers, employees, agents, and representatives, from and against any and all liabilities, losses, damages, demands, claims and costs, including court costs and attorney's fees incurred by the city arising, directly or indirectly, from (a) city's approval and issuance of this **Site Development Plan** (b) city's approval or issuance of any permit or action, whether discretionary or nondiscretionary, in connection with the use contemplated herein, and (c) Developer/Operator's installation and operation of the facility permitted hereby, including without limitation, any and all liabilities arising from the emission by the facility of electromagnetic fields or other energy waves or emissions. This obligation survives until all legal proceedings have been concluded and continues even if the city's approval is not validated.
6. Prior to submittal of the building plans, improvement plans, grading plans, or final map, whichever occurs first, developer shall submit to the City Planner, a 24" x 36" copy of the (**Project Site Plan or other**), conceptual grading plan and preliminary utility plan reflecting the conditions approved by the final decision making body. The copy shall be submitted to the City Planner, reviewed and, if found acceptable, signed by the city's project planner and project engineer. If no changes were required, the approved exhibits shall fulfill this condition.

7. Prior to the issuance of a building permit, the Developer shall provide proof to the Building Division from the **Carlsbad Unified** School District that this project has satisfied its obligation to provide school facilities.
8. This project shall comply with all conditions and mitigation measures which are required as part of the Zone **1** Local Facilities Management Plan and any amendments made to that Plan prior to the issuance of building permits.
9. This approval shall become null and void if building or grading permits are not issued for this project within 24 months from the date of project approval.
10. Building permits will not be issued for this project unless the local agency providing water and sewer services to the project provides written certification to the city that adequate water service and sewer facilities, respectively, are available to the project at the time of the application for the building permit, and that water and sewer capacity and facilities will continue to be available until the time of occupancy.
11. Developer shall pay the Citywide Public Facilities Fee imposed by City Council Policy #17, the License Tax on new construction imposed by Carlsbad Municipal Code Section 5.09.030, and CFD #1 special tax (if applicable), subject to any credits authorized by Carlsbad Municipal Code Section 5.09.040. Developer shall also pay any applicable Local Facilities Management Plan fee for Zone **1** pursuant to Chapter 21.90. All such taxes/fees shall be paid at issuance of building permit. If the taxes/fees are not paid, this approval will not be consistent with the General Plan and shall become void.
12. Developer shall submit to the city a Notice of Restriction executed by the owner of the real property to be developed. Said notice is to be filed in the office of the County Recorder, subject to the satisfaction of the City Planner, notifying all interested parties and successors in interest that the City of Carlsbad has issued a **Site Development Plan** on the property. Said Notice of Restriction shall note the property description, location of the file containing complete project details and all conditions of approval as well as any conditions or restrictions specified for inclusion in the Notice of Restriction. The City Planner has the authority to execute and record an amendment to the notice which modifies or terminates said notice upon a showing of good cause by the Developer or successor in interest.
13. **Prior to issuance of grading permit(s)**, developer shall **make a separate formal landscape construction drawing plan check submittal to the Planning Division** and obtain City Planner approval of a Final Landscape and Irrigation Plan showing conformance with the approved Preliminary Landscape Plan and the city's Landscape Manual. Developer shall construct and install all landscaping and irrigation as shown on the approved Final Plans. All landscaping shall be maintained in a healthy and thriving condition, free from weeds, trash, and debris. All irrigation systems shall be maintained to provide the optimum amount of water to the landscape for plant growth without causing soil erosion and runoff.
14. The first submittal of Final Landscape and Irrigation Plans shall be pursuant to the landscape plan check process on file in the Planning Division and accompanied by the project's building, improvement, and grading plans.

15. Project has been granted additional units and waivers in accordance with density bonus provisions found in Government Code §65915 and Carlsbad Municipal Code (CMC) Chapter 21.86. Per density bonus definitions, the project had a calculation of 145 Base Units. Applicant requested a 50% increase which allows up to an additional 73 units for a total maximum project size of 218 units. Based on the requested density bonus, applicant must provide 15% of the Base Units to very low-income units, as defined by California Health and Safety Code §50053. The project must provide 22 density bonus affordable units based on calculations in Government Code §65915.

Project is also subject to Carlsbad Municipal Code (CMC) Chapter 21.85, requiring 15% of the total residential units be affordable for lower income households. CMC Section 21.85.070 provides alternatives to construction of inclusionary units, including reducing the required number of inclusionary units to twelve and a half percent (12.5%) for projects that restrict all affordable units to extremely low or very-low-income households. Under this alternative, this project is providing very-low-income units pursuant to Government Code §65915, and therefore, the very-low-income inclusionary units shall be 12.5 percent of the total residential units approved by the final decision-making authority, including density bonus units. Therefore, the affordable units required by this code section is 27 units ($218 \times 0.125 = 27.25$ rounded down to 27). Per CMC Section 21.86.130 those affordable dwelling units provided to meet the inclusionary requirement established pursuant to CMC Chapter 21.85 shall be counted toward satisfying the density bonus requirements of this chapter. Twenty-two (22) of the 27 units can count towards the density bonus affordable requirement, as long as they meet the affordability requirement identified in Government Code §65915.

16. Developer shall construct the project’s required affordable units concurrent with the project’s market rate units in accordance with the unit type distribution identified below and in accordance with Section 21.85.100(C) of the Carlsbad Municipal Code (CMC). Affordable units within the project must be distributed evenly across various areas and floors, adhering to the guidelines of California Health and Safety Code Section 17929, rather than being concentrated in specific locations. Additionally, these affordable units should be equipped with equivalent amenities as those found in market rate units, in accordance with CMC 21.85.040 (G).

Unit Type	Total Units	Market Rate Units	Affordable Units	Affordability Level
Studio	15	13	2	50%
One-bedroom	87	76	11	50%
Two-bedroom	90	79	11	50%
Three-bedroom	26	23	3	50%
TOTALS:	218 units	191 units	27 units	

17. Prior to the approval of the final map for any phase of this project, or where a map is not being processed, prior to the issuance of building permits for any lots or units, the Developer shall enter into an Affordable Housing Agreement with the city to **provide and deed restrict 27 dwelling units as affordable to very-low-income households for 55 years, earning up to 50% of the area median income (AMI)** in accordance with the requirements and process set forth in Chapters 21.85 and 21.86 of the Carlsbad Municipal Code. The draft Affordable Housing Agreement shall be submitted to the City Planner no later than 60 days prior to the request to final the map and shall be recorded prior to issuance of the first building permit. The recorded Affordable Housing Agreement shall be binding on all future owners and successors in interest.

18. **Prior to issuance of grading and building permits, Developer shall list the following condition on all grading and building permit construction plans.** Construction activities shall take place during the permitted time and day per Carlsbad Municipal Code Chapter 8.48. Developer shall ensure that construction activities for the proposed project are limited to the hours from 7:00 a.m. to 6:00 p.m. Monday through Friday, and 8:00 a.m. to 6:00 p.m. on Saturdays; no work shall be conducted on Sundays or on federal holidays.
19. All roof appurtenances, including air conditioners, shall be architecturally integrated and concealed from view and the sound buffered from adjacent properties and streets, in substance as provided in Building Department Policy No. 80-6, to the satisfaction of the **City Planner and Building Official**.
20. No outdoor storage of materials shall occur onsite unless required by the Fire Chief. When so required, the Developer shall submit and obtain approval of the Fire Chief and the City Planner of an Outdoor Storage Plan, and thereafter comply with the approved plan.
21. Developer shall submit and obtain City Planner approval of an exterior lighting plan including parking areas. All lighting shall be designed to reflect downward and avoid any impacts on adjacent homes or property.
22. **Prior to issuance of the grading permit and removal of any trees within the public right-of-way, Developer shall inform the Carlsbad Parks and Recreation Department in writing of said requested removal(s) and obtain any required permits, to the satisfaction of the City Planner and Parks & Recreation Director.**
23. **Prior to issuance of a grading permit or the commencement of any ground-disturbing activities, whichever occurs first, Developer shall provide written confirmation to the City Planner that a qualified paleontologist has been retained to draft and implement a paleontological monitoring program in accordance with the provisions of the City of Carlsbad Tribal, Cultural, and Paleontological Resources Guidelines (September 2017) and as further detailed in the Paleontological Assessment (BFS Environmental Services, May 23, 2023) prepared for the project. Prior to release of the grading bonds, the qualified paleontologist shall complete a report describing the methods and results of the paleontological monitoring and data recovery program and file a copy of the report at the San Diego Natural History Museum, to the satisfaction of the City Planner.**
24. **Prior to issuance of a grading permit or the commencement of any ground-disturbing activities, whichever occurs first, Developer shall:**
 - a. **Retain the services of a qualified archaeologist who shall be on-site to monitor ground disturbing activities. In the event cultural resource material is encountered, the archaeologist is empowered to temporarily divert or halt grading to allow for coordination with the Luiseno Native American monitor and to determine the significance of the discovery. The archaeologist shall follow all standard procedures for cultural resource materials that are not Tribal Cultural Resources, in accordance with applicable laws and regulations including but not limited to the Carlsbad Tribal, Cultural and Paleontological Resources Guidelines (2017).**

- b. Enter into a Pre-Excavation Agreement, otherwise known as a Tribal Cultural Resources Treatment and Tribal Monitoring Agreement, with the San Luis Rey Band of Mission Indians or other Luiseno Native American tribe that meets all standard requirements of the tribe for such Agreements, in accordance with applicable laws and regulations including but not limited to the Carlsbad Tribal, Cultural and Paleontological Resources Guidelines (2017). This agreement will address provision of a Luiseno Native American monitor and contain provisions to address the proper treatment of any Tribal Cultural Resources and/or Native American human remains inadvertently discovered during the course of the project. The agreement will outline the roles and powers of the Luiseno Native American monitor and the archaeologist.**
25. Tier 4 or diesel construction equipment with diesel particulate filters shall be utilized for all construction activities. Grading and building plans shall note this requirement and the Developer shall adhere to the condition for all construction phases of the project.

Engineering:

General

26. Prior to hauling dirt or construction materials to or from any proposed construction site within this project, developer shall apply for and obtain approval from, the City Engineer for the proposed haul route.
27. This project is approved upon the express condition that building permits will not be issued for the development of the subject property unless the District Engineer has determined that adequate water and sewer facilities are available at the time of permit issuance and will continue to be available until time of occupancy.
28. Developer shall submit to the City Engineer an acceptable instrument, via CC&Rs and/or other recorded document, addressing the maintenance, repair, and replacement of shared private improvements within this subdivision, including but not limited to private driveways, utilities, landscaping, trees, sidewalks, parking spaces, stormwater treatment quality treatment facilities and street lights located therein and to distribute the costs of such maintenance in an equitable manner among the owners of the properties within this subdivision. The CC&Rs shall include a requirement to provide an annual verification of the effective operation and maintenance of each structural treatment control BMP in accordance with the BMP maintenance agreement and the SWQMP. The annual verification shall be submitted to the enforcement official in a format as approved by the city prior to the start of the rainy season.
29. Developer shall include rain gutters on the building plans subject to the City Engineer's review and approval. Developer shall install rain gutters in accordance with said plans.
30. Developer shall prepare, submit and process for City Engineer approval a **ministerial Parcel Map**. There shall be one **Parcel Map** recorded for this project. Developer shall pay the city standard map review plan check fees.
31. Developer shall install sight distance corridors at all street intersections and driveways in accordance with City Engineering Standards **or applicable standards within the Village & Barrio Master Plan, whichever is more restrictive**. The property owner shall maintain this condition.

32. Developer shall submit to the City Engineer written approval from North County Transit District (NCTD) demonstrating mass-transit improvement requirements for this project have been satisfied.
33. Property owner shall maintain all landscaping (street trees, tree grates, shrubs, groundcover, etc.) and irrigation along the parkway frontage with **Carlsbad Village Drive and Oak Avenue** as shown on the Site Plan.

Fees/Agreements

34. Developer shall cause property owner to execute and submit to the City Engineer for recordation, the city's standard form Geologic Failure Hold Harmless Agreement.
35. Developer shall cause property owner to execute and submit to the City Engineer for recordation the city's standard form Drainage Hold Harmless Agreement.
36. Developer shall cause property owner to submit an executed copy to the City Engineer for recordation a city standard Permanent Stormwater Quality Best Management Practice Maintenance Agreement. **Developer shall include all private permanent stormwater quality best management practices as well as maintenance of the public curb inlet on Oak Avenue that drains storm water to the treatment facility on site.**
37. Developer shall cause property owner to apply for, execute, and submit, to the City Engineer for recordation, an Encroachment Agreement covering private storm drain located over the existing public right-of-way or easements as shown on the site plan. Developer shall pay processing fees **per the city's FY 2022-23 fee schedule, which was effective at the time of the Senate Bill 330 (SB-330) preliminary housing development pre-application submittal date of March 22, 2023.**
38. **The developer may be eligible for a reimbursement agreement for the sewer main and water main replacement on Carlsbad Village Drive and Harding Street, for portions beyond the frontage of the project, as approved by the District Engineer in proportion to generation and use by the development.**

Grading

39. Based upon a review of the proposed grading and the grading quantities shown on the site plan, a grading permit for this project is required. Developer shall prepare and submit plans and technical studies/reports as required by City Engineer, post security and pay all applicable grading plan review and permit fees **per the city's FY 2022-23 fee schedule, which was effective at the time of the Senate Bill 330 (SB-330) preliminary housing development pre-application submittal date of March 22, 2023.**
40. Prior to issuance of the grading permit, the contractor shall submit a Construction Plan to the City Engineer for review and approval. Said Plan may be required to include, but not be limited to, identifying the location of the construction trailer, material staging, bathroom facilities, parking of construction vehicles, employee parking, construction fencing and gates, obtaining any necessary permission for off-site encroachment, addressing pedestrian safety, and identifying time restrictions for various construction activities.

41. Concurrent with the grading plans Developer shall include shoring plans as part of the grading plans to the satisfaction of the City Engineer and Building Official. Structural calculations for all shoring shall be submitted for review and approval by the building division. Developer shall pay all deposits necessary to cover any 3rd party review.

Storm Water Quality

42. Developer shall comply with the city's Stormwater Regulations, latest version, and shall implement best management practices at all times. Best management practices include but are not limited to pollution control practices or devices, erosion control to prevent silt runoff during construction, general housekeeping practices, pollution prevention and educational practices, maintenance procedures, and other management practices or devices to prevent or reduce the discharge of pollutants to stormwater, receiving water or stormwater conveyance system to the maximum extent practicable. Developer shall notify prospective owners and tenants of the above requirements.
43. Developer shall submit for city approval a Tier 3 Storm Water Pollution Prevention Plan (TIER 3 SWPPP). The TIER 3 SWPPP shall comply with current requirements and provisions established by the San Diego Regional Water Quality Control Board and City of Carlsbad Requirements. The TIER 3 SWPPP shall identify and incorporate measures to reduce storm water pollutant runoff during construction of the project to the maximum extent practicable. Developer shall pay all applicable SWPPP plan review and inspection fees **per the city's FY 2022-23 fee schedule, which was effective at the time of the Senate Bill 330 (SB-330) preliminary housing development pre-application submittal date of March 22, 2023.**
44. This project is subject to 'Priority Development Project' requirements and **Trash Capture Requirements**. Developer shall prepare and process a Storm Water Quality Management Plan (SWQMP), subject to City Engineer approval, to comply with the Carlsbad BMP Design Manual latest version. The final SWQMP required by this condition shall be reviewed and approved by the City Engineer with final grading plans. Developer shall pay all applicable SWQMP plan review and inspection fees **per the city's FY 2022-23 fee schedule, which was effective at the time of the Senate Bill 330 (SB-330) preliminary housing development pre-application submittal date of March 22, 2023.**
45. Developer is responsible to ensure that all final design plans (grading plans, improvement plans, landscape plans, building plans, etc.) incorporate all source control, site design, pollutant control BMP and applicable hydromodification measures.

Dedications/Improvements

46. Developer shall cause owner to submit to the City Engineer for recordation a covenant of easement for private reciprocal access, parking and drainage purposes as shown on the site plan. **The offer shall be made by certificate on the parcel map or separate recorded document.** Developer shall pay processing fees **per the city's FY 2022-23 fee schedule, which was effective at the time of the Senate Bill 330 (SB-330) preliminary housing development pre-application submittal date of March 22, 2023.**
47. Developer shall cause owner to dedicate to the city and/or other appropriate entities easements

for public street and utility purposes **as shown on the Site Plan: 8-foot wide along Carlsbad Village Drive, portions of Oak Avenue to construct a cul-de-sac, an unobstructed 4-foot wide fire access easement along the Hope Avenue Alley, a public drainage easement and a water easement for public water and access** purposes as shown on the Site Plan. The offer shall be made by a certificate on the parcel map. All land so offered shall be free and clear of all liens and encumbrances and without cost to the city. Streets that are already public are not required to be rededicated. Additional easements may be required at final design to the satisfaction of the City Engineer. **The city will also vacate a portion of the public right-of-way on Oak Avenue, as shown on the Site Plan.**

48. Developer shall design the private drainage systems, as shown on the site plan to the satisfaction of the City Engineer. All private drainage systems (12-inch diameter storm drain and larger) shall be inspected by the city. Developer shall pay the standard improvement plan check and inspection fees for private drainage systems.
49. Prior to any work in city right-of-way or public easements, Developer shall apply for and obtain a right-of-way permit to the satisfaction of the City Engineer.
50. Developer shall prepare and process public improvement plans; and prior to City Engineer approval of said plans, shall execute a city standard Development Improvement Agreement to install and shall post security in accordance with Carlsbad Municipal Code Section 20.16.070 for public improvements shown on the site plan. Said improvements shall be installed to city standards to the satisfaction of the City Engineer. **As shown on the Site Plan, these improvements include, but are not limited to:**
 - a. **Replace curb, gutter, sidewalk and street trees on Carlsbad Village Drive. Install 10-foot wide sidewalk.**
 - b. **Remove and install new ADA compliant driveways and curb ramps on Carlsbad Village Drive.**
 - c. **Install an Americans with Disabilities Act (ADA) compliant bus stop pad, a bus stop shelter, a bench, trash receptacle, lighting, and connecting sidewalk along Carlsbad Village Drive. All bus stop furniture (i.e., shelter, bench, trash receptacle, etc.) shall consist of the brown color type to the satisfaction of the City Engineer and City Planner.**
 - d. **Install curb, gutter, sidewalk, driveways and street trees along Oak Avenue.**
 - e. **Replace 10-inch sewer main with new 12-inch PVC sewer main in Harding Street and rehabilitate manholes. Sewer replacement diameter to be confirmed by District Engineer during final engineering.**
 - f. **Replace 6-inch VCP sewer main with new 8-inch PVC sewer main in Carlsbad Village Drive and rehabilitate manholes. Sewer replacement diameter to be confirmed by District Engineer during final engineering.**
 - g. **Replace 6-inch AC waterline with new 8-inch PVC water main in Carlsbad Village Drive. Water replacement diameter to be confirmed by District Engineer during final engineering.**
 - h. **Install 8-inch PVC Water in Oak Avenue as shown on the Site Plan.**
 - i. **Install water services, fire Services and sewer services necessary to serve the project.**
 - j. **Install water and sewer appurtenances generally, as shown on the Site Plan.**
 - k. **Provide sewer bypass and water highline service during construction.**
 - l. **Along project frontage, AC grind and overlay full width of Oak Avenue, south half of Carlsbad Village Drive and the east half of the Hope Avenue Alley. Outside of the project frontage, AC grind and overlay per Engineering Standards.**

- m. **Developer shall remove and replace structural section to meet Fire Department standards if existing structural section of the Hope Avenue Alley is found to be insufficient.**
- n. **Developer shall remove and replace existing pedestrian lighting on Carlsbad Village Drive in compliance with city standards.**

Additional public improvements required in other conditions of this resolution are hereby included in the above list by reference. Developer shall pay the standard improvement plan check and inspection fees in accordance with **the city's FY 2022-23 fee schedule, which was effective at the time of the Senate Bill 330 (SB-330) preliminary housing development pre-application submittal date of March 22, 2023.** Improvements listed above shall be constructed within 36 months of approval of the subdivision or development improvement agreement or such other time as provided in said agreement.

- 51. Developer shall design, and obtain approval from the City Engineer, the structural section for the access aisles with a traffic index of 5.0 in accordance with city standards due to truck access through the parking area and/or aisles with an ADT greater than 500. Prior to completion of grading, the final structural pavement design of the aisle ways shall be submitted together with required R-value soil test information subject to the review and approval of the City Engineer.
- 52. Developer is responsible to ensure all existing overhead utilities servicing the subject property are to be undergrounded as shown on the Site Plan and to the satisfaction of the City Engineer. No new or relocated utility poles are allowed.
- 53. Developer is responsible to ensure utility transformers or raised water backflow preventers that serve this development are located outside the right-of-way as shown on the Site Plan and to the satisfaction of the City Engineer. These facilities shall be constructed within the property.
- 54. **Developer shall provide evidence of quitclaim all existing easements from San Diego Gas & Electric that are conflicting with the proposed design of this project.**
- 55. **Developer shall have the public improvement plans reviewed by a Certified Access Specialist person (CASP) for disabled accessibility compliance with local, state and federal regulations and submit a letter from the CASP confirming that the designed plans are in conformance with said regulations.**

Non-Mapping Notes

- 56. Add the following notes to the parcel map as non-mapping data:
 - A. Developer has executed a city standard Development Improvement Agreement and has posted security in accordance with Carlsbad Municipal Code Section 20.16.070 to install public improvements shown on the Site Plan. **As shown on the Site Plan, these improvements include, but are not limited to:**
 - a. **Replace curb, gutter, sidewalk and street trees on Carlsbad Village Drive.**
 - b. **Remove and install new ADA compliant driveways and curb ramps on Carlsbad Village Drive. Install 10-foot wide sidewalk.**
 - c. **Install an American with Disabilities Act (ADA) compliant bus stop pad, a bus stop shelter, a bench, trash receptacle, lighting, and connecting sidewalk along Carlsbad**

Village Drive. All bus stop furniture (i.e., shelter, bench, trash receptacle, etc.) shall consist of the brown color type to the satisfaction of the City Engineer and City Planner.

- d. Install curb, gutter, sidewalk, driveways and street trees along Oak Avenue.**
 - e. Replace 10-inch sewer main with new 12-inch PVC sewer main in Harding Street and rehabilitate manholes. Sewer replacement diameter to be confirmed by District Engineer during final engineering.**
 - f. Replace 6-inch VCP sewer main with new 8-inch PVC sewer main in Carlsbad Village Drive and rehabilitate manholes. Sewer replacement diameter to be confirmed by District Engineer during final engineering.**
 - g. Replace 6-inch AC waterline with new 8-inch PVC water main in Carlsbad Village Drive. Water replacement diameter to be confirmed by District Engineer during final engineering.**
 - h. Install 8-inch PVC Water in Oak Avenue as shown on the Site Plan.**
 - i. Install water services, fire Services and sewer services necessary to serve the project.**
 - j. Install water and sewer appurtenances, generally as shown on the Site Plan.**
 - k. Provide sewer bypass and water highline service during construction.**
 - l. AC grind and overlay full width of Oak Avenue, south half of Carlsbad Village Drive, and east half of the Hope Avenue Alley.**
 - m. Developer shall remove and replace structural section to meet Fire Department standards if existing structural section of the Hope Avenue Alley is found to be insufficient.**
 - n. Developer shall remove and replace existing pedestrian lighting on Carlsbad Village Drive in compliance with city standards.**
- B. Building permits will not be issued for development of the subject property unless the appropriate agency determines that sewer and water facilities are available.
- C. Geotechnical Caution:
- a. The owner of this property on behalf of itself and all of its successors in interest has agreed to hold harmless and indemnify the City of Carlsbad from any action that may arise through any geological failure, ground water seepage or land subsidence and subsequent damage that may occur on, or adjacent to, this subdivision due to its construction, operation or maintenance.
- D. No structure, fence, wall, tree, shrub, sign, or other object may be placed or permitted to encroach within the area identified as a sight distance corridor as defined by City of Carlsbad Engineering Standards or **the applicable standards within the Village & Barrio Master Plan, whichever is more restrictive.**
- E. The owner of this property on behalf of itself and all of its successors in interest has agreed to hold harmless and indemnify the City of Carlsbad from any action that may arise through any diversion of waters, the alteration of the normal flow of surface waters or drainage, or the concentration of surface waters or drainage from the drainage system or other improvements identified in the city approved development plans; or by the design, construction or maintenance of the drainage system or other improvements identified in the city approved development plans.

Utilities

57. Developer shall meet with the Fire Marshal to determine if fire protection measures (fire flows, fire hydrant locations, building sprinklers) are required to serve the project.
58. Developer shall design and agree to construct public facilities within public right-of-way or within easements granted to the district or the City of Carlsbad **as shown on the Site Plan**. At the discretion of the district or City Engineer, wider easements may be required for adequate maintenance, access and/or joint utility purposes.
59. Developer shall install potable water services and meters at locations approved by the District Engineer. The locations of said services shall be reflected on public improvement plans.
60. The developer shall agree to install sewer laterals and clean-outs at locations approved by the City Engineer. The locations of sewer laterals shall be reflected on public improvement plans.
61. The developer shall design and agree to construct public water **and** sewer facilities substantially as shown on the Site Plan to the satisfaction of the District Engineer and City Engineer.
62. The potable water service for this project shall be master-metered which shall be located within a water easement subject to approval by the District Engineer. Developer shall install private sub-meters as necessary for all proposed units in the building. Final meter design, backflow preventer, size, and manufacturer shall be provided to the satisfaction of the District Engineer and shown on public improvement plans.
63. The developer shall modify as needed the existing sewer study, prepared by a registered engineer, that identifies the peak flows of the project, required pipe sizes, depth of flow in pipe, velocity in the main lines, and the capacity of the existing infrastructure to confirm final sewer main replacement diameter for construction plans.

Fire & Life Safety:

64. **The project is subject to an Alternate Materials & Methods Request approved by the Fire & Life Safety Division and dated June 23, 2023.**

Code Reminders

The project is subject to all applicable provisions of local ordinances, including but not limited to the following:

65. **Government Code Section 65589.5, subsection (o), states that, subject to certain exceptions and limitations, a housing development project shall be subject only to the ordinance, policies, and standards adopted and in effect when a complete preliminary application was submitted. The applicant submitted a complete SB 330 preliminary application for the project on March 22, 2023.**
66. Developer shall pay planned local area drainage fees in accordance with Section 15.08.020 of the Carlsbad Municipal Code to the satisfaction of the City Engineer.

67. Developer shall pay traffic impact and sewer impact fees based on Section 18.42 and Section 13.10 of the City of Carlsbad Municipal Code, respectively. The Average Daily Trips (ADT) and floor area contained in the staff report and shown on the site plan are for planning purposes only.
68. Subdivider shall comply with Section 20.16.040(D) of the Carlsbad Municipal Code regarding the undergrounding of existing overhead utilities.
69. Approval of this request shall not excuse compliance with all applicable sections of the Zoning Ordinance and all other applicable city ordinances in effect at time of building permit issuance, except as otherwise preempted by **State law** or specifically provided herein.
70. Any signs proposed for this development shall at a minimum be designed in conformance with the **Village & Barrio Master Plan**, shall require review and approval of the City Planner prior to installation of such signs.
71. Developer acknowledges that the project is required to comply with the city's greenhouse gas (GHG) reduction ordinances and requirements. GHG reduction requirements are in accordance with, but are not limited to, Carlsbad Municipal Code Chapters 18.21, 18.30, and 18.51 in addition to the California Green Building Standards Code (CCR, Title 24, Part 11 – CALGreen), as amended from time to time. GHG reduction requirements may be different than what is proposed on the project plans or in the Climate Action Plan Checklist originally submitted with this project. Developer acknowledges that new GHG reduction requirements related to energy efficiency, photovoltaic, electric vehicle charging, water heating and traffic demand management requirements as set forth in the ordinances and codes may impact, but are not limited to, site design and local building code requirements. If incorporating GHG reduction requirements results in substantial modifications to the project, then prior to issuance of development (grading, building, etc.) permits, Developer may be required to submit and receive approval of a Consistency Determination or Amendment for this project through the Planning Division. Compliance with the applicable GHG reduction requirements must be demonstrated on or with the construction plans prior to issuance of the applicable development permits.

NOTICE

Please take **NOTICE** that approval of your project includes the “imposition” of fees, dedications, reservations, or other exactions hereafter collectively referred to for convenience as “fees/exactions.”

You have 90 days from date of final approval to protest imposition of these fees/exactions. If you protest them, you must follow the protest procedure set forth in Government Code §66020(a) and file the protest and any other required information with the City Manager for processing in accordance with Carlsbad Municipal Code Section 3.32.030. Failure to timely follow that procedure will bar any subsequent legal action to attack, review, set aside, void, or annul their imposition.

You are hereby FURTHER NOTIFIED that your right to protest the specified fees/exactions DOES NOT APPLY to water and sewer connection fees and capacity charges, nor planning, zoning, grading, or other similar application processing or service fees in connection with this project; NOR DOES IT APPLY to any fees/exactions of which you have previously been given a NOTICE similar to this, or as to which the statute of limitations has previously otherwise expired.

PASSED, APPROVED, AND ADOPTED at a regular meeting of the Planning Commission of the City of Carlsbad, California, held on **June 5, 2024**, by the following vote, to wit:

AYES:

NAYES:

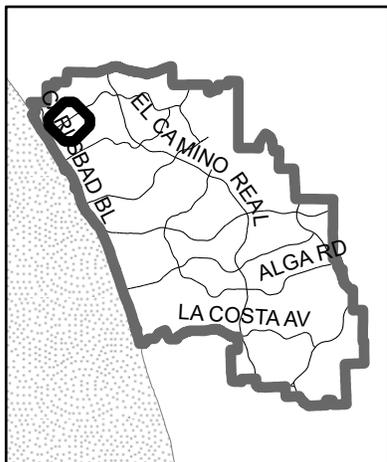
ABSENT:

ABSTAIN:

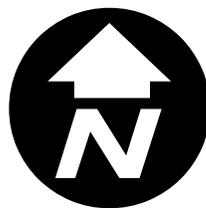
WILLIAM KAMENJARIN, Chairperson
CARLSBAD PLANNING COMMISSION

ATTEST:

ERIC LARDY
City Planner



SITE MAP



NOT TO SCALE

Carlsbad Village Mixed Use SDP 2023-0014



DISCLOSURE STATEMENT
P- 1(A)

Development Services

Planning Division
1635 Faraday Avenue
(442) 339-2610
www.carlsbadca.gov

Applicant's statement or disclosure of certain ownership interests on all applications which will require discretionary action on the part of the City Council or any appointed Board, Commission or Committee.

The following information **MUST** be disclosed at the time of application submittal. Your project cannot be reviewed until this information is completed. Please print.

Note:

Person is defined as "Any individual, firm, co-partnership, joint venture, association, social club, fraternal organization, corporation, estate, trust, receiver, syndicate, in this and any other county, city and county, city municipality, district or other political subdivision or any other group or combination acting as a unit."

Agents may sign this document; however, the legal name and entity of the applicant and property owner must be provided below.

1. **APPLICANT** (Not the applicant's agent)

Provide the **COMPLETE, LEGAL** names and addresses of **ALL** persons having a financial interest in the application. If the applicant includes a corporation or partnership, include the names, titles, addresses of all individuals owning more than 10% of the shares. IF NO INDIVIDUALS OWN MORE THAN 10% OF THE SHARES, PLEASE INDICATE NON-APPLICABLE (N/A) IN THE SPACE BELOW. If a publicly-owned corporation, include the names, titles, and addresses of the corporate officers. (A separate page may be attached if necessary.)

Person Tooley Interests LLC Corp/Part _____
Title Patrick Tooley, Manager Title _____
Address 11661 San Vicente Blvd, Suite 850 Address _____
Los Angeles, CA 90049

2. **OWNER** (Not the owner's agent)

Provide the **COMPLETE, LEGAL** names and addresses of **ALL** persons having any ownership interest in the property involved. Also, provide the nature of the legal ownership (i.e., partnership, tenants in common, non-profit, corporation, etc.). If the ownership includes a corporation or partnership, include the names, titles, addresses of all individuals owning more than 10% of the shares. IF NO INDIVIDUALS OWN MORE THAN 10% OF THE SHARES, PLEASE INDICATE NON-APPLICABLE (N/A) IN THE SPACE BELOW. If a publicly-owned corporation, include the names, titles, and addresses of the corporate officers. (A separate page may be attached if necessary.)

Person GRT Carlsbad Village LLC Corp/Part _____
Title Patrick Tooley, Managing Member Title _____
Address 11661 San Vicente Blvd, Suite 850 Address _____
Los Angeles, CA 90049

RECEIVED

MAY 17 2023

CITY OF CARLSBAD
PLANNING DIVISION

3. **NON-PROFIT ORGANIZATION OR TRUST**

If any person identified pursuant to (1) or (2) above is a nonprofit organization or a trust, list the names and addresses of **ANY** person serving as an officer or director of the non-profit organization or as trustee or beneficiary of the.

Non Profit/Trust _____	Non Profit/Trust _____
Title _____	Title _____
Address _____	Address _____
_____	_____

4. Have you had more than \$500 worth of business transacted with any member of City staff, Boards, Commissions, Committees and/or Council within the past twelve (12) months?

Yes No If yes, please indicate person(s): _____

NOTE: Attach additional sheets if necessary.

I certify that all the above information is true and correct to the best of my knowledge.

ASJ 5/15/23
Signature of owner/date

ASJ 5/15/23
Signature of applicant/date

GRT Carlsbad Village LLC
Print or type name of owner

Tooley Interests LLC
Print or type name of applicant

Jonathan Frankel 5/16/23
Signature of owner/applicant's agent if applicable/date

Jonathan Frankel
Print or type name of owner/applicant's agent

Tooley Interests LLC
11661 San Vicente Blvd, Suite 850
Los Angeles, CA 90049

City of Carlsbad Community Development Department
Planning Division
1635 Faraday Avenue
Carlsbad, CA 90028

Re: Letter of Authorization
Land Use Review Application; 945-1065 Carlsbad Village Drive, APN's: 203-320-53, 203-320-54,
203-320-55, 203-320-56

Dear City of Carlsbad Community Development Department:

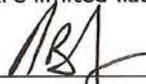
Tooley Interests LLC, a Delaware limited liability company ("TI"), hereby certifies that Patrick Tooley, an individual, is a member of TI and is legally authorized to sign and bind the organization on its behalf.

Sincerely,

"TI"

Tooley Interests LLC
a Delaware limited liability company

By:



Patrick Tooley
Manager

Date: May 15, 2023

PROJECT ANALYSIS
(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

PROJECT ANALYSIS

The project is subject to the following regulations:

- A. Village-Barrio (V-B) General Plan Land Use Designation
- B. Village-Barrio (V-B) Zone (CMC Chapter 21.35), Village & Barrio Master Plan; and Density Bonus Ordinance (CMC Chapter 21.86)
- C. Site Development Plan (CMC Chapter 21.06)
- D. Inclusionary Housing Ordinance (Chapter 21.85)
- E. Growth Management Ordinance (CMC Chapter 21.90) and Local Facilities Management Plan Zone 1

The recommendation for approval of this project was developed by analyzing the project’s consistency with the applicable regulations and policies. The project’s compliance with each of the above regulations is discussed in detail within the sections below.

A. Village-Barrio (V-B) General Plan Land Use Designation

The subject property has a General Plan Land Use designation of Village-Barrio (V-B). As discussed in the Village & Barrio Master Plan, properties within the Village do not have an assigned residential density as it relates to Growth Management Plan compliance. For discussion regarding residential density see section B below. The project’s compliance with the various Elements of the General Plan is outlined in Table “A” below:

TABLE A – GENERAL PLAN COMPLIANCE

ELEMENT	USE, CLASSIFICATION, GOAL, OBJECTIVE, OR PROGRAM	PROPOSED USES & IMPROVEMENTS	COMPLY
Land Use & Community Design	<u>Goal 2-G.2</u> – Promote a diversity of compatible land uses throughout the city, to enable people to live close to job locations, adequate and convenient commercial services, and public support systems such as transit, parks, schools, and utilities.	The mixed-use project provides 13,800 square feet of new neighborhood-serving commercial development that is oriented towards Carlsbad Village Drive in accordance with the Village & Barrio Master Plan, 218 multiple-family residential apartment units (191 market rate and 27 affordable) and 340 parking spaces all located near jobs, schools, beaches, lagoons, and other convenient neighborhood-serving commercial services. The project provides 340 parking spaces onsite and also has access to public transit via the Carlsbad Village Station providing bus, train, and Coaster services, as well as NCTD bus route 315 providing service between Camp Pendelton, Sprinter, and the Carlsbad Village Station.	Yes

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

ELEMENT	USE, CLASSIFICATION, GOAL, OBJECTIVE, OR PROGRAM	PROPOSED USES & IMPROVEMENTS	COMPLY
Land Use & Community Design	<p><u>Goal 2-G.3</u> – Promote infill development that makes efficient use of limited land supply, while ensuring compatibility and integration with existing uses. Ensure that infill properties develop with uses and development intensities supporting a cohesive development pattern.</p>	<p>The mixed-use project is proposed on a developed site surrounded by urban development. In addition to 13,800 square feet of new neighborhood-serving commercial development oriented towards Carlsbad Village Drive, the project will provide future residents with access to convenient neighborhood-serving commercial uses onsite and within the area, as well as providing new housing to support nearby employment centers.</p>	Yes
Land Use & Community Design	<p><u>Goal 2-G.4</u> – Provide balanced neighborhoods with a variety of housing types and density ranges to meet the diverse demographic, economic and social needs of residents, while ensuring a cohesive urban form with careful regard for compatibility.</p>	<p>The multiple-family residential portion of the mixed-use project includes 218 apartments units (191 market rate and 27 affordable) in a variety of bedroom counts and provides much-needed higher density housing located in close proximity to neighborhood-serving commercial uses, nearby employment centers and the mass transit station located in the core of Carlsbad Village.</p>	Yes
Land Use & Community Design	<p><u>Goal 2-G.30</u> Develop a distinct identity for the Village by encouraging a variety of uses and activities, such as a mix of residential, commercial office, restaurants and specialty retail shops, which traditionally locate in a pedestrian-oriented downtown area and attract visitors and residents from across the community by creating a lively, interesting social environment.</p> <p><u>Policy 2-P.70</u> Seek an increased presence of both residents and activity in the Village with new development, particularly residential, including residential as part of a mixed-use development,</p>	<p>The proposal to construct a mixed-use development with 13,800 square feet of commercial and 218 multiple-family residential apartments would enhance the vitality of the Village by providing new residential and updated commercial land uses near the downtown core area. The project reinforces the pedestrian orientation desired for the downtown area by providing residents an opportunity to walk to shopping, restaurants, recreation, and mass transit functions. The project’s proximity to existing bus routes and other mass transit helps to further the goal of providing new</p>	Yes

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

ELEMENT	USE, CLASSIFICATION, GOAL, OBJECTIVE, OR PROGRAM	PROPOSED USES & IMPROVEMENTS	COMPLY
	as well as commercial, entertainment and cultural uses that serve both residents and visitors.	economic development near transportation corridors.	
Mobility	<p><u>Goal 3-G.3</u> – Provide inviting streetscapes that encourage walking and promote livable streets.</p> <p><u>Policy 3-P.5</u> – Require developers to construct or pay their fair share toward improvements for all travel modes consistent with the Mobility Element, the Growth Management Plan, and specific impacts associated with their development.</p>	<p>Along Carlsbad Village Drive, the project is proposing an 8-foot right-of-way dedication and construction of a new 16-foot-wide parkway with 10-foot-wide sidewalks and 6-foot-wide landscape planters with street trees. On Oak Avenue, the project will maintain the existing 10-foot right-of-way along the project frontage, but will replace the existing curb, gutter, and sidewalk with a 10-foot-wide sidewalk reduced down to 5.5 feet at each tree well planter location. Along the north side of Oak Avenue, the project also proposes to improve the dead-end street design to a cul-de-sac pursuant to City of Carlsbad standards. In addition, the proposed project is located approximately 0.4 miles from the Carlsbad Village train station, which provides rail and bus service throughout the day. The project’s proximity to the transit station would provide visitors with new neighborhood-serving commercial and residents with the opportunity to commute to major job centers, thereby reducing vehicle miles traveled (VMTs) and the carbon footprint. Furthermore, the project supports walkability and mobility by locating the project near other existing goods and services within the Village.</p>	Yes

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

ELEMENT	USE, CLASSIFICATION, GOAL, OBJECTIVE, OR PROGRAM	PROPOSED USES & IMPROVEMENTS	COMPLY
Noise	<p><u>Goal 5-G.1</u> – Protect public health and welfare by eliminating existing noise problems where feasible, maintaining an acceptable indoor and outdoor acoustic environment, and preventing significant degradation of the acoustic environment.</p> <p><u>Goal 5-G.2</u> – Ensure that new development is compatible with the noise environment, by continuing to use potential noise exposure as a criterion in land use planning.</p>	<p>Dudek Environmental Consulting conducted a noise study (Dec. 2023) for the proposed project. The principal noise source associated with the proposed project is traffic generated from the Interstate-5 freeway and the surrounding street system. As it relates to exterior use areas, the project will include outdoor usable space within multiple interior courtyards. Due to sound-blocking acoustical shielding afforded by the onsite positions and multi-floor structural arrangement of the proposed buildings, these exterior use areas will not be subject to noise exposure levels exceeding the City’s 65 dBA CNEL standard. As it relates to interior spaces, pursuant to Chapter 18.04.080 of the Carlsbad Municipal Code (CMC), any new residence or addition of one or more habitable rooms to an existing residence located within the noise impact boundary of an airport or freeway must be designed to ensure that internal noise levels due to airport or freeway operations do not exceed 45 dB. The project will not be built without adequately demonstrating compliance with the California Building Code (CBC), as locally amended. Modeling was performed to determine interior noise levels from street-facing units of the project. An assessment was conducted on street-facing units intended for residential occupation since the facade noise levels were modeled above 60 dBA CNEL. As a project design feature, glass assemblies would require a closed window condition to reduce the</p>	Yes

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

ELEMENT	USE, CLASSIFICATION, GOAL, OBJECTIVE, OR PROGRAM	PROPOSED USES & IMPROVEMENTS	COMPLY
		<p>interior noise levels below the City’s 45 dBA CNEL threshold. Therefore, mechanical ventilation (e.g., air conditioning) will be installed in impacted street-facing units to move air within the structure and control temperature when windows are closed. Pursuant to standard building permit procedures, a final interior noise assessment will be conducted to ensure interior noise reductions are met. As is relates to airport noise, the nearest airport is McClellan-Palomar Airport, which is approximately 4.0 miles southeast of the project site and is located outside of all CNEL noise contours outlined in the McClellan-Palomar Airport Land Use Compatibility Plan (ALUCP) and included in the City’s General Plan.</p>	
Public Safety	<p><u>Goal 6-G.1</u> – Minimize injury, loss of life, and damage to property resulting from fire, flood, hazardous material release, or seismic disasters.</p>	<p>The proposed project is not located in any Very High Fire Severity Zone, Flood Zones, or Earthquake Fault Zones. The proposed structural improvements would be required to meet all seismic design standards at time of building permit in accordance with the California Building Code. The Fire & Life Safety Division has approved the proposed conceptual building design with fire sprinklers included throughout per the National Fire Protection Association (NFPA) standards. In addition, an Alternative Means and Method Request (AM&M) plan has been reviewed and approved by the Fire & Life Safety Division (June 23, 2023) and will be implemented at the construction stage of development.</p>	Yes
Public Safety	<p><u>Goal 6-G.4</u> – Maintain safety services that are responsive to</p>	<p>The proposed project will provide all supporting water infrastructure</p>	Yes

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

ELEMENT	USE, CLASSIFICATION, GOAL, OBJECTIVE, OR PROGRAM	PROPOSED USES & IMPROVEMENTS	COMPLY
	citizens' needs to ensure a safe and secure environment for people and property in the community.	in accordance with Fire & Life Safety requirements; fire sprinklers are required and provided for all residential structures; and the dwelling units proposed by this project are all within a five minute emergency response time. In addition, the existing six-inch (6") diameter water line located within the Carlsbad Village Drive project frontage right-of-way is proposed to be replaced with a new eight-inch (8") diameter water line.	
Public Safety	<u>Policy 6-P.6</u> – Enforce the requirements of Titles 18, 20, and 21 pertaining to drainage and flood control when reviewing applications for building permits and subdivisions.	The project has been conditioned to develop and implement a program of “best management practices” for the elimination and reduction of pollutants which enter and/or are transported within storm drainage facilities.	Yes
Public Safety	<u>Policy 6-P.34</u> – Enforce the Uniform Building and Fire codes, adopted by the city, to provide fire protection standards for all existing and proposed structures.	The project is required to comply with all Building and Fire codes to ensure that fire protection standards are met by the proposed structures. The Fire & Life Safety Division has approved conceptual building designs with fire sprinklers included throughout per the National Fire Protection Association (NFPA) standards. In addition, an Alternative Means and Method Request (AM&M) plan has been reviewed and approved by the Fire & Life Safety Division dated June 23, 2023 and shall be implemented at the construction stage of development. Therefore, the proposed project is consistent with the applicable fire safety requirements.	Yes
Public Safety	<u>Policy 6-P.39</u> – Ensure all new development complies with all applicable regulations regarding the	The project is required to construct or pay applicable fees for necessary improvements, public utilities, and facilities in accordance with Growth	Yes

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

ELEMENT	USE, CLASSIFICATION, GOAL, OBJECTIVE, OR PROGRAM	PROPOSED USES & IMPROVEMENTS	COMPLY
	provision of public utilities and facilities.	Management requirements; Fire Station No. 1, located less than 0.25-miles of the project site, is within emergency response timeframes; and the project design will not affect the city’s ability to implement its Emergency Operations Plan. Improvements to existing utilities includes the replacement of an existing six-inch (6”) diameter water line located within the Carlsbad Village Drive project frontage right-of-way with a new eight-inch (8”) diameter water line.	
Sustainability	<u>Policy 9-P.1</u> – Enforce the Climate Action Plan (CAP) as the city’s strategy to reduce greenhouse gas emissions.	The project implements and is consistent with measures identified in the Climate Action Plan (CAP) through the provision of renewable energy generation (photovoltaic systems), energy conservation (Green Building Code), and by accommodating Zero-Emission vehicles and other greenhouse gas reduction measures and features. At building permit, the project will also be reviewed for compliance with the CAP ordinances.	Yes
Housing	<u>Goal 10-G.3</u> – Sufficient new, affordable housing opportunities in all quadrants of the city to meet the needs of current lower and moderate-income households and those with special needs, and a fair share proportion of future lower and moderate-income households.	Per CMC Chapter 21.85, a project is required to provide 15% of the total residential units as affordable for lower income households. Pursuant to City Council Policy No. 57, the percentage can be reduced to 12.5% if the inclusionary units are very low or extremely low-income, the units are located on the same site as the market-rate units and the Developer is not requesting financial assistance from the city. The project proposes 27 very low-income units onsite, and the Developer is not requesting financial assistance from the city. In	Yes

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

ELEMENT	USE, CLASSIFICATION, GOAL, OBJECTIVE, OR PROGRAM	PROPOSED USES & IMPROVEMENTS	COMPLY
		<p>addition, the project is conditioned to enter into an Affordable Housing Agreement with the city to provide and deed restrict 27 dwelling units as affordable to very low-income households for 55 years, earning up to 50% of the area median income (AMI).</p>	
Housing	<p>Policy 10-G.2 – New housing developed with diversity of types, prices, tenures, densities, and locations, and in sufficient quantity to meet the demand of anticipated city and regional growth.</p> <p><u>Policy 10-P.15</u> – Pursuant to the Inclusionary Housing Ordinance, require affordability for lower income households of a minimum 15 percent of all residential ownership and qualifying rental projects. For projects that are required to include 10 or more units affordable to lower income households, at least 10 percent of the lower income units should have three or more bedrooms (lower income senior housing projects are exempt).</p>	<p>The proposed project will increase the diversity of housing in Carlsbad by adding 218 multiple-family apartment units to the city’s housing inventory. In addition, the project will increase housing diversity by providing housing offered at a price affordable to very low-income households. The project is conditioned to enter into an Affordable Housing Agreement with the city to provide and deed restrict 27 dwelling units (12.5%) as affordable to very low-income households for 55 years, earning up to 50% of the area median income (AMI). Additionally, of those 27 affordable units, a minimum of three (3) units will have three-bedrooms, consistent with requirements of the city’s Inclusionary Housing Ordinance.</p>	Yes
Housing	<p><u>Policy 10-P.19</u> – Address the unmet housing needs of the community through new development and housing that is set aside for lower and moderate-income households consistent with priorities set by the Housing and Neighborhood Services Division, in collaboration with the Planning Division, as set forth in the city’s Consolidated Plan.</p>	<p>The city currently has a jobs/housing ratio where there are more jobs than housing. The project replaces an existing commercial shopping center with a new mixed-use development consisting of 13,800 square feet of neighborhood-serving commercial and 218 multiple-family apartments (191 market rate and 27 affordable). The residential portion of this mixed-use project expands the city’s affordable housing stock to assist in meeting its Regional</p>	Yes

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

ELEMENT	USE, CLASSIFICATION, GOAL, OBJECTIVE, OR PROGRAM	PROPOSED USES & IMPROVEMENTS	COMPLY
		Housing Needs Assessment and provides an adequate number of housing units to meet the needs of very low-income households.	

B. Village-Barrio (V-B) Zone (CMC Chapter 21.35), Village & Barrio Master Plan; and Density Bonus Ordinance (CMC Chapter 21.86)

The minimum and maximum densities for residential development are established in the Village & Barrio Master Plan. Table B below identifies the permissible density range for properties located within the Freeway Commercial (FC) District of the Village & Barrio Master Plan.

TABLE B – PROPOSED DENSITY

Gross Acres	Net Acres	Allowable Density Range, Dwelling Units per Acre FC District*	Project Density, Proposed Dwelling Units
4.12	4.12	28-35 du/ac Minimum: 58 dwelling units** Maximum: 145 dwelling units	52.9 du/ac 218 dwelling units

*For density bonus projects fractional units shall be rounded up per Section 21.86.050 of the Carlsbad Municipal Code.

**For mixed-use projects, the minimum density shall be calculated based on fifty percent of the developable area and the maximum density shall be calculated based on the entire developable area (Village & Barrio Master Plan, Chapter 2.4 - Density).

The residential portion of the proposed mixed-use development includes a request to construct a 218-unit, five-story multiple-family residential apartment project. As summarized above, the base number of units for a 4.12-acre parcel at 35 du/ac is 145 dwelling units. In order to construct 218 units, the Developer is requesting approval of a density bonus pursuant to CMC Chapter 21.86, the Density Bonus Ordinance, and California (hereinafter referred to as “State”) Density Bonus Law (Government Code §65915). CMC Chapter 21.86 was established as a means to implement California Government Code §65915 (State Density Bonus Law) and the goals, objectives, and policies of the Housing Element of the General Plan, which includes the provision to provide housing affordable to lower- and moderate-income households.

The legislative intent of State Density Bonus Law is to increase the production of affordable housing by requiring local agencies to grant an increase to the maximum allowable residential density over the otherwise maximum eligible density. The density bonus that is authorized is set on a sliding scale based upon the percentage of affordable units in the project, ranging from 5% to 50% additional units over the number ordinarily permitted. Likewise, State Density Bonus Law provides for a progressive approach to incentives or concessions, allowing up to four incentives or concessions in some cases. It also includes incentivization in the form of waivers from development standards, which do not count as incentives or concessions, and are unlimited. For projects located near transit stops, like the proposed project, additional concessions are afforded, which include restricting the city’s ability to require minimum

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

parking standards under Assembly Bill (AB) 2097 on certain types of private development projects.¹ Parking is discussed in further detail below.

In this instance, to support the implementation of the project, the Developer is requesting a 50% density bonus pursuant to State Density Bonus Law. In exchange for the 50% density bonus, the Developer is required to designate 15% of the 145 base maximum density units, or 22 units, as affordable density bonus units. The affordable density bonus units are required to be affordable to “very low-income households” for a period of 55 years.

In addition to the mandatory affordable housing provisions of State Density Bonus Law, the project is also required to comply with the city’s Inclusionary Housing Ordinance, which is intended to ensure that all residential developments provide a range of housing opportunities for all economic segments of the population, including households of lower and moderate income.² The 22 very low-income units required under State density bonus can also be used to satisfy the affordable housing requirements of the city’s Inclusionary Housing Ordinance pursuant to CMC Section 21.85.040 – Affordable housing standards.

Pursuant to the Inclusionary Housing Ordinance, projects are typically required to designate 15% of their total units as affordable to lower income households. However, pursuant to City Council Policy Statement No. 57, Inclusionary Housing Ordinance – Alternative Means of Compliance, the percentage of units designated as affordable to lower income households can be reduced to 12.5% if all of the following are satisfied:

1. All of the affordable units are made available to very low or extremely low-income households, or combination thereof.
2. The units are located on the same site as the market-rate units; and
3. No financial assistance from the city is required.

For this project, the Developer is proposing to satisfy the affordable housing requirements by providing 12.5% or a total of 27 units (12.5% multiplied by 218 total units = 27.25, rounded down to 27 units) as affordable to very low-income households. The inclusionary units will be located on the same site as the market-rate units and the Developer is not requesting financial assistance from the city. Therefore, the proposal to provide 27 very low-income units onsite complies with the Inclusionary Housing Ordinance, Density Bonus Ordinance and State Density Bonus Law.

Density bonus law stipulates that a request for a density bonus does not constitute a valid basis on which to find a proposed housing development project is not compliant with a general plan. In addition, State Density Bonus Law explicitly requires the city to consider “the density allowed under the land use element of the general plan” in determining maximum allowable residential density. As discussed above, properties designated V-B do not have an assigned residential density in the General Plan.

¹ Refer to Exhibit 5 for an info-bulletin that provides more information on restrictions imposed by the state legislature under AB 2097 that limit the city’s ability to require minimum parking standards on certain private development projects, including residential density bonus projects. In Carlsbad there are currently only two major public transit stops (i.e. Carlsbad Village Station and Poinsettia Station).

² Refer to Exhibit 8 for an info-bulletin that provides more information on how the Inclusionary Housing Ordinance works and Exhibit 9 for City Council Policy Statement Policy No. 57 which provides additional information on options to satisfy the intent of the Inclusionary Housing Ordinance.

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

Therefore, the minimum and maximum densities for residential development are established in the Village & Barrio Master Plan. The proposed density of the project complies with the FC District of the Village & Barrio Master Plan.

Incentives, Concessions or Waivers

The project is requesting incentives, concessions or waivers as allowed under State Density Bonus Law³ and implemented by CMC Chapter 21.86 – Density Bonus Ordinance. A project can request incentives and concessions as defined in State Density Bonus Law, based on the percentage of affordable units.

An incentive or concession may include any of the following:

- A reduction in site development standards or a modification of zoning code or architectural design requirements (excluding State Building Standards), that results in identifiable, financially sufficient and actual cost reductions. A reduction/modification to standards or requirements may include, but is not limited to, a reduction in minimum lot size, setback requirements, and/or in the ratio of vehicular parking spaces that would otherwise be required (if minimum parking standards are required).
- Other regulatory incentives or concessions that result in identifiable, financially sufficient and actual cost reductions.
- The city council may, but is not required to, provide direct financial incentives, including the provision of publicly owned land, or the waiver of fees or dedication requirements.

In addition to incentives or concessions, waiver or reduction of development standards that would “have the effect of physically precluding the construction of a density bonus housing development at the density or with the incentives or concessions permitted by” can also be considered. There is no limit on the number of waivers or reductions of development standards that may be granted, and the granting of a waiver or reduction shall neither increase nor decrease the number of incentives or concessions to which the project is entitled. Waivers or reductions of development standards shall be granted by the city unless certain findings can be made. In order to deny the requested waivers or reductions, the city would have to make any of the following findings in writing based upon substantial evidence:

- The standard(s) requested to be waived or reduced will not have the effect of physically precluding the construction of a housing development at the densities or with the incentives or concessions permitted by this chapter.
- The requested waiver or reduction of development standards would have a specific adverse impact (a significant, quantifiable, direct, and unavoidable impact, based on objective, identified written public health or safety standards, policies, or conditions as they existed on the date the application was deemed complete) upon public health and safety or on any real property that is listed in the California Register of Historical Resources, and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact.

³ Refer to Exhibit 7 for an info-bulletin that provides more information on how State Density Bonus Law works.

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

- The waiver or reduction of development standards would be contrary to state or federal law.

The Developer is requesting five waivers as discussed in Exhibit 6. Staff has found no substantial evidence that any of the above findings apply in this case.

Freeway Commercial (FC) District

The project’s compliance with the development standards specific to the Freeway Commercial (FC) District, as well as the applicable parking and area-wide standards within the Village & Barrio Master Plan are provided in Tables C, D and E below. In addition, the project generally complies with the intent of the Area-Wide Design Guidelines, which are included in Section 2.8 of the Village & Barrio Master Plan. Pursuant to Section 2.8, designers and developers should consider the guidelines as a starting point for quality development and do not comprise every possible strategy for achieving high-quality design.

**TABLE C- VILLAGE & BARRIO MASTER PLAN
FREEWAY COMMERCIAL (FC) DISTRICT COMPLIANCE ANALYSIS**

Standard	Required/Allowed	Proposed	Comply?
Setbacks - Front	Minimum: 10 feet Minimum 15 foot landscaped setback where surface parking areas are located adjacent to a public street.	Lot 1 (fronting Carlsbad Village Drive) Commercial Bldg. A: 16.1 feet Commercial Bldg. B: 16 feet All surface parking spaces, as it relates to Carlsbad Village Drive, are setback greater than 15 feet and landscaped. Lot 2 (fronting Oak Avenue) Residential Bldg. C: 11 feet Residential Bldg. D: 11 feet Parking Structure: 10 feet	Yes
Setbacks - Front	Encroachments permitted within the front setback: <ul style="list-style-type: none"> Maximum 5 feet: Awnings, canopies, upper floor balconies. Up to property line: Plazas, courtyards, and outdoor dining. 	Lot 1 (fronting Carlsbad Village Drive) Commercial Bldgs. A and B both include metal canopies and awnings projecting from the front elevation of the buildings. For both buildings, neither canopy nor awning encroach into the 10-foot setback. Lot 2 (fronting Oak Avenue) the upper floor balconies of the residential units encroach no greater than 5-feet.	Yes
Setbacks - Side	No minimum setback	Lot 1 (East Side) Commercial Bldg. B: 11 feet (approx.) Lot 2 (East Side)	Yes

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

Standard	Required/Allowed	Proposed	Comply?
		Parking Structure: ≥12 feet (approx.) Lot 1 (West Side) Commercial Bldg. A: 32 feet (approx.) Lot 2 (West Side) Residential Bldg. C: 6.9 feet (approx.)	
Setbacks - Rear	10 feet	Lot 1: ≥10 feet Lot 2: ≥10 feet	Yes
Lot Size and Dimension	Not applicable	Lot 1: 0.94 acres Lot 2: 3.11 acres	Yes
Building Height	Maximum 45 feet and 4 stories	Commercial Bldg. A: 24.5 feet / 1 story Commercial Bldg. B: 24.5 feet / 1 story Residential Bldg. C: 56 feet / 5 stories with architectural projections up to 60.5 feet for parapets, 66 feet for stair and elevator towers*. Residential Bldg. D: 53.8 feet / 5 stories with architectural projections up to 58.3 feet for parapets, 63.8 feet for stair towers, and 67.8 feet for elevator towers*. Parking Garage: 53.8 feet / 5 levels with architectural projections up to 58.3 feet for parapets, 63.8 feet for stair towers, and 67.8 feet for elevator towers*. *Developer is requesting a waiver to this standard (VBMP, 2.7.4(G)(1)) pursuant to the Density Bonus Ordinance and State Density Bonus Law. Please see Exhibit 6 for details.	Yes*
Building Height	A maximum of 30% of the 4 th story, street-facing facade can have a minimum 10-foot setback (as measured from property line). The remaining 70% shall be set back a minimum of 15 feet.	100% of the 4 th and 5 th stories of Residential Bldgs. C and D, fronting on Oak Avenue, have been setback 17'-8½".	Yes
Building Height	The total square footage of enclosed 4 th story floor space shall not exceed 80% of the of	*Developer is requesting a waiver to this standard (VBMP, 2.7.4(G)(2)(b)) pursuant to the Density Bonus	Yes*

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

Standard	Required/Allowed	Proposed	Comply?
	the largest enclosed floor space below (floors one, two and three). However, in no case shall the fourth floor enclosed space exceed the amount of the third floor enclosed space.	Ordinance and State Density Bonus Law. Please see Exhibit 6 for details.	
Building Massing	No building facade visible from any public street shall extend more than 50 feet in length without a 5-foot minimum variation in the wall plane, as well as a change in roofline.	Along Oak Avenue, the 4 th and 5 th stories of Residential Bldgs. C and D have been setback 17'-8½", which is 6-feet further beyond the three lower floors they are set over. The proposed roofline breaks in six locations. Along Interstate-5, metal louvered architectural structures project 5-feet beyond the face of the 2 nd , 3 rd , and 4 th levels to provide added screening of cars and to also provide variation along this frontage.	Yes
Property Open Space	A minimum of 20 percent of property must be maintained as open space.	Required: 35,893 SF Proposed: 40,870 SF	Yes
Residential Private Open Space	Minimum of 80 square feet of private open space per unit; minimum dimension of 6 feet in any direction.	All 218 apartment units have a private balcony with a minimum area of 80 SF and minimum dimension of 6 feet in any direction.	Yes
Residential Common Open Space	Minimum of 25 square feet per unit, active or passive: 218 x 25 = 5,450 square feet Minimum dimension, 10 feet x 10 feet	23,110 square feet. Common open space exceeds the minimum dimensions and includes two large courtyards, pool/spa, cabanas with fireplaces, turf areas for lawn games, barbecues and social cooking dining areas, and a roof top deck.	Yes

The project proposes to provide 284 parking spaces for the 218 multiple-family residential apartment units, which equates to 1.3 spaces per unit, thus exceeding the 0.5 space per unit requirement pursuant to State Density Bonus Law. An additional state law imposed by the state legislature, AB 2097, which became effective on January 1, 2023; prohibits cities from requiring minimum parking standards on certain private development projects, including residential density bonus projects, that are located within one-half mile of public transit. Under AB 2097, developers can still provide onsite parking, but the number of parking spaces provided is based on builder preference and market demand, not by city-established minimum parking standards. The parking spaces provided meet the minimum parking location and design standards. A summary of how the project would comply if the city could impose the minimum parking standards under State Density Bonus Law is provided in Table "D" below.

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The project is proposing to utilize the Density Bonus Parking Standards per Government Code §65915(p)(1). Table “D” below compares the parking standards from the Village & Barrio Master Plan Area-Wide Parking Standards versus that being requested in accordance with a density bonus project as implemented by CMC Chapter 21.86. Table “D” below also includes the required amount of parking required and proposed for commercial development. As Table “D” demonstrates, the project is providing more parking onsite than is required by the Village & Barrio Master Plan and State Density Bonus.

TABLE D - PARKING STANDARDS

Type of Use	No. of Res. Units	Commercial	VBMP Required	CMC 21.86 Required	Proposed
Studio	15	--	1 space per unit (15 spaces)	0.5 space per unit (8 spaces)	284 spaces (1.3 spaces per unit)
1-Bdrm	87	--	1 space per unit (87 spaces)	0.5 space per unit (44 spaces)	
2-Bdrm	90	--	1.5 spaces per unit (135 spaces)	0.5 space per unit (45 spaces)	
3-Bdrm	26	--	1.5 spaces per unit (39 spaces)	0.5 space per unit (13 spaces)	
Residential Totals:			276 spaces	110 spaces	284 spaces
Retail	--	9,800 SF	1 space per 415 SF (24 spaces)	--	56 spaces
Restaurant	--	4,000 SF	1 space per 170 SF (24 spaces)	--	
Commercial Totals:			48 spaces	--	56 spaces
Overall Totals:			324 spaces	--	340 spaces

**TABLE E –VILLAGE & BARRIO MASTER PLAN
AREA-WIDE STANDARDS COMPLIANCE ANALYSIS**

Standard	Required/Allowed	Proposed	Comply?
Ingress and Egress	Vehicle access shall be taken off the alley. Driveway apron shall not exceed 20 feet in width.	*Developer is requesting a waiver to this standard (VBMP, 2.6.1(A)(3)) pursuant to the Density Bonus Ordinance and State Density Bonus Law. Please see Exhibit 6 for details.	Yes*
Ingress and Egress	A clear zone shall be provided at the intersection of a street and an alley or driveway to maintain a free line of sight. • The clear zone shall consist of an isosceles right triangle measured 7.5 feet in both directions from the	Clear zones are provided at the intersections with Carlsbad Village Drive at both the drive aisle and the Hope Avenue Alley. Additionally, a clear zone is provided at the intersection with Oak Avenue and the Hope Avenue Alley.	Yes

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

Standard	Required/Allowed	Proposed	Comply?
	<p>intersection of the two property lines.</p> <ul style="list-style-type: none"> The clear zone shall not be occupied by a ground floor building footprint, site features taller than 36 inches, or landscaping that is taller than 30 inches. 		
Property Line Walls and Fences	Fences and walls within the front setback shall be a maximum 3.5 feet tall (42-inches).	No walls or fences located in the front setback along Carlsbad Village Drive or Oak Avenue exceed 3.5 feet (42-inches) in height.	Yes
Building Orientation	Buildings shall be oriented toward primary street frontage.	The project is appropriately oriented towards Carlsbad Village Drive.	Yes
Building Entrances	The primary entrance of a ground floor commercial use shall be oriented towards the primary street frontage.	Both Commercial Bldgs. A and B have the primary entrance oriented towards Carlsbad Village Drive.	Yes
Roof Protrusions	All new development that results in additional height above the building height maximum for the applicable district shall abide by the following regulations:	--	--
Roof Protrusions	No roof structure or any other space or feature shall be allowed for the purpose of providing additional floor space except as provided herein.	The proposed stair and elevator towers projecting above the roof structure do not provide additional floor space beyond the purpose they serve.	Yes
Roof Protrusions	<p>All roof structures (excluding architectural features) shall be set back a minimum five feet from all building faces and shall not exceed the maximum stated heights below. Structures permitted to exceed five feet in height shall be setback at least an additional one foot for every foot above five feet, up to maximum stated heights.</p> <ul style="list-style-type: none"> Guardrails or other barrier for roof decks; roof deck amenities, including permanently affixed equipment and furnishings (such as built-in seating, barbeque islands, counters, and the like) trellises, latticework, screens 	*Developer is requesting a waiver to this standard (VBMP, 2.6.2(C)(4)(b)) pursuant to the Density Bonus Ordinance and State Density Bonus Law. Please see Exhibit 6 for details.	Yes*

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Standard	Required/Allowed	Proposed	Comply?
	<p>(including vegetative screens), or other objects designed or arranged to create a privacy screen or outdoor room; equipment screening, fire or parapet walls; skylights; and similar: Up to 42 inches above maximum height.</p> <ul style="list-style-type: none"> • Roof structures specifically for the housing of elevators, stairways, tanks, ventilating fans or similar equipment required to operate and maintain the building; flagpoles, chimneys, smokestacks, wireless masts and similar structures: Up to 10 feet above maximum height. 		
Roof Protrusions	<ul style="list-style-type: none"> • Architectural features: Up to 10 feet above maximum building height. Architectural features that are within 10 feet of a building face shall be cumulatively limited in width to 30 percent of that building face. 	*Developer is requesting a waiver to this standard (VBMP, 2.6.2(C)(4)(c)) pursuant to the Density Bonus Ordinance and State Density Bonus Law. Please see Exhibit 6 for details.	Yes*
Window Glazing	45 percent minimum glazing of ground-floor facade for retail uses adjacent to Carlsbad Village Drive. Facade is measured from plate height to finish floor.	<p>Area of Glazing: Commercial Bldg. A = 1,007 SF Commercial Bldg. B = 758 SF</p> <p>Percentage of Glazing Provided: Commercial Bldg. A = 61% Commercial Bldg. B = 52%</p>	Yes
Window Glazing	The bottom of any window or product display window shall not be more than 3.5 feet above the adjacent sidewalk.	Storefront glazing for the two commercial buildings fronting Carlsbad Village Drive extends to the ground.	Yes
Window Glazing	Transparent or translucent glazing is required on the ground-floor facade of a commercial or retail use facing a public street. Opaque, reflective, or dark tinted glass is not permitted.	Storefront glazing for the two commercial buildings fronting Carlsbad Village Drive consist of clear glass.	Yes

C. Site Development Plan (CMC Chapter 21.06)

The project requires a Site Development Plan according to section 6.3.3.B of the Village & Barrio Master Plan. A Conditional Use Permit is not required with this project because mixed-use, consisting of multiple-family residential and commercial (i.e., restaurants, retail, markets, etc.), is permitted by right within the Freeway Commercial (FC) District of the Village & Barrio Master Plan. All Site Development Plans within the Village & Barrio Master Plan area are required to be approved by the City Council. Five

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site development plan findings are required for the project. The required findings with justification for each are summarized below and contained in Planning Commission Resolution (Exhibit 1 to the June 5, 2024 Planning Commission Staff Report).

- 1. That the proposed development or use is consistent with the General Plan and any applicable master plan or specific plan, complies with all applicable provisions of Chapter 21.06 of the Carlsbad Municipal Code, and all other applicable provisions of this code.**

The project proposes the demolition of an existing shopping center with approximately 58,735 square feet of commercial uses, and in its place the construction of a new mixed-use development consisting of 13,800 square feet of neighborhood-serving commercial within two one-story buildings, 218 multiple-family residential apartment units (191 market rate and 27 affordable) within two five-story buildings, 289 garage parking spaces within a five-story parking structure, and 51 surface parking spaces, all on a 4.12-acre infill site located within the Freeway Commercial (FC) District of the Village & Barrio Master Plan. Mixed-use, consisting of multiple-family residential and commercial (i.e., restaurants, retail, markets, etc.), is permitted by right within the FC District. The project is consistent with the various elements and objectives of the Village (V) General Plan Land Use designation and the Village & Barrio Master Plan as discussed in the findings below and in the project staff report dated June 5, 2024 incorporated by reference. Although the project's residential density of 53 dwelling units per acre is above the 35 du/ac maximum that is allowed by the FC District, the 218-unit residential project can be found consistent with the General Plan Land Use Policies as discussed in the findings below and in the project staff report dated June 5, 2024 through use of a Density Bonus as allowed under California Law and the Carlsbad Municipal Code. State law encourages cities to provide affordable housing through incentives to developers (i.e., State Density Bonus Law, Government Code §65915). State Density Bonus Law allows a developer to increase density on a property above the maximum density, set under the land use policies of the General Plan, zoning district, and Specific Plan/Master Plan. In this instant, based on the adopted land use density of the Freeway Commercial (FC) District of the Village and Barrio Master Plan, the base number of units allowed for a 4.12-acre parcel at 35 du/ac is 145 dwelling units. The applicant is requesting a 50% increase in density to yield 218 total units. Density Bonus Law stipulates that a request for a density bonus does not constitute a valid basis on which to find a proposed housing development project is not compliant with a general plan. In addition, State Density Bonus Law explicitly requires the city to consider "the density allowed under the land use element of the general plan" in determining maximum allowable residential density and General Plan conformance. Therefore, the proposed density of the project is deemed compliant with the land use density designation of the General Plan and Village and Barrio Master Plan.

- 2. That the requested development or use is properly related to the site, surroundings, and environmental settings, will not be detrimental to existing development or uses or to development or uses specifically permitted in the area in which the proposed development or use is to be located, and will not adversely impact the site, surroundings, or traffic circulation.**

The proposed mixed-use development is located within the Freeway Commercial (FC) District of the Village & Barrio Master Plan. The project will not be detrimental to existing uses or to uses specifically permitted in the area in which the use is located in that mixed-use, consisting of multiple-family residential and commercial (i.e., restaurants, retail, markets, etc.), is permitted by right in the FC District. All the properties surrounding the Site are designated for FC uses under the VBMP, except for properties located to the south of the Site, across Oak Avenue, which are designated as Barrio Perimeter (BP) and

PROJECT ANALYSIS

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those along Harding Avenue to the west, which are designated as Village Center (VC). The project Site is surrounded by a variety of urban uses, including an adjacent gas station, a four-story mixed-use building across Carlsbad Village Drive to the north, one-to-three-story residential (multiple-family and single-family) buildings to the south across Oak Avenue, and various one-story public and commercial uses to the west across the Hope Avenue Alley along Harding Street. The proposed site design places residential adjacent to residential to the south and commercial development adjacent to existing commercial development to the north and west. The proposed mixed-use development will not adversely impact the site, surroundings, or traffic circulation. The mixed-use project is estimated to generate fewer trips (2,242 ADT) than the existing commercial land uses (7,044 ADT) it intends to replace, resulting in a net decrease of 4,802 Average Daily Trips (ADT). Both Carlsbad Village Drive and Oak Avenue are designed to adequately handle the traffic generated by the project, and the project is adequately parked on-site in accordance with State Density Bonus Law and the Village & Barrio Master Plan.

3. That the site for the intended development or use is adequate in size and shape to accommodate the use.

Pursuant to State Density Bonus Law, the Developer has requested waivers to standards in the Village & Barrio Master Plan related to maximum building height and the number of floor levels, height limits on roof structures, height limits on architectural features, limitations on total square footage of the fourth floor, and restricting vehicular access to an alley. The project is entitled to the requested number of units and the density bonus waivers are necessary for the project to be developed at the density. The waivers are permitted pursuant to CMC Chapter 21.86 and State Density Bonus Law.

4. That all yards, setbacks, walls, fences, landscaping, and other features necessary to adjust the requested development or use to existing or permitted future development or use in the neighborhood will be provided and maintained.

The project complies with all applicable development standards for mixed-use projects within the FC District of the Village & Barrio Master Plan, including lot coverage, open space, height, setbacks, and parking, etc., subject to density bonus provisions for concessions and waivers, including reductions of standards. The architecture of the building is compatible with the surrounding residential and commercial development. Landscaping along the street frontages and throughout the project will be provided consistent with the requirements of the city's Landscape Manual and the Village & Barrio Master Plan.

5. That the street systems serving the proposed development or use is adequate to properly handle all traffic generated by the proposed use.

The proposed project is considered a through lot since it has two primary street frontages, Carlsbad Village Drive to the north, and Oak Avenue to the south, both of which are connected by the existing Hope Avenue Alley bordering the entire westerly property line of the subject property. Along Carlsbad Village Drive, the project is proposing 8-feet of right-of-way dedication. With a total of 16-feet from curb to property line, the project proposes a 10-foot-wide sidewalk buffered by a 6-foot-wide landscape planter with street trees. In addition to the pedestrian improvements described, the project will also be updating an existing bus stop on Carlsbad Village Drive with a new Americans with Disabilities Act (ADA) compliant bus stop pad with shelter, bench, trash receptacles and shelter lighting, etc., pursuant to NCTD standards. On Oak Avenue, the project will maintain the existing 10-foot right-of-way along the

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project frontage and will replace the existing curb, gutter, and sidewalk. Along this frontage, the project is proposing a 10-foot-wide sidewalk reduced down to 5.5 feet at each tree well planter location. Along the north side of Oak Avenue, the project also proposes to improve the dead-end street design to a cul-de-sac pursuant to City of Carlsbad standards. Primary vehicular access into the project site will be provided via two existing driveways on Carlsbad Village Drive – one (1) unsignalized right-turn only driveway to the Hope Avenue Alley along the western boundary of the site and one (1) unsignalized private driveway permitting right turns and inbound left turns only. The existing median in Carlsbad Village Drive prohibits left turns out from this driveway. Secondary vehicular access will be provided on Oak Avenue via a full-access driveway located at the end of the cul-de-sac where Oak Avenue terminates west of Interstate-5. The proposed project is estimated to generate fewer trips (2,242 ADT) than the existing land uses (7,044 ADT) it will replace, resulting in a net decrease of 4,802 Average Daily Trips (ADT). Both Carlsbad Village Drive and Oak Avenue are designed to adequately handle the traffic generated by the project. The project site is also located within 0.4 miles of the Carlsbad Village mass transit station which provides bus, train, and Coaster service on a daily basis. Bike lanes are provided on Carlsbad Village Drive. In addition, the Developer will be required to pay traffic impact fees in accordance with Carlsbad Municipal Code Section 18.42 prior to issuance of building permits that will go towards future road improvements.

D. Inclusionary Housing Ordinance (CMC Chapter 21.85)

Pursuant to Carlsbad Municipal Code (CMC) Chapter 21.85, Inclusionary Housing Ordinance, residential developments proposing seven or more units are required to provide affordable housing. The standard inclusionary housing requirement is 15% of the total number of units as affordable to lower income households. However, pursuant to City Council Policy Statement No. 57, Inclusionary Housing Ordinance – Alternative Means of Compliance, the percentage of units designated as affordable to lower income households can be reduced to 12.5% if all of the following are satisfied:

1. All of the affordable units are made available to very low or extremely low-income households, or combination thereof;
2. The units are located on the same site as the market-rate units; and
3. No financial assistance from the city is required.

For this project, the Developer is proposing to satisfy the affordably housing requirements by providing 12.5% or a total of 27 units (12.5% multiplied by 218 total units = 27.25, rounded down to 27 units) as affordable to very low-income households. The inclusionary units will be located on the same site as the market-rate units and the Developer is not requesting financial assistance from the city. Therefore, the proposal to provide 27 very low-income units onsite complies with the Inclusionary Housing Ordinance. Therefore, the project has been conditioned to satisfy the inclusionary housing requirement for very low-income households by entering into an Affordable Housing Agreement (AHA) to provide 27 units onsite as affordable to very low income households for 55 years, earning up to 50% of the area median income (AMI). By entering into the AHA accordingly, the project is providing its fair share of housing affordable to very low-income households and is therefore consistent with the Inclusionary Housing Ordinance. For a family of four living in Carlsbad, the AMI at 50% is \$75,750 per year.

Chapter 21.85 of the CMC also requires that for those developments which are required to provide ten or more units affordable to low-income households, at least 10% of the low-income units, or three units for the proposed project, shall have three or more bedrooms. Of the 27 affordable units proposed,

PROJECT ANALYSIS

(GENERAL PLAN, MUNICIPAL CODE, AND OTHER REGULATIONS)

three, three-bedroom units are currently proposed.

E. GROWTH MANAGEMENT

The proposed project is located within Local Facilities Management Zone 1 in the Northwest Quadrant of the city. The impacts on public facilities created by the project, and its compliance with the adopted performance standards, are summarized in Table “F” below.

TABLE F – GROWTH MANAGEMENT COMPLIANCE

Standard	Impacts/Demand	
City Administration	771 sq. ft.	
Library	411 sq. ft.	
Wastewater Treatment	Commercial: 1,533 GPD Residential: 34,880 GPD	Total: 36,413 GPD
Parks	NW Quadrant	
Drainage	25.4 CFS, Drainage Basin B	
Circulation	Existing: 7,044 ADT Proposed: 2,242 ADT	Net decrease: 4,802 ADT
Fire	Fire Station 1	
Open Space	NA	
Schools	Carlsbad Elementary: 25.51 students Carlsbad Middle: 13.47 students Carlsbad High: 17.2 students	
Sewer Collection System	Commercial: 1,533 GPD Residential: 34,880 GPD	Total: 36,413 GPD
Water	Commercial: 480 GPD Residential: 40,330 GPD	Total: 40,810 GPD

AB 2097

PARKING REQUIREMENTS

This bulletin provides an overview of the restrictions imposed by the state legislature under AB 2097 on the city's ability to require minimum parking standards on certain private development projects.



BACKGROUND

Most cities, including Carlsbad, have historically required that new residential and commercial development provide onsite parking spaces to sufficiently accommodate occupants and customers and reduce impacts to neighboring land uses. The commonly applied parking standard is formula based, where the minimum number of required parking spaces is dependent upon the size and type of use being built. For example, Carlsbad requires one parking space for every 100 square feet of restaurant space, two parking spaces for each apartment unit with two or more bedrooms, and so on.

Based largely on a body of academic research regarding the potential impacts minimum parking ratios have on car ownership, vehicle miles traveled, and use of public transit, the state legislature passed AB 2097, which added Government Code §65863.2, that effectively eliminates parking requirements in new residential and commercial developments when located within a half-mile of a major transit stop.

According to the Assembly Floor Analysis on AB 2097, the study found that in buildings with no on-site parking, only 38% of households owned a car, but in buildings with at least one parking space per unit, the study found that more than 81% of households owned automobiles. As such, by eliminating parking minimums in new development, the state legislature figures that fewer households will rely on the automobile for transportation.

Documents Referenced

Carlsbad Village Station Eligible Parcels; [Map](#)
 Poinsettia Station Eligible Parcels; [Map](#)
 Carlsbad Housing (Element) Plan; [IB-137](#)
 2021-2029 Housing Element; [Plan](#)
 Carlsbad Parking Standards; [§21.44](#)
 EV Charging Station Requirements; [§18.22](#)
 EV Charging Permit Streamlining; [IB-165](#)
 Density Bonus; [IB-112](#)
 Supportive Housing Defined; [§21.04.355.1](#)
 Transitional Housing Defined; [§21.04.362](#)
 CA Coastal Commission Memo; [Policy](#)

Developers could still voluntarily provide onsite parking (and many likely will), but the number of parking spaces provided will be based on builder preference and market demand, not by city-established minimum parking standards.

The state legislature's intent with this action, which is effective January 1, 2023, is that it will help drive down construction costs, reduce vehicle traffic, increase public transit ridership, and promote walkable and bikeable communities so people can get around without a car, which will reduce the greenhouse gas emissions responsible for climate change.

NEW STATE LIMITS ON PARKING

AREAS AFFECTED

Pursuant to Government Code §65863.2(a), a city cannot impose or enforce any minimum automobile parking requirements on a residential, commercial, or other development project (except for hotels, motels, short-term rentals, or other transient lodging --- city parking standards still apply to those) if the project is located within one-half mile of public transit. The state defines public transit as a "major transit stop," containing any one of the following:

- Fixed rail station
- Bus rapid transit stop
- high-quality transit corridor included in a regional transportation plan
- Intersection of two or more major bus routes where buses stop every 15 minutes or less during peak commute periods

The city does not have any high-quality transit corridors per SANDAG’s 2021 Regional Transportation Plan. And North County Transit District’s Breeze bus system, does not meet the bus stop requirements and therefore does not qualify.

Currently, there are only two locations in the city that meet the definition of public transit --- Carlsbad Village Station and Poinsettia Station. Maps have been provided showing those parcels that are subject to §65863.2. See “Documents Referenced” above.

In cases where only a portion of the project site is located within one-half mile of a major transit stop, the following standards must be met in order to be eligible.

- At least 75% of the total project site is located within one-half mile of a major transit stop; and
- At least 90% of the proposed residential units, or 100 units or more, whichever is less, are located one-half mile of a major transit stop.

Projects failing to meet these requirements do not qualify for the allowances under §65863.2 and must meet current city parking standards.



EV CHARGING & ADA PARKING

Irrespective of proximity to public transit, state law allows the city to continue to apply minimum parking standards for electric vehicle (EV) charging stations as well as required parking spaces accessible to persons with disabilities (ADA).

- For EV stations, the required number of EV parking spaces for residential projects is provided in Table 4.106.4.3.1 of Carlsbad Municipal Code (CMC) §18.21.040. For nonresidential projects, EV parking space requirements are provided in Table 5.106.5.3.3 of CMC [§18.21.050](#). The parking requirement is based on the total number of actual parking spaces that would have otherwise applied to

the development if the state code section did not exist. Refer to [IB-165](#) for an overview of the city’s streamlining provisions and alternative standards for EV charging stations.

- For ADA parking, the city applies the standards set forth in Chapter 1109A (multifamily) and Chapter 11B (commercial) of Title 24, Volume 1 of the 2023 CA Building Code. Like EV spaces, the total number of ADA spaces is based on the total number of actual parking spaces that would have otherwise applied to the development.



EXEMPTIONS

The city may apply its minimum parking standards to a proposed project if it makes written findings that failure to impose parking standards will result in one of the following to occur.

- Hinders the city’s ability to meet its share of low- and very low-income housing. Refer to [IB-137](#) for more on the city’s Housing Element and Regional Housing Needs Assessment requirements.
- Hinders the city’s ability to meet any special housing needs for elderly or persons with disability. Refer to the city’s [2021-2029 Housing Element](#) for more information.
- The proposed “housing development project” will negatively impact existing residential or commercial parking that is located within one-half mile of the project. The state defines a “housing development project” as either:
 - A 100% residential development; or
 - Mixed-use development where at least 2/3 of the project is designed for residential use; or

- Project includes [transitional housing](#) or [supportive housing](#).

The city exemption determination must be supported by a preponderance of the evidence in the record showing that not imposing or enforcing minimum parking standards would have a substantially negative impact on the above referenced development.

The exemption finding must be made by the city within 30 days following receipt of a completed application.



- The city may require that the voluntary parking spaces meet established minimum location and design standards.
- If a project voluntarily provides parking spaces, the city can require that the spaces be available to the public.
- If a project voluntarily provides parking spaces, the city can require that a parking fee be charged to residents or customers for use. Conversely, the city cannot require that the voluntarily provided parking spaces be offered to the residents or customers free of charge.



EXCEPTIONS TO THE EXEMPTIONS

Government Code §65863.2(c) provides a list of specific project types that are not subject to the above exemption provisions. In other words, the city cannot impose minimum parking standards on the following housing development projects, irrespective of whether the above discussed exemption findings can be made.

- The housing development project contains fewer than 20 housing units.
- The housing development project dedicates a minimum of 20% of the total housing units to very low-, low-, or moderate-income households, students, elderly, or persons with disabilities.
- The housing development project is subject to parking reductions based on the provisions of any other applicable law. As an example, the proposed development is a density bonus project, which offers reduced parking standards for development projects. Please refer to [IB-112](#) for more on density bonus law.

PARKING SPACES PROVIDED VOLUNTARILY

When a project voluntarily provides parking, the city is limited to only imposing the following parking requirements:

PROPERTIES IN THE COASTAL ZONE

Development in the Coastal Zone requires a Coastal Development Permit consistent with Carlsbad Municipal Code (CMC) Chapter [21.201](#). Following adoption of AB 2097, the California Coastal Commission released on June 30, 2023 a [memorandum](#) discussing how the Commission and local governments can impose other types of conditions in these areas to ensure projects and Local Coastal Programs (LCPs) are consistent with the public access and recreation policies of the Coastal Act (Chapter 3) and certified LCPs. These other types of conditions may assist the city in making the necessary findings to approve required Coastal Development Permits ([§21.201.080\(C\)](#)).

YOUR OPTIONS FOR SERVICE

Questions pertaining to this state law, please contact the Planning Division at 442-339-2600 or via email at Planning@CarlsbadCA.gov.



Carlsbad Village Mixed Use (CVMU) – SDP 2023-0014

Supplemental Application - Density Bonus Program (Revised 2.27.24)

Project Name

Carlsbad Village Mixed Use (CVMU) (the “Project”)

SDP 2023-0014

Project Location

Street Address: 945-1065 Carlsbad Village Drive, Carlsbad, CA 92008 (the “Site”)

APN’s: 203-320-53-00; 203-320-54-00; 203-320-55-00; 203-320-56-00

Site Description

The 4.12-acre Site is currently occupied by a commercial shopping center with a total floor area of 58,735 square feet within five buildings, which are occupied by a variety of retail and restaurant uses, as well as a surface parking lot spread across four (4) parcels. The Site is surrounded by Carlsbad Village Drive to the north, Interstate 5 (I-5) Freeway to the east, Oak Avenue to south, and an unnamed public alley to the west. The property is substantially surrounded by urban uses.

The Site is located 0.4 miles from the Carlsbad Village center and 0.6 miles from the Pacific Ocean. Primary regional access to the Site is provided via the I-5 freeway, which generally runs north/south and is adjacent to the Site. The Site has convenient access to public transportation and is located within a transit priority area, served by the Coaster and Amtrak, as well as numerous bus lines within walking distance. NCTD bus route 315 provides service between Camp Pendleton, Sprinter, and Carlsbad Village Station, with stops immediately adjacent to the Project Site. The closest major transit stop is Carlsbad Village Station, located within 0.5 miles east of the Site.

The Project Site is surrounded by a variety of uses, including an adjacent gas station, a 4-story mixed-use building across Carlsbad Village Drive to the north, multifamily buildings to the south across Oak Avenue, and various public and commercial uses to the west across the public alley.

The Site is designated as Village-Barrio (V-B) in the General Plan and is similarly zoned Village-Barrio (V-B) and is located more specifically within the Freeway Commercial (FC) District of the Carlsbad Village and Barrio Master Plan (VBMP). All the properties surrounding the Site are designated for FC uses under the VBMP, except for properties located to the south of the Site, across Oak Avenue, which are designated Barrio Perimeter (BP).

The FC District permits a wide array of uses associated with urban freeway interchanges including lodging, retail, restaurant, residential and mixed uses.

Project Description:

The Site's existing commercial buildings and paved areas would be removed in order to develop the Project, which would consist of 218 multifamily residential units, including 27 units deed restricted to Very Low-Income (VLI) households, and approximately 13,800 square feet of commercial retail/restaurant uses and the existing (4) legal parcels would be consolidated into two (2) legal lots. The proposed commercial uses would be contained in two, one-story buildings along Carlsbad Village Drive, and the residential units, including the VLI units, would be spread across two five-story residential buildings separated by a landscaped paseo and courtyard. 340 vehicular parking spaces will be provided at-grade surrounding the commercial and residential buildings and within an above-grade parking structure. The new proposed buildings and parking structure would have a maximum building height of 56 feet. Pursuant to Carlsbad Municipal Code §21.46.020, parapets of 4'-6" tall are proposed around the edge of the roof and stair/elevator towers of various heights would extend up to 10 feet above the maximum building height, except for two elevator towers adjacent to the parking structure that would extend 14 feet above the maximum building height to provide elevator access to the rooftop parking level.

Vehicular access to the Site would occur via three access points from the surrounding streets and public alley. One vehicular driveway is provided on the northern boundary of the Site along Carlsbad Village Drive which provides site access via an existing driveway and left turn pocket. Both the commercial at-grade parking stalls and parking structure will be accessed via this existing driveway along Carlsbad Village Drive. Additional vehicular access to the parking structure would be provided from Oak Avenue for tenants only. Access to the Site would also be provided from the public alley.

The Project's commercial space along Carlsbad Village Drive is anticipated to contain a variety of neighborhood serving retail and restaurant uses, including a neighborhood market. The residential building includes variety of ancillary uses and open space, including landscaped courtyards, private balconies and courtyards, and extensive residential amenities such as a 5th floor sky deck, a swimming pool and courtyard, indoor amenity areas, a lobby, leasing office, and mail room.

It is anticipated that the Project will be constructed in a single phase, subject to market conditions.

Density Bonus Calculations (Government Code Section 65915(f)(2)/Carlsbad Municipal Code Chapter 21.86)

Base Maximum Density:	35 du/ac
Base Maximum Units:	4.12 ac x 35 = 144.2 or 145 (round up per CMC Section 21.86.040(G))
Affordable Units:	15% of base units; 15% x 145 = 21.8 or 22 VLI affordable units (50% AMI)
Density Bonus Units:	145 x 50% = 72.5 or 73 density bonus units
Total proposed units:	218 dwelling units (52.9 du/ac)

Inclusionary Housing Requirement (Carlsbad Municipal Code Chapter 21.85)

Total proposed units: 218 dwelling units
Inclusionary units: 218 total units x 12.5% (City Council Policy 57 Section 2(c)) = 27 inclusionary units

Parking

Pursuant to AB 2097, due to the Site's proximity within 0.5 miles to Carlsbad Village Station, a major transit stop, there is no onsite parking requirement. However, the Project is proposing to utilize the Density Bonus Parking Standards per Government Code Section 65915(p)(2), which require a minimum of 0.5 parking spaces per unit.

Waivers Requested

1. *Waiver of VBMP FC Supplemental District Standard 2.7.4(G)(1) limiting maximum building height to 45 feet and 4 stories.*

The proposed Project would include building heights up to five stories, with a maximum building height of 56 feet. However, the fourth and fifth stories of the residential buildings will include varying setbacks, plane breaks, and articulation to minimize the appearance of the fifth floor. Imposing a 45 feet and four story height limit would reduce the Project's developable floor area and would physically preclude construction of the proposed project at the density permitted by CMC Chapter 21.86. A waiver of maximum building height does not result in a specific adverse impact upon public health and safety, the physical environment, or a listed historical property.

2. *Waiver of VBMP Area-Wide Standard 2.6.2(C)(4)(c) to limit architectural features up to 10 feet above maximum building height.*

As a result of the use of a Density Bonus waiver to permit an increased building height of 56 feet, an additional waiver is required for the above referenced standard. Imposing the height limit of architectural features to 10 feet above the maximum building height would physically preclude the project from having a fifth story and therefore would preclude the construction of the proposed project at the density permitted by CMC Chapter 21.86. A waiver of height limits architectural features does not result in a specific adverse impact upon public health and safety, the physical environment, or a listed historical property

3. *Waiver of VBMP Area-Wide Standard 2.6.2(C)(4)(b) to limit roof structures above the building height maximum: (i) guardrails, roof deck amenities, trellises, parapets up to 42 inches above maximum height and (ii) roof structures specifically for the housing of elevators and stairways up to 10 feet above maximum height.*

To accommodate elevator access to the rooftop parking level, two elevator towers will require heights up to 14 feet above the maximum building height of 56 feet. All other roof

top structures will be within 10 feet of the maximum building height. Imposing the 10 foot height limit on roof structures would eliminate access to the upper floors and thereby reduce the Project's developable floor area and would physically preclude the construction of the proposed project at the density permitted by CMC Chapter 21.86. A waiver of height limits on roof structures does not result in a specific adverse impact upon public health and safety, the physical environment, or a listed historical property.

4. *Waiver of VBMP FC Supplemental District Standard 2.7.4(G)(2)(b) to limit the total square footage of enclosed fourth floor space not to exceed 80% of the largest floor space below.*

The proposed Project will include up to five stories. The fourth and fifth stories of the residential building will include varying setbacks and the total enclosed fourth and fifth floor space of the residential buildings will be less than the floors below. Additionally, the residential buildings facing Carlsbad Village Drive are set back approximately 150 feet from the street. Requiring the project to limit its fourth floor area to 80% of the largest floor below would significantly limit the gross floor area of the project and therefore reduce the amount of units able to fit within the project and would physically preclude construction of the proposed Project at the density permitted by CMC Chapter 21.86. A waiver of the requirement limiting total square footage of the enclosed fourth floor space does not result in a specific adverse impact upon public health and safety, the physical environment, or a listed historical property.

5. *Waiver of VBMP Area-Wide Standard 2.6.1(A)(3) to require vehicle access be taken from an alley and limit driveway apron to 20 feet.*

The proposed Project will close two of three existing vehicular access points to the Site along Carlsbad Village Drive. The remaining existing access driveway apron on Carlsbad Village Drive will be reduced from its existing width of approximately 50 feet to 24 feet and will maintain the existing westbound left turn pocket lane on Carlsbad Village Drive into the Project. Further, the Project will maintain access from the alley. Requiring the Project to take all vehicular access from the alley would require additional onsite circulation, driveways, and fire access roadways and would reduce the buildable footprint of site and physically preclude construction of the proposed project at the density permitted by CMC Chapter 21.86. A waiver of the requirement to take all vehicular access from an alley and limit the driveway apron to 20 feet does not result in a specific adverse impact upon public health and safety, the physical environment, or a listed historical property.

Density Bonus

This bulletin outlines the development allowances provided under Govt. Code [§65915](#), commonly referred to as state density bonus law. The bulletin is only intended to summarize the key provisions of state law rather than cite them in total. The document has been updated to include recent state legislation, including [AB 1287](#), the “middle-income homes density bonus law,” which became effective January 1, 2024.

BACKGROUND

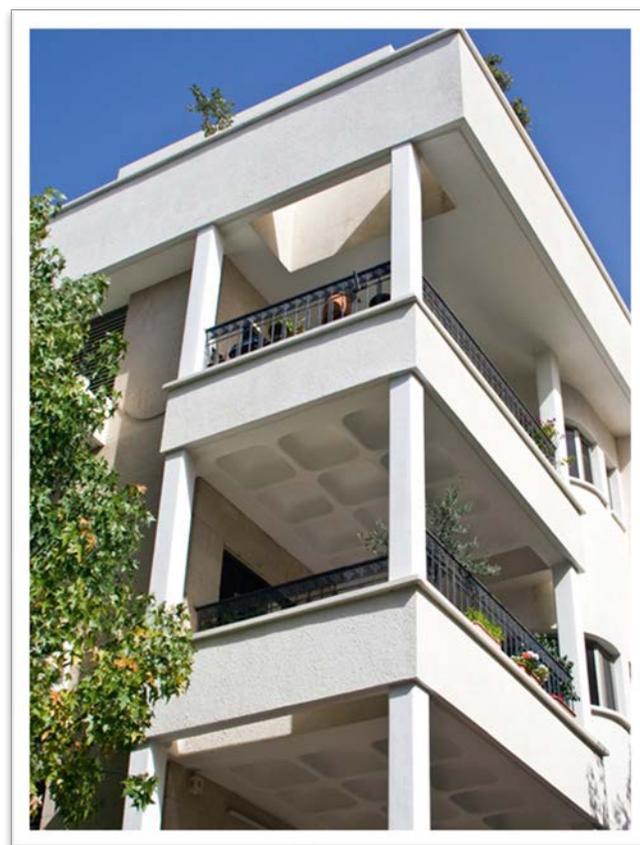
State density bonus law allows a developer to increase density (total number of homes) allowed on a property above the maximum set under a city’s local land use plan (Carlsbad General Plan) by as much as 100%. In addition, qualifying applicants can also receive reductions in required development standards such as setbacks and height limits when those standards prevent the applicant from achieving the density allowed under state law. Other tools include reduced or no parking requirements for certain project types.

In exchange for these benefits, a certain number of the new dwelling units within the development project must be reserved for lower-income households, seniors, or the other eligible affordable housing projects.

Pursuant to Government Code [§65915\(a\)\(1\)](#), each jurisdiction must adopt an ordinance that specifies how compliance with density bonus law will be implemented. Failure to adopt an ordinance does not relieve the city from complying with state density bonus law. As such, the city’s adopted ordinance, Carlsbad Municipal Code [§21.86](#), references state mandates where appropriate (as opposed to repeating state code requirements) and focuses more on the permit processing requirements for density bonus applications.

ELIGIBILITY

Any housing development that proposes five or more units and incorporates at least one of the following is eligible for a density bonus. Note: Accessory Dwelling Units (ADUs) may be included as part of a single-family or multi-family development, but ADUs do not count towards/against the total density allowed under state density bonus. Refer to info-bulletin [IB-111](#) for more on ADUs.



- At least 5% of the housing units are restricted to very low-income residents.
- At least 10% of the housing units are restricted to low-income residents.
- At least 10% of the units in a for-sale housing development are restricted for moderate-income.
- 100% of the housing units (other than manager’s units) are restricted affordable with a maximum of 20% of the units being moderate.
- At least 10% of the housing units are rent restricted at the very low-income level for transitional foster youth, disabled veterans, or homeless persons.
- At least 20% of the housing units are for low-income college students in housing dedicated for full-time students at accredited colleges.
- The project donates at least one acre of land to the city for very low-income units, and the land

has the appropriate permits and approvals and access to needed public facilities.

- The project is a senior citizen housing development; in which case, no affordable units are required.
- The project is a mobile home park that is age-restricted to senior citizens; in which case, no affordable units are required.

AFFORDABILITY DURATION

State density bonus law establishes how long an affordable unit must stay affordable.

- Affordable *rental* units must be restricted at the targeted income level group for at least 55 years.
- Affordable *for-sale* units must be restricted at the targeted income level group for at least 30 years, which starts after the initial sale of the affordable unit. Affordable units may be sold at a market price to other than targeted households provided that the sale results in an equity sharing agreement with the city.



DENSITY BONUS CALCULATIONS

Despite the city’s rounding requirements under CMC [§21.53.230](#) (Table A), for projects utilizing density bonus, Government Code §65915(q) requires that each component of any density calculation resulting in fractional units shall be separately rounded up to the next whole number. In other words, all density related calculations must be rounded up.

Base Density Calculation

Step one in calculating density bonus is to calculate the project’s **base density**, which represents the number of dwelling units allowed under the city’s General Plan, per acre of property. Calculating base

density under density bonus is no different from how the city calculates density for standard residential development projects, with the following exceptions:

- While the city uses developable (or net) acreage in determining density, density bonus law requires cities to use gross acreage. This allowance was clarified in an HCD [technical assistance letter](#) dated July 26, 2023.
- While the city utilizes a “mid-range” density calculation for determining the allowable number of units on a property, state law requires that density bonus be calculated based upon the *maximum* density allowed under the city’s General Plan and zoning ordinance for the subject property.
- Pursuant to SB-330 (Housing Crisis Act of 2019), the city is prohibited from enforcing housing caps. As such, the housing caps in the city’s Growth Management Plan ([GMP](#)) cannot be applied to new housing development projects. Refer to [IB-132](#) for more information on SB-330 and Reso No. [2021-074](#) for the city’s suspension of the GMP cap limits and performance standard moratorium provisions.

Density Bonus Calculation

Step two in calculating density bonus is to calculate the project’s **density increase**, which represents the number of units allowed in addition to the base density units. These additional dwelling units are set per a sliding scale, based upon two primary factors:

- The percentage of units in the project that will be set aside (reserved) as affordable; and,
- The household income category of those affordable units (i.e., very low, low, or moderate income).

For convenience, a Density Bonus Table is included on page six of the city’s Density Bonus Report ([Form P-1\(H\)](#)). As you will see from the table, the number of affordable units (far left column) and the level of affordability (top row) greatly influence the number of density bonus units that can be granted.

For example, a project that reserves 10% of its units as affordable for *very low-income* families is eligible for a 32.5% density bonus, as opposed to a density bonus of only 20% if those same affordable units were reserved for *low-income* families. Refer to info-bulletin [IB-137](#) (Carlsbad’s Housing Plan) for more information on household income and affordability.



INCLUSIONARY HOUSING CALCULATIONS

To help provide local affordable housing, the city in 1993 adopted an inclusionary housing ordinance ([§21.85](#)), which established the legal basis for requiring affordable (inclusionary) housing units in new residential development in the city.

For more information, refer to info-bulletin [IB-157](#) (Inclusionary Housing Program). While the city's inclusionary regulations are separate from density bonus law, there are a few important provisions in the city's inclusionary ordinance that directly affect density bonus projects, as reflected below.

- The city's inclusionary requirements apply to all proposed development projects that include residential units. This means that projects subject to the state density bonus law/city's density bonus ordinance ([§21.86](#)), must also comply with the city's inclusionary housing ordinance ([§21.85](#)).
- Projects proposing seven or more housing units are required to restrict at least 15% of the **total** proposed units for low-income households. The total proposed units include base density **and** density bonus units.
- When calculating inclusionary requirements, fractional units resulting in less than 0.5 are rounded *down* to the next whole number.
- The required affordable inclusionary units satisfy the required affordable density bonus units.

The city's application of its inclusionary code is consistent with [AB 2345](#) and the Department of Housing & Community Development's (HCD) [technical assistance letter](#) dated September 2, 2022 to the City of West Hollywood.

THEORETICAL EXAMPLE

Sometimes showing the math helps folks better understand how density bonus works. The following is a theoretical example on how these different density calculations are applied.

A property 1.003 net acres in size has a zoning designation of R-15 (11.5 to 15 dwelling units per acre). Under density bonus, this results in a maximum base density of 15.05 units for this site (1.003 acres multiplied by 15 units per acre), which rounds up to 16 units.

The applicant proposes that 3 of the 16 units will be reserved for low-income households. This results in 18.7% of the units that will be reserved as affordable housing (3 affordable units divided by 16 base density units), which rounds up to 19%.

Based on the sliding scale found in the Density Bonus Table in the Density Bonus Report ([Form P-1\(H\)](#)), with 19% of the affordable units reserved for low-income families, the project's base density can increase by 33.5% or 5.36 units (16 base density units multiplied by 33.5%), which rounds up to 6 density bonus units for a total of 22 units for this project.

To satisfy the city's inclusionary ordinance, a total of 3.3 affordable low-income units are required for this project (15% inclusionary requirement multiplied by 22 total units), which rounds down to 3 units.

In this example, the 3 affordable low-income units satisfy the requirements under state density bonus law as well as the city's inclusionary housing ordinance.

SECONDARY DENSITY BONUS

AB 1287 (Alvarez, 2023) amended state density bonus law by requiring jurisdictions to award an additional (or second) density bonus for projects that have allocated a certain amount of affordable housing for very-low income, low-income, or moderate-income units, as summarized in the section below.

Minimum Eligibility

The proposed density bonus project must comply with one of the following affordability requirements to be eligible for an additional density bonus.

- A minimum of 15% of the base units are reserved for very low-income households; or
- A minimum of 24% of the base units are reserved for low-income households; or
- A minimum of 44% of the base units are reserved for moderate-income households.

Density Increase

Under state law, the city must grant the developer an additional density bonus if additional units are set aside for either very low or moderate-income households. Like traditional density bonus calculations, the secondary density bonus is also on a sliding scale, based on the project's base density.

For reference, the secondary density bonus allowances have been included in the Density Bonus Table found on page six of the city's Density Bonus Report ([Form P-1\(H\)](#)). To help illustrate how this secondary density bonus is applied, let us relook at the previous theoretical example.

Instead of 3 units, the applicant now proposes that 5 of the 16 units will be reserved for low-income households. This results in 31.2% of the units that will be reserved as affordable housing, which rounds up to 32%.

With 32% of the affordable units reserved for low-income households, the project's base density can increase by 50% or 8.0 units (16 base density units multiplied by 50%), for a total of 24 units.

Since more than 24% of the base units are being reserved for low-income households (31.2%, specifically), this project is eligible for a second density bonus. In this example, the applicant proposes to reserve an additional 15% of the base units for moderate-income households, which results in 2.4 units (16 base density units multiplied by 15%) that rounds up to 3 units.

With 15% of the affordable units reserved for moderate-income households, the project's base density can increase by an additional 50% or 8.0 units (16 base density units multiplied by 50%).

As a result, the total project size increases to 32 total units, 8 of which will be restricted affordable.

It is important to highlight that AB 1287 caps the affordable set aside at 50%. For projects that allocate the maximum of moderate-income units (44%), they would only be eligible to set aside another 6% of very-low income or moderate-income units to receive an additional bonus of 23.75% or 22.5%, respectively.



REMOVAL OF EXISTING RENTAL UNITS

Under density bonus law, projects that include the demolition or removal of affordable rental units are ineligible for density bonus unless the units are replaced concurrent with the development of the project. This provision applies to the following types of rental units:

- Units subject to recorded restrictions
- Units subject to rent control
- Units occupied by very low- or low-income households

If household rental income cannot be determined, the city may assume households are occupied by low-income households in the same proportion as low-income renters in the city, consistent with [AB 2556](#).

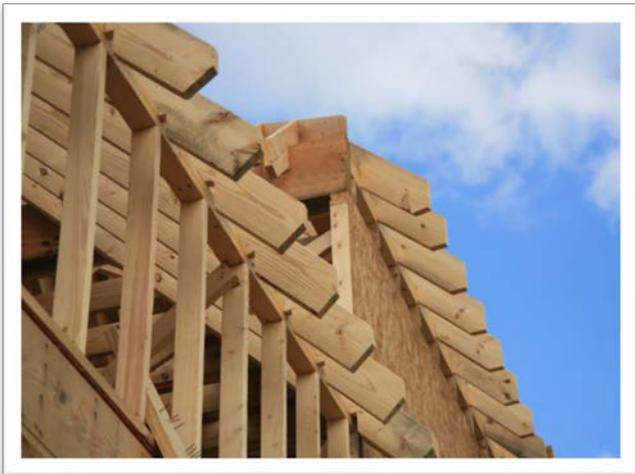
Additionally, under state law the affordable units required under density bonus may also be used to satisfy any replacement unit requirements. This allowance was clarified in an HCD [technical assistance letter](#) dated December 14, 2023.

DEVELOPMENT STANDARD DEVIATIONS

Traditional development projects must be designed to comply with city established development standards and design regulations such as building height limitations, setback requirements, minimum parking ratios, and on-site open space directives.

However, under state density bonus law, applicants can deviate from these development standards when found that the standards prevent the applicant from achieving the density allowed under the state law. There are two types of tools available to applicants:

- Incentives & Concessions
- Waivers



Incentives & Concessions

Incentives and concessions, as defined under state density bonus law, allow a developer to deviate from those requirements when modifying such regulations would provide “identifiable and actual cost reductions” to provide for affordable housing costs and rents. This requirement was clarified in the court decision of *Schreiber v. City of Los Angeles* (later codified as part of the passage of AB 1287) and the sections below reflect the holdings in that case.

Application

A few key considerations regarding the application of incentives or concessions:

- Under the government code, the terms “incentives” and “concessions” are used interchangeably. As such, the city considers them one in the same (“incentives/concessions”).
- A density bonus project is entitled to incentives/concessions even without a request for a density bonus --- if a developer provides the affordable housing specified under density bonus law, they are eligible for incentives/concessions.
- The city applies incentives/concessions to the development standards or design regulations requiring deviation, not to the individual situation.

For example, say a project proposes three separate buildings with each building requiring an increase in the city’s maximum building height standard. In this example, the city would require one incentive/concession for this deviation, even though the deviation applies to three separate buildings.

If that same project requires a deviation from the building height and rear yard setback
June 5, 2024

standards, the city will require two incentives/concessions since these are considered two different development standards.

- Pursuant to the *Schreiber* case, which was later codified as part of the passage of AB 1287, a developer is not required to provide financial evidence (i.e., pro forma) documenting that a requested incentive/concession will result in actual cost reductions. However, applicants need to reasonably document “why” the requested incentive/concession will reduce affordable housing development costs. As such, the city requires applicants to provide reasonable documentation to show that a requested incentive/concession will result in identifiable cost reductions to provide for affordable housing costs or rents.

Number Authorized

The number of incentives/concessions that can be requested by a developer varies by the amount and type of affordable units being proposed, as reflected below.

INCOME	% OF AFFORDABLE UNITS ¹				
Very Low	5%	10%	15%	16%	≥80%
Low	10%	17%	24%	---	≥80%
Moderate	10%	20%	30%	45%	20%
Student ²	20%	---	---	---	---
Incentives	1	2	3	4	5³

¹ The % of a project’s affordable units must be at least equal to the listed %.

² Lower-income student in a dedicated student housing development.

³ To qualify for 5 incentives, a project must reserve at least 80% of the units for lower income households (very low, low, or combination thereof). The remaining 20% may be reserved for moderate income households. The applicant shall also receive a height increase of up to three additional stories, or 33 feet.

As noted in footnote #1, when determining the appropriate number of incentives/concessions, a project’s percentage of affordable units must be “at least” equal to the percentages shown in the table above (§65915(d)(2)). In other words, the percentages in the table are minimums.

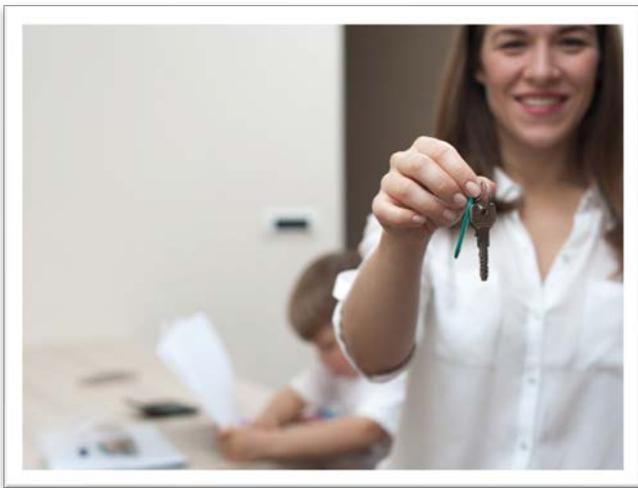
So, in the case of the theoretical example project that reserved 19% of the units for low-income, the applicant is eligible to receive two incentives/concessions. If affordable units are provided to satisfy the city’s inclusionary housing obligation above required density bonus affordable units, the total number of affordable units count

when determining the number of incentives/concessions allowed.

Grounds for Denial

Under the Schreiber case, the city must grant a requested incentive/concession unless it finds, under a preponderance of evidence, the following:

- The incentive/concession does not result in identifiable and actual cost reductions to provide for affordable housing costs or rents.
- Granting the incentive/concession would have a specific adverse impact on public health or safety or on property listed on the California historical register, which cannot be mitigated, or would be contrary to state or federal law.



Waivers

Density bonus law offers another form of assistance to developers, separate from concessions/waivers, in the form of “waivers.” A waiver is a modification or reduction to established development standards or design regulations when those requirements potentially cause the construction of the development project physically infeasible, if not approved.

Application

A few key considerations regarding waivers.

- Waivers do not count as an incentive/concession and can be used in concert (combined) with incentives/concessions.
- The developer must provide sufficient documentation justifying why the city’s established development standard(s) or design regulation(s) physically preclude construction of the project and why the waiver(s) is necessary.

Sufficient documentation may include a written explanation of the physical constraints accompanied with an exhibit showing the site and developable envelope.

Number Authorized

Unlike concessions/incentives, applicants are entitled to waive any established development standards or design regulations that would physically preclude the development from achieving the allowances authorized under density bonus law. In other words, there is no limit in the number of waivers an applicant can request.

Grounds for Denial

The city is not required to grant or otherwise authorize a waiver if it finds that the requested deviation or modification causes a specific adverse impact on public health or safety and cannot be mitigated, would have an adverse impact on property listed on the California historical register, or would otherwise violate state or federal law.

Like incentives/concessions, the city must include a showing of substantial evidence when making a finding of denial on a waiver request.



PARKING ALLOWANCES

Despite the city’s parking requirements under CMC [§21.44](#), the city may not require more than the following parking ratios for a density bonus project (including parking for persons with disabilities):

Unit Type	Required Parking
Studio	1 space
One Bedroom	1 space
Two Bedroom	1.5 spaces
Three Bedroom	1.5 spaces
Four Bedroom	2.5 spaces



State law further limits parking requirements for specified projects as reflected below.

- 0.5 spaces per unit for projects with at least 11% very low income, 20% lower income, or 40% moderate income, when located within ½ mile of accessible major transit stop, which in the city is the Carlsbad Village Coaster Station or Poinsettia Coaster Station.
- No parking spaces are required for projects meeting the following:
 - 100% affordable to lower income residents, within ½ mile of a major transit stop, which in the city is the Carlsbad Village Coaster Station or Poinsettia Coaster Station.
 - 100% senior or special needs rental project affordable to lower income, either with paratransit service or within ½ mile of an accessible bus route that operates at least eight times per day.
 - Rental supportive housing development that is 100% affordable to lower income households.

Parking requirements may be satisfied by providing individual parking stalls or in tandem, so long as the stalls are provided onsite.

Requesting these parking standards does not count as an incentive/concession or waiver; however, an applicant may request further parking standard reductions using the incentive/concession or waiver allowances.

DENSITY BONUS APPLICATIONS

The city's Density Bonus Ordinance can be found in CMC [§21.86](#) and applicants should follow the permit submittal requirements and processes set forth in the Land Use Review Application ([Form P-1](#)).

Pursuant to changes in state density bonus law that went into effective in 2019, the city developed a supplemental form outlining the information that must be submitted for a complete density bonus application; referred to as the Density Bonus Report [Form P-1\(H\)](#). This includes project location, property description, project description, density calculations, and information on any requested incentives/concessions or waivers.

Once a development application is determined to be complete, the city, under state law, will notify the applicant of the level of density bonus and parking ratio the development is eligible to receive.



PROJECTS IN THE COASTAL ZONE

When a density bonus project is proposed in the coastal zone, legislation that went into effect in 2019 attempted to strike a balance between the state goals of promoting housing and protecting the coast.

Density bonuses, incentives/concessions, waivers, and parking reductions are to be permitted so that they are consistent with both density bonus law and the California Coastal Act. Granting of a density bonus or an incentive does not require a general plan, zoning, or local coastal plan amendment.

YOUR OPTIONS FOR SERVICE

To schedule an appointment to submit an application or to learn more about density bonus, please contact the Planning Division at 442-339-2600 or via email at Planning@carlsbadca.gov.

NOTE: State density bonus law is regularly updated and revised by the state legislature and the city may not be able to timely update this bulletin to reflect the most current provisions. Please refer to current state law ([§65915 et seq.](#)).

Inclusionary Housing Program

When housing prices spiked in the 1990s, many cities looked for ways to help make housing more affordable. One such tool that many jurisdictions implemented was **INCLUSIONARY HOUSING REGULATIONS**. Under these laws, developers are required to set aside a certain number of units within their residential development project and make them affordable to lower income households.

As part of their inclusionary housing program, many cities also included provisions that allow developers to deviate from the strict adherence of the policy, so long as it is found that the alternative means of compliance meets the intent of the jurisdiction's inclusionary housing policies, and is consistent with the housing affordability and fair housing choice goals specified in its long-range housing plan.

This info-bulletin provides an overview of Carlsbad's Inclusionary Housing Program and describes how it helps address affordable housing needs while advancing equitable development goals consistent with the city's adopted Housing Element.



NEED FOR LOCAL INCLUSIONARY HOUSING

The state faces a serious housing problem that not only threatens its economic security, the lack of access to affordable housing can have a direct impact upon the health, safety, diversity, and welfare of Carlsbad residents. To retain a healthy livable environment and meet state mandated housing goals, more needs to be done to accommodate locally available and affordable housing stock.

To help address this need, the city's inclusionary housing ordinance, originally adopted in 1993, established the legal basis for requiring affordable (inclusionary) housing units in new residential development in the city. The law applies to all proposed development projects that include residential units and requires that a minimum of 15% of the units within a project be affordable to lower-income households.

Since its implementation, the inclusionary housing ordinance has proven to be extremely effective. From 1995 to 2020, the city produced 19,026 housing units, of which roughly 13% were made affordable through this program. And it is anticipated that the city will generate over 500 additional affordable units over the next eight-year period.

HOUSEHOLD INCOME & AFFORDABILITY

We hear this a lot --- How do we make housing more affordable? The term "affordable housing" can be used to describe housing that receives some form of subsidy/restriction that forcibly keeps rents and mortgages low. It can also mean housing that's naturally affordable simply because of market supply and demand. In order to make a meaningful difference in providing affordable housing, the solution should not be looked at as an "either-or" approach between privately produced housing and subsidized housing...it requires both.

To help understand what qualifies as affordable, the U.S. Department of Housing and Urban Development (HUD) establishes income ranges for different household types, which they have grouped into five "income categories:" extremely low, very low, low, moderate and above-moderate household income. The household income for each of these categories is based on a percentage of the region's Area Median Income or AMI.

Carlsbad falls within the San Diego County region, which has an AMI of \$95,100 (2021) per year for a four-person household. In comparison, the city's actual median income is closer to \$108,000, but under state law the city must use the county AMI. To help illustrate, the table below shows the income levels for a family of four.

Income Category	% of AMI	Household Income ^{1,2}
Extremely Low	<30%	<\$28,500
Very Low	30 - 50%	\$28,500 - \$47,600
Low	51 - 80%	\$47,600 - \$76,000
Moderate	81 - 120%	\$77,000 - \$114,100
Above Moderate	>120%	>114,100

¹ AMI as of April 2021 was \$95,100 for family of four; ² Figures rounded.

For housing costs to be considered affordable, a family’s monthly rent/mortgage payment should not exceed 30% of the gross annual household income of any given income category. So, a low-income family of four with a gross annual income of \$55,000 should pay no more than \$1,375 per month for housing.

For a rental unit, total housing costs include the monthly rent payment as well as consideration for a utility allowance. With for-sale units, total housing costs include the mortgage payment, homeowner association dues, property taxes, mortgage insurance and any other related assessments.

To learn more about HCD and housing, please see our Info-Bulletin: Carlsbad Housing Plan ([IB-137](#)).



INCLUSIONARY HOUSING REQUIREMENTS

The city’s Inclusionary Housing Ordinance ([CMC §21.85](#)) was passed by the City Council in 1993, and established the legal basis for requiring inclusionary housing in new residential development in the city. The following provides a summary of key standards required under the ordinance for new residential development in the city.

Number of units required

- All residential development projects proposing seven or more housing units are required to provide at least 15% of the total units (including density bonus units) to be restricted for low-income households.

For projects proposing six or fewer units, refer to Alternative #1 under the “Alternative Means of Compliance” section of this info-bulletin.

- Inclusionary unit requirements apply to all residential development projects (rental or for-sale product), including mixed-use projects.
- When calculating, fractional units ≥ 0.5 must be rounded up to the next whole number.

Example: An applicant proposes to satisfy its inclusionary requirements for a 112-unit residential development by reserving 15% of the units for low-income households, or 17 units ($112 \times 0.15 = 16.8$, rounded up to 17). The remaining 95 units (112 units minus 17) may be sold at market rate.

Duration units protected

- Inclusionary *rental* units shall remain restricted and affordable to the designated income group for not less than 55 years.
- Inclusionary *for-sale* units shall remain restricted and affordable to the designated income group for not less than 30 years.

Development standards

- Inclusionary units should be located throughout the development rather than clustered in one area.
- The inclusionary units must be indistinguishable from the market-rate units in the development, at least outwardly.
- The inclusionary units must be constructed prior to or concurrent with development of the market-rate units, and prior to final building permit approval of the market-rate units.
- Residents of the inclusionary units must have access to the same amenities – such as pool, fitness center and parking – as residents of the market-rate units.
- The inclusionary units must include a similar mix and number of bedrooms as the market-rate units.
- When ten or more inclusionary units are required, at least ten percent of those required units must provide at least three bedrooms.
- To the extent possible, projects using for-sale units to satisfy inclusionary requirements shall be designed to be compatible with conventional mortgage financing programs.



ALTERNATIVE MEANS OF COMPLIANCE

The city understands that various constraints may frustrate a developer’s ability to meet the strict letter of the city’s inclusionary housing regulations. As such, the ordinance allows for the City Council to authorize “alternative means of compliance” when it is found that the alternative meets the intent of the city’s Inclusionary Housing Ordinance ([CMC §21.85](#)) and the goals and policies of the city’s [2021-2029 Housing Element](#).

Council Policy Statement No. 57 ([Policy 57](#)) lists those alternatives that have been found to meet the intent of the city inclusionary code and help address its affordable housing needs. These alternative means of compliance are summarized in the sections below, but please refer to [Policy 57](#) for the specific requirements.

Alternative #1: Payment of in-lieu fee

Rather than constructing the inclusionary unit as part of the development project, applicants proposing ≤six units may instead pay a fee.

- \$8,529 for a new single-family detached home
- \$15 per square foot of net building area for each proposed market-rate unit.

The rates above are effective 2022. Please check the city’s [Fee Schedule](#) for the most current rates.

“Net building area” means the aggregate gross floor area of all the unrestricted dwelling units within a development, excluding areas outside the dwelling unit’s

habitable space such as garages, carports, parking areas, porches, patios, open space, and excluding common areas such as lobbies, common hallways, stairways, elevators and equipment spaces.

Collected in-lieu fees are deposited into the city’s Housing Trust Fund, and applied towards the furthering of the city’s affordable housing needs pursuant to Council Policy Statement No. 90 ([Policy 90](#)).

Alternative #2: Varying housing affordability

In addition to providing more housing available for low income families, the city also recognizes a need to increase housing stock for other targeted and needed housing affordability levels such as moderate, very low- and extremely low-income households. As such, so long as the total average gross income restriction for the required inclusionary units does not exceed 80% of the AMI, the makeup of the inclusionary units can be comprised on any combination of income categories.



Alternative #3: Increases in residential density

While not specific to the city’s inclusionary housing ordinance, sometimes there is a request from a property owner to increase their residential density above what is authorized under the city’s current land use plans. In exchange for the increased density, the city will require that any future development on the site meet the following additional inclusionary housing requirements.

- At least 20% of the total residential units are restricted for low-income households; or,
- A least 15% of the total residential units are restricted for low-income households and an additional 10% are restricted for moderate-income households; or,
- At least 15% of the total residential units are restricted for very low-income households.

This has been applied to properties that received an increase in residential density as part of the [2015 General Plan](#) update and the [2021-29 Housing Element](#).

Alternative #4: Reduction credit

Residential development (for-sale or rental product) can reduce its inclusionary housing obligations from 15% to 12.5%, under the following conditions.

- All affordable units must be made available to very low- or extremely low-income households.
- Affordable units are located on the same site as the market-rate units.
- No financial assistance from the city is provided.

Example: “A 78-unit residential development is proposed, which requires that 15% of the units be reserved for low-income (12 units). If the developer voluntarily agrees to make inclusionary units available to very low-income households, then the developer may receive an incentive reduction credit. Under this scenario, 10 units in a 78-unit development projects equates to 12.8%, which meets the minimum inclusionary housing requirement.

Alternative #5: Use of accessory dwelling units

Pursuant to [CMC §21.85.070.B](#), construction of up to 15 accessory dwelling units (ADU) can be used to satisfy inclusionary housing requirements. Refer to our info-bulletin on ADUs ([IB-111](#)) for additional information. The standards below provide additional specifications for when this allowance can be used.

- The project proposes ≥ 200 residential detached dwelling units.
- The ADU may be an attached or detached product type (Junior ADU prohibited) .
- The ADUs shall have an affordability term ($\leq 80\%$ AMI with rents $\leq 70\%$ AMI) of at least 30 years.

Notwithstanding the above, for projects proposing ≤ 6 units, the in-lieu fee may be waived if a detached or attached ADU or Junior ADU is constructed concurrent with construction of the market-rate unit, deed restricted for low-income households for 30-years, and occupied by income-qualified families.

Alternative #6: Off-site construction

Circumstances may arise in which the public interest would be better served by allowing some or all of the required inclusionary units to be developed at an



alternative site. This is referred to as a “Combined Inclusionary Housing Project” or “Combined Project.” To qualify, the following requirements must be met.

- The inclusionary calculation requirements shall be based on the total number of market-rate units to be provided, as opposed to the total number of residential units in the project. See example below.

Example: An applicant proposes to satisfy its inclusionary requirements for a 112-unit project by building 15% of the units for low-income households off-site. This leaves 85% of the units for market rate, for a total residential unit count of 132 units ($112 \div 0.85 = 131.8$, rounded to 132). Using the total residential unit count, the number of inclusionary units required is 20 ($132 \times 0.15 = 19.8$, rounded to 20).

- The decision-making authority of the permit application may approve a Combined Project subject to the following findings.
 - The site has not or will not receive a density increase or density bonus.
 - Site conditions make it physically infeasible to accommodate the inclusionary units on-site.
 - Significant price and product type disparities make it financially infeasible to accommodate the inclusionary units on-site.
 - There is a documented lack of development capacity to deliver affordable housing on-site.
 - The off-site option provides greater financial feasibility/cost effectiveness than the on-site.
 - The off-site option provides better access to jobs, schools, transit, and services.
 - The off-site option supports housing goals and policies in the city’s Housing Element.

Notwithstanding, the City Council shall retain final permit approval authority, including approval of the Affordable Housing Agreement, on any Combined Project that requires financial assistance from the city.



Alternative #7: Inclusionary housing credit bank

For projects that build inclusionary units in excess of code requirements, and where the city is financially participating in the project, the city can sell those excess units in the form of credits to other developers to satisfy their inclusionary requirement. The proceeds from the credit sales are deposited into the city's Housing Trust Fund and redistributed pursuant to [Policy 90](#).

- **Bank Creation.** The City Council may approve the creation of an inclusionary housing credit bank, subject to the following findings.
 - The inclusionary units are in excess of the minimum number of units required under the city's Inclusionary Housing Ordinance; or
 - The inclusionary units are part of a 100% affordable housing project; and
 - The inclusionary units are constructed and received final inspection; and
 - The city financially contributed to the construction of the project.
- **Credit Purchase Eligibility.** Projects proposing more than seven, but no more than 50 residential units are eligible to purchase credits.
- **Credit Price.** The price for each inclusionary unit shall be determined by dividing the city's financial contribution by the total number excess inclusionary units, subject to annual CPI adjustments.

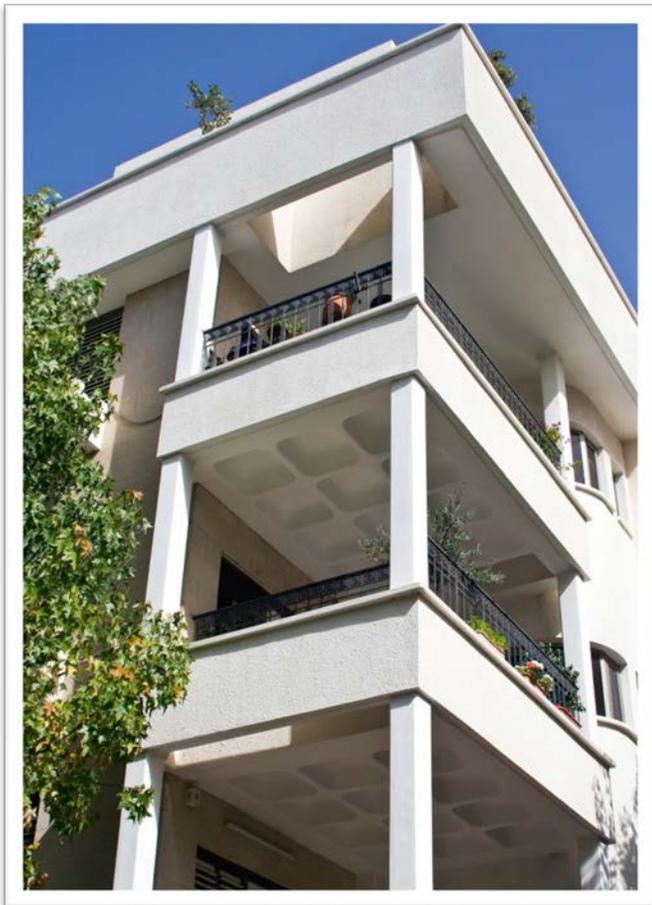
- **Credit Bank.** Applicants must pay the credit price of available credits from the oldest established inclusionary housing bank that is located within the same city quadrant in which the market-rate units are located, or if none available, sites which are contiguous to the quadrant in which the units are proposed.
- **Credit Purchase Ratio** – The amount of credits to be purchased shall be based on the following.
 - 7 to 20 units: 1.0 credit/inclusionary unit
 - 21 to 35 units: 1.5 credits/inclusionary unit
 - 36 to 50 units: 2.0 credits/inclusionary unit
- **Credit Purchase Approval.** The decision-making authority of the development project is authorized to approve credit purchases, so long as the findings below are met. Should a request to purchase credits occur after discretionary approval, the Community Development Director or the Housing & Homeless Services Director shall have the authority to approve credits, subject to the same findings.
 - The project site is located within the same [Growth Management Plan](#) quadrant that the housing credit is located, or if nonavailable, sites which are contiguous (share a common boarder) with the quadrant in which the units are proposed.
 - Sufficient housing credits available to purchase.
- **Credit Price Payment.** Payment of credit purchases shall be due prior to recordation of the final map or issuance of a building permit, in situations where the project does not include a subdivision. If the project entitlements expire, credits will be made available to another eligible project(s).



Alternatives not listed, but may be considered

The City Council may approve other alternatives means of compliance not listed in [Policy 57](#) when evidence is provided that the alternative helps achieve relevant Housing Element policies and goals and assists the city in meeting its state housing requirements.

As part of the City Council's consideration, the applicant must also show why compliance with current inclusionary housing regulations would be infeasible or present unreasonable hardship in light of such factors as project size, site constraints, market competition, price and product type disparity, available financial subsidies, and approved alternatives listed in [Policy 57](#).



AFFORDABLE HOUSING AGREEMENTS

An Affordable Housing Agreement (AHA) is a legally binding agreement between the developer and the city to ensure that the inclusionary requirements of a residential development are satisfied. [CMC §21.85.140](#) provides the specific requirements of the AHA, but the more significant components are listed below.

- A project condition shall be added to projects subject to the inclusionary ordinance and [Policy 57](#) requiring that an AHA be reviewed, approved, and

recorded prior to Final Map or issuance of building permits, whichever is first.

- Among other items, the AHA must include the number of required inclusionary units, the unit sizes, location, affordability tenure, required findings, terms and conditions of affordability and unit production schedule.
- The AHA and all relevant terms and conditions shall be recorded against the entire development.
- The AHA shall bind all future owners and successors in interest for the term of years specified therein.

APPROVAL AUTHORITY

The approval authority for the development project subject to these standards is as follows:

- The decision-making authority for the underlying permit application(s) shall have the authority to approve projects found consistent with [Policy 57](#).
- Development projects that propose an alternative means of compliance that is not specifically provided for in the CMC or [Policy 57](#) shall be considered by the Housing Commission for a recommendation to the City Council.
- Approval authority of the AHA is as follows:
 - AHA that are consistent with [CMC §21.85](#) and [Policy 57](#) and do not request financial assistance from the city shall first be considered by the Affordable Housing Policy Team for a recommendation to the H&HS Director.
 - Affordable Housing Agreements that propose a deviation(s) to [Policy 57](#) or request financial assistance shall first be considered by the Affordable Housing Policy Team and Housing Commission for a recommendation to the City Council, which has the authority to issue final approval of the alternative.

YOUR OPTIONS FOR SERVICE

To schedule an appointment or to learn more about this program, please contact the Planning Division at 442-339-2600 or via email at Planning@carlsbadca.gov or the Housing & Homeless Services Department at 442-339-4721 or via email at Housing@carlsbadca.gov.





City Council

POLICY STATEMENT

Policy No. 57
 Date Issued 3-22-22
 Resolution No. 2022-078

Subject: Inclusionary Housing Ordinance – Alternative Means of Compliance

Purpose

Carlsbad Municipal Code (CMC) §21.85.100, of the city's Inclusionary Housing Ordinance, sets forth the City Council's ability to make available certain allowances and provisions for development applications that would result in meeting the city's affordable housing needs. To assist in achieving this end, the City Council developed this policy to provide alternatives means of compliance for meeting the city's growing affordable housing needs. The provisions contained in Policy No. 57 (Off-site and Combined Inclusionary Housing Projects), Policy No. 58 (Sale of Affordable Housing Credits), and Policy No. 68 (Inclusionary Housing Incentive Credit Schedule) have been combined into this revised Policy No. 57. Policy Nos. 58 and 68 were then subsequently rescinded to have a single City Council Policy that administers the inclusionary housing program.

Background

The city's Inclusionary Housing Ordinance (CMC §21.85) was passed by the City Council in April 1993 and established the legal basis for requiring inclusionary housing in new residential development in the city. Under the program, all residential developments are required to comply with the code, projects with seven or more units are required to restrict 15 percent of the total number of homes as affordable to lower income households. This obligation is fulfilled through construction of rental or ownership units, or through other alternative means of compliance.

Statement of policy

It is the policy of the City Council that the following Alternative Means of Compliance satisfy the intent and requirements of CMC §21.85, 2021-2029 Housing Element Goals 10-G.2 and 10-G.4, and Housing Element Policies 10-P.13 and 10-P.35.

1. In-Lieu Fee

Pursuant to CMC §21.85.110, developments proposing a total of six or fewer units may satisfy their affordable housing obligation through payment of inclusionary housing in-lieu fees. As an alternative for property owners who are constructing one single-family residence on a single lot, an Accessory Dwelling Unit (detached or attached) or Junior Accessory Dwelling Unit may be constructed concurrent with construction of the market-rate unit. The ADU or JADU must be deed restricted for occupancy by income and rent qualified low-income households for 30-years.

2. Inclusionary Affordability Requirement and Credit Adjustment

CMC §21.85.030.B states that not less than 15% of the total units in a residential development of seven or more units shall be restricted to “lower-income households.” Household income qualification is defined as 80% or below of the area median income for San Diego County. The affordability is set at the 70% income level for rental households and 80% for ownership units. To assist the city in providing housing at varying affordability levels (e.g., moderate, low-, very- and extremely low-income), developers shall have the following options for satisfying the inclusionary ordinance:

- a. At least 15% of the total units are restricted for low-income households (default requirement as defined in CMC §21.85.020); or,
- b. At least 15% of the total units are restricted at any combination of income categories (e.g., moderate, low, very low, and/or extremely low-income) as long as the total average gross income restriction does not exceed 80% of the area median income for San Diego County as determined annually by the U.S. Department of Housing and Urban Development (affordable housing levels are defined in CMC §21.85.020); or,
- c. A residential development can receive an incentive credit of 1.2 per inclusionary unit, thereby reducing the inclusionary housing requirement from 15% to 12.5%¹, under the following conditions.
 1. All affordable units must be made available to very low or extremely low-income households, or combination thereof.
 2. The units are located on the same site as the market-rate units.
 3. No financial assistance from the city is required.

3. Use of Accessory Dwelling Units

Pursuant to CMC §21.85.070.B, an applicant may construct up to 15 accessory dwelling units (ADU) to satisfy their inclusionary requirements. The standards below provide additional specifications for when this allowance can be used.

- a. The project proposes 200 or more residential detached units; and,
- b. The ADUs are an attached or detached housing product type; and,
- c. The ADU shall have an affordability term (low-income) of at least 30 years; and,

¹ **Example.** An applicant proposes a 78-unit residential development, which requires that 15% of the units be reserved for low-income (12 inclusionary units). If the above standards are applied, the inclusionary units can be adjusted by 1.2, thereby reducing the total number of affordable units from 12 to 10 (12 divided by 1.2 equals 10). Under this scenario, 10 units in a 78-unit development projects equates to 12.8%, which meets the minimum inclusionary housing requirement of 12.5%.

- d. The property owner shall be required to income qualify the tenant under 80% of the area median income for San Diego County and restrict rents to not exceed the affordable rent for 70% of the area median income for the unit size.

Refer to Section 1 of this policy on the use of an ADU to satisfy inclusionary requirements on a single lot to be developed with one single-family residence.

4. Combined Inclusionary Housing (Off-Site Compliance)

Circumstances may arise in which the public interest would be better served by allowing some or all inclusionary units associated with a proposed residential development project to be produced and operated at an alternate off-site(s) location. This alternative is described as a "Combined Inclusionary Housing Project" or "Combined Project."

a. Inclusionary Housing Calculation

The inclusionary housing calculation requirement shall be based on 15% of the total number of market-rate units to be provided on-site and the inclusionary/affordable units to be provided off-site.²

b. Required Findings

Consistent with CMC §21.85.080.B, the City Council delegates approval authority of any proposed Combined Projects to the decision-making authority of the proposed development project so long as the findings below are met. Notwithstanding, the City Council shall retain final approval authority of the Affordable Housing Agreement on any Combined Project that requires financial assistance from the city. The findings below shall be included as part of the Affordable Housing Agreement decision documents.

1. The site has not or will not receive a density increase or density bonus to increase the number of total residential units permitted on the master development site.
2. Site conditions make it physically infeasible to accommodate the inclusionary units on-site.
3. Significant price and product type disparities make it financially infeasible to accommodate the inclusionary units on-site.

² **Example:** An applicant proposes to satisfy its inclusionary requirements for a 112-unit project by building 15% of the total units for low-income households off-site. If the 112 units are to remain as the total market-rate units on the site, the total number of residential units needs to be recalculated before calculating the required number of inclusionary units to be provided off-site. If 85% of the total units for market rate are now 112 units then the "total residential unit count" is recalculated and becomes 132 units ($112 \div 0.85 = 131.8$, rounded up to 132). Using the new total residential unit count of 132 units the total number of inclusionary units required for this project becomes 20 units ($132 \times 0.15 = 19.8$, rounded to 20), which results in an off-site inclusionary housing compliance requirement of 17.9% for this project ($20 \div 112 = 17.85\%$).

4. There is a documented lack of development capacity to deliver affordable housing on-site.
5. The off-site option provides greater financial feasibility and cost effectiveness than the on-site alternative.
6. The off-site option provides better access to jobs, schools, transit, and services.
7. The off-site option supports affordable housing goals and policies as expressed in the city's Housing Element.

5. Inclusionary Housing Credit Purchase Program

In certain circumstances, it may be in the city's best interest to financially invest in projects or ventures that further increase the availability and overall number of affordable housing units in the city. Pursuant to §21.85.090, inclusionary units created which exceed the final requirement for a project may, subject to City Council approval in the affordable housing agreement, be utilized by a developer to satisfy other inclusionary requirements for which it is obligated or market the units to other developers as a combined project subject to the requirements of Section 21.85.080.

a. Affordable Housing Credit Bank – Establishment

1. Bank Establishment

Consistent with §21.85.090, this policy authorizes the establishment of an affordable housing credit bank where affordable units that are constructed in excess of inclusionary requirements can be purchased from the city by qualifying developers to satisfy their respective inclusionary housing requirements. The City Council shall approve the creation of an affordable housing credit bank, subject to the following findings.

- i. The affordable units intended for purchase are in excess of the minimum number of units required under the city's Inclusionary Housing Ordinance; or
- ii. The affordable units intended for purchase are part of a 100% affordable housing project; and
- iii. The affordable excess units are constructed and received final inspection; and
- iv. The city financially contributed to the construction of the affordable housing project that the affordable excess units are a part.

This policy does not authorize a developer to sell excess affordable units that

are constructed without city financial assistance. However, developers may pursue approval for such an action, consistent with CMC §21.85.090 and Section 6 of this policy.

2. Credit Pricing

The price for each unit (credit) shall be determined by dividing the “local financial contribution” by the total number of affordable units that are in excess of inclusionary requirements. The local (city’s) financial contribution shall consist of all city financial assistance provided to the project (e.g., loans, expenditures, hard/soft costs, and accrued interest). The credit price shall be adjusted annually by the Consumer Price Index (CPI) and set by the City Council as part of their annual approval of the Master Fee Schedule.

b. Affordable Housing Credit Bank – Administration

1. Project Eligibility

Projects proposing more than seven, but no more than 50 residential units are eligible to purchase affordable housing credits to satisfy their inclusionary housing requirements. Projects greater than 50 residential units may pursue approval of eligibility to purchase affordable housing credits consistent with CMC §21.85.070 and Section 6 of this policy.

2. Credit Purchase Ratio

The amount of credits to be purchased shall be based on the following scale.

- i. 7 to 20 units – 1.0 credit for each required inclusionary unit
- ii. 21 to 35 units – 1.5 credits for each required inclusionary unit
- iii. 36 to 50 units – 2.0 credits for each required inclusionary unit

3. Credit Purchase Findings

Authority to approve a credit purchase from an established affordable housing credit bank shall be given to the decision-making authority of the proposed development permit, so long as the findings below are met. Should a request to purchase credits occur after discretionary approval, the Community Development Director and the Housing & Homeless Services Director shall have the authority to approve credits, subject to the findings below. If the findings can be made, the Community Development Director, or designee shall determine via a substantial compliance determination that the credit purchase satisfies the project condition to construct the inclusionary unit, without the need to process an amendment to the permit.

- i. The affordable housing credit bank project site is located in the same city quadrant in which the market-rate units are located, or is contiguous to the quadrant in which the market-rate units are proposed; and,
- ii. There are sufficient housing credits available to purchase.

4. Proximity to Bank

CMC §21.85.080 states that the credit purchase must be from an affordable housing credit bank that is within the same city quadrant, or contiguous quadrant in which the proposed market-rate units are proposed. This policy shall clarify that "contiguous" includes quadrants that share a common border (e.g., the northeast and southwest quadrants are considered contiguous to the northwest quadrant because they share a common border).

5. Credit Payment

Considering how credit pricing is established (Section 5.a.2), it is anticipated that the credit purchase price will vary. As such, applicants must pay the credit price of available credits from the oldest affordable housing credit bank project that is located within the same city quadrant in which the market-rate units are located, or is contiguous to the quadrant in which the market-rate units are proposed. Funds from housing credit deposits/purchases shall be deposited to the Housing Trust Fund.

- i. Payment of housing credit purchases shall be due prior to recordation of the final map or issuance of a building permit, in situations where the project does not include a subdivision.
- ii. Payments shall be deposited to the Housing Trust Fund pursuant to Council Policy No. 90.
- iii. If the project entitlements or above-mentioned due dates expire, the credits will be made available to another project(s), subject to this process.

6. Other Alternative Means of Compliance not Specified

The City Council may approve, pursuant to CMC §21.85.070, alternatives that are not authorized by this policy where the proposed alternative supports specific housing element policies and goals and assists the city in meeting its state housing requirements. Alternatives may include, but are not limited to, acquisition and rehabilitation of affordable units, conversion of existing market-rate units to affordable units, construction of special needs housing projects or programs (e.g., shelters, transitional housing), the

construction of accessory dwelling units, or the purchase of housing credits for projects greater than 50 residential units.

7. Approval Authority

Approval authority for development projects subject to this policy is as follows:

- a. The decision-making authority for the underlying permit application(s) shall have the authority to consider and approve projects found consistent with this policy; this includes consideration and approval of the findings for Combined Inclusionary Housing Projects (Section 5.b.3).
- b. Development projects that propose an alternative means of compliance that is not consistent with this policy shall be considered by the City Council, after review and recommendation of the Affordable Housing Policy Team and the Housing Commission.
- c. A project condition shall be added to all projects subject to this policy requiring that an Affordable Housing Agreement, consistent with §21.85.140, be reviewed, approved, and recorded prior to Final Map or issuance of building permits, whichever comes first. Approval authority of the Affordable Housing Agreement is as follows:
 1. Affordable Housing Agreements that are consistent with §21.85 and this policy and do not request financial assistance from the city shall be considered for approval by the Housing and Homeless Services Director, after review and recommendation of the Affordable Housing Policy Team.
 2. Affordable Housing Agreements that propose a deviation(s) to this policy or include a request for financial assistance shall be considered by the City Council, after review and recommendation of the Affordable Housing Policy Team and the Housing Commission.

Related Policy

- a. Policy No. 90

**CARLSBAD VILLAGE MIXED USE (SDP 2023-0014)
ENHANCED STAKEHOLDER OUTREACH REPORT**

August 10, 2023

City of Carlsbad
Planning Division
1635 Faraday Avenue
Carlsbad, CA 90028
ATTN: Jason Goff

**SUBJECT: Enhanced Stakeholder Outreach Report for Carlsbad Village Mixed Use – 945-1065
Carlsbad Village Drive – Project No. SDP 2023-0014**

GRT Carlsbad Village, LLC conducted an Enhanced Stakeholder Outreach Program for Carlsbad Village Mixed Use (SDP 2023-0014) in accordance with Carlsbad Council Policy No. 84, Development Project Public Involvement Policy.

REQUIREMENTS

Applicants of certain types of larger development projects must conduct an Enhanced Stakeholder Outreach Program following submittal of project applications to the city, as outlined in the *Guidance on Development Project Involvement (P-21)* document written by the City of Carlsbad (City). The proposed project meets the following requirement as a project that must engage in Enhanced Stakeholder Outreach: *Residential Development consisting of 50 or more residential lots or residential units.*

The City recognizes that a one-size-fits-all approach doesn't work for all projects and gives applicants options for how to engage stakeholders in a discussion on their projects. The Carlsbad Village Mixed Use project conducted the following outreach option provided by the City to engage the community on the project: *A project website that enables the public to learn about the project and understand how to provide input.*

PROJECT WEBSITE

The project entitlement application was submitted to the City on May 17, 2023 and a project website was created following the requirements in the form P-21 guidance.

In accordance with City Council Policy No. 84, a Notice of Project Application (Appendix A) was mailed to 132 property owners within 600 feet of the project site on June 15, 2023. The mailed notice included a URL to the project website (www.carlsbadvillagemixeduse.com) for those interested in learning more about the project and/or providing feedback.

The website includes information about the project, conceptual project images, and a fillable form to provide feedback (Exhibit 1). On the website landing page, there are several direct links that lead users to the feedback form (Exhibit 2).

As of August 10, 2023, seven (7) comments have been received from members of the public. These comments are included in Appendix B. The general nature of the comments focused on the project's height.

The project website will continue to remain active and allow stakeholders to visit and/or provide feedback throughout the approval process.

SUMMARY/CONCLUSION

While community outreach efforts for the project will continue throughout the application and approval process, the Applicant has demonstrated compliance with both Part A and Part B of City Council Policy No. 84. If you have any questions or need additional information, please do not hesitate to contact me at the contact information below.

Regards,

Jonathan Frankel – Applicant’s Representative
Atlantis Group
2488 Historic Decatur Road, Suite 220
San Diego, CA 92106
925-708-3638

EXHIBIT 1 – PROJECT WEBSITE FEEDBACK FORM

PLEASE PROVIDE YOUR COMMENTS AND INFO SO WE CAN KEEP YOU UPDATED ON THE PROJECT.

We can also be reached at
info@carlsbadvillagemixeduse.com.

FIRST NAME(REQUIRED)

LAST NAME(REQUIRED)

EMAIL(REQUIRED)

PHONE(REQUIRED)

ADDRESS(REQUIRED)

CITY(REQUIRED)

STATE(REQUIRED)

ZIP(REQUIRED)

COMMENTS(REQUIRED)

EXHIBIT 2 – PROJECT WEBSITE LANDING PAGE

CARLSBAD VILLAGE MIXED USE

[Home](#) [Overview](#) [Images](#) [Provide Feedback](#)

Carlsbad Village Mixed Use is a sustainable, transit-oriented mixed use project that will bring a diverse mix of uses to an aging site and provide affordable housing and local retail amenities, including a planned neighborhood market.

[Share your Thoughts >](#)



APPENDIX A

NOTICE OF PROJECT APPLICATION

NOTICE OF PROJECT APPLICATION

GRT CARLSBAD VILLAGE LLC

June 13, 2023

SUBJECT: Notice of Project Application: SDP 2023-0014 – Carlsbad Village Mixed Use

Dear Neighbors and Interested Stakeholders:

Per City Council Policy No. 84, this is to inform you that GRT CARLSBAD VILLAGE, LLC has submitted the following development application to the City of Carlsbad:

PROJECT NUMBER: SDP 2023-0014

PROJECT NAME: Carlsbad Village Mixed Use

PROJECT ADDRESS: 945-1065 Carlsbad Village Drive

ASSESSOR PARCEL NO: 203-320-53, -54, -55 and -56

DATE OF APPLICATION: May 17, 2023

PROJECT DESCRIPTION: Site Development Plan to redevelop an existing shopping center and construct a 218-unit mixed use project with 13,800 square feet of neighborhood serving retail on a 4.12-acre site. The project will include 22 very-low-income affordable units. The retail component will be contained in two one-story buildings and the residential component in two five-story buildings. The project will include approximately 350 parking spaces at-grade and in a parking structure, as well as open space and landscaped areas. See attached site plan and elevations.

APPLICANT CONTACT: Jonathan Frankel
Atlantis Group
2488 Historic Decatur Road
San Diego, CA 92106
619-796-2205
jfrankel@atlantissd.com

CITY PROJECT PLANNER: Jason Goff, Senior Planner
Jason.Goff@carlsbadca.gov

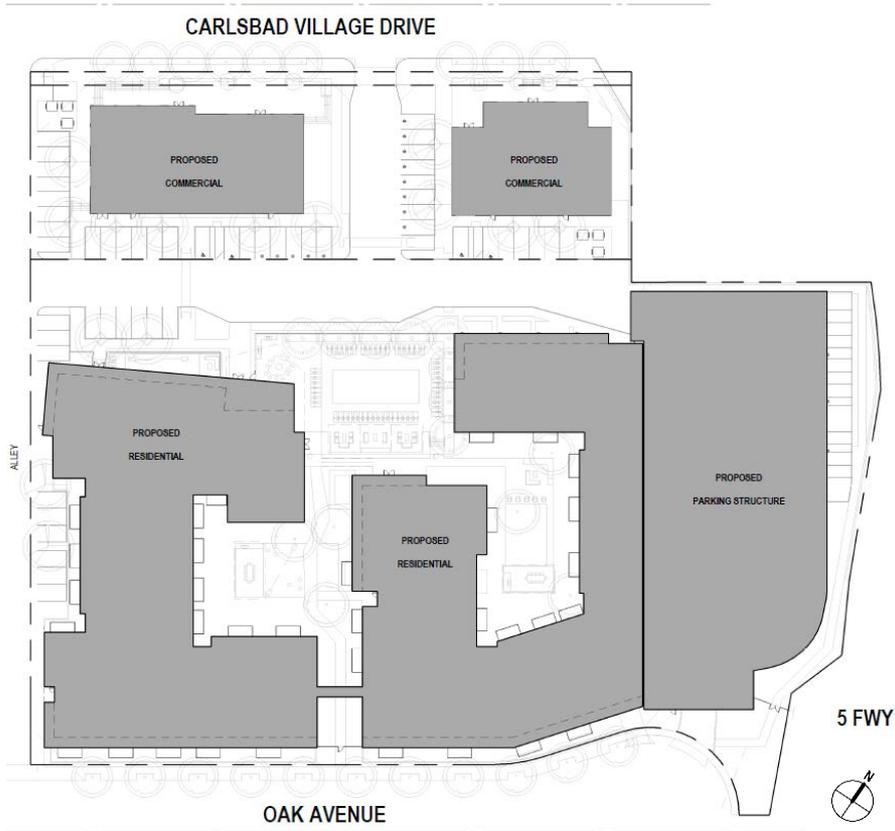
If you would like more information or would like to provide input on the project, please do not hesitate to contact me at the email address above. The project is currently under review by the City and your input is welcome. You can also provide your feedback by visiting www.carlsbadvillagemixeduse.com. A decision to approve or deny this application will be made by the City at a future public hearing and you will be notified of the hearing in advance.

Sincerely,

Jonathan Frankel
Atlantis Group

Attachment: Site Plan and Building Elevations

SITE PLAN AND BUILDING ELEVATIONS



Site Plan.



View from Carlsbad Village Drive, looking south.



View from internal drive aisle, looking east.

**CERTIFICATIONS
EQUALIZED ASSESSMENT INFORMATION
NOTICE OF PROJECT APPLICATION MAILING
AND SIGN POSTING**

I certify ALL of the following:

JF
Initial

The radius map and property owners list utilized for the Notice of Project Application represent the latest information from the County Assessor's Equalized Assessment Roll.

JF
Initial

The "Notice of Project Application" was mailed on June 15, 2023 (DATE) to property owners within a 600-foot radius of the subject property, affected property/homeowner's association(s) (if any), those notified for Part B Stakeholder Outreach if the Notices were combined, and, if in the Coastal Zone, occupants within a 100-foot radius of the subject property. A complete copy of the mailed notice (and included exhibits, if any), radius map and mailing lists are attached to this certification.

JF
Initial

The "Notice of Project Application" sign has been posted at a conspicuous location on the site on June 15, 2023 (DATE). A photograph of the posted sign is attached.

SIGNATURE: Jonathan Frankel

PRINT NAME: Jonathan Frankel

PROJECT NAME: Carlsbad Village Mixed Use (CVMU)

PROJECT NUMBER: SDP 2023-0014

LOCATION: 945-1065 Carlsbad Village Drive

Carlsbad CA 90028

RETURN TO: Jason Goff
(Project Planner)

CITY OF CARLSBAD
PLANNING DIVISION
1635 Faraday Avenue
Carlsbad, CA 92008-7314

Certified Property Owner's
Affidavit

I, MICHAEL HILGERTSON

Hereby certify that the attached list contains the names and addresses of all persons to whom all property is assessed as they appear on the latest available assessment roll of the county within the area described on the attached application and for all properties within 600 feet from the exterior boundaries of the property described on the attached application, as of 5/12/22

Subject Parcel number 203 320 53, 54, 55, 56

I certify under penalty of perjury the forgoing is true and correct to the best of my knowledge.

(Signed) [Signature]

Name MICHAEL HILGERTSON

Address 11354 MCKENNA VALLEY RD PENN VALLEY 45946

Phone # 800 567 7104

	APN	NAME	ADDRESS	CITY	STATE	ZIP
1	203-320-53	G R T CARLSBAD VILLAGE LLC	2001 WILSHIRE BLVD #420	SANTA MONICA	CA	90403
1	203-320-54	G R T CARLSBAD VILLAGE LLC	2001 WILSHIRE BLVD #420	SANTA MONICA	CA	90403
1	203-320-55	G R T CARLSBAD VILLAGE LLC	2001 WILSHIRE BLVD #420	SANTA MONICA	CA	90403
1	203-320-56	G R T CARLSBAD VILLAGE LLC	2001 WILSHIRE BLVD #420	SANTA MONICA	CA	90403
2	156-180-15	ARDIS ZANDER/SHICK FAMILY TRUST	5230 LAS VIRGENES RD #200	CALABASAS	CA	91302
3	156-180-26	ARDIS ZANDER/SHICK FAMILY TRUST	5413 FAIRGREEN WAY	BONSALL	CA	92003
4	156-180-28	ARDIS ZANDER/SHICK FAMILY TRUST	5230 LAS VIRGENES RD #200	CALABASAS	CA	91302
5	156-180-30	ARDIS ZANDER/SHICK FAMILY TRUST	5230 LAS VIRGENES RD #200	CALABASAS	CA	91302
6	156-180-41	CITY OF CARLSBAD	1200 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
7	156-180-42	ANEWALT FAMILY TRUST 01-21-04	11835 SPRINGSIDE RD	SAN DIEGO	CA	92128
8	156-180-43	LYONS REVOCABLE FAMILY TRUST 02-04-90	7515 MIRAMAR AVE	LA JOLLA	CA	92037
9	156-180-44	GOLABATON L L C	P O BOX 911005	SAN DIEGO	CA	92191
10	156-180-45	PHOENIX RETAIL PARTNERS LLC	10721 TREENA ST #200	SAN DIEGO	CA	92131
11	156-180-46	CITY OF CARLSBAD	1200 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
12	203-130-20	E S A P PORTFOLIO LLC	POB 49550 PROP TAX	CHARLOTTE	NC	28277
13	203-130-22	A B 3 LLC	20 W 64TH ST #9V	NEW YORK	NY	10023
14	203-130-23	RODRIQUEZ FAMILY TRUST	1010 GRAND AVE	CARLSBAD	CA	92008
15	203-130-24	JACK B MILLS	3110 AZAHAR ST	CARLSBAD	CA	92009
16	203-130-27	AMAPOLO L L C	3117 WILDFLOWER SMT	ENCINITAS	CA	92024
17	203-130-34	E S A P PORTFOLIO LLC	POB 49550 PROP TAX	CHARLOTTE	NC	28277
18	203-202-13	CHOSEN WISE PROPERTIES LLC	3144 EL CAMINO REAL #104	CARLSBAD	CA	92008
19	203-202-18-01	HEKMAT FAMILY TRUST 02-24-06	17234 SILVER GUM WAY	SAN DIEGO	CA	92127
20	203-202-18-02	UCKER FAMILY LIVING TRUST 11-01-02	800 GRAND AVE #102	CARLSBAD	CA	92008
21	203-202-18-03	HAVILUK BARBARA D REVOCABLE LIVING TRUST 11-26-84	800 GRAND AVE #103	CARLSBAD	CA	92008
22	203-202-18-04	KROENER REVOCABLE TRUST 11-30-83	2516 MONTGOMERY AVE	CARDIFF	CA	92007
23	203-202-18-05	MICHAEL & DAWN B OBRIEN	800 GRAND AVE #105	CARLSBAD	CA	92008
24	203-202-18-06	BURNETT JAMES D & KIMBERLY REVOCABLE TRUST 02-06-06	208 GIBSON PT	SOLANA BEACH	CA	92075
25	203-202-18-07	SAFFO DR & MRS KARL S TRUST 09-14-20	800 GRAND AVE #107	CARLSBAD	CA	92008
26	203-202-18-08	BROSNAN JAMES J & PAMELA A BROSNAN TRUST 03-03-20	800 GRAND AVE #108	CARLSBAD	CA	92008
27	203-202-18-09	GARCIA-ROSS FAITH M 2019 TRUST 05-20-19	800 GRAND AVE #109	CARLSBAD	CA	92008
28	203-202-18-10	LOPEZ-PANAMA MIGUEL A & LOPEZ MARIVIC T	2308 MENDOTA WAY	ROSEVILLE	CA	95747
29	203-202-18-11	JOHNSON LISA K FAMILY TRUST 01-10-03	3306 E KACHINA DR	PHOENIX	AZ	85044
30	203-202-18-12	BUCKMASTER CHAD & AMANDA FAMILY TRUST 02-01-12	3119 CIRCA DE TIERRA	ENCINITAS	CA	92024
31	203-202-18-13	DAVID R WILLIAMS	5041 N CAMINO SUMO	TUCSON	AZ	85718
32	203-202-18-14	MELANIE MAURO	800 GRAND AVE #203	CARLSBAD	CA	92008
33	203-202-18-15	REED CALVIN L & LINDA E REVOCABLE LIVING SURVIVORS	800 GRAND AVE #204	CARLSBAD	CA	92008

34	203-202-18-16	MAKATURA FAMILY TRUST 10-25-01	2052 KIRKLAND CIR	THOUSAND OAKS	CA	91360
35	203-202-18-17	BERGAMASCHI RICHARD J & MARCHELE A FAMILY TRUST	800 GRAND AVE #206	CARLSBAD	CA	92008
36	203-202-18-18	THUM FAMILY TRUST 07-18-12	800 GRAND AVE #207	CARLSBAD	CA	92008
37	203-202-18-19	JOSEPH S & JOYCE M QUINCE	800 GRAND AVE #208	SAN DIEGO	CA	92109
38	203-202-18-20	BUBB FAMILY 2012 TRUST 04-23-12	800 GRAND AVE #209	CARLSBAD	CA	92008
39	203-202-18-21	SUSAN M BERGER	22506 SE 45TH LN	ISSAQUAH	WA	98029
40	203-202-18-22	ZARNOW FAMILY TRUST 02-14-03	800 GRAND AVE #211	CARLSBAD	CA	92008
41	203-202-18-23	MARK A & JULIA G BUTCHKO	7534 SEDGE MEADOW DR	INDIANAPOLIS	IN	46278
42	203-202-18-24	HAVER FAMILY 2011 TRUST 05-18-11	16520 SENTERRA DR	DELRAY BEACH	FL	33484
43	203-202-18-25	STIDMAN VALERIE H REVOCABLE TRUST 07-25-16	6504 CHEROKEE TRL	MINNEAPOLIS	MN	55439
44	203-202-18-26	JAN E & MARGARETA JANSSON	800 GRAND AVE #304	CARLSBAD	CA	92008
45	203-202-18-27	STADNICK FAMILY TRUST 07-03-00	27684 N 71ST WAY	SCOTTSDALE	AZ	85266
46	203-202-18-28	KING TRUST 12-22-10	61341 TRIPLE KNOT RD	BEND	OR	97702
47	203-202-18-29	HERBERT LEE & BEVERLY ANN JONES	800 GRAND AVE #307	CARLSBAD	CA	92008
48	203-202-18-30	WARWICK S & NOLA M FORD	142 W END AVE #26N	NEW YORK	NY	10023
49	203-202-18-31	SHEW STEVEN & TIFFANY FAMILY TRUST 04-04-20	800 GRAND AVE #309	CARLSBAD	CA	92008
50	203-202-18-32	GABRIELE COURT LLC	328 VISTA VILLAGE DR #D	VISTA	CA	92083
51	203-202-18-33	WILSON CORY A & SUSAN J TRUST 12-06-18	1330 LONG LAKE RD	FLORENCE	WI	54121
52	203-202-20	CARLYLE RESIDENCES COMMUNITY ASSN	5075 SHOREHAM PL #280	SAN DIEGO	CA	92122
53	203-320-02	CARLSBAD VILLAGE II LLC	3444 CAMINO DEL RIO N #202	SAN DIEGO	CA	92108
54	203-320-20	CARLSBAD VILLAGE II LLC	3444 CAMINO DEL RIO N #202	SAN DIEGO	CA	92108
55	203-320-27	ALLAHYARI FAMILY TRUST 07-17-02	19 TIERRA VISTA	LAGUNA HILLS	CA	92653
56	203-320-31	ALLAHYARI FAMILY TRUST 07-17-02	3008 EMINENCIA DEL SUR	SAN CLEMENTE	CA	92673
57	203-320-39	DENNYS INC	P O BOX 260888	PLANO	TX	75026
58	203-320-40	CARLSBAD VILLAGE II LLC	6700 TOWER CIR #1000	FRANKLIN	TN	37067
59	203-320-41	CARLSBAD VILLAGE II LLC	6700 TOWER CIR #1000	FRANKLIN	TN	37067
60	203-320-48	CARLSBAD VILLAGE II LLC	5120 SHOREHAM PL #150	SAN DIEGO	CA	92122
61	203-320-49	CARLSBAD VILLAGE LLC	3444 CAMINO DEL RIO N #202	SAN DIEGO	CA	92108
62	203-320-50	G6 HOSPITALITY PROPERTY LLC	3444 CAMINO DEL RIO N #200	SAN DIEGO	CA	92108
63	203-320-51	CARLSBAD VILLAGE II LLC	5120 SHOREHAM PL #150	SAN DIEGO	CA	92122
64	203-320-52	G6 HOSPITALITY PROPERTY LLC	3444 CAMINO DEL RIO N #200	SAN DIEGO	CA	92108
65	203-352-02	CAVALEA 2023 TRUST	3640 FELIZ CREEK RD	HOPLAND	CA	95449
66	203-352-03	CAVALEA 2023 TRUST	3640 FELIZ CREEK RD	HOPLAND	CA	95449
67	203-352-04	CAVALEA 2023 TRUST	3640 FELIZ CREEK RD	HOPLAND	CA	95449
68	203-352-11	3095 HARDING L L C	10951 SORRENTO VALLEY RD #2A	SAN DIEGO	CA	92121
69	203-352-12	CITY OF CARLSBAD	1200 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
70	203-352-13	GASTELUM FAMILY SURVIVORS TRUST 11-16-94	323 HILL DR	VISTA	CA	92083

71	203-352-14	ZMACK LLC	1868 CREST DR	ENCINITAS	CA	92024
72	203-352-15	EZRA MINISTRIES INC	825 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
73	203-352-18	EZRA MINISTRIES INC <DBA MISSION CHURCH>	825 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
74	203-352-19	P G P CARLSBAD SENIORS LTD	10951 SORRENTO VALLEY RD #2A	SAN DIEGO	CA	92121
75	203-353-04	BUCCHARE ERIC T & TRACY L TRUST 03-22-12	275 VILLAGE RUN W	ENCINITAS	CA	92024
76	203-353-05	CITY OF CARLSBAD	1200 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
77	203-353-06	CITY OF CARLSBAD	1200 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
78	203-353-07	CITY OF CARLSBAD	1200 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
79	203-353-09	JACK IN THE BOX INC/PACIFIC COAST INVESTMENTS GP	P O BOX 7099	NEWPORT BEACH	CA	92658
80	203-354-01	KENNETH J DAVIES/GEORGE W MERKLE	1576 PRIMERA ST	LEMON GROVE	CA	91945
81	203-354-05	3740 LP	P O BOX 33628	SAN DIEGO	CA	92163
82	203-354-07	GRAND & HARDING LLC	P O BOX 3277	BEVERLY HILLS	CA	90212
83	203-354-08	SCANLON FAMILY TRUST 05-16-16	3410 CAMINO ALEGRE	CARLSBAD	CA	92009
84	203-354-13	ORANGE LAND HOLDINGS LLC	264 PACIFIC AVE	SOLANA BEACH	CA	92075
85	203-354-15	BANK OF CALIFORNIA	P O BOX 12734	LA JOLLA	CA	92039
86	203-354-16	HELIX REAL ESTATE INVESTMENT TRUST	P O BOX 15453	SAN DIEGO	CA	92175
87	203-354-19	UEBER HAUN I LLC/GRANT HOLDINGS LLC	43650 LA CRUZ DR	TEMECULA	CA	92590
88	203-355-01	STARDUST HOMES L L C	P O BOX 2100	COLTON	CA	92324
89	203-355-02	JACKSON FAMILY TRUST 09-14-05	2504 MANCHESTER AVE	CARDIFF	CA	92007
90	203-355-03	JACKSON FAMILY TRUST 09-14-05	2504 MANCHESTER AVE	CARDIFF	CA	92007
91	203-355-04	2952 HARDING LLC	4899 CASALS PL	SAN DIEGO	CA	92124
92	203-355-05	G & M GAPCO LLC	16868 A LN	HUNTINGTN BCH	CA	92647
93	204-032-01	HATTER LARRY W & CATHLEEN E 1986 TRUST	5315 AVENIDA ENCINAS #130	CARLSBAD	CA	92008
94	204-032-10	DARCUIEL FAMILY TRUST 04-15-15	601 E PALOMAR ST #C-243	CHULA VISTA	CA	91911
95	204-032-12	CARLSBAD COMMUNITY CHURCH	3175 HARDING ST	CARLSBAD	CA	92008
96	204-100-06	CITY OF CARLSBAD	1200 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
97	204-110-01	MARY J JONES/JEANNE M STAMPER	1727 FIDDLERS RIDGE DR	FLEMING ISLAND	FL	32003
98	204-110-02	MARY J JONES/JEANNE M STAMPER	1727 FIDDLERS RIDGE DR	FLEMING ISLAND	FL	32003
99	204-110-32	COLLEEN Y MATSUBARA	983 PINE AVE	CARLSBAD	CA	92008
100	204-110-33	KATZ FAMILY LIMITED PARTNERSHIP	3302 NW PANORAMA DR	BEND	OR	97703
101	204-110-37	ROOSTER HOUSE LIMITED PARTNERSHIP	5252 BALBOA AVE #605	SAN DIEGO	CA	92117
102	204-111-01	LEONE LAEL TRUST 08-15-22	985 OAK AVE	CARLSBAD	CA	92008
103	204-111-02	CITY OF CARLSBAD	1200 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
104	204-111-03	OAK 51 APARTMENTS LLC	14336 OLDE HIGHWAY 80	EL CAJON	CA	92021
105	204-111-04	OAK 47 APARTMENTS LLC	P O BOX 21010	EL CAJON	CA	92021
106	204-111-05	OAK 47 APARTMENTS LLC	P O BOX 21010	EL CAJON	CA	92021
107	204-111-06	RACHEL 2015 TRUST 04-02-15	931 OAK AVE	CARLSBAD	CA	92008

108	204-111-09	STERNBERG TANYA 1998 TRUST	3156 HARDING ST	CARLSBAD	CA	92008
109	204-111-10	MARKS 2004 FAMILY TRUST 10-31-04	3166 HARDING ST	CARLSBAD	CA	92008
110	204-111-11	SMERUD FAMILY TRUST 03-27-97	928 PINE AVE	CARLSBAD	CA	92008
111	204-111-12	SMERUD FAMILY TRUST 03-27-97	1099 BUENA VISTA WAY	CARLSBAD	CA	92008
112	204-111-13	SCHICK LUAN TRUST 11-30-99	938 PINE AVE	CARLSBAD	CA	92008
113	204-111-14	INTERLAGOS INVESTMENTS L L C	7424 CIRCULO SEQUOIA	CARLSBAD	CA	92009
114	204-111-15	SEIDERMAN BRIAN & JOYCE TRUST 04-21-00	1415 FOREST AVE	CARLSBAD	CA	92008
115	204-111-16	ANDY H ANSON	982 PINE AVE	CARLSBAD	CA	92008
116	204-111-17	996 PINE AVE LLC	P O BOX 624	SOLANA BEACH	CA	92075
117	204-111-18	DOKKEN MARY T LIVING TRUST 03-08-99/DIANA DOKKEN	1022 PINE AVE	CARLSBAD	CA	92008
118	204-111-19	DOKKEN MARY T LIVING TRUST 03-08-99/RICHARD & BARBARA BOOTH	1022 PINE AVE	CARLSBAD	CA	92008
119	204-111-20	CARLSBAD COMMUNITY CHURCH	3175 HARDING ST	CARLSBAD	CA	92008
120	205-020-26	DONALD L KING	1155 OAK AVE	CARLSBAD	CA	92008
121	205-020-27	SCHNEIDER FAMILY TRUST 03-20-19	P O BOX 891	CARLSBAD	CA	92018
122	205-020-29	MAST POKEY FAMILY TRUST	1095 OAK AVE	CARLSBAD	CA	92008
123	205-020-30	N & G LLC	7848 PRAIRIE SHADOW RD	SAN DIEGO	CA	92126
124	205-020-31	ST JOHN FAMILY TRUST 11-01-11	3657 MONROE ST	CARLSBAD	CA	92008
125	205-020-32	SCANLON FAMILY TRUST 05-16-16	3410 CAMINO ALEGRE	CARLSBAD	CA	92009
126	205-020-33	MARIANO HERNANDEZ	1325 HIGGINS ST	OCEANSIDE	CA	92058
127	205-020-34	DANIEL & ELIZABETH GLASSER	1210 PINE AVE	CARLSBAD	CA	92008
128	205-020-53	TAMMY G MCDARIS	1103 OAK AVE	CARLSBAD	CA	92008
129	205-020-54	BABE & BUNNY TRUST 11-16-22	1109 OAK AVE	CARLSBAD	CA	92008
130	205-020-55	BABE & BUNNY TRUST 11-16-22	1109 OAK AVE	CARLSBAD	CA	92008
131	205-111-27	CITY OF CARLSBAD	1200 CARLSBAD VILLAGE DR	CARLSBAD	CA	92008
132	760-252-15	CARLSBAD VETERAN HOUSING L.P.	13520 EVENING CREEK DR N #160	SAN DIEGO	CA	92128



NOTICE OF PROJECT APPLICATION

PROJECT NAME: Carlsbad Village Mixed-Use (CVMU)

PROJECT NUMBER: SDP 2023-0014

PROJECT DESCRIPTION: Mixed use development to demolish an existing shopping center and construct two commercial buildings containing 13,800 square feet and two residential buildings containing 218-units (including 22 affordable units) on a 4.12 acre site.

CITY OF CARLSBAD
PLANNING DIVISION

JASON GOFF, SENIOR PLANNER
JASON.GOFF@CARLSBADCA.GOV
442-339-2643

PROJECT APPLICANT/REPRESENTATIVE
GRT CARLSBAD VILLAGE, LLC

JONATHAN FRANKEL
JFRANKEL@ATLANTISSD.COM
925-788-3438



NOTICE
This property is for sale. All offers must be in writing and submitted to the listing agent. No verbal offers or agreements. For more information, please contact the listing agent at [phone number].

RENTAFENCE.COM

RENTAFENCE.COM

APPENDIX B

PROJECT WEBSITE COMMENTS

Andrew Cerrina

From: Carlsbad Village <donotreply@carlsbadvillagemixeduse.com>
Sent: Friday, June 23, 2023 7:25 AM
To: info@carlsbadvillagemixeduse.com
Subject: Carlsbad Village Web Inquiry

First Name
Don
Last Name
King
Email
dkingconstruction1155@gmail.com
Phone
7606725000
Address
1155 Oak Ave.
City
Carlsbad
State
CA
Zip
92008
Comments
<p>You've already ruined the view and the feel of being near the beach. By putting a huge ugly building at the off ramp at Carlsbad Village Drive. Stop putting tall huge structures at our beach cities. Quit always letting money be the main concern of every greedy land owner. I can't believe you even have to ask. Leave the little beach town the way it is. You damaged it horribly already. And why is this the first notice i've ever recieved . When there are a bunch of tall buildings that have gone up?</p>

Andrew Cerrina

From: Carlsbad Village <donotreply@carlsbadvillagemixeduse.com>
Sent: Tuesday, June 27, 2023 11:37 PM
To: info@carlsbadvillagemixeduse.com
Subject: Carlsbad Village Web Inquiry

First Name
Sam
Last Name
Doan
Email
samdoan23@yahoo.com
Phone
7608897706
Address
3985 hibiscus circle
City
Carlsbad
State
CA
Zip
92008
Comments
<p>I think that all this development is happening way too fast the housing project across the street where dennys used to be just got finished and now there's another one being planned.</p> <p>Smart and final is the only grocery store in the village and that parking lot is always full of cars from people using the services that poinsettia plaza provides. It is very questionable to me that we need more housing in the village especially right next to a freeway on ramp. There's hundreds if not thousands of homes being built along el Camino between tamarack and cannon.</p> <p>I really hope this project gets turned down at least for now because doesn't benefit any of our long standing local residents who are effectively being forced out of their neighborhoods as all these developments keep happening</p>

Andrew Cerrina

From: Carlsbad Village <donotreply@carlsbadvillagemixeduse.com>
Sent: Tuesday, July 11, 2023 9:02 AM
To: info@carlsbadvillagemixeduse.com
Subject: Carlsbad Village Web Inquiry

First Name
Candy
Last Name
Kressman
Email
candy@yum-yoga.com
Phone
760-529-2767
Address
390 Oak Ave
City
Carlsbad
State
Ca
Zip
92008
Comments
I would like more information on this project

Andrew Cerrina

From: Carlsbad Village <donotreply@carlsbadvillagemixeduse.com>
Sent: Tuesday, July 11, 2023 11:07 AM
To: info@carlsbadvillagemixeduse.com
Subject: Carlsbad Village Web Inquiry

First Name
June
Last Name
Pasini
Email
junetune@att.net
Phone
760 213-2449
Address
233o Byron Place
City
Carlsbad
State
CA
Zip
92008
Comments
5 story buildings do NOT belong in our beautiful coastal city.

Andrew Cerrina

From: Carlsbad Village <donotreply@carlsbadvillagemixeduse.com>
Sent: Tuesday, July 11, 2023 9:07 AM
To: info@carlsbadvillagemixeduse.com
Subject: Carlsbad Village Web Inquiry

First Name
Shari
Last Name
Roberts
Email
slkahnroberts@gmail.com
Phone
2018199596
Address
2787 State St
City
Carlsbad
State
CA
Zip
92008
Comments
<p>The Height of 5 stories is an eyesore. If the height is reduced 2-3 stories this project would be more aesthetically pleasing.</p> <p>The many residents of Carlsbad whom I spoke to, were not happy when discussing this development. Personally, I think if it was smaller in scope it would be an 'easier pill to swallow' and you would have the support of the community.</p>

Andrew Cerrina

From: Carlsbad Village <donotreply@carlsbadvillagemixeduse.com>
Sent: Wednesday, July 12, 2023 11:09 AM
To: info@carlsbadvillagemixeduse.com
Subject: Carlsbad Village Web Inquiry

First Name
Rosie
Last Name
Marks
Email
roromarks@gmail.com
Phone
760-845-7479
Address
3166 Harding Street
City
Carlsbad
State
CA
Zip
92008
Comments
<p>What will happen to all of the small businesses that have been there 20+ years (El Puerto, French Pastry Cafe) and everyone who works at the grocery store while this project happens? Will they get to remain open and employed? Will everyone now have to walk to Von's on Tamarack for groceries? We already can see the top of the unattractive relatively NEW 4 story apartment building next to the freeway. This project means we get to look at two 5-story buildings A LOT closer to us. And what exactly is AFFORDABLE these days with every new housing project in the village selling for \$900k up???? (the Carlyle on Grand, the Fives on Madison, etc.). We really do feel like the we are getting squeezed out like the house in the Pixar movie UP!</p>

Andrew Cerrina

From: Carlsbad Village <donotreply@carlsbadvillagemixeduse.com>
Sent: Monday, July 17, 2023 2:58 PM
To: info@carlsbadvillagemixeduse.com
Subject: Carlsbad Village Web Inquiry

First Name

Rob

Last Name

Schupp

Email

rob.schupp2@gmail.com

Phone

760-583-0265

Address

2735 Wilson St.

City

Carlsbad

State

CA

Zip

92008

Comments

Thanks for posting information on your project. Some questions:

What is the current square footage of the shopping center?

What is the square footage of the proposed market and the square footage of the existing Smart & Final?

Will residences be for rent or for sale?

Please estimate rent/sales price ranges.

Looks like 7/11 stays, right?

I would suggest that while the project may reduce traffic trips to the shopping center, those same trips will be taken to other businesses much further away.

Jason Goff

From: Stacie <staciebeal@gmail.com>
Sent: Wednesday, May 22, 2024 12:16 PM
To: Jason Goff
Subject: Fwd: Notice of Public Hearing for SDP 2023-0014 - Carlsbad Village Mixed-Use
Attachments: SDP20230014.jg.Notice of Public Hearing.pdf

Dear Jason Goff,

I am writing to express my strong opposition to the proposed housing development of 218 multiple-family residential apartment units within two five-story buildings, and a five-story above-grade parking structure on a 4.12-acre property located at 945-1065 Carlsbad Village Drive. While I understand the need for affordable housing in our city, this project would have a detrimental impact on our community.

Primarily, the proposed development is simply too large for our area. The increase in population density would put a strain on our already overburdened infrastructure, leading to increased traffic congestion, noise pollution, and strain on our public services. Additionally, the construction of this project would result in significant environmental damage, destroying natural habitats and putting wildlife at risk.

Furthermore, the type of housing being proposed is simply not in keeping with the character of our neighborhood. It would also drastically alter the aesthetic of our area, replacing the existing skyline and open space with a monolithic, high-density housing complex.

Finally, I am deeply concerned about the impact this development would have on property values in the surrounding area. The influx of high-density housing residents could result in a decline in property values, making it difficult for current residents to sell their homes and move elsewhere.

In conclusion, I strongly urge you to reconsider this proposed housing development. While I recognize the need for affordable housing, this project is simply not the right fit for our neighborhood. Thank you for your attention to this matter.

Sincerely, Stacie Beal

Carlsbad Village Resident



----- Forwarded message -----

From: **Carla Flores** <Carla.Flores@carlsbadca.gov>
Date: Wed, May 22, 2024 at 11:51 AM
Subject: Notice of Public Hearing for SDP 2023-0014 - Carlsbad Village Mixed-Use



May 14, 2024

The Planning Commission
City of Carlsbad

Dear Planning Commissioners:

On behalf of the Carlsbad Chamber of Commerce Board of Directors, I am writing in support of the upcoming Carlsbad Village Mixed Use development in Carlsbad at the site where Smart & Final currently resides (Carlsbad Village Shopping Center). Tooley Interests has a long history of quality developments and they plan long-term ownership of this project. The proposed development would bring fresh retail space to an aged property and much needed housing.

The project being proposed is consistent with Carlsbad's General Plan. The two retail concepts it has will keep a market located on site, which is preferred due to the proximity of many local residents, and it will add a restaurant or coffee shop.

The great value here is in the 218 rental units it will add. This supply will include 27 affordable units for "Very Low" income levels, a product desperately needed in our community. The planned amenities here are exceptional. The project parks itself fully. And the sidewalk widening bring beautiful landscaping and increased walkability to our booming village.

For these reasons and more, we would like you to approve the Carlsbad Village Mixed Use application as proposed.

Sincerely,

A handwritten signature in black ink, appearing to read "Bret Schanzenbach", followed by a horizontal line.

Bret Schanzenbach
President and CEO

May 12, 2024

Jason Goff
City Project Manager

Dear Mr. Goff,

From the moment I read the yellow notice on the Miso Restaurant building I knew myself and neighbors were going to be shocked and upset to learn of the entire shopping center was going to be torn down with a new center being built.

This one is very personal to all of us. But this isn't a personal issue, its a business arrangement. The two don't mix do they...And we need the city's help to find a solution that keeps the valuable center in tact.

I understand that you have heard all the reasons this project of the Carlsbad Village Shopping Center rebuild should not be allowed but please hear me out.

If the city allows this to move forward the way it is right now, we will be putting our neighbor's stores and shops out of business. Did you know many of them are residents of Carlsbad? They have homes here, they pay taxes. Most importantly they are a vital part of our community. The center survived Covid, safely keeping their stores open to serve us and pay their own bills.

I suggest another environmental impact study to be done. I know that has been passed on as the center as it stands now has already had the study when it was built. But we need another impact study. How many more cars, producing emissions into the air, making CVD more difficult to drive because of too many cars on the road at once. Presently, travel in that area is very difficult at any time of the day.

One could say that this new center would provide a wonderful location for train travel to work, or pleasure. Maybe, in a perfect world....that parking lot is already full from morning to night. Are the new residents going to walk to the station? Maybe a few but the reality is that most would drive to the station, making the streets and parking lots fuller than they are currently. Some new residents won't use the train regularly at all. Once again, more cars clogging CVD.

The point of train travel is to help save the environment by omitting cars on the road. That's a good thing but what about the folks that use the Smart and Final Center? If it's torn down for the new project, folks are going to have to drive a lot further to find similar shopping needs. This is a big deal for those of us who live close.

This is not the right area for such a huge area of buildings/condos to cram into the space. Maybe good for the development company but not for a single citizen who uses this center currently.

Most Sincerely,

Heather Sager
Carlsbad Village Resident

Jason Goff

From: Paul Miller <millerpa06@gmail.com>
Sent: Monday, May 6, 2024 5:40 PM
To: Jason Goff
Subject: Village Plaza

Dear Mr. Goff,

I'm totally and completely opposed to the proposed redevelopment of the Carlsbad Village Plaza into a large, mixed-use condominium complex. (Project Number SDP 2023-0014)

I've been a resident in this fine city for over 26 years and my family has frequented the businesses in the Plaza weekly over the years. Both of my daughters were raised in Carlsbad and were properly educated at Jefferson and CHS. This proposed development is not in the best interest of Carlsbad and the residents of Carlsbad Village. It will destroy the only grocery store in the village, turning the village into a food desert. It will destroy the only pharmacy in the village, which is important to seniors and residents with limited mobility. The Denault's Ace Hardware has been a staple in the community, employing experienced seniors and creating a small town feel that you do not get at Lowe's or Home Depot. I cannot count how many birthday cakes we've ordered from the French bakery. I have to add the Golden Tee, a classic watering hole that many of us have spent evenings at. These stores will not return. The retail space will be reduced by 77% and the new space will be much more costly. These stores are an important resource for the village, does that not mean anything to the City?

What's more, this development is inconsistent with the vision documented in the Carlsbad Village and Barrio Master Plan. Specifically, this plan calls for the village to "serve as the historic heart of the city, honoring Carlsbad's past and creating a strong sense of community" in a way that "encourages preservation of each neighborhood's character" and "provides for the daily needs of nearby residents."

I understand that progress is inevitable, but this is the wrong project for this location. It will irreversibly alter the character of the Carlsbad Village, the character that I and so many residents wish to preserve. Do the right thing, and do not approve this project.

Please notify me when any public hearings are scheduled regarding this project. Thank you.

Sincerely,
Paul A. Miller
5133 El Arbol Drive
760-271-4457

CAUTION: Do not open attachments or click on links unless you recognize the sender and know the content is safe.

Jason Goff

From: Tracy Faris <tlfaris@yahoo.com>
Sent: Sunday, May 5, 2024 7:09 AM
To: Jason Goff
Cc: Planning; Council Internet Email; Manager Internet Email; Melanie Burkholder; Priya Bhat-Patel; Carolyn Luna; Teresa Acosta; Keith Blackburn
Subject: Project Number SDP 2023-0014 - Save Carlsbad Village Plaza

Dear Mr. Goff,

I am opposed to the proposed redevelopment of the Carlsbad Village Plaza into a large, mixed-use condominium complex. (Project Number SDP 2023-0014)

I have lived in Carlsbad for 20 years and love the quaint feel of the village. We cannot let it turn into the mess that Encinitas has. Its disgraceful and does not reflect the "Life's Rad in Carlsbad" vibe that we all know and love.

This proposed development is not in the best interest of Carlsbad and the residents of Carlsbad Village. It will destroy the only grocery store in the village, turning the village into a food desert. It will destroy the only pharmacy in the village, which is important to seniors and residents with limited mobility. The hardware store will go, as will the French bakery, the cleaners, the CRC resale store, the laundromat, and other local businesses - many of them family owned and operated. These stores will not return. The retail space will be reduced by 77% and the new space will be much more costly. These stores are an important resource for the village and must stay.

What's more, this development is inconsistent with the vision documented in the Carlsbad Village and Barrio Master Plan. Specifically, this plan calls for the village to "serve as the historic heart of the city, honoring Carlsbad's past and creating a strong sense of community" in a way that "encourages preservation of each neighborhood's character" and "provides for the daily needs of nearby residents."

Further, the proposed development will significantly increase traffic in the area, making mobility more difficult for residents, especially cyclists.

I understand that progress is inevitable, but this is the wrong project for this location. It will irreversibly alter the character of the Carlsbad Village, the character that I and so many residents wish to preserve. Do not approve this project.

Please notify me when any public hearings are scheduled regarding this project. Thank you.

Sincerely,

Tracy Faris

Carlsbad resident and homeowner

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Jason Goff

From: decroce@aol.com
Sent: Friday, May 3, 2024 6:48 PM
To: Planning; Eric Lardy; Scott Chadwick; Jason Goff; Melanie Burkholder; Keith Blackburn
Subject: CARLSBAD VILLAGE PLAZA

Ladies and Gentlemen:

I am strongly against the destruction of the Carlsbad Village Plaza. The Village does not need another mixed-use condominium complex. (Project Number SDP 2023-0014). I have lived in Carlsbad since 1986 and have raised three children here. I live at 3759 Greccourt Way which is considered part of the Village. I regularly play pickle ball at St. Mike's church with my husband and we regularly ride our bikes there. As much as I would like it to be, I know nothing stays the same forever. But our lovely, quaint community Village is being destroyed and dismantled street by street. Three story apartment and condo buildings have taken residence on too many of our streets..... Grand Avenue, State Street, Madison Avenue and the largest eyesore of them all, the mixed-use apartment complex on Carlsbad Village Drive directly across from the Carlsbad Village Plaza. The Village is tapped out.

This proposed development is not in the best interest of Carlsbad and the residents of Carlsbad Village. It will destroy the only grocery store in the village, turning the village into a food desert. It will destroy the only pharmacy in the village, which is important to seniors and residents with limited mobility. The hardware store will go, as will the French bakery, the cleaners, the CRC resale store, the laundromat, and other local businesses - many of them family owned and operated. These stores will not return. The retail space will be reduced by 77% and the new space will be much more costly. These stores are an important resource for the village and must stay.

What's more, this development is inconsistent with the vision documented in the Carlsbad Village and Barrio Master Plan. Specifically, this plan calls for the village to "serve as the historic heart of the city, honoring Carlsbad's past and creating a strong sense of community" in a way that "encourages preservation of each neighborhood's character" and "provides for the daily needs of nearby residents."

Further, the proposed development will **significantly increase** traffic in the area, making mobility more difficult and dangerous for residents, especially cyclists.

I understand that progress is inevitable, but this is the wrong project for this location. It will irreversibly alter the character of the Carlsbad Village, the character that I and so many residents wish to preserve. Please listen to the citizen's you represent and do not approve this project.

Please notify me when any public hearings are scheduled regarding this project. Thank you.

Sincerely,

Deborah Foley
3759 Greccourt Way
Carlsbad, CA 92008
decroce@aol.com
760-415-8024

Jason Goff

From: Maria <mnovida@yahoo.com>
Sent: Thursday, May 2, 2024 9:17 PM
To: Jason Goff
Subject: Project Number SDP 2023-0014)

Dear Mr. Goff,

I am opposed to the proposed redevelopment of the Carlsbad Village Plaza into a large, mixed-use condominium complex. (Project Number SDP 2023-0014)

My family and I moved to Carlsbad from Vista in 2009 for the reason of having a better school district and living in better Carlsbad neighborhoods. We bought our home in the Hidden Ridge neighborhood off of El Camino Real across Robertsons Ranch. We frequently go to the village to eat and shop. My college age daughter has worked in the village the past years for summer jobs and will return again this summer to work. We love the village and we think this proposed development is not in the best interest of Carlsbad and the residents of Carlsbad Village.

It will destroy the only grocery store in the village, turning the village into a food desert.

Just today, after buying a gift card from Blue Water Grill Restaurant, I stopped by Smart and Final to buy ingredients for dinner and how wonderful it is for that grocery store to be there. As it is, the closest grocery store from us is the Vons by Carlsbad Shoppes Mall. We need Smart and Final grocery store in this area.

Additionally, the proposed development will destroy the only pharmacy in the village, which is important to seniors and residents with limited mobility. The hardware store will go, as will the French bakery, the cleaners, the CRC resale store, the laundromat, and other local businesses - many of them family owned and operated. These stores will not return. The retail space will be reduced by 77% and the new space will be much more costly. These stores are an important resource for the village and must stay.

What's more, this development is inconsistent with the vision documented in the Carlsbad Village and Barrio Master Plan. Specifically, this plan calls for the village to "serve as the historic heart of the city, honoring Carlsbad's past and creating a strong sense of community" in a way that "encourages preservation of each neighborhood's character" and "provides for the daily needs of nearby residents."

Further, the proposed development will significantly increase traffic in the area, making mobility more difficult for residents, especially cyclists.

I understand that progress is inevitable, but this is the wrong project for this location. It will irreversibly alter the character of the Carlsbad Village, the character that I and so many residents wish to preserve. Do not approve this project.

Please notify me when any public hearings are scheduled regarding this project. Thank you.

Sincerely,

Maria Leupold
2357 Summerwind Place,
Carlsbad, Ca 92008
(760) 500-1220

Sent from my iPhone

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Jason Goff

From: Melissa Furioli <melissa.furioli@gmail.com>
Sent: Thursday, May 2, 2024 7:59 PM
To: Jason Goff
Cc: Carolyn Luna; Council Internet Email; Manager Internet Email; Planning
Subject: Save the Carlsbad Village Plaza

Dear Mr. Goff,

I am opposed to the proposed redevelopment of the Carlsbad Village Plaza into a large, mixed-use condominium complex. (Project Number SDP 2023-0014)

I was brought home from the hospital to my first home on La Portalada Dr., near the intersection of Tamarack and El Camino. My family purchased that home in 1975 and we have all been here since. Certainly we've seen many changes to the city in almost 50 years, but I strongly believe that this proposed development is not in the best interest of Carlsbad and the residents of Carlsbad Village. It will destroy the only grocery store in the village, turning the village into a food desert. It will destroy the only pharmacy in the village, which is important to seniors and residents with limited mobility. The hardware store will go, as will the French bakery, the cleaners, the CRC resale store, the laundromat, and other local businesses - many of them family owned and operated. These stores will not return. The retail space will be reduced by 77% and the new space will be much more costly. These stores are an important resource for the village and must stay.

What's more, this development is inconsistent with the vision documented in the Carlsbad Village and Barrio Master Plan. Specifically, this plan calls for the village to "serve as the historic heart of the city, honoring Carlsbad's past and creating a strong sense of community" in a way that "encourages preservation of each neighborhood's character" and "provides for the daily needs of nearby residents." Further, the proposed development will significantly increase traffic in the area, making mobility more difficult for residents, especially cyclists.

I understand and appreciate that progress is inevitable, but this is the wrong project for this location. It will irreversibly alter the character of the Carlsbad Village, the character that I and so many residents wish to preserve. I do not approve this project. Please notify me when any public hearings are scheduled regarding this project. Thank you so much for your time and attention in this matter!

Warmly,

Melissa Furioli
Homeowner in District 2
3704 Strata Drive
760-703-5973

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Jason Goff

From: Enchanted Seashells <enchantedseashells@gmail.com>
Sent: Thursday, May 2, 2024 5:37 PM
To: Jason Goff; Planning; Melanie Burkholder; Council Internet Email; Manager Internet Email
Subject: STOP THE DESTRUCTION OF CARLSBAD VILLAGE PLAZA

RE: Carlsbad Village Plaza

Enough is enough.

I am vehemently opposed to the proposed redevelopment of the Carlsbad Village Plaza into a large, mixed-use condominium complex. (Project Number SDP 2023-0014).

I've lived in Carlsbad since 1985.

This proposed development is not in the best interest of Carlsbad and the residents of Carlsbad Village.

It will destroy the only grocery store in the village, turning the village into a food desert. It will destroy the only pharmacy in the village, which is important to seniors and residents with limited mobility.

The hardware store will go, as will the French bakery, the cleaners, the CRC resale store, the laundromat, and other local businesses - many of them family owned and operated. These stores will not return. The retail space will be reduced by 77% and the new space will be much more costly. These stores are an important resource for the village and must stay.

What's more, this development is inconsistent with the vision documented in the Carlsbad Village and Barrio Master Plan. Specifically, this plan calls for the village to "serve as the historic heart of the city, honoring Carlsbad's past and creating a strong sense of community" in a way that "encourages preservation of each neighborhood's character" and "provides for the daily needs of nearby residents."

You can't deny that the proposed development will significantly increase traffic in the area, making mobility more difficult for residents, especially cyclists.

I understand that progress is inevitable, but as usual for Carlsbad's elected officials, this is the wrong project for this location. It will irreversibly alter the character of the Carlsbad Village, the character that I and so many residents wish to preserve.

Do not approve this project.

Please notify me when any public hearings are scheduled regarding this project.

\
Rosanne Bentley 619-200-7417

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Contact Us : Entry # 14

First Name

Katherine

Last Name

Sparrow

Email

kittysparrow.cbts@gmail.com

Phone

760

Address

2855 Carlsbad Boulevard

City

Carlsbad

State

CA

Zip

92008

Comments

I live at Carlsbad by the Sea Retirement community on Coast Hwy. Most of us are still very active and enjoy the walk down to the pharmacy, dry cleaner, hardware store etc. in the shopping center on Carlsbad Village Drive by the freeway. We do have cars, but it is SO nice to leave them at home to access these businesses and not drive several miles to shop at similar businesses. I'm sure that others who live in the Village feel the same way. If you are making a large purchase, it is nice to have the parking lot as well. Closing them for a year or so during construction would be VERY difficult - especially for the pharmacy! Please consider the needs of the businesses and residents when you take on this project.

Notes



Admin Notification (ID: 648a8a4dd8c1e)

added September 14, 2023 at 6:28 pm

WordPress successfully passed the notification email to the sending server.

Contact Us : Entry # 18**First Name**

Alejandro

Last Name

Ortega

EmailOrtega1989@att.net**Phone**

760-889-5606

Address

3908 Sierra Morena Ave

City

Carlsbad

State

CA

Zip

92010

Comments

I am highly against this so-called modernization of Carlsbad. It's completely ruining and destroying the true identity of what "The Village" is. I grew up here, I still remember when Pizza Port used to be the Hostess Factory. I went through Pine School Elementary before it was demolished. I played football on the old dirt field at Carlsbad High School. I remember things about Carlsbad that made it unique and drew tourists from all over.

Over the last 10 years, I have watched our city leaders and officials sell the identity of this city to the highest bidder which indicates to me they either aren't from here or are blinded by the almighty dollar to allow these ridiculous projects to happen.

First off, we are not Newport Beach, if you want to build 3 and 4 story buildings then take that to La Costa or build those inland. The three story "shoe boxes" that have been built in the village so far are ridiculous, their outside esthetics don't blend in with Carlsbad at all with our cozy look. The project for Village Plaza is destroying a longstanding icon that I remember when I was a kid. I remember going to Big Bear then when it became Albertsons where my father worked for over 10 years. Texas liquor has always been there since before I was born and you want to take local locations that have memories for thousands to build unaffordable housing. What is deemed "affordable" isn't affordable and will only make it more difficult, nearly impossible, for any families to move into Carlsbad to raise their kids.

The proposed project at the corner of Carlsbad Village and State St. is ridiculous. Crackheads and Choice Juicery are very popular establishments for locals and tourists alike. That is a unique establishment that constantly draws in crowds and offers a fun and relaxed atmosphere for both adults and kids to play. Yet you want to propose to demolish that and replace it with more store fronts (when you can't even fill numerous available store fronts around Carlsbad) and more unaffordable shoe box condominiums.

Contact Us : Entry # 18

Stop this nonsense, Carlsbad has already lost a significant portion of its unique identity and continuing with these ridiculous projects will certainly destroy the very essence that makes Carlsbad, Carlsbad.

Notes



Admin Notification (ID: 648a8a4dd8c1e)

added January 24, 2024 at 2:35 am

WordPress successfully passed the notification email to the sending server.

Contact Us : Entry # 19**First Name**

Susan

Last Name

Cleveland

Emailsusanjcleveland@gmail.com**Phone**

760-672-5989

Address

1390 Yourell Ave

City

Carlsbad

State

CA

Zip

92008

Comments

I am opposed to the proposed redevelopment of the Carlsbad Village Plaza into a large, mixed-use condominium complex. (Project Number SDP 2023-0014)

My husband, I and our son, who was going into the fourth grade, moved to Carlsbad in 1995 as we loved the community, schools and the quaint charm of the village and downtown Carlsbad.

This proposed development is not in the best interest of Carlsbad and the residents of Carlsbad Village. It will destroy the only grocery store in the village, turning the village into a food desert. It will destroy the only pharmacy in the village, which is important to seniors and residents with limited mobility. The hardware store will go, as will the French bakery, the cleaners, the CRC resale store, the laundromat, and other local businesses - many of them family owned and operated. These stores will not return. The retail space will be reduced by 77% and the new space will be much more costly. These stores are an important resource for the village and must stay.

What's more, this development is inconsistent with the vision documented in the Carlsbad Village and Barrio Master Plan. Specifically, this plan calls for the village to "serve as the historic heart of the city, honoring Carlsbad's past and creating a strong sense of community" in a way that "encourages preservation of each neighborhood's character" and "provides for the daily needs of nearby residents."

Further, the proposed development will significantly increase traffic in the area, making mobility more difficult for residents, especially cyclists.

Contact Us : Entry # 19

I understand that progress is inevitable, but this is the wrong project for this location. It will irreversibly alter the character of the Carlsbad Village, the character that I and so many residents wish to preserve. Do not approve this project.

Notes



Admin Notification (ID: 648a8a4dd8c1e)

added February 8, 2024 at 12:48 am

WordPress successfully passed the notification email to the sending server.

Contact Us : Entry # 22

First Name

Sandy

Last Name

Rogerson

Email

slrfunsun@yahoo.com

Phone

7608059060

Address

4481 Coastline

City

Carlsbad

State

CA

Zip

92008

Comments

Please do not develop the Carlsbad Village shopping center with the French Cafe and Ace Hardware. We love shopping there and it has been part of the village for many years.

Notes



Admin Notification (ID: 648a8a4dd8c1e)

added February 18, 2024 at 2:19 am

WordPress successfully passed the notification email to the sending server.

Jason Goff

From: JackCumming@aol.com
Sent: Wednesday, April 3, 2024 11:17 AM
To: Jason Goff
Subject: Carlsbad Village Plaza

The “Village Plaza” project will change the character of Carlsbad Village forever, and together with “The Lofts” it will form the gateway to the historic core of Carlsbad. The early sketches are commonplace and “boring” to borrow a phrase from an astute commentary on Tooley developments ([Click here](#)).

Of course, boring is nothing new in this era of rectilinear structures designed for compliance more than artful humanity. Why is it that European buildings seem more inviting, more human than the T-square and triangle buildings of America? It’s those little adornments... bay windows, French balconies, and drive-through arches that provide character and charm. A working premise is that “villages” are characterized by the prominence and convenience of attractive shops and eateries even though they may also be residential centers. Carlsbad Village is, perhaps, the only refuge in Carlsbad where people can live without an automobile. That’s precious.



American architects, with their most-revenue-space-for-the-least-investment purpose, often dismiss those minor amenities with the derisive German term, “Klebearchitektur,” which translates as “glued on architecture.” Still, in an era when many people, including architects, work from home, a desk built into a bay window can be an attractive, space-saving amenity even as it lends character to the façade.

That brings us to “Village Plaza” which is characterized by massive five story structures with two small shops in front facing Carlsbad Village Drive. It is defined by five boxes, carefully crafted to maximize the footprint with small, landscaped setbacks and shaft-like air spaces to bring fresh air and light into the residential units.

It is a transformation of a shopping area into a residential area. One might echo Gary Nessim who warned that the village would go to high priced residential if developers and planners were left to the status quo. Without an imaginative overall concept, each parcel stands on its own with only municipal codes as guidance. Planners are pretty much limited to statutory compliance. Human appeal is secondary.

Meanwhile, we're to go from a full-service grocery store, Smart & Final Extra, to a small 5,000 square foot convenience store (think booze and lottery sales) even as the nearby population needing groceries is slated to mushroom. That need not be. That should not be.

Consider the following. Leave the residential and parking structures as they are. Of course, they could be improved, much as self-storage facilities are often improved with Klebearchititektur elements. Facing Carlsbad Village Drive you might allow a two-story residential bonus over larger (Smart & Final size) ground level retail, with an arched through-the-building vehicle entrance for vehicles entering the property from Carlsbad Village Drive westbound.

Allow sufficient ground level retail space to accommodate Smart & Final, or another mainstream grocer, while trying to help Smart & Final to bridge to the new, build-to-suit grocery store. Add aesthetics to give a village feeling.

Let's make Carlsbad, and especially Carlsbad Village, all that it can be, instead of just another seaside beach community undifferentiated from California boring.

You can do better than this picture, and the picture is better than the Village Plaza renderings.

The biggest objection to this project will come from people who dislike the loss of grocery shopping, and the relegation of village denizens to the less safe and unkempt Von's shopping center on Tamerack. Keeping Smart & Final in place will put an end to those objections. Giving character to the planned buildings will gain support.

Jack Cumming
2855 Carlsbad Blvd.
Carlsbad, CA 92008

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Jason Goff

From: susan gutierrez <schneb6@gmail.com>
Sent: Sunday, February 18, 2024 11:26 AM
To: Jason Goff; Manager Internet Email; Melanie Burkholder
Subject: Carlsbad Village Plaza

February 17, 2023

Dear Mr. Goff,

We are opposed to the proposed redevelopment of the Carlsbad Village Plaza into a large, mixed-use condominium complex. (Project Number SDP 2023-0014)

In 1985 my husband and I moved to Carlsbad with our sons. We've raised our family here, participated in community activities such as Carlsbad Historical Society Board Membership, Historic Preservation Commission, Carlsbad Police Department JJP membership, and homework zone tutoring. In 2017 we received the Carlsbad Citizens of the Year recognition. Carlsbad has been our home for decades, and we've always strived to contribute to making it the best it can be for all residents.

Our youngest son bought a home in Carlsbad in 2014, and is raising his family here.

It does not appear that this proposed development is in the best interest of Carlsbad and the residents of Carlsbad Village. It will destroy the only grocery store in the Village, turning it into a food desert. This particularly concerns us as many residents are unable to drive to grocery stores due to lack of autos, or funds to purchase an auto, or due to age. Many are prohibited in using services such as Lyft or Uber for the same reason. An ongoing study has documented poor nutritional resources in cities that lack easy access to full service grocery stores.

As a city that supports and promotes shopping at locally owned downtown small business merchants, this project will destroy many of these same enterprises that are currently enjoying resident patronage. This location has the only pharmacy in the village. It may not seem important to those with transportation access or fully functioning mobility, but a small local pharmacy is important to seniors and residents with limited mobility or who want a relationship with their pharmacists, who has intimate knowledge of their medications, and looks out for their interests.

The hardware store will go, and the next nearest is located miles away. Carlsbad has been in the forefront promoting active participation in reducing our carbon foot print. This hardware store is a treasure to residents, and many of us use it frequently. It's easy and quick to reach. Shopping there reduces congestion on the already impacted El Camino Real. The same holds true for the French bakery. My family visit all three of these businesses at the very minimum once a week. These stores are an important resource for the village and must stay.

What's more, this development is inconsistent with the vision documented in the Carlsbad Village and Barrio Master Plan. Specifically, this plan calls for the Village to "serve as the historic heart of the city, honoring Carlsbad's past and creating a strong sense of community" in a way that "encourages preservation of each neighborhood's character" and "provides for the daily needs of nearby residents."

The proposed development will significantly increase traffic in the area, making mobility more difficult for residents, increasing the risk to cyclists and potentially creating an impact for Interstate 5. Years ago, the Carlsbad High Homecoming Parade-- Lancer Day-- had to reroute their parade route due to the significant back up on the freeway entrance and exits on Carlsbad Village Drive. That was a one day event, and I hate to think how this project will add to the congestion.

I understand that progress is inevitable, but the proposed scope of this project is wrong for this location. It will alter the character of the Carlsbad Village, and create daily accessibility hardships rather than offering any significant benefits for residents. Do not approve this project as proposed. Thank you.

Sincerely,

Susan and Germán Gutierrez

1864 Palisades Drive

Carlsbad CA 92008

760-822-8035

--

Sue Gutierrez
760-434-5308- home
760-822-8035-cell

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Jason Goff

From: len rogerson <slrfunsun@yahoo.com>
Sent: Saturday, February 17, 2024 8:20 PM
To: Jason Goff
Subject: Please save Carlsbad Village Plaza

This project is not in the best interest of Carlsbad residents and is not keeping with Carlsbad Village plans. It would take away the only large grocery store, hardware store, and laundromat. We love the French Bakery which has been there for years, We have lived here for 22 years and are very disappointed in all the 4 story projects and the taking away of the village aspect. Sandy and Len Rogerson Coastline Ave, Carlsbad.

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Jason Goff

From: Peter Ladouceur <prladouceur@att.net>
Sent: Thursday, February 22, 2024 2:23 PM
To: Jason Goff
Subject: Fwd: Project:Carlsbad Village Plaza

Sent from my iPhone

Begin forwarded message:

From: Peter Ladouceur <prladouceur@att.net>
Date: February 22, 2024 at 2:02:04 PM PST
To: jasangoff@carlsbadca.gov
Subject: Project:Carlsbad Village Plaza

Dear Mr. Goff, Carlsbad Manager and Members of the City Council,

We are opposed to the redevelopment of the Carlsbad Village Plaza into a large, mixed use condo complex. (Project Number SDP 2023-0014).

We support keeping the Village history and its character alive and well! This proposed development will just be one more four story plus monstrosity that destroys the Carlsbad Village in look and feel and will impact traffic and safety issues and ignore the needs of its citizens. Doing away with the only supermarket in the village and the only hardware and pharmacy will certainly be a hardship to the city's voters; seniors, those without transportation, tourists visiting the beach and hotels, and the homeless, and many of us that live near the Village and prefer not to clog the freeway and El Camino Real, which are already impacted with heavy traffic. The thought that the city would ignore its citizens needs and clutter our prized Village with yet more ugly architecture that stays vacant for months or years because of high rental prices that no one can afford is both economically untenable, but also the destruction of a village atmosphere that will turn its people to spend its time in other cities that respect the history and the needs of its citizens with the California beach community preservation.

There is still time to show your voters that you care by denying these developments that are unacceptable both visually, aesthetically, and are environmentally unacceptable and lack consideration as to the needs of its citizens. The impact on traffic, lack of parking, safety issues, etc. are all of great importance. We notice the city has invested in a huge fire engine that can now serve 4 story buildings in readiness to changing the village, and destroying its character! How about stop building these monsters and keep the village at its original height limits! If they want more housing, how about building East of the downtown. How come the city is not destroying the La Costa area with these tall boxes? Or is it because they would have fought "tooth and nail" against it. I wonder if the city would have forced the new height and forced this "look" on the La Costa area "Village". Or do their votes mean more to the City Council than ours?

We hope you and the Commission and the City Council respond in good faith to the citizens of Carlsbad and vote no to this project.

Sincerely,

Sue and Peter Ladouceur

3460 Charter Oak Drive
Carlsbad, CA 92008

(760) 729-6555
prladouceur@att.net
Sent from my iPhone

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Jason Goff

From: Susan Cleveland <susanjcleveland@gmail.com>
Sent: Wednesday, February 7, 2024 4:46 PM
To: Jason Goff
Cc: Manager Internet Email; Melanie Burkholder
Subject: Save the Carlsbad Village Plaza

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Mr. Goff,

I am opposed to the proposed redevelopment of the Carlsbad Village Plaza into a large, mixed-use condominium complex. (Project Number SDP 2023-0014)

My husband, I and our son, who was going into the fourth grade, moved to Carlsbad in 1995 as we loved the community, schools and the quaint charm of the village and downtown Carlsbad.

This proposed development is not in the best interest of Carlsbad and the residents of Carlsbad Village. It will destroy the only grocery store in the village, turning the village into a food desert. It will destroy the only pharmacy in the village, which is important to seniors and residents with limited mobility. The hardware store will go, as will the French bakery, the cleaners, the CRC resale store, the laundromat, and other local businesses - many of them family owned and operated. These stores will not return. The retail space will be reduced by 77% and the new space will be much more costly. These stores are an important resource for the village and must stay.

What's more, this development is inconsistent with the vision documented in the Carlsbad Village and Barrio Master Plan. Specifically, this plan calls for the village to "serve as the historic heart of the city, honoring Carlsbad's past and creating a strong sense of community" in a way that "encourages preservation of each neighborhood's character" and "provides for the daily needs of nearby residents."

Further, the proposed development will significantly increase traffic in the area, making mobility more difficult for residents, especially cyclists.

I understand that progress is inevitable, but this is the wrong project for this location. It will irreversibly alter the character of the Carlsbad Village, the character that I and so many residents wish to preserve. Do not approve this project.

Please notify me when any public hearings are scheduled regarding this project. Thank you.

Sincerely,
Susan Cleveland
1390 Yourell Ave.

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Jason Goff

From: Alejandro Ortega JR <ortega1989@att.net>
Sent: Wednesday, February 7, 2024 9:31 AM
To: Jason Goff
Cc: Melanie Burkholder
Subject: Save the Carlsbad Village Plaza

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Mr. Goff,

I want to take the time to hopefully provide valuable insight as to why I oppose the proposed redevelopment of the Carlsbad Village Plaza into a large mixed-use condominium complex (Project number SDP 2023-0014).

While I am not currently a resident of Carlsbad, but currently close to being able to purchase in the next year or two, I grew up in Carlsbad. I have more fond memories of what this city is/was and the identity that this city has. I went through the K-12 system and was one of the last classes to go through Pine Elementary School before it was demolished. I remember when Pizza Port used to be the Hostess Factory and when Smart & Final, the site where you plan to turn it into a mixed-use complex, was Big Bear and my father used to work there. I have memories of going there to see him as well as many other sites before they were demolished or repurposed. I'm a CHS alumni from '07 and once I do make my way back into Carlsbad as a resident, I'll be the third generation in my family to call Carlsbad home.

With that said, this proposed development is not in the best interest of the city nor its residents. Starting with the current retail establishments there, the grocery store has been a staple of the Village for decades. Looking around, there is no other grocery store located in the vicinity that serves the local downtown Carlsbad community. As mentioned previously, this grocery store has a history from Big Bear to Albertsons to Kroger to now Smart & Final. The pharmacy has been there for as long as I can remember and the French Bakery has been operating in that same spot since I was a kid. Texas Liquor has also been there since my parents were in high school. All these establishments, minus Smart & Final, are family owned. You're talking about removing a piece of Carlsbad's identity to bring in a project that will reduce the retail space to build condominiums that, quite frankly, are ridiculously priced.

Additionally, these new project developments do not align with Carlsbad's identity as "The Village." The identity of "The Village" is being destroyed with these new modern architecture designs that do not align with what Carlsbad is known for. Three story shoe-box condominiums are not Carlsbad. The metal and glass architecture is not Carlsbad. 3+ story buildings when The Village was known only for two-story max buildings. The Village is why tourists come from all over to visit. The Tudor style architecture that you see blended with 1950's American brick design IS Carlsbad. As someone who has lived their entire life in this city, it is a mixture of frustration and disappointment to see a city that I love dearly become another replica of Newport Beach when that is not our identity.

Lastly, adding more homes in a small space does not create solutions, just problems. Carlsbad already averages \$1 million dollars to buy a home, how do you expect to attract young families to move into the city when the price for a condo is out of reach for new families to come in? At this point, it just looks like greed to build more on less and charge insane prices for condos. Adding the hundreds of condos on the Carlsbad Village Plaza alone will create a surge in traffic and congestion in an area that is not meant for it.

Change is inevitable, I understand that, it's something that has and must happen. But I will end my stance in stating that nobody is against growth of the city, but stop destroying what the identity and image of the downtown area is and salvage what's left. Preserving the identity and history of downtown Carlsbad is what matters, not turning the downtown area into Newport Beach 2.0.

Please keep me informed of any upcoming public hearings for proposed projects.

Kind Regards,
Alex Ortega Jr
(760) 889-5606

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Jason Goff

From: David Chrisman <davidchrisman@googlemail.com>
Sent: Wednesday, February 7, 2024 10:02 AM
To: Jason Goff; Manager Internet Email; Melanie Burkholder
Subject: Opposition to 2023-0014 development

Follow Up Flag: Follow up
Flag Status: Completed

Dear Mr. Goff,

I am opposed to the proposed redevelopment of the Carlsbad Village Plaza into a large, mixed-use condominium complex. (Project Number SDP 2023-0014).

I have lived at 2440 Buena Vista Circle in Carlsbad for more than 8 years. In that time, I've lost count of the number of development projects that this city has approved. Typically, I don't involve myself in this type of construction because it is a natural part of the city's evolution. But the Plaza project is actually going to adversely affect the quality of life in the village as it removes retail establishments that are actually used by locals. As a part of our municipal government, I think you have a responsibility to put the financial interests of investors aside and represent and protect the people of Carlsbad. The last thing this town needs is another condominium.

Who benefits from another condominium? People who don't live here. Who suffers when you tear down a grocery store and a hardware store? The people who live here. Who benefits when you tear down a grocery store and a hardware store? Walmart and Home Depot.

Moreover, if this project is approved, how will you win a re-election against a candidate who runs on a promise to finally put an end to this type of development? Is there a less defensible position? Is there an issue in this city more important than the future of the city itself? In other words, why hold office in the first place if your objective is to basically sell the city to developers parcel by parcel? I really don't get it. People vote, people talk, people organize and at the moment, the people are not happy.

Please protect this city from overdevelopment and please notify me when any public hearings are scheduled regarding this development.

Thank you,

David Chrisman

760-696-2894

2440 Buena Vista Circle

Carlsbad, CA

92008

Jason Goff

From: Janell Cannon <blancofelis@att.net>
Sent: Thursday, February 8, 2024 5:25 PM
To: Jason Goff
Cc: Manager Internet Email; Melanie Burkholder; Eric Lardy
Subject: Carlsbad Village Plaza

Dear Mr. Goff—

I have been a long time Carlsbad Villager, and frequent the Carlsbad Village Plaza for grocery shopping, hardware purchases bakery goods, pharmacy, thrift store, occasional laundromat use, Mexican food, and the occasional visit to our old dive, The Golden Tee. Texas Liquor has a great array of craft beers, wine and spirits. This all-purpose center has been here before I arrived 48 years ago, and in those days I depended on the Plaza for groceries (Big Bear) before I had a car.

The place is plain, well worn, but has an important role in the Village and Barrio for being able to get essentials quickly and conveniently. No need to own a car, or for car owners to drive across town or to another city.

It also has a place in the hearts of the community members who go there regularly and often see one another and end up having a gab session. A social hub of sorts.

I and many other locals were horrified to hear that a large mixed-use condo complex is planned to replace the Plaza. We have seen many similar complexes installed in the village. Most are bleak in design, and lack adequate parking. They are insanely expensive, and in one of the more recently built mixed-use projects on Jefferson and Grand, the floor level shop space has been vacant for months. I am assuming that the rent in these new spaces is exorbitant, and they also lack the space many retail businesses need for storage.

Further east on Grand are 100s of new apartments being built, also with inadequate parking space. As the village becomes ever more crowded, is it not even more important that the neighborhood has essential stores and services within walking distance?

I have heard the City say innumerable times that they want the village to remain a small town, friendly walkable community where people can get what they need right around the corner.

This plan for the destruction of Carlsbad Village Plaza is absolutely contradictory to any of that rhetoric, and is just one more step to creating a place that is crowded, bleak, unaffordable to most, frustrating to find parking (NO—people are not ever giving up their cars any time soon!), and eventually it will become a place to avoid.

Please rethink putting high density housing in a retail center that is essential for the Village and Barrio.
Unless all of the existing stores and services provided by existing businesses there are replicated in affordable-to-lease-large-enough sites in the new development, you will effectively gut our village's basic shops and services.

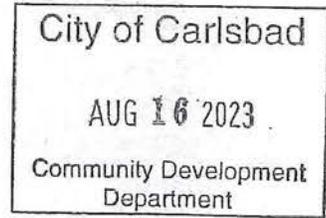
Respectfully,

Janell Cannon

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August 15, 2023

Carlsbad City Planning Commission
City Hall - Council Chamber
1200 Carlsbad Village Drive
Carlsbad, California 92008



RE: Carlsbad Village Plaza proposed development

Ladies & Gentlemen:

I am writing in regard to the proposed development for the property of Carlsbad Village Plaza just west of Interstate 5. I am vehemently opposed to this plan!! I am a lifetime resident of Carlsbad and in general have found our growth to be well done and planned.

Unfortunately in the last few years more and more of our quaint, "hometown style" downtown area has been taken over by overly large (particularly tall) new buildings. These have obscured the once welcoming area and made it appear more and more like a big city is overtaking my hometown. This proposal not only eliminates the one remaining small hardware store that everyone (including me) relies on for many items, but also the only remaining "grocery" store in the area. The idea of a five story building as you get off the freeway is repugnant and should not be allowed. Calling this "transit oriented" is just another way of saying the developer wants to use every loophole possible to cram more units into a small space.

I urge you to reject this plan and look at something more in line with our small town image that has been in place since the incorporation of Carlsbad.

Yours truly,

A handwritten signature in cursive script that reads "Rebecca A. Doty-Rogers".

Rebecca A. Doty-Rogers
2292 Bryant Drive
Carlsbad, CA 92008

Jason Goff

From: Jason Goff
Sent: Monday, July 17, 2023 10:17 AM
To: Jason Goff
Subject: FW: Smart & Final Shopping Center Redevelopment

From: Rob Schupp <rob.schupp@icloud.com>
Sent: Saturday, July 15, 2023 1:26:37 PM
To: Melanie Burkholder <Melanie.Burkholder@carlsbadca.gov>
Subject: Smart & Final Shopping Center Redevelopment

Hi. Congrats on your election. Can you send me any links to info on the planned Smart & Final shopping center redevelopment?

Thanks.

Rob Schupp
Carlsbad 92008
760-583-0265

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Jason Goff

From: Kellan Bellchamber <kellanbellchamber@gmail.com>
Sent: Friday, June 30, 2023 12:52 PM
To: Jason Goff
Subject: URGENT: Project SDP-2023-0014

Good afternoon,

My name is Kellan Bellchamber and I am a resident of the Carlsbad area. I have lived in North County my entire life. I have seen the plans to demolish the shopping center on Carlsbad Village drive that includes the Smart & Final and Texas Liquor. I want to express my concern and dissatisfaction with destroying these essential businesses in favor of building unaffordable apartments that will crowd Carlsbad even more. In the last decade I have seen Carlsbad morph from the humble village where culture and small businesses thrived into an unaffordable tourist trap meant to push locals out. Building these apartments will not improve Carlsbad, but make it even more crowded and expensive.

Not only that, but these businesses are needed. Residents of the village will have no option but to either drive at least 15 minutes away to a more affordable grocery store or drive to Vons, where prices are ridiculously high and not sustainable for middle class citizens. This is opposed to many village residents being able to walk to their local Smart & Final grocery, DeNault's hardware and Carlsbad Village pharmacy. The Golden Tee, a place where many Carlsbad residents like to spend their time and develop a sense of community, would disappear. The loss of a local bakery and a thrift shop to help the homeless and less fortunate would be surely noticed. These businesses provide a sense of community that building a suffocating high rise would destroy. I know I am not the only resident feeling this way and I strongly urge you to reconsider destroying Carlsbad even more. Thank you for your time, I hope we can all work together to make a better Carlsbad and North County that locals and transplants can all enjoy.

Most Sincerely,
Kellan Bellchamber
760-682-8119
kellanbellchamber@gmail.com

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Jason Goff

From: EJ SHIRING <ejshiring@aol.com>
Sent: Tuesday, June 27, 2023 4:51 PM
To: Jason Goff
Subject: Fw: project CVMU #SDP 2023-0014

Can you tell me if this project has received final approval yet and if not when it is schedule to go to the planning commission and any other information you may have? This seems like a very high density project for a location like this. I understand Sacramento's push for projects like these, but 218 units on that street will destroy the traffic in the Village not to mention any remaining semblance of "a village."

Thank you

EJ SHIRING

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Jason Goff

From: Jason Goff
Sent: Monday, June 26, 2023 11:55 AM
To: Jason Goff
Subject: RE: CARLSBAD VILLAGE MIXED-USE (SDP 2023-0014) - City Manager's Update: June 22, 2023

From: Ellie Bonner <elliebonner@yahoo.com>
Sent: Thursday, June 22, 2023 5:23 PM
To: Communications <Communications@CarlsbadCA.gov>
Subject: Re: City Manager's Update: June 22, 2023

Really appreciate receiving these updates. Thanks for the effort to produce them.

A question: I read something about the shopping center on Carlsbad Village Drive being demolished and replaced with housing. Is that true? Wouldn't it be counter-productive to build more housing and remove the only grocery store downtown? Thanks for any insight you can provide. Ellie Bonner

Sent from Yahoo Mail for iPad

Jason Goff

From: Jason Goff
Sent: Thursday, June 22, 2023 1:32 PM
To: Jason Goff
Subject: RE: CARLSBAD VILLAGE MIXED-USE (SDP 2023-0014) - Defacing Carlsbad

From: Randy L. Atkinson <randypadre3@hotmail.com>
Sent: Thursday, June 22, 2023 8:30:02 AM
To: Keith Blackburn <Keith.Blackburn@carlsbadca.gov>
Subject: Defacing Carlsbad

Dear Sir: What the hell is going on with you folks ? I just heard that you guys have approved a housing and affordable housing eyesore where Smart & Final, Ace/True Value, French Bakery , Cleaners , etc are located now . Is this true ? What a horrible idea — you don't mandate and take away (continually) the charm of a nice coastal city for a 3-4 story eyesore !!!!!!!!!!! You folks already put 3 of these in around the same area recently — no more . We have had it with this shit . Stop ruining the coast ... put this stuff in Vista and Escondido like areas . Oceanside has already been ruined — Carlsbad is next .

Randy & Kathy . My family has been in Carlsbad/Oceanside/Encinitas/Solana Beach for over 125 yrs !!!

Sent from my iPhone

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Jason Goff

From: Wanda <wmagana@sbcglobal.net>
Sent: Wednesday, June 21, 2023 4:15 PM
To: Eric Lardy
Subject: Please cancel building of 210 units

Please don't destroy our village.

Best Regards,
Wanda Magaña Adamson

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Jason Goff

From: Tida Hamilton <tidahamilton@gmail.com>
Sent: Wednesday, June 21, 2023 2:24 PM
To: Melanie Burkholder; Eric Lardy
Cc: Jason Goff
Subject: Project SDP-2023-0014

To whom it may concern:

I'm very concerned about the proposed development going in the Smart & Final shopping center on Carlsbad Village Dr.

This is the first thing people see when they exit the freeway. We already have one concrete monstrosity that was just built on the north side of Carlsbad Village Dr. People are going to be welcomed into our City being sandwiched between two large concrete buildings.

They are also going to be closing the Las Flores exit at some point. With all those housing units right off the freeway on both sides of Carlsbad Village Dr, plus an additional proposed 156 units at the Motel 6 location, it is going to be an absolute disaster. The intersection at Carlsbad Village Dr and Pio Pico can't handle our current situation, cars are constantly blocking the intersection trying to get to the freeway or heading west. How can we add more traffic to that?

I think the best use would be to revitalize the shopping center like the old shopping center on Encinitas Blvd and El Camino Real. They gave it an East Coast Hampton-style facelift. That would look so good there as it is the first thing everyone sees as they get off the freeway.

--

Tida Hamilton

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Jason Goff

From: mennenj@aol.com
Sent: Wednesday, June 21, 2023 2:01 PM
To: Jason Goff
Subject: Proposed housing development at Smart & Final Shopping Center

As a long time resident of Carlsbad, I would like to express my discontent at the proposed project for the shopping center at Carlsbad Village Drive and I-5. I strongly propose to keep the shopping center with the Smart and Final grocery store and the Denaults Hardware store.

Respectfully,

Julie Mennen, Ph.D.
Carlsbad Resident

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Jason Goff

From: Tom Brenner <thomashbrenner@gmail.com>
Sent: Tuesday, June 20, 2023 2:36 PM
To: Eric Lardy
Subject: Carlsbad Village Shopping Center

Follow Up Flag: Follow up
Flag Status: Flagged

Good Day Mr. Lardy, I am contacting you regarding the Carlsbad Village Shopping Center (Smart and Final Store - Denault's Hardware etc.) being rezoned for housing. Myself and many of my Carlsbad neighbors are opposed to this change. We feel there is already many new housing projects in the immediate area. Please let us know when there will be a Town Hall meeting regarding this matter, so we can attend and voice our concerns. Your help with this matter is greatly appreciated. Aloha Nui Loa, Tom Brenner 760-550-2054

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Jason Goff

From: Stacie <staciebeal@gmail.com>
Sent: Wednesday, June 21, 2023 12:36 PM
To: Eric Lardy
Subject: Proposed shopping center mixed use Carlsbad Village Drive

Dear Mr. Lardy,

I am contacting you regarding the Smart & Final Carlsbad Village Shopping Center being rezoned for mixed use and. Myself and many of my Carlsbad neighbors are opposed to this change. We feel there is already to many new housing projects in the immediate area. Please let us know when there will be a Town Hall meeting regarding this matter, so we can attend and voice our concerns. Your help with this matter is greatly appreciated. Thank you

Stacie Beal
916-730-0412

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Jason Goff

From: Jason Goff
Sent: Thursday, June 22, 2023 2:43 PM
To: Jason Goff
Subject: RE: CARLSBAD VILLAGE MIXED-USE (SDP 2023-0014) - Multi use on Carlsbad Village Dr

From: Sug Brunhaver <mssugb@gmail.com>
Sent: Tuesday, June 20, 2023 12:23:24 PM
To: Melanie Burkholder <Melanie.Burkholder@carlsbadca.gov>
Subject: Multi use on Carlsbad Village Dr

What is wrong with you and the City Council. I have lived here since 1987 and this is the worst thing you people have done.

Miriam Brunhaver

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Jason Goff

From: Gigi Berklite <ggberklite@gmail.com>
Sent: Tuesday, June 20, 2023 4:06 PM
To: Jason Goff
Subject: STOP CVMU - Carlsbad Village Mixed-Use SDP 2023-0014

Dear Mr. Goff,

I am a longtime resident of Carlsbad. My husband worked for the Carlsbad Fire Department for 32 years and we raised our family here. I am reaching out to you in regard to a new development application that is in process, for the shopping center where the Smart N Final is. I strongly oppose another MAMMOTH "Mixed -Use" residential building being developed, ruining our Village. Please **STOP CVMU !**

Thank you.

Best regards,

Gigi Berklite

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Jason Goff

From: Jason Goff
Sent: Thursday, June 22, 2023 2:41 PM
To: Jason Goff
Subject: RE: CARLSBAD VILLAGE MIXED-USE (SDP 2023-0014) - Keep smart and final.

From: Mary Fox <maryagnesfox@gmail.com>
Sent: Tuesday, June 20, 2023 10:42:02 AM
To: Melanie Burkholder <Melanie.Burkholder@carlsbadca.gov>
Subject: Keep smart and final.

40 year residents of Carlsbad here. Very unhappy about plans to remove smart and final shopping center. We use those shops daily. As citizens what can we do to protest or stop this project

Mary Fox

Scott Sharp.

Peter Sharp.

Hannah Sharp

3425 woodland way

Carlsbad 92008.

Sent from my iPhone

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Jason Goff

From: David & Beth Geesey <safari123089@gmail.com>
Sent: Monday, June 19, 2023 6:24 AM
To: Jason Goff
Cc: Melanie Burkholder
Subject: Carlsbad Village Permit - SDP 2023-0014

Jason-

Can you please provide details of the project application advertised on the old Miko Sushi building adjacent to the 7-Eleven? What is the timeline for final project approval? Is there an opportunity for public comment? Is there an overall plot map?

Thank you

--David Geesey

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From: [Sean Jean \(WEO\)](#)
To: [Jason Goff](#)
Cc: [Keith Blackburn; Council Internet Email](#)
Subject: Notice of Public Hearing for SDP 2023-0014 - Carlsbad Village Mixed-Use
Date: Thursday, May 23, 2024 9:21:20 AM

Dear Jason Goff,

I am a resident of North Carlsbad and I live within a half mile of the proposed housing development of 218 multiple-family residential apartment units within two five-story buildings, and a five-story above-grade parking structure on a 4.12-acre property located at 945-1065 Carlsbad Village Drive. I (along with my family and many friends) are strongly opposed to this development.

While I understand the need for affordable housing, a high-density mid-rise housing project is not appropriate for our beach community. This will add traffic, noise, parking issues and ultimately cause environmental damage. Has an EIR been approved for this project? If so, please direct me to a copy of the approved EIR so I can understand the environmental impact.

In addition to straining the Carlsbad Village limits of traffic, population, and environmental impact, this project will likely decrease property value for those living in proximity.

I, as well as my friends, are all voters in Carlsbad and we strongly encourage you to reconsider this mid-rise high-density housing project.

Thank you,

Sean Jean
Carlsbad Village Resident

CAUTION: Do not open attachments or click on links unless you recognize the sender and know the content is safe.

From: [Planning](#)
To: [Cynthia Vigeland](#)
Subject: FW: Reject SDP 2023-0014 Carlsbad Village Mixed Use Project
Date: Wednesday, May 29, 2024 10:52:22 AM

From: Martin Danner <martin@danner.org>
Sent: Wednesday, May 29, 2024 10:29 AM
To: Planning <planning@carlsbadca.gov>
Cc: Jason Goff <jason.goff@carlsbadca.gov>
Subject: Reject SDP 2023-0014 Carlsbad Village Mixed Use Project

Dear Carlsbad Planning Commission Members,

I am opposed to the proposed redevelopment of the Carlsbad Village Plaza into a large, mixed-use high-density residential complex. (Project Number SDP 2023-0014)

As a longtime resident of Carlsbad, living just a mile and a half from Carlsbad Village Plaza, I frequently use my bicycle to run errands due to our household's decision to maintain only one car. The proposed redevelopment would significantly increase my reliance on car travel, contravening the city's efforts to reduce car trips, or force me to cycle further on larger, busier, more dangerous streets.

This proposed development is not in the best interest of Carlsbad and the residents of Carlsbad Village. It will destroy the only grocery store in the village, forcing the residents into more expensive and distant options. It will destroy the only pharmacy in the village, which is important to seniors and residents with limited mobility. The hardware store will go, as will the French bakery, the cleaners, the CRC resale store (which benefits battered women), the laundromat, and other local businesses - many of them family owned and operated. These stores will not return. The retail space will be reduced by 77% and the new space will be much more costly. These stores are an important resource for the village and must stay.

Further, the traffic analysis submitted for this project is seriously flawed. The project is more than 1/2 mile away from the Carlsbad Village Station transit center. Also, the Vehicle Miles Driven (VMT) is miscalculated. This project will not decrease VMT by ~20,000. Rather, it will increase VMT by ~20,000. As a result, a full CEQA VMT analysis should have been conducted.

What's more, this development is inconsistent with the vision documented in the

Carlsbad Village and Barrio Master Plan. Specifically, this plan calls for the village to “serve as the historic heart of the city, honoring Carlsbad’s past and creating a strong sense of community” in a way that “encourages preservation of each neighborhood’s character” and “provides for the daily needs of nearby residents.”

I understand that progress is inevitable, but this is the wrong project for this location. It will irreversibly alter the character of the Carlsbad Village, the character that I and so many residents wish to preserve.

In light of these points, I urge you to act in the community’s best interest by rejecting the proposed development of SDP 2023-0014. Please consider the long-term effects such a project would have on the fabric of our community and the daily lives of its residents.

Sincerely,

Martin Danner
1826 Magnolia Ave
Carlsbad, CA 92008
760-492-5916

CAUTION: Do not open attachments or click on links unless you recognize the sender and know the content is safe.

CEQA DETERMINATION OF EXEMPTION

Subject: This California Environmental Quality Act (CEQA) Determination of Exemption is in compliance with Carlsbad Municipal Code Section 19.04.060. An appeal to this determination must be filed in writing with the required fee within ten (10) calendar days of the City Planner's decision consistent with Carlsbad Municipal Code Section 21.54.140.

Project Number and Title: SDP 2023-0014 (DEV2023-0078) – CARLSBAD VILLAGE MIXED-USE

Project Location - Specific: 945-1065 Carlsbad Village Drive,
Assessor Parcel Numbers (APNs) 203-320-53, 203-320-54, 203-320-55, and 203-320-56

Project Location - City: Carlsbad **Project Location - County:** San Diego

Description of Project: Site Development Plan (SDP 2023-0014) to allow for the demolition of an existing commercial shopping center with five (5) buildings consisting of approximately 58,735 square feet; consolidation of four (4) legal parcels totaling 4.12 acres (gross) into two (2) lots (Lot 1, 0.94 acres net/ Lot 2, 3.11 acres net); and development of a mixed use project consisting of approximately 13,800 square feet of neighborhood serving commercial uses and 218 multifamily residential dwelling units (for-rent)(53 dwelling units per acre).

The Project is intended to comply with provisions of the State Density Bonus Law (Government Code sections 65915-65918) (inclusive) which provides that a local government shall grant a density bonus, incentives/concessions, and/or waivers of development standards to a developer of a housing development constructing a specified percentage of affordable housing units and the Project will provide 15% of the base density as affordable to very low-income (VLI) households to comply with the provisions of the State Density Bonus Law. However, the Project applicant continues to work with the city to determine the exact amount of low-, very low-, or moderate-income housing units that will be included, while still satisfying and complying with the requirements of both the city's Inclusionary Housing Ordinance and the State of California Density Bonus Law.

The proposed commercial uses will be contained in two (2), one-story buildings with plazas and patios fronting Carlsbad Village Drive, and the residential buildings will be contained in two (2) five-story buildings separated by a landscaped paseo and courtyard fronting along Oak Avenue. The project is proposing to include approximately 40,870 square feet of open space, including courtyards, a 5th floor sky deck, private balconies, a swimming pool and pool deck courtyard, interior courtyards, and indoor amenity areas. While the project is not subject to minimum parking requirements pursuant to Assembly Bill 2097, the project is providing 340 vehicular parking spaces at-grade surrounding the commercial and residential buildings and within an above-grade five-level parking structure with rooftop parking.

The project site is located approximately 0.4 miles from Carlsbad Village center and approximately 0.6 miles from the Pacific Ocean. The site is surrounded by Carlsbad Village Drive to the north, Interstate 5 (I-5) Freeway to the east, Oak Avenue to south, and an unnamed public alley to the west. The project site is surrounded by a variety of urban uses, including an adjacent gas station, a four-story mixed-use building across Carlsbad Village Drive to the north, one-to-three-story residential (multiple-family and single-family) buildings to the south across Oak Avenue, and various one-story public and commercial uses to the west across the public alley.

Primary regional access to the site is provided via the I-5 freeway, which generally runs north/south and is adjacent to the site. The site has access to public transportation and is located within a transit priority area, served by the Coaster and Amtrak, as well as numerous bus lines within walking distance. NCTD Bus Route No. 315 provides service between Camp Pendleton, Sprinter, and the Carlsbad Village Coaster Station, with stops immediately adjacent to the project site. The closest major transit stop is the Carlsbad Village Coaster Station, located within 0.5 miles east of the site. Access into the site would be provided by Carlsbad Village Drive (an Arterial Connector Street) and Oak Avenue (a Neighborhood Connector Street).

Proposed earthwork quantities consist of approximately 10,000 cubic yards of cut, 1,200 cubic yards of fill, and 7,520 cubic yards of export. Anticipated remedial excavations will be approximately 2-3 feet deep and soils will be removed and replaced on site. Remedial quantities will consist of 12,880 cubic yards.

Name of Public Agency Approving Project: City of Carlsbad

Name of Person or Agency Carrying Out Project: City of Carlsbad

Name of Applicant: Tooley Interests, LLC (Attn: Andrew Cerrina)

Applicant's Address: 11661 San Vicente Boulevard, Suite 850, Los Angeles, CA 90049

Applicant's Telephone Number: (424) 291-6582

Name of Applicant/Identity of person undertaking the project (if different from the applicant above):
Jonathan Frankel, Atlantis Group, (925) 708-3638

Exempt Status: Categorical Exemption: Class 32, Section 15332 (In-fill Development Projects)

Reasons why project is exempt: The Project qualifies for a Class 32 categorical exemption under the California Environmental Quality Act (CEQA). Pursuant to CEQA Guidelines Section 15332, Class 32 categorical exemptions can be used for projects characterized as in-fill development meeting the following conditions: (1) general plan and zoning consistency; (2) project is within city limits on a site of no more than 5 acres and is substantially surrounded by urban uses; (3) project site has no value as habitat for endangered, rare, or threatened species; (4) project would not result in significant effects to traffic, noise, air quality, or water quality; and (5) the site can be adequately served by all required utilities and public services. Additionally, in order to qualify for a categorical exemption, a project cannot meet any of the "exceptions to exemptions" enumerated in CEQA Guidelines Section 15300.2.

Land Use Consistency: The Project is consistent with the applicable general plan designation and all applicable general plan policies as well as with the applicable zoning designation and regulations.

Land Use and Zoning Designations

The Project site is located on four parcels (Assessor's Parcel Numbers 203-320-53-00, 203-320-54-00, 203-320-55-00, and 203-320-56-00) and is designated as Village Barrio (V-B) in the City's General Plan and similarly has a zoning designation of Village-Barrio (V-B). The V-B zoning designation is implemented by the Village and Barrio Master Plan (Master Plan). The Project site is located in the Freeway Commercial (FC) District of the Master Plan.

City of Carlsbad General Plan

The City of Carlsbad General Plan sets forth goals and policies that guide both Citywide and community specific land use policies. The General Plan is comprised of a variety of State-mandated elements, including, but not limited to, Land Use and Community Design and Mobility.

Land Use and Community Design Element

The Land Use and Community Design Element establishes general policies and the vision for the future of the City. Land use, housing, urban form and neighborhood design, open space, economic development, transportation, and infrastructure and public services are all addressed in the context of accommodating future development. The Project is consistent with many of the Land Use and Community Design Element's goals and policies.

- Goal G.2. Promote a diversity of compatible land uses throughout the city, to enable people to live close to job locations, adequate and convenient commercial services, and public support systems such as transit, parks, schools, and utilities.
- Goal G.3. Promote infill development that makes efficient use of limited land supply, while ensuring compatibility and integration with existing uses. Ensure that infill properties develop with uses and development intensities supporting a cohesive development pattern.
- Goal G.6. Allow a range of mixed-use centers in strategic locations that maximize access to commercial services from transit and residential areas.
- Goal G.29. Maintain and enhance the Village as a center for residents and visitors with commercial, residential, dining, civic, cultural, and entertainment activities.
- Goal G.30. Develop a distinct identity for the Village by encouraging a variety of uses and activities, such as a mix of residential, commercial, office, restaurants and specialty retail shops, which traditionally locate in a pedestrian-oriented downtown area and attract visitors and residents from across the community by creating a lively, interesting social environment.
- Policy P.6. Encourage the provision of lower and moderate-income housing to meet the objectives of the Housing Element.
- Policy P.12. Encourage residential uses mixed in conjunction with commercial development on commercially designated sites and within the Village.
- Policy P.13. Encourage medium to higher density residential uses located in close proximity to commercial services, employment opportunities and major transportation corridors.
- Policy P.70. Seek an increased presence of both residents and activity in the Village with new development, particularly residential, including residential as part of mixed-use development, as well as commercial, entertainment and cultural uses that serve both residents and visitors.
- Policy P.77. Promote new investment by allowing opportunities for medium and high-density infill residential development, strategically located in the neighborhood consistent with the Land

Use Map. Ensure that development is designed to enhance neighborhood quality, character, and vitality, and is sensitive to historic and cultural resources.

The General Plan Land Use and Community Design Element identifies specific planning areas on the city's Land Use Map. The V-B (Village-Barrio) land use designation is a land use category defined in the General Plan that is applied in the village area where comprehensive planning is important to achieve a desired vision. The Village and Barrio Specific Plan (discussed further below) implements this designation and identifies the permitted and conditionally permitted land uses for the village area, as well as residential densities permitted. The residential designations provide for a range of housing types and densities. Densities are stated as number of dwelling units per acre of developable land. Residential development is required to be within the development range as identified in the Village and Barrio Master Plan Land Use Map. The maximum density permitted in the Freeway Commercial (FC) district is 35 dwelling units per acre, unless otherwise preempted by State law. The Project site is 4.12 acres. The maximum density allowed is 145 units (4.12-acres multiplied by 35 dwelling units per acre = 144.2 or 145 units). State law encourages cities to provide affordable housing through incentives to developers (i.e., State Density Bonus Law). State Density Bonus Law allows a developer to increase density on a property above the maximum density in exchange for the provision of affordable housing. In this instance the Project proposes 218 units. Density Bonus Law stipulates that a request for a density bonus does not constitute a valid basis on which to find a proposed housing development project is not compliant with a general plan. In addition, State Density Bonus Law explicitly requires the city to consider "the density allowed under the land use element of the general plan" in determining maximum allowable residential density. Therefore, the proposed density of the Project is deemed compliant with the land use density designation of the General Plan.

Noise Element Consistency

The Noise Element of the City's General Plan includes policies that are designed to ensure protection from noise for sensitive uses. The Noise Element provides acceptable limits of noise for various land uses for both exterior and interior environments from transportation sources. The noise levels associated with exterior roadway traffic were modeled according to the City's Noise Guidelines.

Exterior Use Areas

The Project would include outdoor usable space within multiple interior courtyards. Due to sound-blocking acoustical shielding afforded by the onsite positions and multi-floor structural arrangement of the proposed Project buildings, these exterior use areas will not be subject to noise exposure levels which exceeds the City's 65 dBA CNEL standard. Locations and orientations of the exterior spaces are compatible with better noise-sensitive site design recommendations appearing in Figures V-1 and V-2 of the City's Noise Guidelines Manual (City of Carlsbad 2013b).

Interior Spaces

Pursuant to Chapter 18.04.080 of the Carlsbad Municipal Code, any new residence or addition of one or more habitable rooms to an existing residence located within the noise impact boundary of an airport or freeway must be designed to ensure that internal noise levels due to airport or freeway operations do not exceed 45 dB. This standard may be satisfied by performing the acoustical analysis or by employing the prescribed construction methods described in this section. The Project would not be built without adequately demonstrating compliance with the California Building Code, as locally amended.

Modeling was also performed to determine interior noise levels from street-facing units of the Project. An assessment was conducted on street-facing units intended for residential occupation since the facade noise levels were modeled above 60 dBA CNEL. As a Project design feature, glass assemblies would require a closed window condition to reduce the interior noise levels below the City's 45 dBA CNEL threshold. Therefore, mechanical ventilation (e.g., air conditioning) will be installed in impacted street-facing units to move air within the structure and control temperature when windows are closed. Once the final architectural plans are available, a final interior noise assessment will be conducted to ensure interior noise reductions are met.

Airport Noise

There are no airports or private airstrips within the vicinity of the Project. The nearest airport is the Carlsbad McClellan-Palomar Airport which is approximately 4.0 miles southeast of the Site. Further, the Project is outside all CNEL noise contours from the McClellan-Palomar Airport Land Use Compatibility Plan (ALUCP) included in the General Plan.

Therefore, the Project is consistent with the applicable policies included in the Noise Element of the General Plan.

City of Carlsbad Housing Element Consistency

The Housing Element of the City's General Plan provides the City with long-term goals, policies and programs for promoting the production and conservation of safe, decent, and affordable housing within the community. The Project is consistent with many of the goals and policies of the Housing Element:

- Goal G.1. Housing Opportunities. New housing developed with diversity of types, prices, tenures, densities, and locations, and in sufficient quantity to meet the demand of anticipated city and regional growth and to meet or exceed the city's established Regional Housing Needs Allocation (RHNA).

- Policy P.4. Encourage increased integration of housing with nonresidential development where appropriate and where residential development can be implemented in a way that is compatible with existing and planned uses.

- Policy P.7. Encourage distribution of development of affordable housing throughout the city to avoid over concentration in a particular area, excluding areas lacking necessary infrastructure or services.

- Goal G.2. Housing Implementation. Sufficient new, affordable housing opportunities in all quadrants of the city to meet the needs of current lower- and moderate-income households and those with special needs, and a fair share proportion of future lower- and moderate-income households.

- Policy P.16. Address the unmet housing needs of the community through new development and housing that is set aside for lower- and moderate income households consistent with priorities set by the Housing Services Division, in collaboration with the Planning Division, and as set forth in the city's Consolidated Plan.

Policy P.17. Encourage the development of an adequate number of housing units suitably sized to meet the needs of lower- and moderate income larger households.

Program 1.8: Mixed Use. The city will encourage mixed-use developments that include a residential component that provides housing for lower- and moderate-income households...Major commercial centers should incorporate, where appropriate, mixed commercial/residential uses, with a focus on the production of lower-income units.

A measure of compliance with state housing element law is the ability of a jurisdiction to accommodate its share of the region's housing needs. The State Department of Housing and Community Development (HCD) projected a need for 171,685 new housing units in the San Diego region for an 8.8-year projection period between June 30, 2020 and April 15, 2029. The City of Carlsbad's share of the RHNA is 3,873 units and is allocated among the following income distribution:

Very Low-income: 1,311 units (34 percent)

Low-income: 784 units (20 percent)

Moderate Income: 749 units (19 percent)

Above Moderate Income: 1,029 units (27 percent)

The Project would develop a mixed-use multifamily and commercial development on an underutilized infill/housing site. The Project has a proposed density of approximately 53 dwelling units per acre (du/acre), and the Project applicant continues to work with the City to determine the percentage of low-, very low-, or moderate-income housing units that will be included in order to qualify for the incentives/concessions and waivers allowed under the State Density Bonus Law. 218 new multifamily residential units, including very low-income units, would be added to the City's housing stock.

As discussed above, the Project would be developed consistent with the Village-Barrio (V-B) General Plan land use designation in the City's General Plan, the Freeway Commercial (FC) designation in the Village and Barrio Master Plan and is consistent with the goals, policies, and programs of the City's Housing Element. Therefore, the proposed Project would meet this criterion.

Village and Barrio Master Plan

The Project site is in an area governed by the Carlsbad Village and Barrio Master Plan (Master Plan). Goals and policies have been established for the Master Plan to reinforce the vision. The Master Plan area encompasses much of the area west of Interstate 5 between Oak Avenue and Laguna Drive and extends to the Pacific Ocean along Garfield Avenue and parts of Ocean Street. The Master Plan goals and policies fall into four categories, Land Use and Community Character, Mobility and Parking, Connectivity, and Placemaking. The Master Plan carries out the policies of the General Plan by classifying and regulating the types and intensities of development and land uses within the Village and Barrio planning area, ensuring that they are consistent with the policies and objectives of the General Plan. The Project would be developed consistent with the development standards set forth in the City's Municipal Code which includes development standards and regulations for the Freeway Commercial District (FC) of the Master Plan, as described further below. Based on the provision of 15% VLI units, the Project is also eligible for a 50 percent market rate density bonus as well as incentives/concessions and waivers of development standards permitted by the State Density Bonus Law (Government Code section 65915 et. seq.).

Land Use and Community Character

- Goal A.1. Support a dynamic mix of uses and facilities, including a commercial center, mixed and standalone residential uses, and new, inviting public spaces.
- Goal A.2. Encourage mixed use development projects in the Village Center, with an emphasis on pedestrian-oriented retail uses on the ground floor, and office, other non-residential, and residential uses on the upper floors.
- Goal A.5. Locate residential uses within convenient walking and cycling distance of the Carlsbad Village Station.
- Goal B.1. Encourage a range of housing types, including medium density single-family, two-family and small scale multi-family development in the Barrio center, and higher density multi-family housing in the perimeter, consistent with the General Plan Land Use Map.

Mobility and Parking

- Goal A.1. Capitalize on the Village and Barrio's proximity to the Carlsbad Village Station by improving sidewalks and bicycle facilities on city streets leading to the transit center and by improving adjacent public alleys.

Placemaking

- A.4. Ensure that new development creates a continuous and interesting façade along the street with an emphasis on pedestrian-scaled features.
- B.1. Integrate plazas, courtyards, outdoor seating or dining areas, and other semi-public spaces into new development, where feasible.

The Project would develop a new mixed-use multifamily and commercial development with 218 multifamily residential units and 13,800 square feet of neighborhood serving commercial uses. Multifamily, commercial, and mixed uses are permitted in the FC District. The Project would provide residential uses that enables residents to walk to nearby public transit and to the amenities of the Village and Barrio area, and commercial uses that would provide ground floor pedestrian-orientated retail.

The maximum density permitted in the FC district is 35 dwelling units per acre. A project that meets the eligibility requirements of the State Density Bonus Law is entitled to a density bonus, incentives/concessions, development standard waivers, and reduced parking ratios (Government Code, section 65915 et. seq.)). The Project application indicates that 15% of the Project's base density, would be set aside for VLI housing to comply with State Density Bonus Law and the Project's provision of affordable units allows up to a 50 percent density bonus (Gov. Code § 65915(f)(2)), resulting in a proposed density of 53 du/ac (including density bonus units). Therefore, the proposed density of 53 units per acre would be consistent with the FC district standards of the Master Plan, the State Density Bonus Law and the City's Density Bonus Implementing Ordinance (Carlsbad Municipal Code Chapter 21.86).

Site Size and Location/Surrounding Land Uses: The proposed development occurs within City limits on a Project site of no more than 5 acres substantially surrounded by urban uses.

The Project site is located entirely within the City of Carlsbad, on a site that is 4.12 acres and is surrounded by established commercial and multi-family residential urban uses. The Project site is within the Village and Barrio neighborhood which lies between Carlsbad State Beach and the Interstate (I-5) freeway. The Project is in a transit priority area and is served by the North County Transit District (NCTD) bus service; the nearest bus stop to the Project site is located at the northwest corner of the Project site along Carlsbad Village Drive which serves route 315. Further, the Project is within 0.5 miles of the Carlsbad Village Station, a major transit stop as defined by Public Resources Code §21064.3.

Land uses surrounding the Project site are described as follows:

North

The Project site is bordered directly to the north by Carlsbad Village Drive and I-5. Further to the north is a 4-story mixed-use residential and commercial building, and a mix of commercial and multifamily development.

East

The Project site is bordered directly to the east by I-5. Further to the east, is a mixture of commercial and single- and multi-family development.

South

The Project site is bordered directly to the south by Oak Avenue; past this road are a variety of multifamily residential uses.

West

The Project site is bordered directly to the west by an unnamed public alley. Across the alley is The Harding Community Center and commercial development. Further to the west are a variety of locally serving commercial and restaurant uses.

As demonstrated, the Project site is substantially surrounded by urban uses and therefore meets the criteria for site size and location.

Habitat: The Project site has no value as habitat for endangered, rare, or threatened species.

The Project site and adjacent properties are highly developed and surrounding land uses include a mix of commercial, office, and residential uses. The site is almost completely covered with existing pavement or structures; it is developed with several commercial buildings and surface parking lot. Vegetation on the site is limited to a row of decorative trees scattered throughout the Project site that are not known to support any candidate, sensitive, or special-status species. No native habitat is located on the Project site or on adjacent properties. Based on the urbanized nature of the Project site and adjacent properties, in conjunction with a lack of suitable habitat for special-status species, the Project site has no value as habitat for endangered, rare, or threatened species and thus meets the Class 32 categorical exemption criteria for lack of habitat. Furthermore, the site is not within mapped areas of potential critical habitat as depicted in the City's General Plan. For these reasons, the Project has no value as habitat for endangered, rare, or threatened species.

Traffic, Noise, Air Quality, & Water Quality: Approval of the Project would not result in any significant effects relating to traffic, noise, air quality, or water quality.

Traffic

The City’s VMT Analysis Guidelines (City of Carlsbad 2022) provides a VMT analysis screening criteria for development projects, which identifies the level of VMT analysis methodology, level of significance, and mitigation based on the development’s trip generation, the type of development, the location of the development within a transit priority area, and other elements. A Vehicle Miles Traveled (VMT) Screening Memorandum (Attachment A) was prepared by Linscott, Law, and Greenspan Engineers in compliance with California Senate Bill 743 and the City’s guidelines. Per the City of Carlsbad VMT Analysis Guidelines, residential, retail, or office projects or projects that have a mix of those uses whose project site boundaries are within one half mile of an existing or planned major transit stop or a stop/transit center along a high-quality transit corridor would be presumed to have a less than significant transportation impact. In the City of Carlsbad and per City VMT Analysis Guidelines, this would only apply to projects within one half mile of the Carlsbad Village Station or Carlsbad Poinsettia Coaster Station, as well as projects within one-half mile of the Plaza Camino Real transit center. The proposed project is within one-half mile of Carlsbad Village Station. Further, the Project would result in a net decrease of 4,802 average daily car trips as compared to the existing commercial uses. Therefore, the Project is presumed to have a less than significant traffic impact provided there is no other evidence suggesting a VMT impact.

Since the Project includes the removal of existing local serving uses that may not have substitutes available in the Carlsbad Village area at the time of this analysis, specifically the grocery store and hardware store uses, additional quantitative analysis was performed to further demonstrate that the Project will not result in a net increase in VMT. To calculate the net change in total VMT, the total VMT to be generated by the proposed Project land uses was deducted from the total VMT generated by the existing shopping center. In addition, and in the interest of producing the most conservative analysis, because the existing shopping center provides certain neighborhood-serving retail uses which will be removed that may not have substitutes available in the Carlsbad Village area, specifically the grocery and hardware store uses, a portion of these trips are considered “displaced” to substitute locations and additional VMT associated with these displaced trips were also considered and added as a part of the Project VMT. As shown in Table 1, the existing shopping center is estimated to generate 45,786 daily VMT. The proposed Project’s VMT reducing land uses, which include infill housing, affordable housing and new neighborhood serving commercial uses, plus the displaced trip VMT calculated totals 25,397 VMT. This is a reduction of 20,389 VMT compared to the existing site. Per City of Carlsbad VMT Analysis Guidelines, redevelopment projects that generate less VMT than the existing project they are replacing would be presumed to have a less than significant impact on VMT. Given that the proposed Project is close to transit, reduces total VMT, and is providing less than 50,000 square feet of neighborhood serving commercial uses, no significant VMT impact is triggered.

Table 1. Redevelopment VMT Quantitative Analysis

Type	Land Use	Quantity	Daily Volumes (ADT)		ATL ^a	Total VMT
			Rate	Volume		
Project	Residential	218 DU	6/DU	1,308	10.5	13,734
	Market	5.8 KSF	150/KSF	870	6.5	5,655
	Quality Restaurant	6 KSF	100/KSF	600	6.5	3,900

	Retail / Strip Commercial	2 KSF	40/KSF	80	6.5	520
	<i>Subtotal Project</i>	—	—	2,858	—	23,809
	Internal Capture ^b	—	—	616	—	5,132
	Project External Trips	—	—	2,242	—	18,677
Displaced	Hardware Store (displaced)	9 KSF	60 /KSF	540	4.0	2,160
	Grocery Store (displaced)	19 KSF	120 /KSF	2,280	2.0	4,560
	Total Project + Displaced	—	—	—	—	25,397
Existing (to be removed)	Shopping Center	58.7 KSF	120/KSF	7,044	6.5	45,786
Net Change in VMT						-20,389

Notes:

^a Average trip lengths from StreetLight Data platform. See Attachment B.

^b Internal capture estimated using NCHRP 8-51 Internal Trip Capture Estimation Tool. VMT reduction calculated by multiplying captured ADT by weighted average trip length of unadjusted Project trips.

In conclusion, the proposed Project meets the City of Carlsbad Screening Criteria and is therefore calculated to have a less than significant transportation VMT impact per the City's guidelines.

Noise

A noise impact analysis (report) has been prepared by Dudek (Attachment B). The report assesses potential noise impacts that could occur under the Project. The report included the following components: documentation of existing noise conditions, discussion of noise modeling methodology and procedure, and analysis of construction noise, traffic noise, and onsite stationary noise sources (e.g., outdoor-exposed rooftop HVAC systems and low-speed parking garage traffic).

Through noise modeling and analysis, the report concluded that noise impacts related to Project construction would be less than significant and that mitigation would not be required.

Although neither the City's Municipal Code or its Noise Guidelines offer a quantitative decibel limit for construction noise, within allowable construction hours per the City of Carlsbad General Plan Noise Element (City of Carlsbad 2015), a relative threshold is utilized herein to evaluate a potential construction noise impact at an NSR (i.e., residential land use) of up to a 10 dB increase over existing ambient sound level, akin to CEC assessment of potentially significant impact for long-duration changes to the outdoor sound environment. Because the Project Site is already characterized by existing outdoor ambient levels ranging from 67 to 68 dBA Leq, this means that construction noise would be considered a significant impact if greater than the position-dependent existing sound level by more than 10 dB. In other words, based on these measured outdoor ambient Leq samples, the allowable construction noise levels would be 77 to 78 dBA hourly Leq values. For context, these construction noise magnitudes are slightly less than the 80 dBA noise threshold that FTA guidance recommends at the exterior of a receiving residence.

The report states that construction could temporarily increase noise levels at nearby offsite residential land uses, but such increases would be less than 10 dB.

Further, in accordance with City of Carlsbad Municipal Code Section 8.48.010, the City restricts the times of day when construction may occur. Construction is anticipated to occur over a single phase lasting between 20

and 24 months and would result in temporary increases in noise levels in the Project area on an intermittent basis. Construction of the Project would take place within the hours specified in the City’s Municipal Code.

Table 2. Predicted Construction Noise at Indicated Receptors per Activity Phase

Construction Phase (and Equipment Types Involved)	Predicted Construction Noise (dBA, hourly L_{eq}) at the NSR	
	ST1	ST3
<i>Construction Noise Threshold*</i>	77.1	77.6
Demolition (concrete saw/industrial saw, rubber-tired dozer, excavator)	71.2	71.7
Site preparation (dozer, tractor, loader, backhoe)	69.1	69.5
Grading (excavator, grader, rubber-tired dozer, tractor, loader, backhoe)	70.4	70.9
Building construction (crane, forklift, generator set, tractor, loader, backhoe, welder)	67.7	68.2
Paving (cement and mortar mixer, paver, paving equipment, roller, tractor, loader, backhoe)	71.8	72.2
Architectural coating (air compressor)	58.5	58.9
<i>Do any predicted phase noise levels exceed the threshold?</i>	<i>no</i>	<i>no</i>

Notes: L_{eq} = equivalent noise level; dBA = A-weighted decibels; NSR = noise-sensitive receptor.

* 10 dB greater than the measured sample of outdoor ambient noise level at the NSR.

As presented in Table 2, the estimated construction noise levels are predicted to be as high as 72 dBA L_{eq} hourly L_{eq} at the nearest NSR during site demolition and paving phases. Although these nearby occupied properties to the north and south of the Project would be exposed to elevated construction noise levels, the increased noise levels would typically be relatively short term and, as shown in Table 2, represent a temporary and less than 10 dB increase of the outdoor ambient sound level. Thus, construction-related noise impacts would be considered less than significant.

The report also modeled noise impacts from changes to nearby roadway traffic from the Project on off-site receptors. The modeling concluded that changes in noise would be a nominal amount (less than 1 dBA reduction) and would thus be considered less than significant.

The report further explains that onsite operational noise sources associated with the Project would include those from the planned above-ground five-level parking garage (i.e., vehicles arriving and leaving, occasional vehicle horns and alarms), background music playback and adults conversing at common areas (e.g., pool and fifth-floor exposed decks), and outdoor-exposed rooftop HVAC equipment. Onsite Project noise events would also include occasional delivery trucks, trash pick-up trucks, and landscape equipment; however, these are intermittent short-duration sound sources that are already part of the pre-Project existing environment. The report concludes that the aggregate operational noise does not exceed the City of Carlsbad daytime or nighttime exterior noise level standards for non-transportation noise sources.

For these reasons, the report concludes that noise impacts related to Project construction, traffic, and operation would be less than significant.

Air Quality

An air quality technical memorandum has been prepared by Dudek (Attachment C). The construction analysis includes demolition of the existing commercial buildings, removal of existing pavement, and construction of the proposed new commercial buildings, and the operational analysis includes operational activities associated with the residential development.

Construction and operational emissions in the technical memorandum were analyzed for both regional and local air quality impacts through the use of the California Emissions Estimator Model (CalEEMod) computer modeling software. The Project's potential impacts to air quality and GHG were quantified using CalEEMod and compared to the CEQA thresholds of significance. Based upon this modeling and the estimated maximum daily construction emissions associated with construction of the Project were less than significant for all criteria air pollutant emissions as shown in Table 3.

Table 3. Estimated Maximum Daily Construction Criteria Air Pollutant Emissions

Year	VOC	NO _x	CO	SO _x	PM ₁₀	PM _{2.5}
	Pounds per Day					
2024	1.42	24.82	44.14	0.10	6.14	3.12
2025	1.13	14.2	26.14	0.05	1.65	0.50
2026	18.75	25.94	45.10	0.08	2.33	0.79
<i>Maximum</i>	<i>18.75</i>	<i>25.94</i>	<i>45.10</i>	<i>0.10</i>	<i>6.14</i>	<i>3.12</i>
<i>SDAPCD threshold</i>	<i>137</i>	<i>250</i>	<i>550</i>	<i>250</i>	<i>100</i>	<i>55</i>
Threshold exceeded?	No	No	No	No	No	No

Notes: VOC = volatile organic compound; NO_x = oxides of nitrogen; CO = carbon monoxide; SO_x = sulfur oxides; PM₁₀ = coarse particulate matter; PM_{2.5} = fine particulate matter; SDAPCD = San Diego Air Pollution Control District.

See Appendix A for complete results.

The values shown are the maximum summer or winter daily emissions results from CalEEMod.

As shown in Table 3, daily construction emissions for the Project would not exceed SDAPCD's significance thresholds for VOCs, NO_x, CO, SO_x, PM₁₀, or PM_{2.5}. Therefore, the Project would result in a less than significant impact.

Further, for operational emissions, the net maximum daily operational emissions would not exceed thresholds for any criteria air pollutant emissions as shown in Table 4 and 5.

Table 4. Estimated Maximum Daily Operational Criteria Air Pollutant Emissions – Existing Land Uses

Source	VOC	NO _x	CO	SO _x	PM ₁₀	PM _{2.5}
	Pounds per Day					
Area	1.39	<0.01	0.02	0	<0.01	<0.01
Energy	<0.01	0.04	0.03	<0.01	<0.01	<0.01
Mobile	9.08	8.25	71.86	0.14	15.66	4.24
Total	10.47	8.29	71.91	0.14	15.66	4.24

Notes: VOC = volatile organic compound; NO_x = oxides of nitrogen; CO = carbon monoxide; SO_x = sulfur oxides; PM₁₀ = coarse particulate matter; PM_{2.5} = fine particulate matter; SDAPCD = San Diego Air Pollution Control District. <0.01 = reported value is less than 0.01.

See Appendix A for complete results.

The values shown are the maximum summer or winter daily emissions results from CalEEMod.

Table 5. Estimated Maximum Daily Operational Criteria Air Pollutant Emissions – Net Project Emissions Compared to SDAPCD Thresholds

Source	VOC	NO _x	CO	SO _x	PM ₁₀	PM _{2.5}
	Pounds per Day					
Area	7.02	0.21	18.00	<0.01	0.10	0.10
Energy	0.08	0.74	0.46	<0.01	0.06	0.06
Mobile	7.57	6.94	60.47	0.12	13.29	3.60
Total	14.67	7.89	78.93	0.12	13.45	3.76
<i>Net (Project – Existing)</i>	<i>4.20</i>	<i>-0.40</i>	<i>7.02</i>	<i>-0.02</i>	<i>-2.21</i>	<i>-0.48</i>
<i>SDAPCD threshold</i>	<i>137</i>	<i>250</i>	<i>550</i>	<i>250</i>	<i>100</i>	<i>55</i>
Threshold exceeded?	No	No	No	No	No	No

Notes: VOC = volatile organic compound; NO_x = oxides of nitrogen; CO = carbon monoxide; SO_x = sulfur oxides; PM₁₀ = coarse particulate matter; PM_{2.5} = fine particulate matter; SDAPCD = San Diego Air Pollution Control District. <0.01 = reported value is less than 0.01.

See Appendix A for complete results.

The values shown are the maximum summer or winter daily emissions results from CalEEMod.

As shown in Table 5, the net maximum daily operational emissions would not exceed SDAPCD's thresholds for VOCs, CO, NO_x, SO_x, PM₁₀, or PM_{2.5} during the operation of the Project. Therefore, the Project would result in a less than significant impact.

Tables 6 and 7 shows the annual operational emissions estimated for the existing land uses and the Project, respectively.

Table 6. Estimated Annual Operational Criteria Air Pollutant Emissions– Existing Land Uses

Source	VOC	NO _x	CO	SO _x	PM ₁₀	PM _{2.5}
	Tons per Year					
Area	0.25	<0.01	<0.01	0	<0.01	<0.01
Energy	<0.01	0.01	0.01	<0.01	<0.01	<0.01
Mobile	1.24	1.17	10.10	0.02	2.21	0.60
Total	1.49	1.18	10.11	0.02	2.21	0.60

Notes: VOC = volatile organic compound; NO_x = oxides of nitrogen; CO = carbon monoxide; SO_x = sulfur oxides; PM₁₀ = coarse particulate matter; PM_{2.5} = fine particulate matter; SDAPCD = San Diego Air Pollution Control District. <0.01 = reported value is less than 0.01.

See Appendix A for complete results.

Table 7. Estimated Annual Operational Criteria Air Pollutant Emissions– Net Project Emissions Compared to SDAPCD Thresholds

	VOC	NO _x	CO	SO _x	PM ₁₀	PM _{2.5}
Source	Tons per Year					
Area	1.23	0.02	1.62	<0.01	0.01	0.01
Energy	0.02	0.14	0.08	<0.01	0.01	0.01
Mobile	1.122	1.09	9.34	0.02	2.08	0.57
Total	2.37	1.25	11.04	0.02	2.10	0.59
<i>Net (Project – Existing)</i>	<i>0.88</i>	<i>0.07</i>	<i>0.93</i>	<i>0</i>	<i>-0.11</i>	<i>-0.01</i>
<i>SDAPCD threshold</i>	<i>13.7</i>	<i>40</i>	<i>100</i>	<i>40</i>	<i>15</i>	<i>10</i>
Threshold exceeded?	No	No	No	No	No	No

Notes: VOC = volatile organic compound; NO_x = oxides of nitrogen; CO = carbon monoxide; SO_x = sulfur oxides; PM₁₀ = coarse particulate matter; PM_{2.5} = fine particulate matter; SDAPCD = San Diego Air Pollution Control District. <0.01 = reported value is less than 0.01.

See Appendix A for complete results.

As shown in Table 7, the annual operations emissions for the Project do not exceed SDAPCD’s significance thresholds for VOCs, CO, NO_x, SO_x, PM₁₀, or PM_{2.5}. Therefore, the Project would result in a less than significant impact.

Further in analyzing cumulative impacts, the Project would not result in a cumulatively considerable contribution to regional ozone concentrations or other criteria pollutant emissions. Cumulative impacts would be less than significant for the Project.

Water Quality

The Project is not anticipated to have a substantial adverse effect on water quality. CEQA threshold questions pertaining to water quality (from Appendix G of the CEQA Guidelines) are addressed below.

Would the Project violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?

Construction

The Project site is located within a developed urbanized and commercial area and does not contain any streams, rivers, or waterbodies. Construction activities associated with the Project are subject to implementation of stormwater BMPs. To avoid adverse impacts on water quality, the applicant and their construction contractors would be required to conduct construction activities in accordance with the statewide Construction General Permit (Order No. 2022-0057-DWQ/CAS000002, as amended). This would include compliance with the Phase I Regional Municipal Separate Storm Sewer System (MS4) Permit (NPDES Permit No. CAS0109266), which requires regulation of surface water quality.

In addition, pursuant to the City’s Municipal Code Chapter 15.16, grading activities would be required to be performed in compliance with the NPDES requirements and must implement BMPs prior to commencement of grading activities. Compliance with all applicable federal, state, and local requirements concerning the handling, storage, and disposal of hazardous waste would reduce the potential for the release of contaminants

into the groundwater and would not cause a violation of regulatory water quality standards related to surface water or groundwater. Further, implementation of BMPs to minimize erosion and sedimentation would ensure that Project construction would not substantially degrade surface or groundwater quality.

Operation

Project operations would not introduce any significant industrial discharges, and therefore, would not violate any water quality standards or waste discharge requirements related to non-stormwater discharges. Under current conditions, the Project site produces nonpoint source pollutants associated with stormwater runoff. The existing Project site is developed with several commercial structures that would be demolished prior to Project construction. The site is primarily impervious, and the existing drainage is a sheet flow from the parking lot (impervious area). Under current conditions, approximately 93.6% of the Project site is paved and impervious. The primary stormwater pollutants that may occur under existing conditions are spilled or leaked petroleum products from parked vehicles on the site, household hazardous materials used for maintenance and cleaning at the existing and proposed commercial buildings, and sediments from landscaping planters.

During redevelopment of the Project site, modern stormwater runoff design requirements and operational practices would be required pursuant to City regulatory requirements. Compliance with such requirements may reduce the volume of stormwater runoff from the site and would likely improve the quality of such runoff. A Stormwater Quality Management Plan (SWQMP) shall accompany all development permit applications. Therefore, the Project would capture and convey stormwater consistent with applicable regulations and would not substantially degrade surface or groundwater quality.

Upon Project implementation, the site would be covered with two five-story residential structures, two single-story commercial structures, a five-story parking structure, and landscaped areas. Stormwater collected onsite would be directed to onsite stormwater treatment BMPs in accordance with the City's BMP Design Manual and would comply with the City's stormwater regulations.

Would the Project substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would result in substantial erosion or siltation on- or off-site or create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

Onsite drainage patterns are expected to remain substantially the same following Project construction. The existing development of paved surface will be removed and replaced with new pavement surface.

The Project site does not contain any streams, rivers, or waterbodies. Upon compliance with the regulatory requirements described above, the proposed Project is not anticipated to result in substantial erosion or siltation, to increase the rate or amount of surface runoff from the site or create runoff that would exceed the capacity of the stormwater drainage system. Due to the developed nature of the Project site and required compliance with existing regulations, any alterations to the existing drainage pattern on the Project site would not result in significant, adverse impacts.

Would the Project conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

In 2014, California enacted the Sustainable Groundwater Management Act (SGMA) to bring the state's groundwater basins into a more sustainable regime of pumping and recharge. The legislation provides for the sustainable management of groundwater through the formation of local groundwater sustainability agencies and the development and implementation of Groundwater Sustainability Plans (GSPs). In San Diego County, the State has designated three of the County's groundwater basins as medium- or high-priority and subject to SGMA, including the Borrego Valley (Borrego Springs Subbasin), San Luis Rey Valley (Upper San Luis Rey Valley Subbasin), and San Pasqual Valley (San Diego County Planning and Development Services 2021), none of which underlie the project site.

As noted above, the Project is not expected to violate any water quality standards, and measures would be taken both during construction and throughout operation to prevent potential contaminants from leaving the site by runoff. Through compliance with Regional Water Quality Control Board requirements and implementation of a SWPPP (construction phase), the Project would not conflict with or obstruct implementation of the San Diego Sustainable Groundwater Management Act. Thus, the proposed Project would not result in substantial conflict nor obstruction of the implementation of a water quality control plan or sustainable groundwater management plan. Additionally, the Project site is primarily impervious under existing conditions and is not considered a significant groundwater recharge area. Therefore, no significant, adverse impacts would be caused due to conflict with a water quality control plan or sustainable groundwater management plan.

Summary

In conclusion, development of the proposed Project has been evaluated for its potential to result in significant effects relating to traffic, noise, air quality, and water quality. No significant effects were identified, as described above and further substantiated in Attachments A, B, and C to this memorandum. As such, the Project meets the Class 32 categorical exemption criteria for not having significant impacts to traffic, air quality, noise, or water quality.

Utilities & Public Services: The Project site can be adequately served by all required utilities and public services.

The Project is located in an infill urban area served by existing public utilities and services and is situated on a site previously developed with a commercial shopping center.

A Water Study was prepared by Dexter Wilson Engineering (Attachment D). Water service to the Project will be provided by the Carlsbad Municipal Water District. There is an existing 8-inch public water line south of the Project in Oak Avenue and an existing 6-inch water line north of the Project in Carlsbad Village Drive east of Harding Street that increases to an 8-inch water line east of the fire hydrant near the northeast corner of the Project Site. The Project will connect to the existing public water lines in Carlsbad Village Drive and Oak Avenue. The Project will upsize the Carlsbad Village Drive water pipeline along the Project's frontage to an 8-inch diameter to be consistent with the City's standard engineering design criteria outlined in the City of Carlsbad Engineering Standards – Volume 2 (2022).

A Sewer Study was prepared by Dexter Wilson Engineering (Attachment E). Existing public sewer service to the site is provided by the City of Carlsbad. The Project proposes to connect to the existing sewer line in Carlsbad Village Drive near the northwest corner of the Project site. Wastewater from the existing sewer line in Carlsbad Village Drive is conveyed west to Harding Street, north in Harding Street to Grand Avenue, then west in Grand Avenue to the existing trunk sewer in Jefferson Street.

Using temporary sewer flow monitoring data provided by the City, the flow depth criteria for 12" and smaller diameter sewer mains is exceeded under pre-project conditions (without the addition of project flows) and, therefore, the existing 6-inch sewer line in Carlsbad Village Drive, from Harding Street to the northeast corner of the project site requires upsizing to an 8-inch sewer line and the existing 10-inch sewer line in Harding Street, from Carlsbad Village Drive to Grand Avenue requires upsizing to a 12-inch sewer line based on the City of Carlsbad Engineering Standards – Volume 2 (2022). The Project's cost contribution to construct a larger pipe will be based on the proportionate share of Project Equivalent Dwelling Units ("EDUs") in accordance with Title 13 of the Carlsbad Municipal Code (13.08.035 and 13.08.040).

When Project flows are further evaluated for the Master Plan buildout condition, which assumes all sites contributing to the sewer basin are developed to the maximum densities allowed for the zoning assumed in the 2019 Sewer Master Plan, the analysis also shows that the existing 10-inch sewer main in Harding Street between Carlsbad Village Drive and Grand Avenue requires upsizing to a 12-inch sewer line based on City engineering standards.

Therefore, the Project site, which has been previously developed, can and will be served by all public utilities and services with the proposed improvement of the water and sewer systems and is consistent with the General Plan. Therefore, the Project meets this requirement.

CEQA Section 15300.2: Exceptions to the Use of Categorical Exemptions

There are five exceptions that must be considered in order to find a project exempt under Class 32:

(a) Cumulative Impacts. All exemptions for these classes are inapplicable when the cumulative impact of successive projects of the same type in the same place, over time is significant.

There is no evidence to conclude that significant impacts will occur based on past project approvals or that the proposed Project's impacts are cumulatively considerable when evaluating any cumulative impacts associated with construction air quality, noise, transportation, or water quality in the area surrounding the proposed Project. The Project, and all future projects, will be required to comply with all applicable local, regional, and state laws, regulations, and guidelines, and as described above, any potential impact cause by the Project's construction and operation would continue to be less than significant and would not contribute significantly to regional cumulative impact in the broader project region.

Therefore, this exception does not apply.

(b) Significant Effect Due to Unusual Circumstances. A categorical exemption shall not be used for an activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances.

The Project proposes a mixed-use, transit proximate development that includes market rate and affordable housing with neighborhood serving commercial uses in an area zoned and designated for such development. The surrounding area is developed with a mixture of residential and commercial uses; as such, the proposed Project is not unusual in character for the area. The Project site is 4.12 acres and almost entirely covered by impervious surfaces. Moreover, development of the Project would result in a net reduction in vehicular trips due to the removal of the existing commercial shopping center. As described above, the proposed Project has been studied for its potential to cause environmental impacts in a variety of categories, including air quality, noise, traffic, and water quality. No significant effects were identified in those categories.

As indicated above, the Project would not result in impacts to biological resources as none exist on the Project site or surrounding area. Also, the Project site is not located in or near a state responsibility area or lands classified as very high fire hazard severity zones by CALFIRE¹. The Project is located with a X Flood Zone as designated by the Federal Emergency Management Agency². This designation indicates that the Project area is subject to inundation by a 0.2-percent-annual-chance flood event; and the area is subject to 1-percent-annual-chance of flood with average depth less than one foot or with drainage areas of less than one square mile. This zone designation, and its implications, does not represent an unusual circumstance.

There is no substantial evidence that this Project will cause a significant impact. The Project site has been designated for residential and commercial development by the City of Carlsbad General Plan and is consistent with the policies and regulations contained in the Village and Barrio Master Plan. The circumstances of the Project, which involve redevelopment of an underutilized commercial lot in an infill location near transit with a new mixed use, mixed income project with complementary neighborhood serving commercial uses, are no different than the general circumstances of other projects covered by this exemption classification, within the Master Plan area or elsewhere in the city where this exemption may be applied appropriately. There are no distinguishing features of the Project that give rise to the reasonable possibility that the Project's size, density and intensity, bulk and mass, and use will impact the environment as compared to other projects. Thus, there are no unusual circumstances which may lead to a significant effect on the environment, and this exception does not apply.

(c) Scenic Highways. A categorical exemption shall not be used for a project which may result in damage to scenic resources, including but not limited to, trees, historic buildings, rock outcroppings, or similar resources, within a highway officially designated as a state scenic highway.

There are no designated State Scenic Highways in the City of Carlsbad. Therefore, the Project would not create any impacts within a designated state scenic highway, and this exception does not apply.

(d) Hazardous Waste Sites. A categorical exemption shall not be used for a project located on a site which is included on any list compiled pursuant to Section 65962.5 of the Government Code.

According to EnviroStor, the State of California's database of Hazardous Waste Sites, the Project site is not identified as a hazardous waste site. The adjacent property located at 1089 Carlsbad Village Drive, currently

¹ CAL FIRE (California Department of Forestry and Fire Protection). 2022. San Diego County – State Responsibility Area Fire Hazard Severity Zones. November 21, 2022. Fire Hazard Severity Zones in State Responsibility Area - San Diego County (ca.gov)

² FEMA (Federal Emergency Management Agency). 2022. FEMA Flood Map Service Center, Carlsbad, City of. Effective May 2012 & December 2019. FEMA's National Flood Hazard Layer (NFHL) Viewer (arcgis.com)

occupied by 7-Eleven, is identified as a hazardous waste site (gas station).³ This facility has one closed LUST cleanup associated with it. The government records search indicated that the former gas station at 1089 Carlsbad Village Drive, located immediately north of the Project site, is listed on the State Water Board Leaking Underground Storage Tank (LUST) database with one case that is indicated as “case closed.” However, prior soil and groundwater contamination has been appropriately treated and did not induce significant impact to the subsurface environment of the Project site.

There are no active LUST cleanup sites or other sites identified with potential environmental concern within the immediate vicinity (less than 0.125 miles) of the Project site. According to EnviroStor, no locations in the neighborhood within close proximity to the Project site are considered to pose any environmental threat to the subject property.

Therefore, the Project site is not identified as a hazardous waste site and is not in the vicinity of a hazardous waste site, and this exception does not apply.

(e) Historical Resources. A categorical exemption shall not be used for a project which may cause a substantial adverse change in the significance of a historical resource.

A historic resources technical report (Attachment F) was prepared by Dudek to determine if the Project would impact any historical resources pursuant CEQA. The report included the results of a survey of the Project site by a qualified architectural historian; building development and archival research; development of an appropriate historic context for the evaluation of the Project site; and recordation and evaluation of one commercial property over 45 years old for historical significance and integrity in consideration of National Register of Historic Places (NRHP), California Register of Historical Resources (CRHR), and City of Carlsbad designation criteria and integrity requirements. The report concluded that none of the buildings on the Project site are listed or eligible to be listed in any national, state, or local landmark or historic district programs. As such, development of the Project would not cause a substantial adverse change in the significance of a historical resource, and this exception does not apply.

Carlsbad Municipal Code Chapter 19.04 – Additional Exceptions to a Categorical Exemption

Grading and clearing activities affecting sensitive plant or animal habitats, which disturb, fragment or remove such areas as defined by either the California Endangered Species Act (Fish and Game Code Sections 2050 et seq.), or the Federal Endangered Species Act (16 U.S.C. Section 15131 et seq.); sensitive, rare, candidate species of special concern; endangered or threatened biological species or their habitat (specifically including sage scrub habitat for the California Gnatcatcher); or archaeological or cultural resources from either historic or prehistoric periods; or

The Project site is located in a developed part of the city and is surrounded by commercial and residential uses. The Project site is developed with a series of existing buildings which would be demolished and removed during construction. Vegetation on the site is limited to a row of decorative trees scattered throughout the Project site that are not known to support any candidate, sensitive, or special-status species. No native habitat is located on the Project site or on adjacent properties. Based on the urbanized nature of the Project site and

³ California Department of Toxic Substances Control. 2022. EnviroStor. Web Mapping Application. Search by map location “945 Carlsbad Village Drive, Carlsbad, CA.” Accessed December 8, 2022. <https://envirostor.dtsc.ca.gov/public/>.

adjacent properties, in conjunction with a lack of suitable habitat for special-status species, the Project site has no value as habitat for endangered, rare, or threatened species and thus meets the Class 32 categorical exemption criteria for lack of habitat. Furthermore, the site is not within mapped areas of potential critical habitat as depicted in the City's General Plan⁴. For these reasons, the Project has no value as habitat for endangered, rare, or threatened species.

A built environment survey was conducted by Dudek to determine if the Project would impact any historical resources pursuant CEQA. The report included the results of a survey of the Project site by a qualified architectural historian; building development and archival research; development of an appropriate historic context for the evaluation of the Project site; and recordation and evaluation of one commercial property over 45 years old for historical significance and integrity in consideration of National Register of Historic Places (NRHP), California Register of Historical Resources (CRHR), and City of Carlsbad designation criteria and integrity requirements. The report concluded that neither the building nor structure on the Project site are currently listed under any national, state, or local landmark or historic district programs.

A records search was reviewed from the South Coastal Information Center (SCIC) at San Diego State University (SDSU) to identify previously discovered cultural resources in the Project vicinity (Attachment G). The SCIC records search was negative for the presence of previously recorded cultural resources within the Project boundaries. Further, a pedestrian survey of the Project site did not reveal any archaeological or cultural resources. In addition, the Project site is currently developed with a commercial shopping center. Therefore, the likelihood that intact archaeological or cultural resources exist on the Project site is low due to previous site disturbance. Furthermore, the Project will be conditioned to require archaeological monitoring of ground-disturbing activities during Project construction in compliance with standard City regulatory procedures outlined in the Carlsbad Tribal, Cultural, and Paleontological Resources Guidelines (City of Carlsbad 2017). Therefore, earth-moving activities associated with the Project would not affect archaeological or cultural resources from either historic or prehistoric periods and this exception does not apply.

Parcel maps, plot plans and all discretionary development projects otherwise exempt but which affect sensitive, threatened or endangered biological species or their habitat (as defined above), archaeological or cultural resources from either historic or prehistoric periods, wetlands, stream courses designated on U.S. Geological Survey maps, hazardous materials, unstable soils or other factors requiring special review, on all or a portion of the site. (Ord. NS-593, 2001)

Biological Resources

As discussed above, the proposed Project would not affect sensitive, threatened, or endangered biological species or their habitat. This exception does not apply.

Cultural Resources

As discussed above, the Project would not affect known archaeological or cultural resources from either historic or prehistoric periods and the Project will be conditioned to require archaeological and Native American monitoring of ground-disturbing activities during Project construction in compliance with standard

⁴ City of Carlsbad. 2015 City of Carlsbad General Plan – Open Space, Conservation, and Recreation Element. Adopted September 2015. <https://www.carlsbadca.gov/home/showpublisheddocument/3424/637434861099030000>

City regulatory procedures outlined in the Carlsbad Tribal, Cultural, and Paleontological Resources Guidelines (City of Carlsbad 2017). This exception does not apply.

Wetlands and Streams

As described above, no evidence of vernal pool or wetland features are present on site or in adjacent area. Further, no streams are located within the vicinity of the site. Thus, this exception does not apply.

Hazardous Materials

Based on the hazardous materials record searches, the Project site is not identified as a hazardous waste site and no hazardous materials with an "open" cleanup case are located on or in the immediate vicinity of the Project site.

Further, a Phase I Environmental Site Assessment was completed by GSI Environmental Inc. on August 8, 2023 and concluded the existing site conditions are not a constraint to future mixed-use development. Finally, the Project will be conditioned to comply with all existing standard applicable regulatory requirements related to hazardous materials. Therefore, this exception does not apply.

Unstable Soils

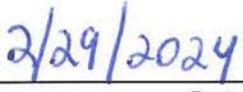
A review of the City's General Plan Safety Element and California Geological Survey's Earthquake Zones of Required Investigation concluded that the Project site is not located in an area with potential for seismic hazards.

Conclusion

For the reasons described above, the Project meets all the criteria for a Class 32 Categorical Exemption.

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ERIC LARDY, City Planner


Date

Attachments:

- Exhibit 1: Vehicle Miles Travelled
- Exhibit 2: Noise
- Exhibit 3: Air Quality
- Exhibit 4: Water
- Exhibit 5: Sewer
- Exhibit 6: Historical
- Exhibit 7: Archeological
- Exhibit 8: Paleontological

LIST OF ACRONYMS AND ABBREVIATIONS

This is a list of acronyms and abbreviations (in alphabetical order) that are commonly used in staff reports.

Acronym	Description	Acronym	Description
APA	American Planning Association	LCPA	Local Coastal Program Amendment
APN	Assessor Parcel Number	LOS	Level of Service
AQMD	Air Quality Management District	MND	Mitigated Negative Declaration
BMP	Best Management Practice	NCTD	North County Transit District
CALTRANS	California Department of Transportation	ND	Negative Declaration
CC	City Council	PC	Planning Commission
CCR	Conditions, Covenants and Restrictions	PDP	Planned Development Permit
CEQA	California Environmental Quality Act	PEIR	Program Environmental Impact Report
CFD	Community Facilities District	PUD	Planned Unit Development
CIP	Capital Improvement Program	ROW	Right of Way
COA	Conditions of Approval	RWQCB	Regional Water Quality Control Board
CofO	Certificate of Occupancy	SANDAG	San Diego Association of Governments
CT	Tentative Parcel Map	SDP	Site Development Plan
CUP	Conditional Use Permit	SP	Specific Plan
DIF	Development Impact Fee	SWPPP	Storm Water Pollution Prevention Program
DISTRICT	City Council Member District Number	TM	Tentative Map
EIR	Environmental Impact Report	VBMP	Village & Barrio Master Plan
EIS	Environmental Impact Statement (federal)	ZC	Zone Change
EPA	Environmental Protection Agency		
FEMA	Federal Emergency Management Agency		
GP	General Plan		
GPA	General Plan Amendment		
GIS	Geographic Information Systems		
HCA	Housing Crisis Act 2019		
IS	Initial Study		



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CVMU
945-1065 CARLSBAD VILLAGE DRIVE
CARLSBAD, CA 92008

GRT CARLSBAD VILLAGE, LLC
11661 SAN VICENTE BLVD,
SUITE 850
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SUBMITAL:

SDP 2023-0014

JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISIONS:

SHEET TITLE:
**PROJECT
INFORMATION**

SHEET NUMBER:
G01

Item #1 177 of 269

PROJECT INFORMATION

PROJECT NAME: CVMU (CARLSBAD VILLAGE MIXED-USE)
ADDRESS: 945-1065 CARLSBAD VILLAGE DRIVE CARLSBAD CA 92008
OWNER: GRT CARLSBAD VILLAGE, LLC
PROJECT DESCRIPTION: THE PROJECT PROPOSES TO DEVELOP A MIXED-USE DEVELOPMENT CONSISTING OF 218 MULTI-FAMILY UNITS (27 UNITS WOULD BE RESERVED FOR VERY LOW INCOME HOUSEHOLDS), AS WELL AS 13,800 SQUARE FEET OF COMMERCIAL SPACE, INCLUDING OVER 40,000 SQUARE FEET OF OPEN SPACE ON A 4.12 ACRE SITE. THE PROPOSED COMMERCIAL USES WOULD BE CONTAINED IN TWO, ONE-STORY BUILDINGS ALONG CARLSBAD VILLAGE DRIVE, AND THE RESIDENTIAL UNITS, INCLUDING THE AFFORDABLE UNITS, WOULD BE SPREAD ACROSS TWO FIVE STORY RESIDENTIAL BUILDINGS. 340 VEHICULAR PARKING SPACES WILL BE PROVIDED AT-GRADE SURROUNDING THE COMMERCIAL AND RESIDENTIAL BUILDINGS AND WITHIN AN ABOVE-GRADE PARKING STRUCTURE. THE SITE IS CURRENTLY OCCUPIED BY A RETAIL SHOPPING CENTER WITH A TOTAL FLOOR AREA OF APPROXIMATELY 58,735 SQUARE FEET, AS WELL AS A SURFACE PARKING LOT THAT WOULD BE DEMOLISHED DURING PROJECT CONSTRUCTION.

CONSTRUCTION TYPE: 1 STORY TYPE V-A AND 5 STORY TYPE III-A, 5 STORY TYPE IA
GENERAL PLAN: VILLAGE-BARIO (V-B)
ZONING: VILLAGE-BARRIO ZONE WITHIN THE "FC" FREEWAY COMMERCIAL DISTRICT OF THE VILLAGE & BARRIO MASTER PLAN (VBMP)
FLOOD ZONE: X
GROSS SITE AREA: 179,312 SF (4.12 ACRES)
REQUIRED SETBACKS: FRONT: 10 FEET
SIDE: 0 FEET
REAR: 10 FEET
PROPOSED BLDG HEIGHT: -BLDG C: 56'-0" AND 5 STORIES
T.O. PARAPET 60'-6", T.O. STAIR 66'-0", T.O. ELEV TOWER 60'-0"
-BLDG D: 53' - 10" AND 5 STORIES
T.O. PARAPET 58'-4", T.O. STAIR 63'-10", T.O. ELEV TOWER 67'-10"
-COMMERCIAL BLDGS: 24'-6" AND 1 STORY
T.O. PARAPET 20'-0", TO EQUIP. WELL 15'-6"
-GARAGE: 53' - 10" AND 5 STORIES
T.O. PARAPET 58'-4", T.O. STAIR 63'-10", T.O. ELEV TOWER 67'-10"
OCCUPANCY TYPE: R-2 RESIDENTIAL, R-2 ACCESSORY, S-2 PARKING, A ASSEMBLY, M MERCANTILE
AREA OF SITE WHICH IS UNDEVELOPABLE: 0 SF PER ZONING ORDINANCE SECTION 21.53.230
SEWER DISTRICT: CITY OF CARLSBAD WASTEWATER DIVISION
WATER DISTRICT: CARLSBAD MUNICIPAL WATER DISTRICT
SCHOOL DISTRICT: CARLSBAD UNIFIED SCHOOL DISTRICT
CLIMATE ACTION PLAN : 0.90 ALLOWED OUTDOOR LIGHTING POWER
0.95 ENERGY BUDGET
ON-SITE RENEWABLE ENERGY PHOTOVOLTAIC: PER CEC SECTION 170.1(f) AND 141.2
HOT WATER HEATING: CEC SECTION 120.11 AND 150(n)5

VICINITY MAP



LEGAL DESCRIPTION

TRACT ONE: FEE
PARCELS A THROUGH D AS SHOWN ON "CITY OF CARLSBAD M.S. 2018-0014", IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO PARCEL MAP THEREOF NO. 21699 FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, JULY 16, 2019.

TRACT TWO: LEASEHOLD
PARCEL 1: LOTS 1 TO 16 INCLUSIVE, IN BLOCK 74 AND THOSE PORTIONS OF BLOCK 74, TOGETHER WITH PORTIONS OF 6TH STREET AND 7TH STREET AND OAK AVENUE AS CLOSED AND VACATED TO PUBLIC USE AND THE ALLEYS ADJOINING AND LYING WITHIN THE SAID BLOCKS IN THE TOWN OF CARLSBAD, IN THE CITY OF CARLSBAD, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF NO. 535, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, THE ABOVE DESCRIBED BLOCKS AND PORTIONS THEREOF BEING ALSO A PORTION OF TRACT 115 OF THE TOWN OF CARLSBAD, ACCORDING TO AMENDED MAP THEREOF NO. 775, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, LYING WITHIN THE FOLLOWING DESCRIBED LAND: (SEE SURVEY FOR DESCRIPTION)
PARCEL 2: ALL OF LOTS 17 TO 32 INCLUSIVE IN BLOCK 66 OF TOWN OF CARLSBAD, IN THE CITY OF CARLSBAD, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF NO. 535, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, MAY 2, 1888, AND AMENDED MAP NO. 775, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, FEBRUARY 15, 1894.
TOGETHER WITH THAT PORTION OF 6TH STREET AS SHOWN ON THE SAID MAP NOS. 535 AND 775, AND MORE PARTICULARLY DESCRIBED AS FOLLOWS: THE SOUTHWESTERLY HALF OF 6TH STREET AS SHOWN ON MAPS ABUTTING THE SAID LOTS 17 TO 32 INCLUSIVE, IN THE SAID BLOCK 66 AS SHOWN ON THE MAP NOS. 535 AND 775.

ALSO KNOWN AS: PARCELS A THROUGH D AS SHOWN ON "CITY OF CARLSBAD M.S. 2018-0014", IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO PARCEL MAP THEREOF NO. 21699 FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, JULY 16, 1990.

REQUIRED PARKING

RETAIL:	PER VILLAGE BARRIO MASTER PLAN
Retail: 1/1415 SF	9,800 / 415 = 23.6 = 24
Restaurant: 1/170 SF	4,000 / 170 = 23.5 = 24
SUBTOTAL:	48 STALLS
RESIDENTIAL:	PER GOVERNMENT CODE SECTION 65915(p)(2)
STUDIO: 5 / unit	15 x 5 = 8 STALLS
1-BR: 5 / unit	87 x 5 = 44 STALLS
2-BR: 5 / unit	90 x 5 = 45 STALLS
3-BR: 5 / unit	26 x 5 = 13 STALLS
SUBTOTAL:	110 STALLS
TOTAL REQUIRED:	158 STALLS
REQUIRED ACCESSIBLE PARKING	
RETAIL:	
ACCESSIBLE	3 OF 56 = 3 LESS 1 VAN = 2
VAN ACCESSIBLE	1 OF 3 = 1
RESIDENTIAL:	
ACCESSIBLE	8 OF 284 = 7 STALLS
VAN ACCESSIBLE	1 OF 8 = 1 STALLS
REQUIRED EV PARKING PER CALGREEN	
RETAIL:	
EV CHARGERS	3 OF 57 = 3 STALLS
EV CAPABLE	13 OF 57 LESS 3 INSTALLED = 10 STALLS
RESIDENTIAL:	
EV CHARGERS	5% OF 284 = 15 STALLS
EV CAPABLE	10% OF 284 LESS 15 INSTALLED = 14 STALLS
EV READY	25% OF 284 = 71 STALLS

*PER AB 2097, PROJECT IS NOT SUBJECT TO MINIMUM PARKING REQUIREMENTS

PROPOSED PARKING

LOCATIONS OF PROVIDED PARKING	
PARKING STRUCTURE	SURFACE PARKING
LEVEL 1: 53 SPACES	TOTAL: 51 SPACES
LEVEL 2: 46 SPACES	
LEVEL 3: 46 SPACES	
LEVEL 4: 47 SPACES	
LEVEL 5: 48 SPACES	
ROOF: 49 SPACES	
TOTAL: 289 SPACES	
RETAIL:	56 STALLS
RESIDENTIAL:	284 STALLS
TOTAL PROVIDED:	340 STALLS

45 STANDARD, 11 COMPACT STALLS
277 STANDARD, 7 COMPACT STALLS

OPEN SPACE REQUIRED

TOTAL OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 20% of site = 35,862 SF
PRIVATE OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 80 SF per Unit (min. dimension 6 ft)
	80 SF x 218 units = 17,440 SF
COMMON OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 25 SF per Unit (min. dimension 10 ft)
	25 SF x 218 units = 5,450 SF
TOTAL REQUIRED:	22,890 SF

OPEN SPACE PROVIDED

TYPE	LEVEL	QTY	
COMMON OUTDOOR SPACE: COURTYARD	LEVEL 1	1	22,517 SF
COMMON OPEN SPACE: ROOF DECK	LEVEL 5	1	593 SF
COMMON OPEN SPACE SUBTOTAL: 23,110 SF			
PRIVATE OPEN SPACE (PATIOS)	LEVEL 1	35	2,800 SF
PRIVATE OPEN SPACE	LEVEL 2	49	3,920 SF
PRIVATE OPEN SPACE	LEVEL 3	49	3,920 SF
PRIVATE OPEN SPACE	LEVEL 4	46	3,680 SF
PRIVATE OPEN SPACE	LEVEL 5	43	3,440 SF
PRIVATE OPEN SPACE SUBTOTAL: 17,760 SF			
GRAND TOTAL: 40,870 SF			
= 23% OF SITE			

BUILDING CODES USED

2022 CALIFORNIA BUILDING CODE (CBC), PART 2, TITLE 24 C.C.R. AND AMENDMENTS
2022 CALIFORNIA ELECTRICAL CODE (CEC), PART 3, TITLE 24 C.C.R. AND AMENDMENTS
2022 CALIFORNIA MECHANICAL CODE (CMC), PART 4, TITLE 24, C.C.R. AND AMENDMENTS
2022 CALIFORNIA PLUMBING CODE (CPC), PART 5, TITLE 24, C.C.R. AND AMENDMENTS
2022 CALIFORNIA FIRE CODE (CFC), PART 9, TITLE 24, C.C.R. AND AMENDMENTS
2022 CALIFORNIA REFERENCED STANDARDS CODE, PART 12, TITLE 24 C.C.R.
2022 CALIFORNIA GREEN BUILDING CODE, TITLE 24, PART 6

GROSS AREA

COMMERCIAL A:	8,000 SF
COMMERCIAL B:	5,800 SF
COMMERCIAL SUBTOTAL: 13,800 SF	
PARKING LEVEL 1	22,894 SF
PARKING LEVEL 2	22,894 SF
PARKING LEVEL 3	22,894 SF
PARKING LEVEL 4	22,894 SF
PARKING LEVEL 5	22,894 SF
PARKING SUBTOTAL: 114,470 SF	
RESIDENTIAL LEVEL 1	52,394 SF
RESIDENTIAL LEVEL 2	52,521 SF
RESIDENTIAL LEVEL 3	52,521 SF
RESIDENTIAL LEVEL 4	50,614 SF
RESIDENTIAL LEVEL 5	47,405 SF
RESIDENTIAL SUBTOTAL: 255,455 SF	
GRAND TOTAL: 383,725 SF	



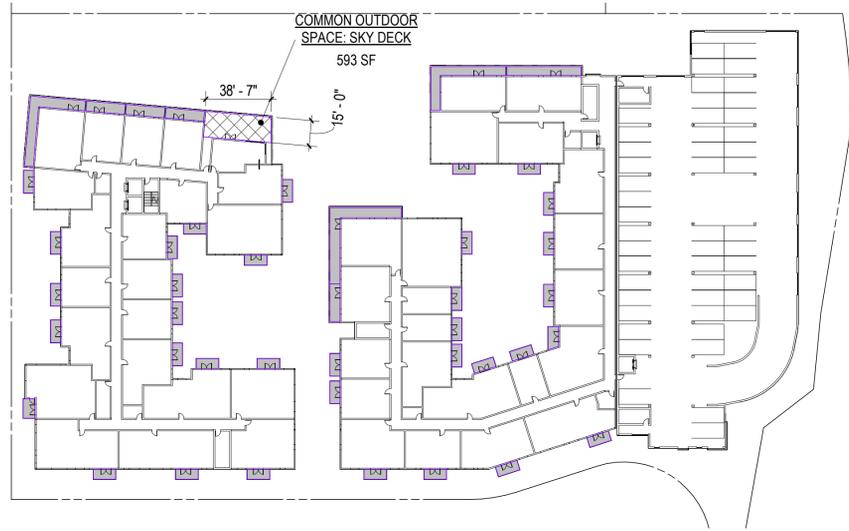
LEGEND

- PROPOSED GROUND FLOOR FOOTPRINT
- EXISTING BUILDINGS (NOT A PART)
- PROPOSED UPPER FLOORS FOOTPRINT
- EXISTING BUILDINGS TO BE DEMOLISHED

PLOT PLAN

GENERAL NOTES

- THE WORK SHALL CONFORM TO THE APPLICABLE BUILDING CODE, AND ALL OTHER ORDINANCES, CODES, AND REGULATIONS LISTED IN THE SPECIFICATIONS OR ON THE DRAWINGS, AND REQUIRED BY LOCAL BUILDING AUTHORITIES. THE GOVERNING CODES, RULES, AND REGULATIONS ARE COLLECTIVELY REFERRED TO AS "THE CODE". CONTRACTOR SHALL REPORT ANY INCONSISTENCIES, CONFLICTS OR OMISSIONS HE MAY DISCOVER TO THE ARCHITECT FOR INTERPRETATION PRIOR TO PERFORMING THE WORK.
 - CONSTRUCTION SHALL COMPLY WITH PERTINENT HEALTH AND SAFETY REGULATIONS FOR REQUIRED METHODS PROTECTING PUBLIC AND CONSTRUCTION WORKER'S HEALTH AND SAFETY DURING THE CONSTRUCTION PERIOD.
 - BEFORE ORDERING ANY MATERIAL, OR DOING ANY WORK, THE CONTRACTOR SHALL VERIFY ALL MEASUREMENTS AT THE BUILDING SITE AND SHALL BE RESPONSIBLE FOR CORRECTNESS OF SAME.
 - DIMENSIONS SHALL BE AS INDICATED ON THE DRAWINGS. CLARIFICATIONS, IF REQUIRED, SHALL BE OBTAINED FROM THE ARCHITECT. THE DRAWING SHALL NOT BE SCALED.
 - ALL ITEMS MARKED NIC ON THE SPECIFICATIONS AND DRAWINGS MEANS NOT IN CONTRACT.
 - DO NOT MAKE ANY SUBSTITUTIONS WITHOUT PERMISSION OF THE ARCHITECT OR THE OWNER.
 - WORK INDICATED ON THE DRAWINGS OR IN THE SPECIFICATIONS AS NIC, OR BY SEPARATE CONTRACTORS, IS NOT PART OF THIS CONTRACT. CONTRACTOR SHALL COOPERATE FULLY WITH ALL SEPARATE CONTRACTORS EMPLOYED BY THE OWNER.
 - THE CLIENT, ARCHITECT, CONSULTANTS, AND ALL INSPECTORS FROM PERTINENT AGENCIES SHALL BE PERMITTED ACCESS TO THE JOB SITE AT ALL TIMES DURING NORMAL WORKING HOURS.
 - WINDOW AND DOOR DIMENSIONS FOR PREFABRICATED WINDOW AND DOOR UNITS HAVE BEEN ROUNDED TO THE NEAREST INCH ON THE DRAWINGS. SPECIFIC DIMENSIONS BY MANUFACTURERS MAY VARY FROM THE DRAWINGS.
 - THE CONTRACTOR SHALL VERIFY LOCATION AND SIZE OF ALL FLOOR, ROOF, AND WALL OPENINGS WITH ALL APPLICABLE DRAWINGS.
 - DETAILS ARE INTENDED TO SHOW THE INTENT OF THE DESIGN. MINOR MODIFICATIONS MAY BE REQUIRED TO SUIT THE FIELD DIMENSIONS OR CONDITIONS, AND SUCH MODIFICATIONS SHALL BE INCLUDED AS PART OF THE WORK OF THE CONTRACT.
 - THE CONTRACT DRAWINGS AND SPECIFICATIONS REPRESENT THE FINISHED STRUCTURE AND DO NOT INDICATE THE METHODS OF CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES, AND PROCEDURES INCLUDING, BUT NOT LIMITED TO BRACING AND SHORING. OBSERVATION VISITS TO THE SITE BY FIELD REPRESENTATIVES OF THE ARCHITECT AND/OR ENGINEER SHALL NOT INCLUDE INSPECTIONS OF THE PROTECTIVE MEASURES TO THE CONSTRUCTION PROCEDURES.
- ANY SUPPORT SERVICES PERFORMED BY THE ARCHITECT AND/OR ENGINEER DURING THE CONSTRUCTION SHALL BE DISTINGUISHED FROM CONTINUOUS AND DETAILED INSPECTION SERVICES WHICH IS FURNISHED BY OTHERS. THESE SUPPORT SERVICES PERFORMED BY THE ARCHITECT AND/OR ENGINEER, WHETHER OF MATERIAL OR WORK, AND WHETHER PERFORMED PRIOR TO, DURING, OR AFTER COMPLETION OF CONSTRUCTION, ARE PERFORMED SOLELY FOR THE PURPOSE OF ASSISTING IN QUALITY CONTROL AND IN ACHIEVING GENERAL CONFORMANCE WITH CONTRACT DOCUMENTS, BUT DO NOT GUARANTEE CONTRACTOR'S PERFORMANCE AND SHALL NOT BE CONSTRUED AS SUPERVISION OF CONSTRUCTION.



5TH FLOOR OPEN SPACE PLAN
1" = 50'-0"

5



4TH FLOOR OPEN SPACE PLAN
1" = 50'-0"

4



3RD FLOOR OPEN SPACE PLAN
1" = 50'-0"

3



2ND FLOOR OPEN SPACE PLAN
1" = 50'-0"

2



COMMON OPEN SPACE:
COURTYARD
22517 SF

1ST FLOOR OPEN SPACE PLAN
1" = 50'-0"

1

NOTES

REQUIRED OPEN SPACE

TOTAL OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 20% of site = 35,862 SF
PRIVATE OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 80 SF per Unit (min. dimension 6 ft)
	80 SF x 218 units = 17,440 SF
COMMON OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 25 SF per Unit (min. dimension 10 ft)
	25 SF x 218 units = 5,450 SF
TOTAL REQUIRED:	22,890 SF

PROPOSED OPEN SPACE

TYPE	LEVEL	QTY	
COMMON OUTDOOR SPACE: COURTYARD	LEVEL 1	1	22,517 SF
COMMON OPEN SPACE: SKY DECK	LEVEL 5	1	593 SF
COMMON OPEN SPACE SUBTOTAL: 23,110 SF			
PRIVATE OPEN SPACE (PATIOS)			
PRIVATE OPEN SPACE	LEVEL 1	35	2,800 SF
PRIVATE OPEN SPACE	LEVEL 2	49	3,920 SF
PRIVATE OPEN SPACE	LEVEL 3	49	3,920 SF
PRIVATE OPEN SPACE	LEVEL 4	46	3,680 SF
PRIVATE OPEN SPACE	LEVEL 5	43	3,440 SF
PRIVATE OPEN SPACE SUBTOTAL: 17,760 SF			
GRAND TOTAL: 40,870 SF			
= 23% OF SITE			

SHEET NOTES

- PROPERTY OPEN SPACE**
- A. A MINIMUM OF 20 PERCENT OF PROPERTY MUST BE MAINTAINED AS OPEN SPACE.
 - B. PUBLIC AND RESIDENTIAL PRIVATE AND COMMON OPEN SPACE MAY BE COUNTED TOWARD ACHIEVING THE PROPERTY OPEN SPACE MINIMUM REQUIREMENT.
 - C. OPEN SPACE MAY BE DEDICATED TO LANDSCAPE PLANTERS, OPEN SPACE POCKETS AND/OR CONNECTIONS, ROOF GARDENS/PATIOS, BALCONIES, OTHER PATIOS, AND/OR OUTDOOR EATING AREAS.
 - D. NO PARKING SPACES OR AISLES ARE PERMITTED IN THE OPEN SPACE.

- RESIDENTIAL PRIVATE OPEN SPACE**
- A. PRIVATE OPEN SPACE SHALL BE PROVIDED AT A MINIMUM OF 80 SQUARE FEET PER UNIT WITH A MINIMUM DIMENSION OF 6 FEET IN ANY DIRECTION. THIS REQUIREMENT MAY BE SATISFIED BY MORE THAN ONE PRIVATE OPEN SPACE AREA.

- RESIDENTIAL COMMON OPEN SPACE**
- A. RESIDENTIAL COMMON OPEN SPACE SHALL BE PROVIDED FOR PROJECTS WITH MORE THAN 10 UNITS.
 - B. COMMON OPEN SPACE SHALL BE PROVIDED AT A MINIMUM OF 25 SQUARE FEET PER UNIT WITH A MINIMUM DIMENSION OF 10 FEET IN ANY DIRECTION.
 - C. COMMON OPEN SPACE SHALL BE PURPOSEFULLY DESIGNED AS ACTIVE OR PASSIVE RECREATIONAL FACILITIES.
 - D. ROOFTOP OPEN SPACE MAY SATISFY THIS REQUIREMENT, PROVIDED IT IS AVAILABLE FOR USE BY ALL RESIDENTS.

LEGEND

- COMMON OPEN SPACE: COURTYARD
- COMMON OUTDOOR SPACE: ROOF DECK
- COMMON OUTDOOR SPACE: SKY DECK
- PRIVATE OPEN SPACE



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SDP 2023-0014

JOB NUMBER: 2022-004.00

DATE: 11.13.2023

REVISION:

SHEET TITLE:
OPEN SPACE CALCULATIONS

SHEET NUMBER:
G03



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JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISION:

SHEET TITLE:
BUILDING AREA

SHEET NUMBER:
G04

Item #1 179 of 269

NOTES

GROSS AREA

COMMERCIAL A:	8,000 SF
COMMERCIAL B:	5,800 SF
COMMERCIAL SUBTOTAL: 13,800 SF	
PARKING LEVEL 1	22,894 SF
PARKING LEVEL 2	22,894 SF
PARKING LEVEL 3	22,894 SF
PARKING LEVEL 4	22,894 SF
PARKING LEVEL 5	22,894 SF
PARKING SUBTOTAL: 114,470 SF	
RESIDENTIAL LEVEL 1	52,394 SF
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RESIDENTIAL LEVEL 4	50,614 SF
RESIDENTIAL LEVEL 5	47,405 SF
RESIDENTIAL SUBTOTAL: 255,455 SF	
GRAND TOTAL: 383,725 SF	

SHEET NOTES

AREA IN SQUARE FEET CONFINED WITHIN THE EXTERIOR WALLS OF A BUILDING, BUT NOT INCLUDING THE AREA OF THE FOLLOWING: EXTERIOR WALLS, STAIRWAYS, SHAFTS, ROOMS HOUSING BUILDING EQUIPMENT OR MACHINERY, PARKING AREAS WITH ASSOCIATED DRIVEWAYS AND RAMPS, SPACE FOR THE LANDING AND STORAGE OF HELICOPTERS, AND BASEMENT STORAGE AREAS.

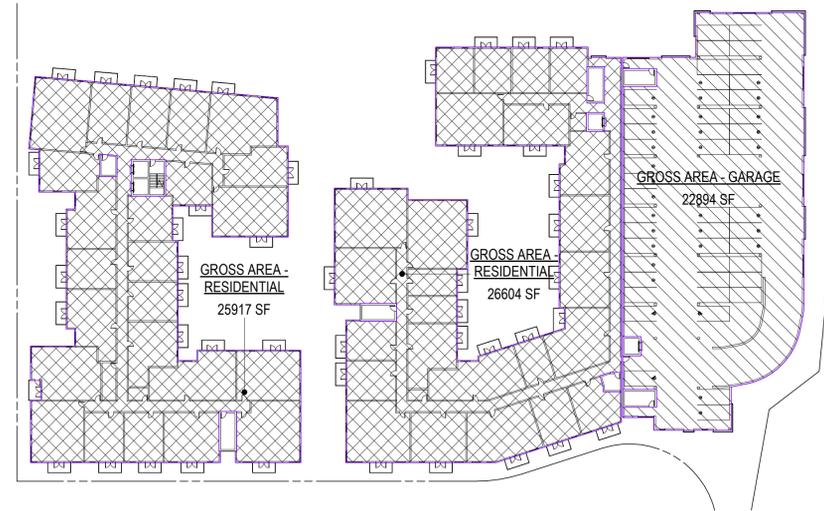
LEGEND

- GROSS AREA - COMMERCIAL A
- GROSS AREA - COMMERCIAL B
- GROSS AREA - GARAGE
- GROSS AREA - RESIDENTIAL



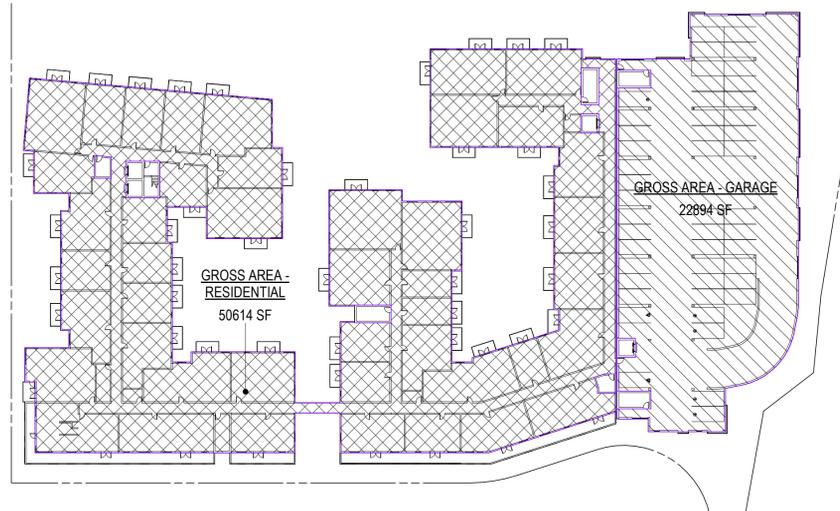
5TH FLOOR GROSS AREA PLAN
1" = 50'-0"

5



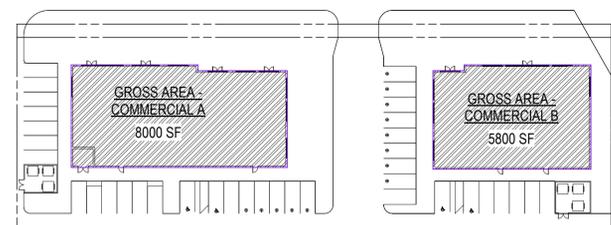
2ND FLOOR GROSS AREA PLAN
1" = 50'-0"

2



4TH FLOOR GROSS AREA PLAN
1" = 50'-0"

4



1ST FLOOR GROSS AREA PLAN
1" = 50'-0"

1



3RD FLOOR GROSS AREA PLAN
1" = 50'-0"

3

LEGAL DESCRIPTION

PARCELS A THROUGH D AS SHOWN ON "CITY OF CARLSBAD M.S. 2018-0014", IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO PARCEL MAP THEREOF NO. 21699 FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, JULY 16, 2019.

BASIS OF BEARINGS

CALIFORNIA COORDINATE SYSTEM OF 1983, ZONE 6, EPOCH 1991.35 GRID BEARING BETWEEN FIRST-ORDER G.P.S. STATION NO. 119 AND FIRST-ORDER G.P.S. STATION NO. 121 PER ROS 17271

I.E., NORTH 40°22'26" WEST
DISTANCE SHOWN HEREON ARE GROUND LEVEL DISTANCES. THE COMBINED SCALE FACTOR AT G.P.S. STATION NO. 121 IS 0.999960210.
GRID DISTANCE = GROUND DISTANCE X COMBINED SCALE FACTOR.
QUOTED BEARING FROM REFERENCE DEEDS/MAPS MAY OR MAY NOT BE IN TERMS OF SAID SYSTEM.

BENCHMARK

CITY OF CARLSBAD CONTROL POINT NO. 121, NGVD-29 ELEVATION 80.303'
2.5" DISK IN WEST CURB OPPOSITE 3334 EUREKA STREET AT ANGLE POINT IN CURB AT NORTH END OF PARALLEL PARKING SPACES. 17' SOUTHWEST OF POWER POLE NO. 24625.

ASSESSOR'S PARCEL NO.

203-320-53 THRU 56

UTILITIES

WATER-----CARLSBAD MUNICIPAL WATER DISTRICT
SEWER-----CITY OF CARLSBAD
STORM DRAIN-----CITY OF CARLSBAD
GAS AND ELECTRIC-----SAN DIEGO GAS & ELECTRIC
TELEPHONE-----SBC TELEPHONE
CABLE T.V.-----TIME WARNER CABLE
SCHOOL-----CARLSBAD UNIFIED SCHOOL DISTRICT

GENERAL NOTES

- EXISTING GENERAL PLAN DESIGNATION: VILLAGE-BARRIO (V-B)
- PROPOSED PROJECT IS LOCATED IN LOCAL FACILITIES MANAGEMENT PLAN ZONE 1.
- TOPOGRAPHY SHOWN ON THIS SURVEY IS BASED UPON AN AERIAL SURVEY BY TERRASCRIBE INC., IN SEPTEMBER 9TH, 2022
- ALL CUT AND FILL SLOPES ARE TO BE 2:1 MAXIMUM, EXCEPT AS OTHERWISE SHOWN ON PLAN.
- GRADING SHOWN HEREON IS PRELIMINARY AND IS SUBJECT TO REVISIONS MADE DURING THE FINAL DESIGN PROCESS, IN ACCORDANCE WITH ENGINEERING POLICY NO. 30, AND APPROVAL FROM THE CITY ENGINEER AND CITY PLANNER.
- ALL LOT DIMENSIONS SHOWN ARE APPROXIMATE. ALL LOTS TO MEET MINIMUM DIMENSIONS OF UNDERLYING ZONING.
- EARTHWORK QUANTITIES (GEOMETRIC): 10,000 C.Y. CUT, 1,200 C.Y. FILL
BASED ON THE GEOTECHNICAL REPORT, ANTICIPATED REMEDIAL EXCAVATIONS WILL BE APPROXIMATELY 2FT-3FT DEEP AND EXTEND 5FT OUTSIDE HORIZONTALLY BENEATH BUILDING PADS, 2FT DEEP AND 2FT OUTSIDE HORIZONTALLY BENEATH PEDESTRIAN HARDSCAPE AND 1FT-2FT OUTSIDE HORIZONTALLY BENEATH VEHICULAR PAVEMENTS. REMEDIAL SOILS WILL BE REMOVED AND REPLACED ONSITE.

REMEDIAL QUANTITY: 12,880 C.Y. (QUANTITY WILL SHRINK APPROXIMATELY 10%)
EXPORT: 7,520 C.Y. (AFTER REMEDIAL SHRINKAGE IS ACCOUNTED FOR)

- THE BEARINGS AND DISTANCES SHOWN WERE DETERMINED FROM A FIELD SURVEY DATED SEPTEMBER 2022.

FIRE HYDRANT FLOW REQUIREMENT IS 2,500 GPM.

- FLOOD ZONE DESIGNATION: X

PROPOSED 2-LOT PARCEL MAP

LOT 1 - COMMERCIAL: 0.94 ACRES
LOT 2 - RESIDENTIAL: 3.11 ACRES
PROJECT TOTAL: 4.05 ACRES

- MASTER DRAINAGE BASIN: BASIN B
PROPOSED DRAINAGE DISCHARGE: 25.4 CFS

WATER DEMAND:

LAND USE	QUANTITY	DEMAND FACTOR	AVERAGE DEMAND
MULTI-FAMILY RESIDENTIAL	218 DUs	185 GPD/DU	40,330 GPD
COMMERCIAL	0.32 AC	1,500 GPD/AC	480 GPD
TOTAL			40,810 GPD

PER TABLE 1, AVERAGE DAY DEMAND FOR THE CARLSBAD VILLAGE MIXED USE PROJECT IS 40,810 GPD OR 28.3 GPM. THE MAXIMUM DAY DEMAND IS 65,296 GPD OR 45.3 GPM (ADD X 1.6). THE PEAK HOUR DEMAND IS 118,349 GPD OR 82.2 GPM (ADD X 2.9).

SEWER GENERATION:

LAND USE	QUANTITY	SEWER GENERATION FACTOR	AVERAGE DAILY FLOW
MULTI-FAMILY RESIDENTIAL	218 DUs	160 GPD/DU	34,880 GPD
RETAIL SPACE	13,800 SQ. FT.	200 GPD/1,800 SQ. FT.	1,533 GPD
TOTAL			36,413 GPD

PER TABLE 2, AVERAGE DAILY SEWER FLOW FOR THE CARLSBAD VILLAGE MIXED USE PROJECT IS 36,413 GPD OR 25.3 GPM. THE PEAK DAILY FLOW IS 91,033 GPD OR 63.2 GPM (AVERAGE DAILY FLOW X 2.5).

ADT TABLE

Project Trip Generation Summary															
Carlsbad Village Mixed Use															
Land Use	Quantity	Daily Volumes									PM Peak Hour				
		AM Peak Hour			PM Peak Hour			AM Peak Hour			Rate	Split	In	Out	Total
		Rate a	Volume	Rate	Split	In	Out	Total	Rate	Split	In	Out	Total		
Proposed Project															
Apartment (Multi-family units >20 DU/acre) b	218 DU	6 /DU	1,308	8%	20%	80%	21	84	105	9%	70%	30%	83	35	118
Supermarket	5.8 KSF	150 /KSF	870	4%	70%	30%	25	10	35	10%	50%	50%	44	43	87
Retail / Strip Commercial	2 KSF	40 /KSF	80	3%	60%	40%	1	1	2	9%	50%	50%	4	3	7
Quality Restaurant	6 KSF	100 /KSF	600	1%	60%	40%	4	2	6	8%	70%	30%	34	14	48
Project Subtotal			2,858				51	97	148				165	95	260
Mixed Use Reduction c			-616				-4	-4	-8				-39	-41	-80
Final Project Trips			2,242				47	93	140				126	54	180
Existing to be replaced															
Neighborhood Shopping Center d	58.7 KSF	120 /KSF	7,044	4%	60%	40%	169	113	282	10%	50%	50%	352	352	704
Net New Trip Generation			-4,802				-122	-20	-142				-226	-298	-524

Footnotes:

- Trip generation rates from SANGAG's (Nat So) Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region, April 2002. ("SANDAG Brief Guide")
 - The site acreage is 4.12 therefore the residential density calculates to 53 DU/acre (218 / 4.12 = 53).
 - Mixed use reduction for AM/PM peak hours calculated using NCHRP 8-51 Internal Trip Capture Estimation Tool. Daily reduction estimated using relationship between peak hours and daily volumes for unadjusted project trips.
 - Per SANDAG this land use is typically less than 15 acres, less than 125,000 sq. ft., w/ usually grocery & drugstore, cleaners, beauty & barber shop, & fast food services)
- The existing 4.12 acre, 58.7 KSF site contains all of these individual uses including: grocery, drugstore, cleaners, barber shop, and eating establishments.

LEGEND

DESCRIPTION	SYMBOL
PROPOSED IMPROVEMENTS	
PROJECT BOUNDARY	---
PROP. LOT LINE	---
PROP. EASEMENT	---
PROP. CONTOURS	---
PROP. CUT SLP (2:1 MAX.) (UNLESS SHOWN OTHERWISE)	Y
PROP. FILL SLP (2:1 MAX.) (UNLESS SHOWN OTHERWISE)	Y
PROP. DAYLIGHT LINE	---
PROP. DISTURBANCE LIMIT	D
PROP. RETAINING WALL (FOR COLOR AND FINISH, REFER TO ARCHITECTURAL PLANS)	---
PROP. CURB AND GUTTER	---
PROP. CURB	---
PROP. RIBBON GUTTER	---
PROP. SIDEWALK	---
PROP. TURF	---
PROP. DRIVEWAY	---
PROP. PEDESTRIAN RAMP	---
PROP. STANDARD PARKING STALL	---
PROP. VAN ACCESSIBLE PARKING STALL	---
PROP. CONC. BROW DITCH	---
PROP. SWALE	---
PROP. PROP. RIPRAP	---
PROP. SLOPE GRADE	4%
PROP. FINISH FLOOR ELEVATION	FF=XX.XX
PROP. FINISH SURFACE ELEVATION	XX.XX FS
PROP. WATER SERVICE BUILDING POC	---
PROP. WATER SERVICE	---
PROP. PRIVATE WATER	---
PROP. WATER METER	---
PROP. WATER RPDA	---
PROP. WATER METER AND BACKFLOW ABOVE GROUND ASSEMBLY	---
PROP. FIRE SERVICE BUILDING POINT OF CONNECTION (STANDPIPE WHERE NOTED)	---
PROP. FIRE SERVICE	---
PROP. PRIVATE FIRE WATER	---
PROP. FIRE RPDA	---
PROP. IRRIGATION POINT OF CONNECTION	---
PROP. SEWER MAIN	---
PROP. SEWER MANHOLE	---
PROP. SEWER SERVICE BUILDING POINT OF CONNECTION	---
PROP. SEWER GREASE INTERCEPTOR	---
PROP. PRIVATE STORM DRAIN	---
PROP. TRENCH DRAIN	---
PROP. A-4 SD CLEANOUT	---
PROP. CURB INLET	---
PROP. PERMANENT BMP MODULAR WETLANDS UNIT SIZE 8'X16'	---
PROP. PERMANENT BMP MODULAR WETLANDS UNIT SIZE 8'X8'	---
RAISED BIOFILTRATION PLANTER	---
UNDREGROUND DETENTION VAULT	---
PROP. BROOKS BOX WITH PARKWAY GRATE	---
TRANSFORMER	---
EXISTING IMPROVEMENTS	
CALTRANS RIGHT-OF-WAY	---
EX. CONTOURS	---
EX. RETAINING WALL	---
EX. CURB AND GUTTER	---
EX. RIBBON GUTTER	---
EX. DRIVEWAY	---
EX. STORM DRAIN	---
EX. EASEMENT	---
EX. CONTOUR	---
EX. FENCE	---
EX. CALTRANS GUARD RAIL	---
EX. STORM DRAIN PIPE	---
EX. STORM DRAIN STRUCTURE	---
EX. WATER PIPE	---
EX. WATER SERVICE	---
EX. WATER RPDA	---
EX. FIRE WATER PIPE	---
EX. FIRE WATER SERVICE	---
EX. FIRE WATER RPDA	---
EX. GATE VALVE	---
EX. SEWER PIPE	---
EX. SEWER MANHOLE	---
EX. SEWER SERVICE	---
EX. GAS PIPE	---
EX. ELECTRICAL UNDERGROUND	---
EX. OVERHEAD ELECTRICAL	---
EX. JOINT TRENCH	---
EX. OVERHEAD ELECTRICAL POLE	---
EX. OVERHEAD ELECTRICAL POLE WITH ST. LIGHT	---
EX. ORNAMENTAL ST. LIGHT	---
EX. BOLLARDS	---



MARINA NATASHA WURST, R.C.E. C67517	DATE	REGISTRATION	DESIGNED	PM
	5/11/2023	EXPIRES 6-30-25	BY: AM/JT	REVIEW: MW

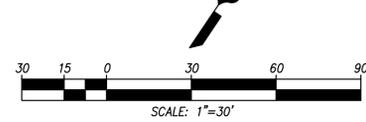
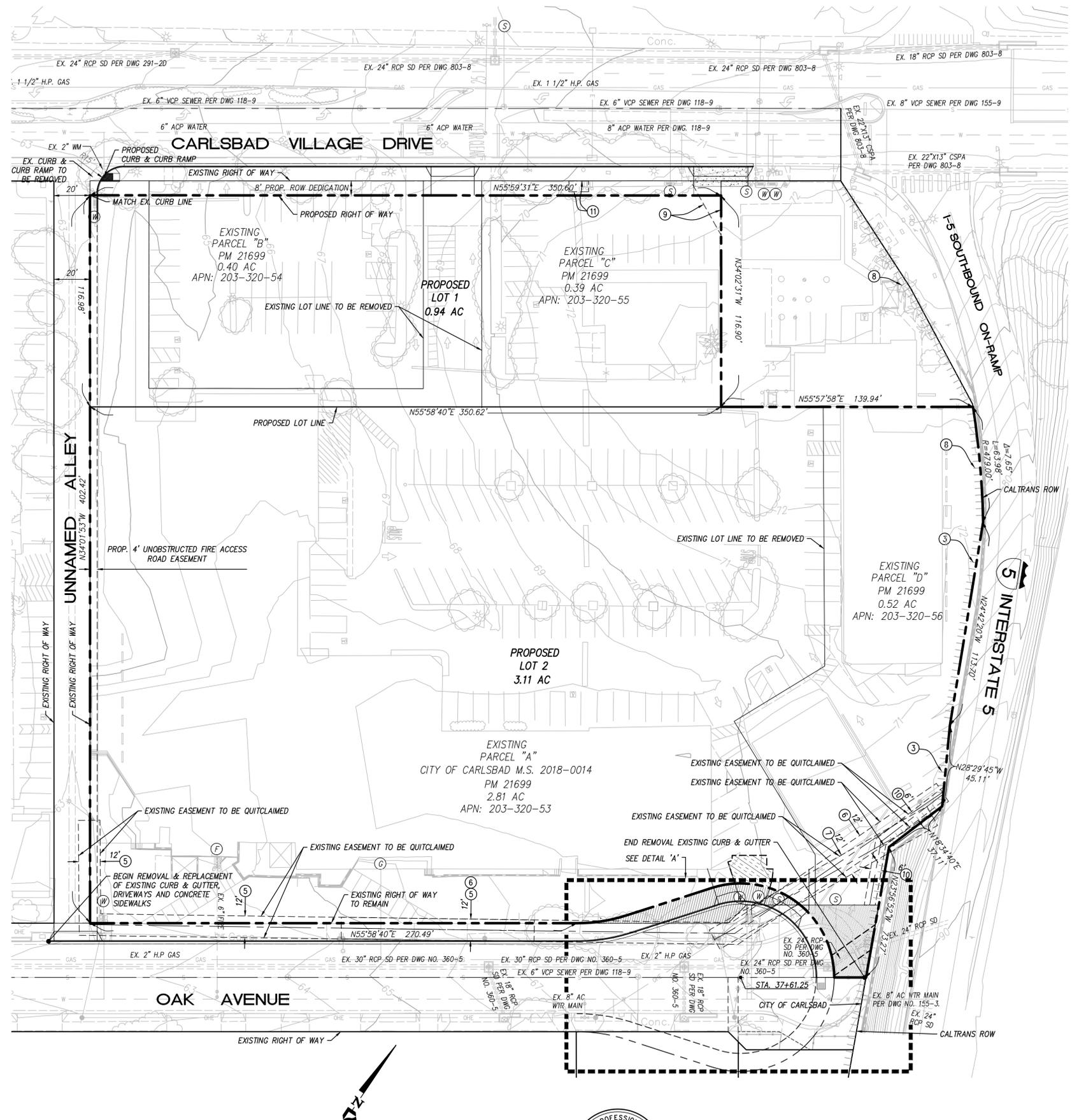
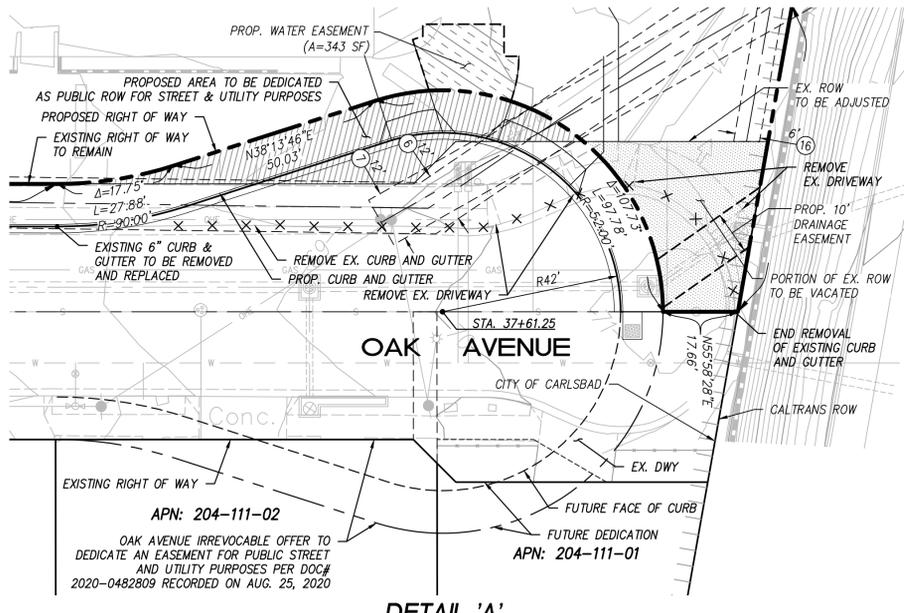
EASEMENTS/ENCUMBRANCES

NOTE: (F) INDICATES A PLOTTABLE ENCUMBRANCE.

THIS INFORMATION IS BASED ON COMMITMENT ORDER NO. NCS-1177654-ONT1 ISSUED BY FIRST AMERICAN TITLE COMPANY DATED AS OF MAY 03, 2023. ENCUMBRANCE ITEMS SUCH AS TAXES, LIENS, RIGHTS, LEASES, FACTS, AGREEMENTS AND TRANSACTIONS APPEARING IN SAID POLICY AS ITEMS NO.'S 1-13 HAVE BEEN REVIEWED AND ARE NOT CONSIDERED SURVEY RELATED, AND THEREFORE HAVE NOT BEEN SHOWN HEREON.

- (3) ABUTTER'S RIGHTS OF INGRESS AND EGRESS TO OR FROM FREEWAY HAVE BEEN RELINQUISHED IN THE DOCUMENT RECORDED APRIL 04, 1952 AS BOOK 4425, PAGE 246 OF OFFICIAL RECORDS. STATUS: TO REMAIN
- (5) AN EASEMENT FOR EITHER OR BOTH POLE LINES, FOR THE PURPOSES OF TRANSMISSION AND DISTRIBUTION OF ELECTRICITY, TOGETHER WITH THE RIGHT OF INGRESS AND EGRESS AND INCIDENTAL PURPOSES, RECORDED APRIL 13, 1964 AS INSTRUMENT NO. 66432 OF OFFICIAL RECORDS.
IN FAVOR OF: SAN DIEGO GAS AND ELECTRIC COMPANY
AFFECTS: AS DESCRIBED THEREIN
STATUS OF EASEMENT: TO BE PARTIALLY QUITCLAIMED PER SEPARATE DOCUMENT
- (6) AN EASEMENT FOR EITHER OR BOTH POLE LINES, FOR THE PURPOSES OF TRANSMISSION AND DISTRIBUTION OF ELECTRICITY, TOGETHER WITH THE RIGHT OF INGRESS AND EGRESS AND INCIDENTAL PURPOSES, RECORDED APRIL 13, 1964 AS INSTRUMENT NO. 66433 OF OFFICIAL RECORDS.
IN FAVOR OF: SAN DIEGO GAS AND ELECTRIC COMPANY
AFFECTS: AS DESCRIBED THEREIN
STATUS OF EASEMENT: TO BE QUITCLAIMED PER SEPARATE DOCUMENT
- (7) AN EASEMENT FOR EITHER OR BOTH POLE LINES, FOR THE PURPOSES OF TRANSMISSION AND DISTRIBUTION OF ELECTRICITY, TOGETHER WITH THE RIGHT OF INGRESS AND EGRESS AND INCIDENTAL PURPOSES, RECORDED DECEMBER 30, 1968 AS INSTRUMENT NO. 228130 OF OFFICIAL RECORDS.
IN FAVOR OF: SAN DIEGO GAS AND ELECTRIC COMPANY
AFFECTS: AS DESCRIBED THEREIN
STATUS OF EASEMENT: TO BE QUITCLAIMED PER SEPARATE DOCUMENT
- (8) ABUTTER'S RIGHTS OF INGRESS AND EGRESS TO OR FROM FREEWAY HAVE BEEN RELINQUISHED IN THE DOCUMENT RECORDED APRIL 01, 1969 AS INSTRUMENT NO. 54612 OF OFFICIAL RECORDS. STATUS: TO REMAIN
- (9) NON-EXCLUSIVE EASEMENT FOR INGRESS AND EGRESS DESIGNATED AS "PARCEL 2" IN AN UNRECORDED LEASE DATED OCTOBER 09, 1968, EXECUTED BY PETER D. AGUILAR AND LUISE W. AGUILAR, HIS WIFE AS LESSOR AND TEXACO INC., A DELAWARE CORPORATION AS LESSEE, AS DISCLOSED BY A MEMORANDUM OF LEASE RECORDED JUNE 25, 1969 AS INSTRUMENT NO. 114508 OF OFFICIAL RECORDS. STATUS OF EASEMENT: TO REMAIN
- (10) AN EASEMENT FOR A LINE OF PIPE AND ALL NECESSARY AND PROPER FIXTURES AND EQUIPMENT FOR USE IN CONNECTION THEREWITH, FOR THE TRANSMISSION AND DISTRIBUTION OF GAS AND FOR ALL PURPOSES CONNECTED THEREWITH TOGETHER WITH THE RIGHT OF INGRESS THERETO AND EGRESS THEREFROM AND INCIDENTAL PURPOSES, RECORDED JANUARY 19, 1970 AS INSTRUMENT NO. 9649 OF OFFICIAL RECORDS.
IN FAVOR OF: SAN DIEGO GAS AND ELECTRIC COMPANY, A CORPORATION
AFFECTS: AS DESCRIBED THEREIN
NOTE: APPROXIMATE LOCATION PLOTTED HEREON BASED UPON PM 21699
STATUS OF EASEMENT: TO BE QUITCLAIMED PER SEPARATE DOCUMENT
- (11) AN EASEMENT FOR THE TRANSMISSION AND DISTRIBUTION OF ELECTRICITY FOR ALL PURPOSES, UNDERGROUND FACILITIES CONSISTING OF CONDUITS, MANHOLES, HANDHOLES AND JUNCTION BOXES WITH WIRES AND CABLES PLACED THEREIN AND ABOVEGROUND STRUCTURES CONSISTING OF PADS, TRANSFORMERS, AND OTHER ABOVEGROUND STRUCTURES NECESSARY FOR THE OPERATION OF SAID UNDERGROUND FACILITIES AND ALL MISCELLANEOUS EQUIPMENT, AND MATERIAL CONNECTED THEREWITH, TOGETHER WITH THE RIGHT OF INGRESS THERETO AND EGRESS AND INCIDENTAL PURPOSES, RECORDED DECEMBER 27, 1971 AS INSTRUMENT NO. 301142 OF OFFICIAL RECORDS.
IN FAVOR OF: SAN DIEGO GAS AND ELECTRIC COMPANY, A CORPORATION
AFFECTS: AS DESCRIBED THEREIN
STATUS OF EASEMENT: TO BE QUITCLAIMED PER SEPARATE DOCUMENT

DETAIL 'A' NOTE:
SEE SHEET 4 FOR PROPOSED GRADING FOR NEW CURB CUL-DE-SAC.



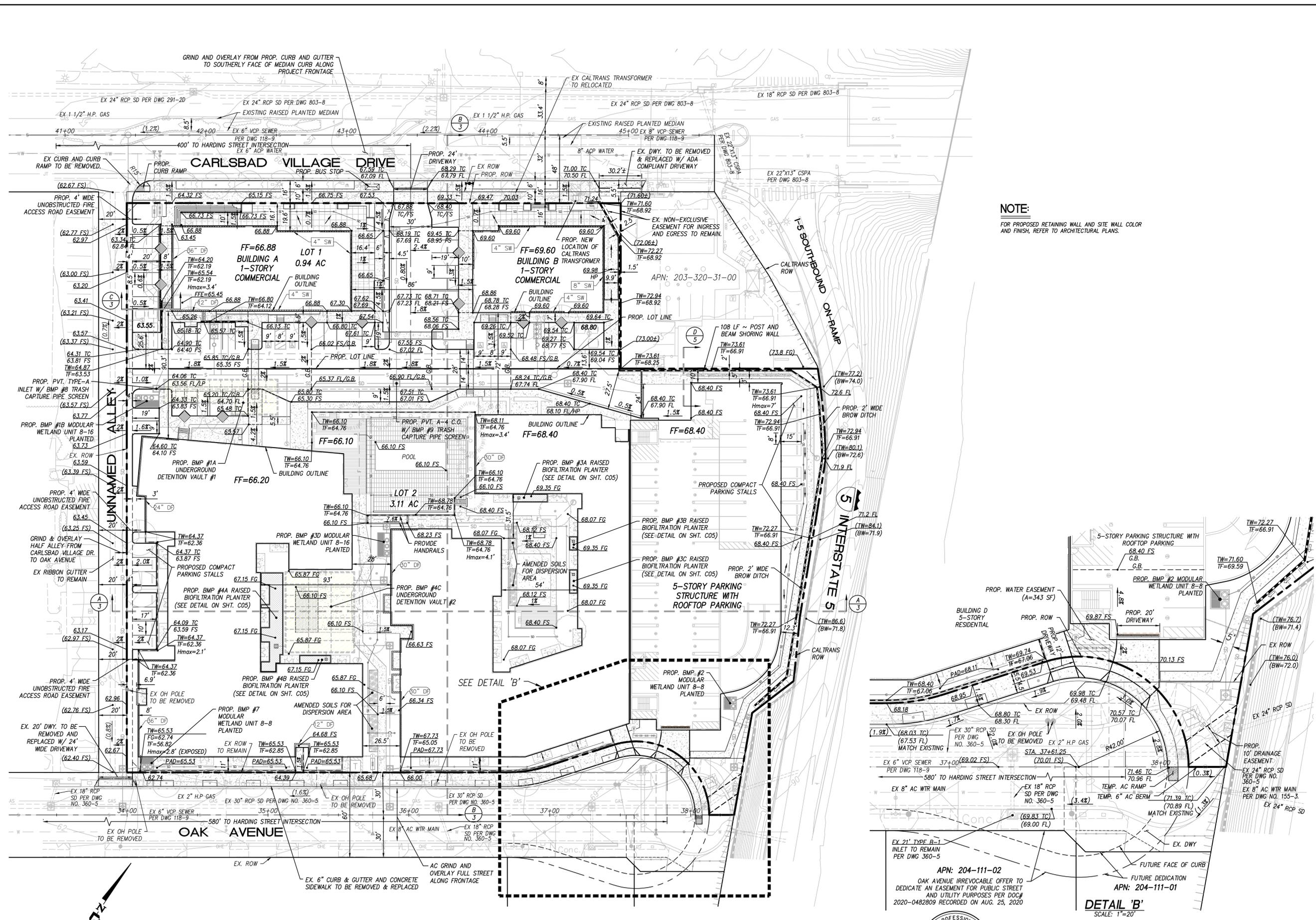
MARINA NATASHA WURST, R.C.E. C67517	DATE REGISTRATION EXPIRES 6-30-25	DESIGNED BY: AM/JT	PM REVIEW: MW
<i>Ma wurst</i>	5/11/2023		

PROJECT DESIGN CONSULTANTS
a BOWMAN company
Planning | Landscape Architecture | Engineering | Survey

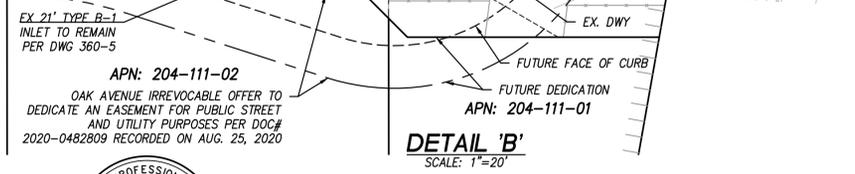
CVMU
945-1065 CARLSBAD VILLAGE DRIVE
CARLSBAD, CA, 92008
GRT CARLSBAD VILLAGE LLC
11661 SAN VICENTE BLVD, SUITE 850
LOS ANGELES CA 90049

SUBMITAL:
SDP 2023-0014
JOB NUMBER:
4488.00
DATE:
11.13.2023
REVISIONS:

SHEET TITLE:
PDC EXISTING ENCROACHMENTS, PROPOSED LOTS & DEDICATIONS
SHEET NUMBER:
C02



NOTE:
FOR PROPOSED RETAINING WALL AND SITE WALL COLOR AND FINISH, REFER TO ARCHITECTURAL PLANS.

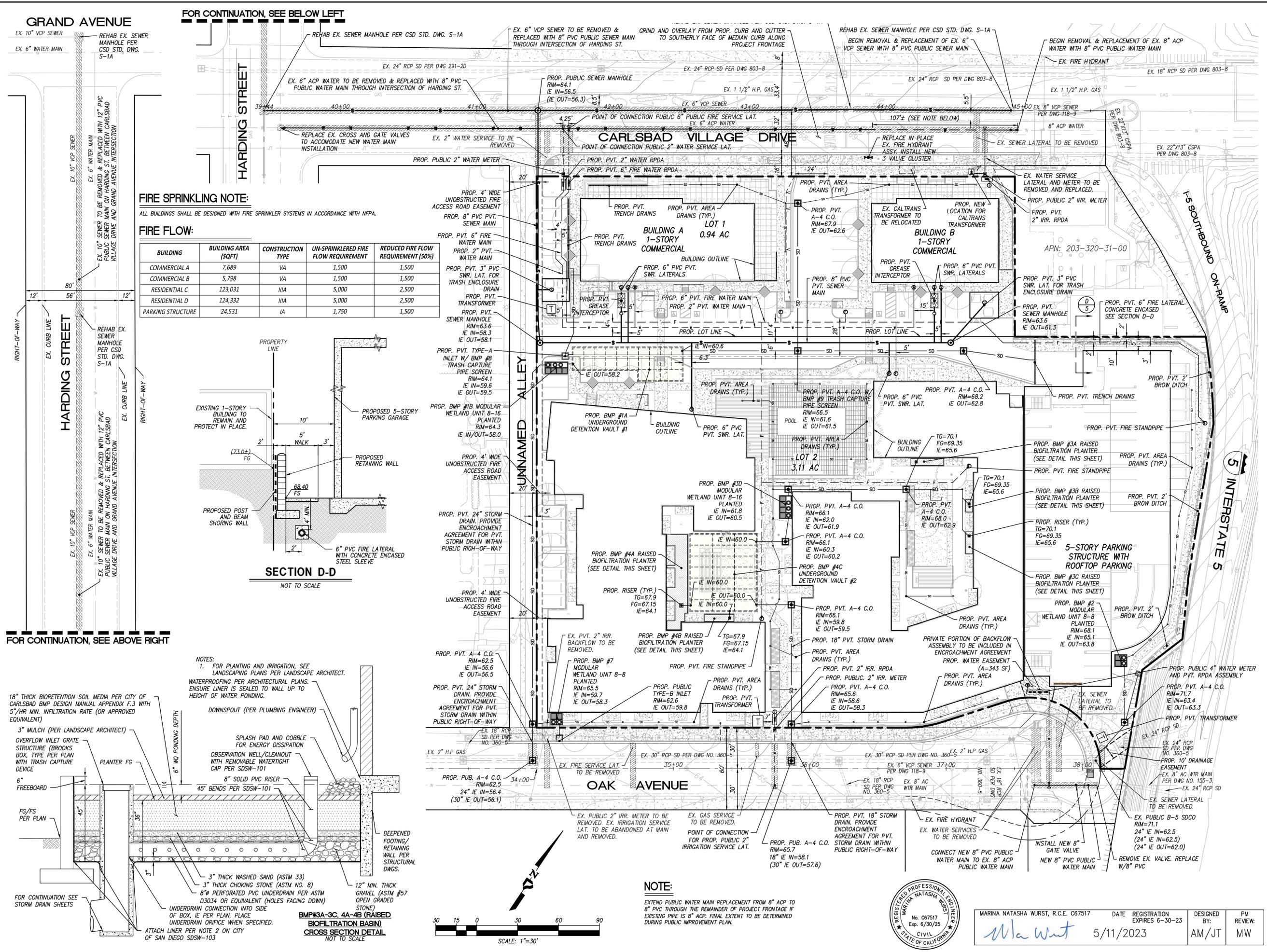


APN: 204-111-02
OAK AVENUE IRREVOCABLE OFFER TO DEDICATE AN EASEMENT FOR PUBLIC STREET AND UTILITY PURPOSES PER DOC# 2020-0482809 RECORDED ON AUG. 25, 2020

APN: 204-111-01

REGISTERED PROFESSIONAL ENGINEER
MARINA NATASHA WURST
No. 057517
Exp. 6/30/25
CIVIL
STATE OF CALIFORNIA

MARINA NATASHA WURST, R.C.E. C67517
DATE REGISTRATION EXPIRES 6-30-23
DESIGNED BY: AM/JT
PM REVIEW: MW

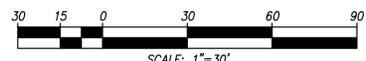
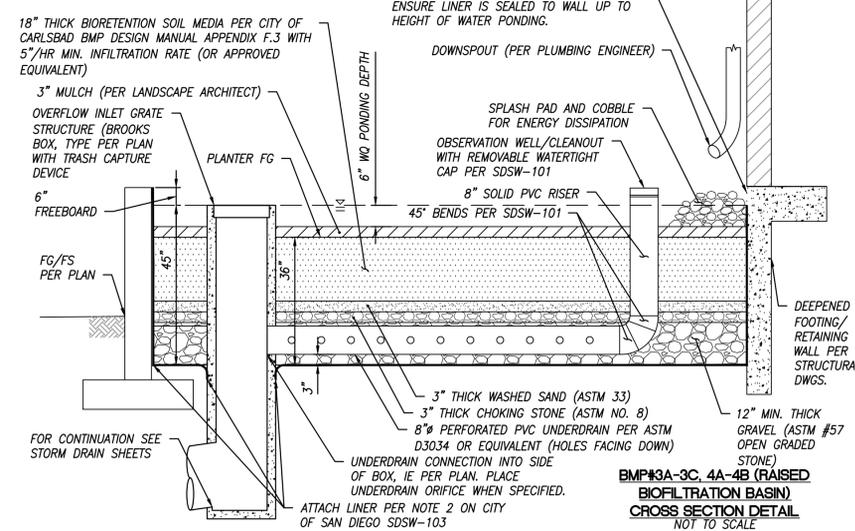


FIRE SPRINKLING NOTE:
ALL BUILDINGS SHALL BE DESIGNED WITH FIRE SPRINKLER SYSTEMS IN ACCORDANCE WITH NFPA.

FIRE FLOW:

BUILDING	BUILDING AREA (SQFT)	CONSTRUCTION TYPE	UN-SPRINKLERED FIRE FLOW REQUIREMENT	REDUCED FIRE FLOW REQUIREMENT (50%)
COMMERCIAL A	7,689	VA	1,500	1,500
COMMERCIAL B	5,798	VA	1,500	1,500
RESIDENTIAL C	123,031	IIIA	5,000	2,500
RESIDENTIAL D	124,332	IIIA	5,000	2,500
PARKING STRUCTURE	24,531	IA	1,750	1,500

NOTES:
1. FOR PLANTING AND IRRIGATION, SEE LANDSCAPING PLANS PER LANDSCAPE ARCHITECT.
WATERPROOFING PER ARCHITECTURAL PLANS. ENSURE LINER IS SEALED TO WALL UP TO HEIGHT OF WATER PONDING.





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CULVER CITY, CA 90232
310.399.7975
KFALOSANGELES.COM

CVMU
945-1065 CARLSBAD VILLAGE DRIVE
CARLSBAD, CA 92008

GRT CARLSBAD VILLAGE, LLC
11661 SAN VICENTE BLVD., SUITE 850
LOS ANGELES CA 90049

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SDP 2023-0014

JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISIONS:

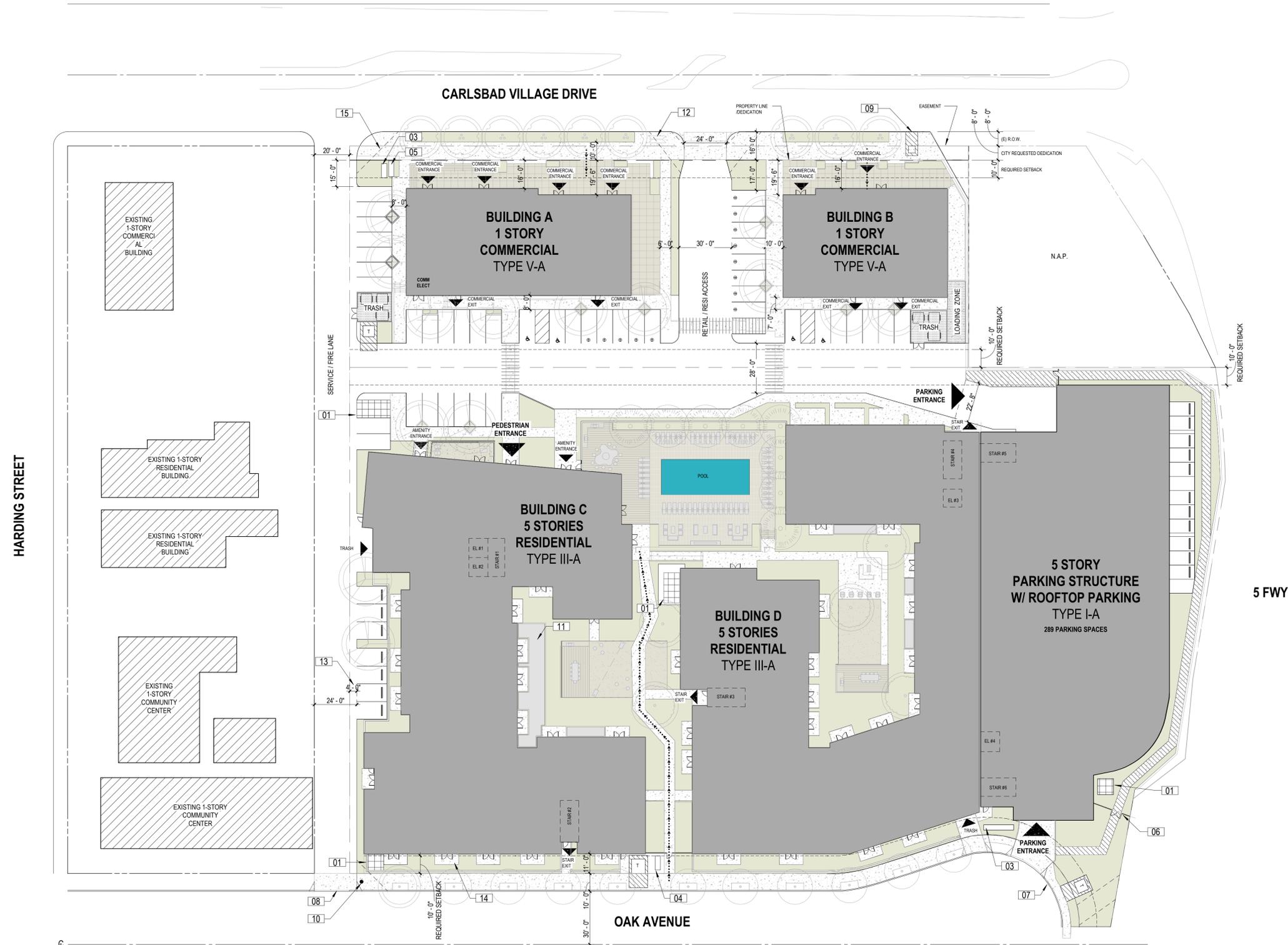
SHEET TITLE:
SITE PLAN

SHEET NUMBER:
A00

Item #1 186 of 269

NOTES

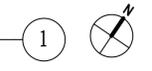
- 01 MODULAR WETLAND UNIT
- 02 STANDPIPE
- 03 DOMESTIC BACKFLOW PREVENTER
- 04 IRRIGATION BACKFLOW PREVENTER
- 05 FIRE BACKFLOW PREVENTER
- 06 KNOX BOX FOR FIRE DEPARTMENT
- 07 NEW CUL-DE-SAC CURB, SEE CIVIL DRAWINGS
- 08 (E) CURB CUT
- 09 RELOCATED CALTRANS TRANSFORMER
- 10 (E) POWER POLE TO BE REMOVED
- 11 BMP RAISED FLOW THRU PLANTER, SEE CIVIL AND LANDSCAPE DRAWINGS
- 12 (E) NCTD BUS STOP LOCATION
- 13 UNOBSTRUCTED FIRE ACCESS ROAD EASEMENT
- 14 TENANT PRIVATE COURTYARD, TYP.
- 15 VISIBILITY TRIANGLE, TYP.



LEGEND

- TRANSFORMER WITH REQUIRED CLEARANCE
- GROUND FLOOR FOOTPRINT
- EXISTING BUILDING
- ACCESSIBLE PATH OF TRAVEL
- HOSE PULL

SITE PLAN
1" = 30'-0"



CARLSBAD VILLAGE DRIVE



FIRST FLOOR PLAN
3/64" = 1'-0"

NOTES

- 01 MODULAR WETLAND UNIT
- 02 STANDPIPE
- 03 DOMESTIC BACKFLOW PREVENTER
- 04 IRRIGATION BACKFLOW PREVENTER
- 05 FIRE BACKFLOW PREVENTER
- 06 EXTENT OF GARAGE STRUCTURE ABOVE
- 07 TRANSFORMER
- 08 ROOF OVERHANG ABOVE
- 09 COMPOSITE TRELLIS ABOVE
- 10 PEDESTRIAN GATE AND FENCE
- 11 TRASH ENCLOSURE, SEE 1/A21
- 12 TRASH ENCLOSURE, SEE 2/A24
- 13 TENANT PRIVATE COURTYARD, TYP.
- 14 CLEAR ZONE LINE OF SIGHT PER VBM 2.6.1(A)(4), TYP.
- 15 METAL FENCE
- 16 OVERHEAD ROLLING GATE
- 17 METAL CANOPY
- 18 PARKING BARRIER GATE

SHEET NOTES

- A. ALL PARKING STALLS TO HAVE A MAXIMUM OF 5% SLOPE PER CITY ENGINEERING STANDARDS. ADA PARKING STALLS AND STRIPING NOT TO EXCEED 2%.
- B. ALL DOORWAY CLEAR LANDINGS NOT TO EXCEED 2% SLOPE

UNIT COUNT

3	STUDIO
13	1 BR
22	2 BR
1	3 BR
TOTAL UNITS: 39	

PARKING LEGEND

RES	RESIDENTIAL
COMM	COMMERCIAL
S	STANDARD
DAS	ACCESSIBLE
C	COMPACT
INS	EV CHARGER INSTALLED
CAP	EV CAPABLE
RDY	EV READY



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SDP 2023-0014

JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISIONS:

SHEET TITLE:
FIRST FLOOR PLAN

SHEET NUMBER:
A10



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JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISIONS:

SHEET TITLE:

SECOND FLOOR PLAN

SHEET NUMBER:

A11

Item #1 188 of 269

NOTES

01 FUTURE EQUIPMENT AREA

SHEET NOTES

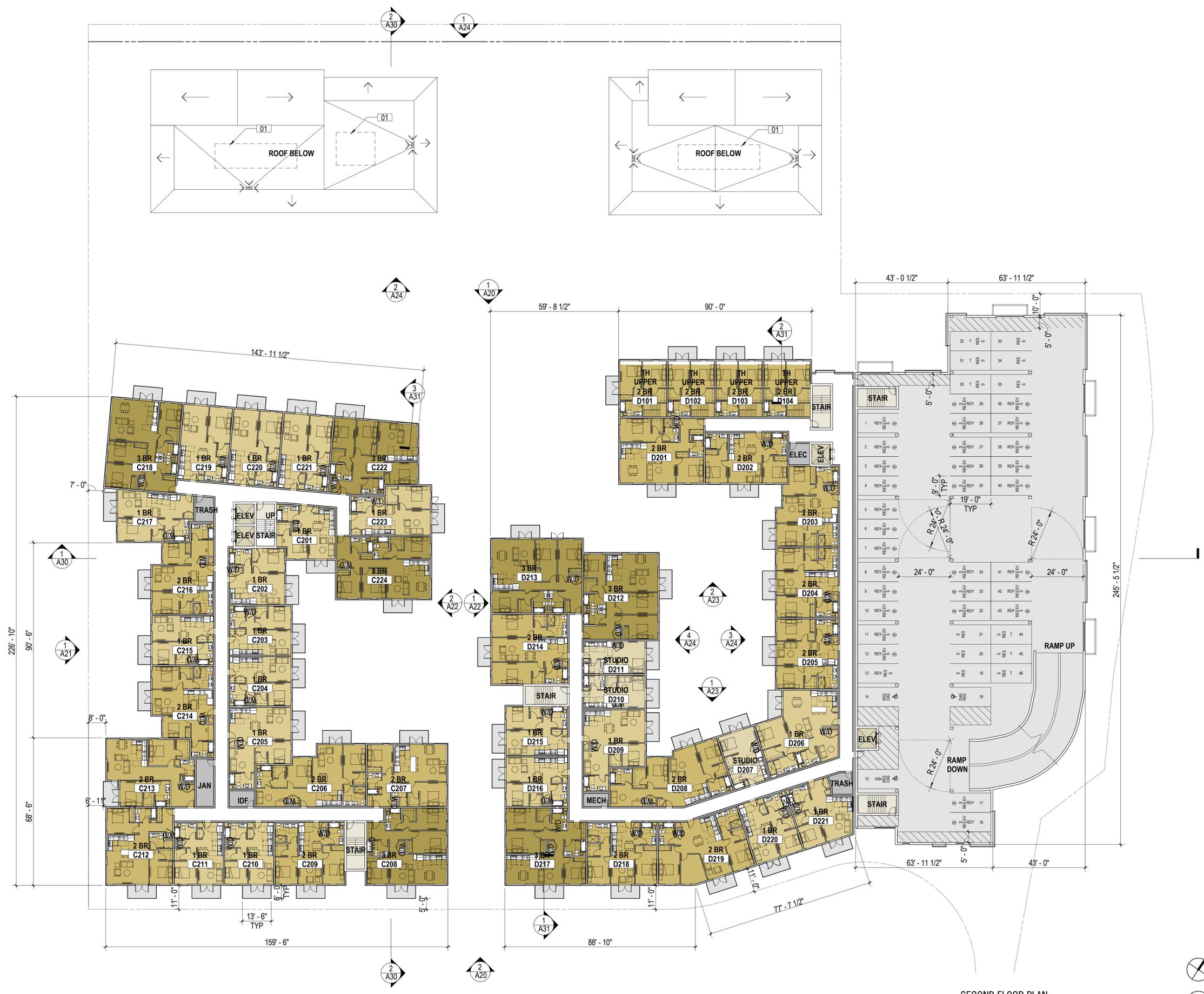
- A. ALL PARKING STALLS TO HAVE A MAXIMUM OF 5% SLOPE PER CITY ENGINEERING STANDARDS. ADA PARKING STALLS AND STRIPING NOT TO EXCEED 2%.
- B. ALL DOORWAY CLEAR LANDINGS NOT TO EXCEED 2% SLOPE

UNIT COUNT

2ND FLOOR	* UNITS D101, D102, D103, D104 ARE 2-STORY UNITS THAT ARE COUNTED ON SHEET A10.
3 STUDIO	
19 1 BR	
16 2 BR	
7 3 BR	
TOTAL UNITS: 45 *	

PARKING LEGEND

RES	RESIDENTIAL
COMM	COMMERCIAL
S	STANDARD
DAS	ACCESSIBLE
C	COMPACT
INS	EV CHARGER INSTALLED
CAP	EV CAPABLE
RDY	EV READY



SECOND FLOOR PLAN
3/64" = 1'-0"



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JOB NUMBER:
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DATE:
11.13.2023

REVISIONS:

SHEET TITLE:
THIRD FLOOR PLAN

SHEET NUMBER:
A12

Item #1 189 of 269

NOTES

SHEET NOTES

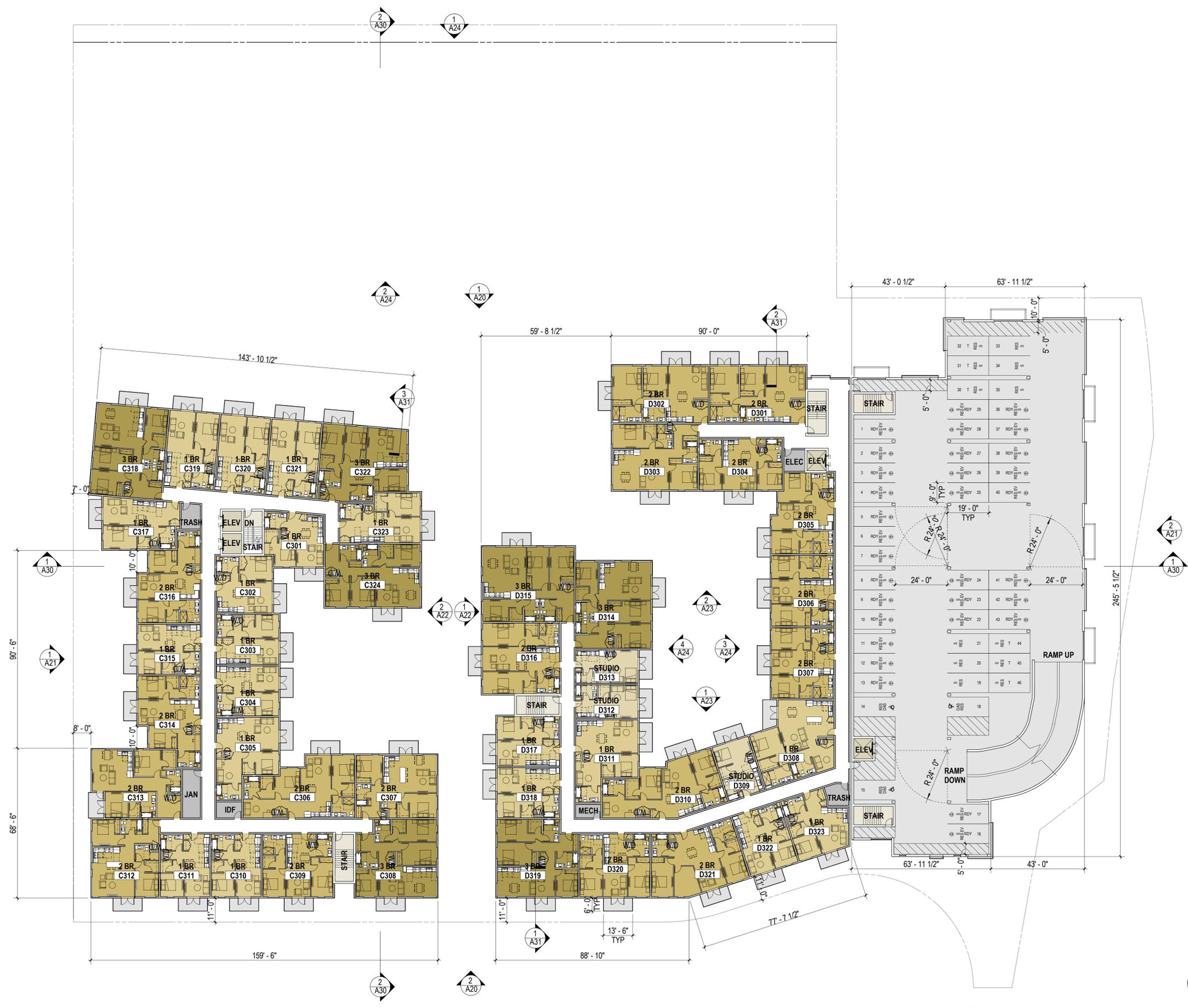
- A. ALL PARKING STALLS TO HAVE A MAXIMUM OF 5% SLOPE PER CITY ENGINEERING STANDARDS. ADA PARKING STALLS AND STRIPING NOT TO EXCEED 2%.
- B. ALL DOORWAY CLEAR LANDINGS NOT TO EXCEED 2% SLOPE

UNIT COUNT

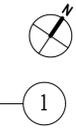
3RD FLOOR	
3 STUDIO	
19 1 BR	
18 2 BR	
7 3 BR	
TOTAL UNITS: 47	

PARKING LEGEND

RES	RESIDENTIAL
COMM	COMMERCIAL
S	STANDARD
DAS	ACCESSIBLE
C	COMPACT
INS	EV CHARGER INSTALLED
CAP	EV CAPABLE
RDY	EV READY



THIRD FLOOR PLAN
3/64" = 1'-0"





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JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISIONS:

SHEET TITLE:
FOURTH FLOOR PLAN

SHEET NUMBER:
A13

Item #1 190 of 269

NOTES

01 PER VILLAGE BARRIO MASTER PLAN 2.7.4.G.2.a,
70% OF 4TH STORY SHALL BE SETBACK 15'-0"

SHEET NOTES

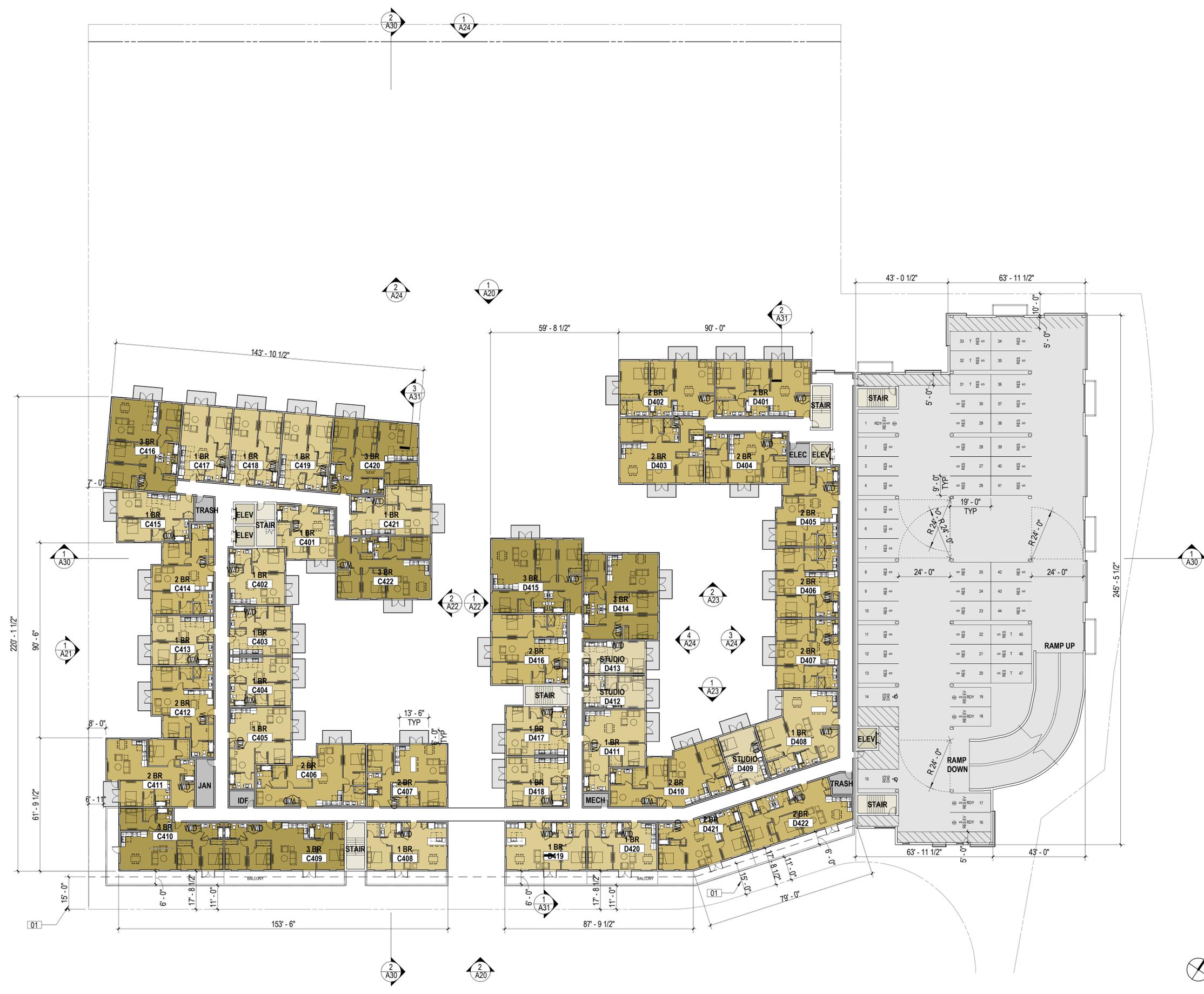
- A. ALL PARKING STALLS TO HAVE A MAXIMUM OF 5% SLOPE PER CITY ENGINEERING STANDARDS. ADA PARKING STALLS AND STRIPING NOT TO EXCEED 2%.
- B. ALL DOORWAY CLEAR LANDINGS NOT TO EXCEED 2% SLOPE

UNIT COUNT

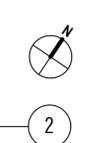
4TH FLOOR	
3 STUDIO	
18 1 BR	
16 2 BR	
7 3 BR	
TOTAL UNITS: 44	

PARKING LEGEND

RES	RESIDENTIAL
COMM	COMMERCIAL
S	STANDARD
DAS	ACCESSIBLE
C	COMPACT
INS	EV CHARGER INSTALLED
CAP	EV CAPABLE
RDY	EV READY



FOURTH FLOOR PLAN
3/64" = 1'-0"





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JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISION:

SHEET TITLE:
FIFTH FLOOR PLAN

SHEET NUMBER:
A14

Item #1 191 of 269

NOTES

SHEET NOTES

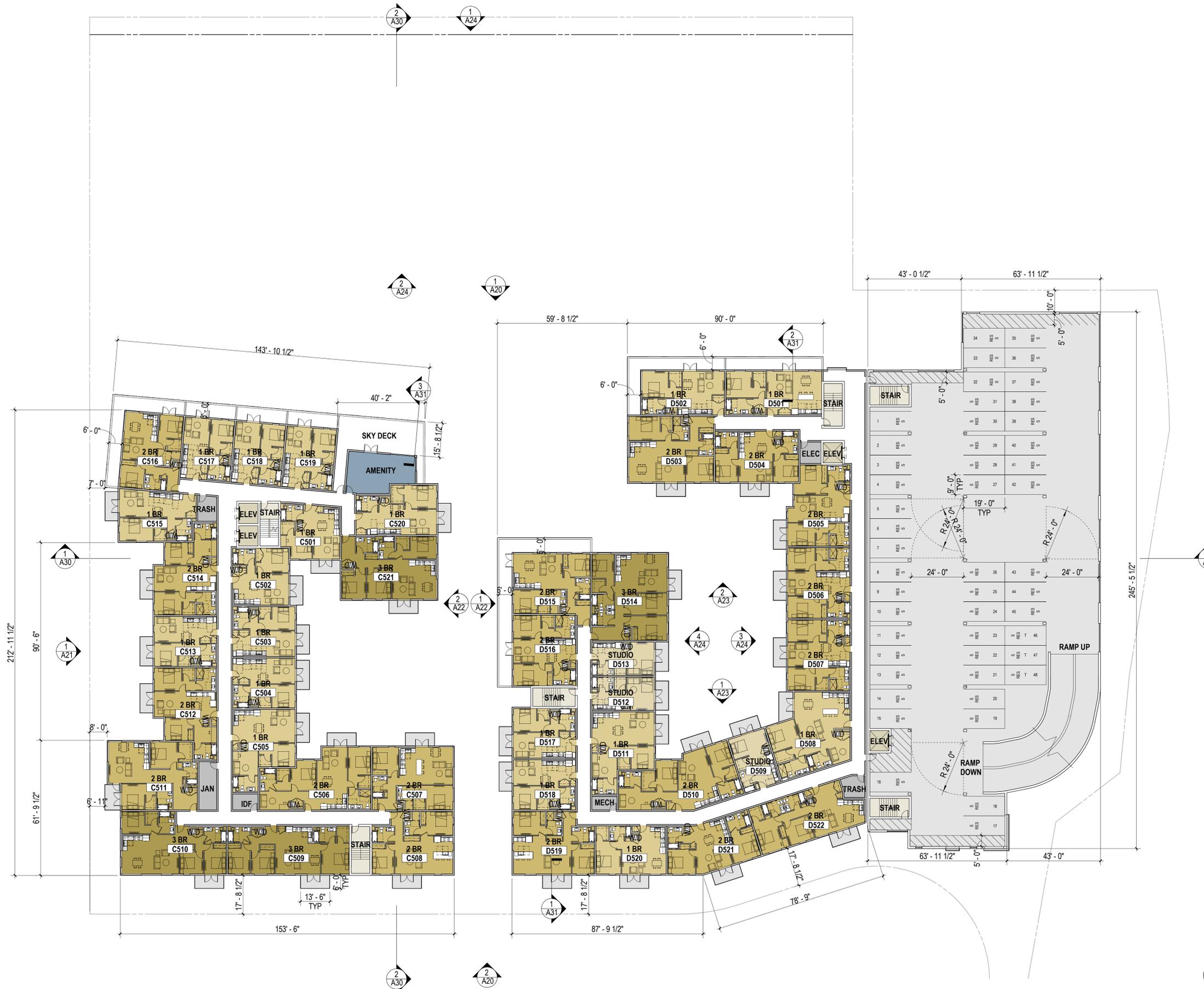
- A. ALL PARKING STALLS TO HAVE A MAXIMUM OF 5% SLOPE PER CITY ENGINEERING STANDARDS. ADA PARKING STALLS AND STRIPING NOT TO EXCEED 2%.
- B. ALL DOORWAY CLEAR LANDINGS NOT TO EXCEED 2% SLOPE

UNIT COUNT

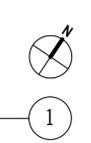
5TH FLOOR
3 STUDIO
18 1 BR
18 2 BR
4 3 BR
TOTAL UNITS: 43

PARKING LEGEND

RES	RESIDENTIAL
COMM	COMMERCIAL
S	STANDARD
DAS	ACCESSIBLE
C	COMPACT
INS	EV CHARGER INSTALLED
CAP	EV CAPABLE
RDY	EV READY

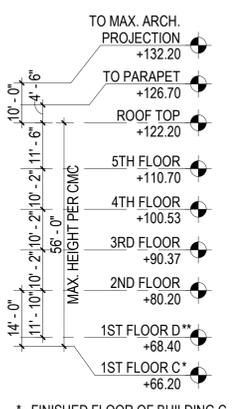


FIFTH FLOOR PLAN
3/64" = 1'-0"



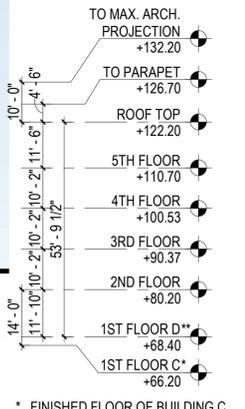


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Elevation NORTH
3/64" = 1'-0"

1



Elevation SOUTH
3/64" = 1'-0"

2

MATERIAL LEGEND

(PL1) STUCCO, COLOR WHITE	(VS1) VERTICAL SIDING BOARD, COLOR WHITE	(WN1) VINYL WINDOW COLOR DARK BRONZE
(PL2) STUCCO, COLOR LIGHT GRAY	(R1) METAL ROOF, COLOR DARK GRAY	(WN2) STOREFRONT WINDOW COLOR DARK BRONZE
(PL3) STUCCO, COLOR LIGHT BEIGE	(T1) COMPOSITE TRELLIS COLOR DARK GRAY / WHITE	
(PL4) STUCCO, COLOR DARK GRAY	(T2) METAL LOUVERS, COLOR BROWN	
(PL5) STUCCO, COLOR DARK BEIGE	(GR1) METAL GUARDRAIL, COLOR WHITE	
(HS1) HORIZONTAL SIDING BOARD, COLOR WHITE	(GR2) METAL GUARDRAIL, COLOR GRAY	
(HS2) HORIZONTAL SIDING BOARD, COLOR BEIGE	(F1) METAL FENCE, COLOR BLACK	
(HS3) HORIZONTAL SIDING BOARD, COLOR BROWN	(S1) COMPOSITE SHUTTERS COLOR LIGHT BEIGE	

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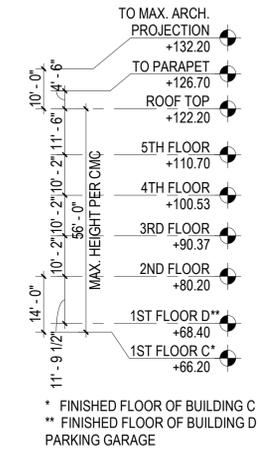
REVISIONS:

SHEET TITLE: ELEVATIONS

SHEET NUMBER: **A20**

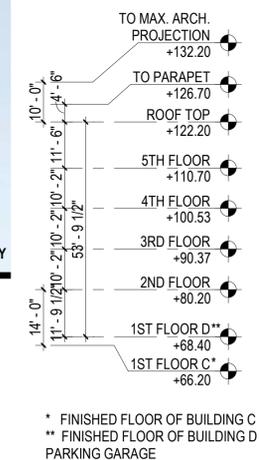


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Elevation NORTH - Courtyards
3/64" = 1'-0"

1



Elevation SOUTH - Courtyards
3/64" = 1'-0"

2

MATERIAL LEGEND

PL1	STUCCO, COLOR WHITE	VS1	VERTICAL SIDING BOARD, COLOR WHITE	WN1	VINYL WINDOW COLOR DARK BRONZE
PL2	STUCCO, COLOR LIGHT GRAY	R1	METAL ROOF, COLOR DARK GRAY	WN2	STOREFRONT WINDOW COLOR DARK BRONZE
PL3	STUCCO, COLOR LIGHT BEIGE	T1	COMPOSITE TRELLIS COLOR DARK GRAY / WHITE		
PL4	STUCCO, COLOR DARK GRAY	T2	METAL LOUVERS, COLOR BROWN		
PL5	STUCCO, COLOR DARK BEIGE	GR1	METAL GUARDRAIL, COLOR WHITE		
HS1	HORIZONTAL SIDING BOARD, COLOR WHITE	GR2	METAL GUARDRAIL, COLOR GRAY		
HS2	HORIZONTAL SIDING BOARD, COLOR BEIGE	F1	METAL FENCE, COLOR BLACK		
HS3	HORIZONTAL SIDING BOARD, COLOR BROWN	S1	COMPOSITE SHUTTERS COLOR LIGHT BEIGE		

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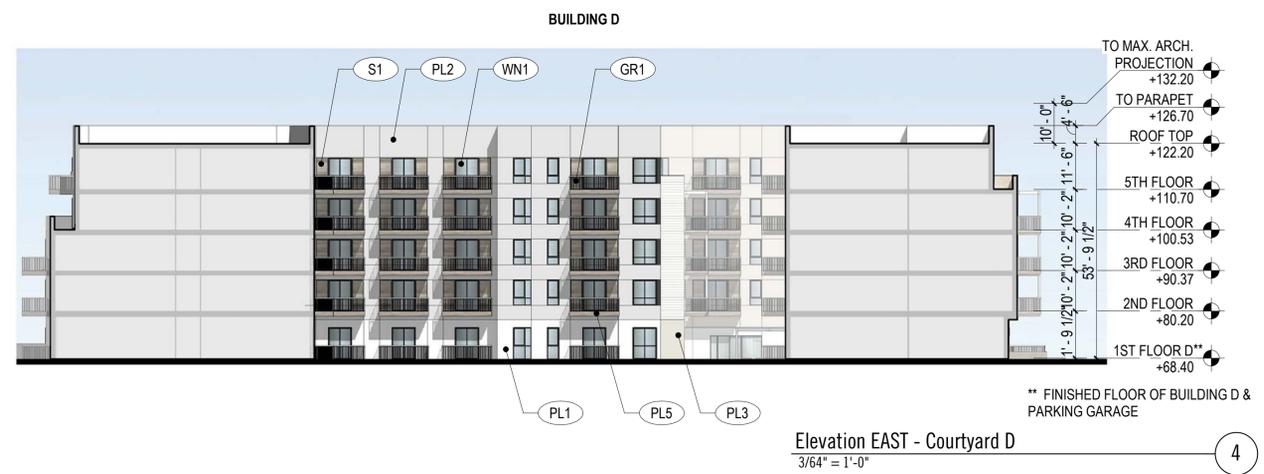
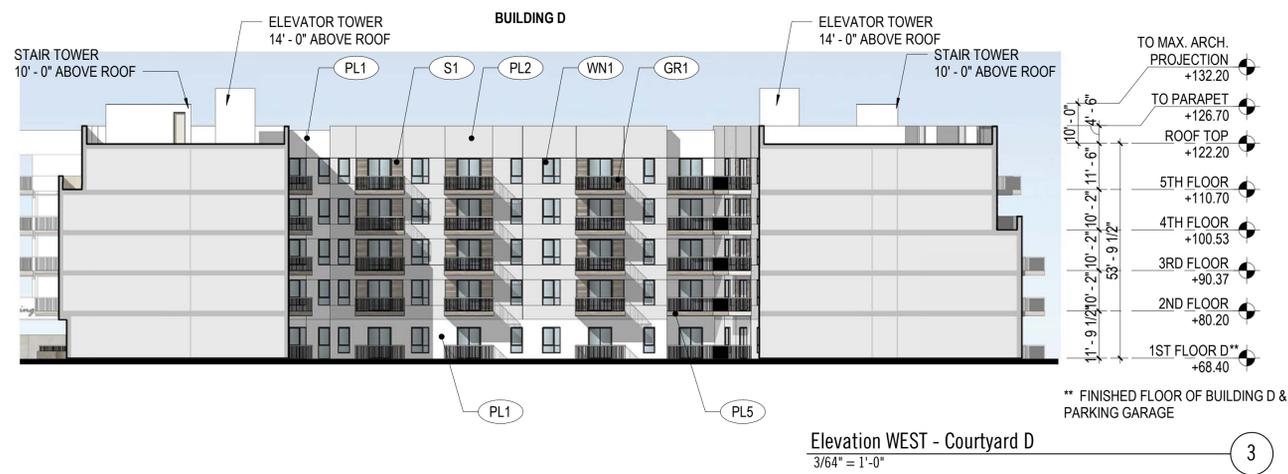
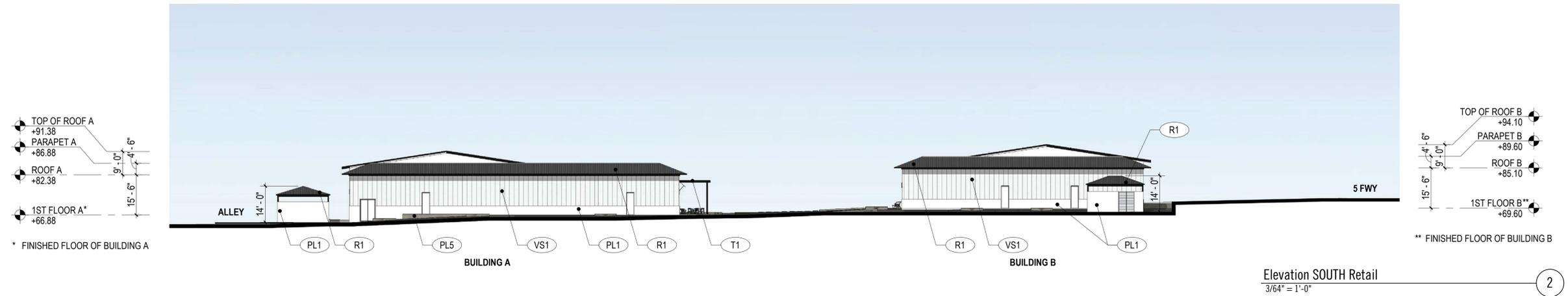
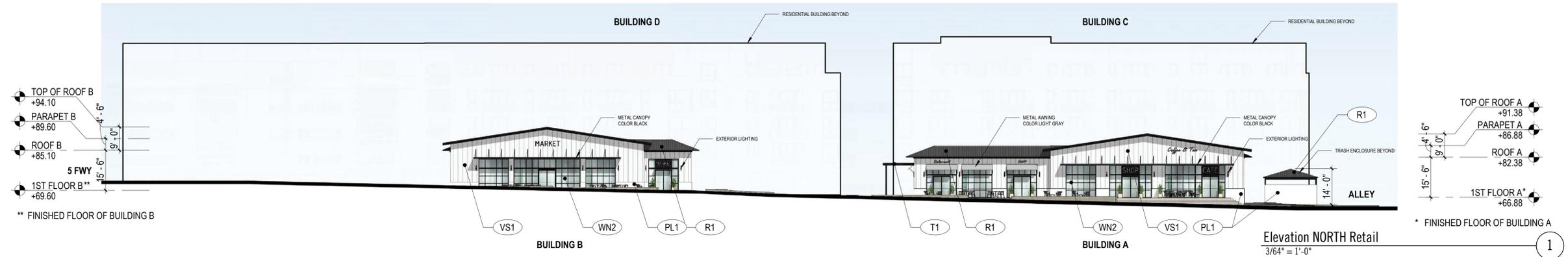
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ELEVATIONS

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A24

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MATERIAL LEGEND

PL1	STUCCO, COLOR WHITE	VS1	VERTICAL SIDING BOARD, COLOR WHITE	WN1	VINYL WINDOW COLOR DARK BRONZE
PL2	STUCCO, COLOR LIGHT GRAY	R1	METAL ROOF, COLOR DARK GRAY	WN2	STOREFRONT WINDOW COLOR DARK BRONZE
PL3	STUCCO, COLOR LIGHT BEIGE	T1	COMPOSITE TRELLIS COLOR DARK GRAY / WHITE		
PL4	STUCCO, COLOR DARK GRAY	T2	METAL LOUVERS, COLOR BROWN		
PL5	STUCCO, COLOR DARK BEIGE	GR1	METAL GUARDRAIL, COLOR WHITE		
HS1	HORIZONTAL SIDING BOARD, COLOR WHITE	GR2	METAL GUARDRAIL, COLOR GRAY		
HS2	HORIZONTAL SIDING BOARD, COLOR BEIGE	F1	METAL FENCE, COLOR BLACK		
HS3	HORIZONTAL SIDING BOARD, COLOR BROWN	S1	COMPOSITE SHUTTERS COLOR LIGHT BEIGE		



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A27





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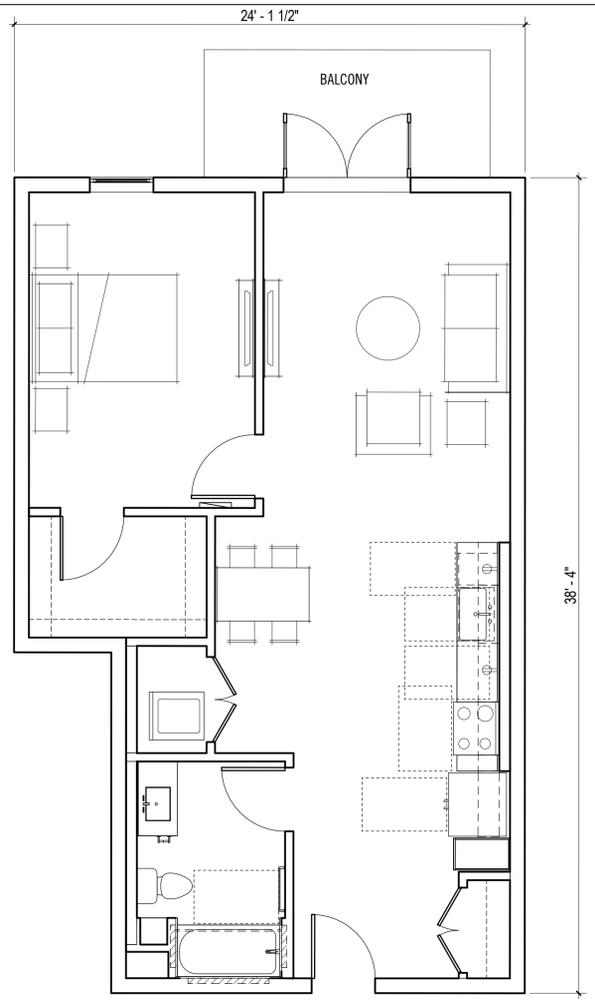
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RENDERING

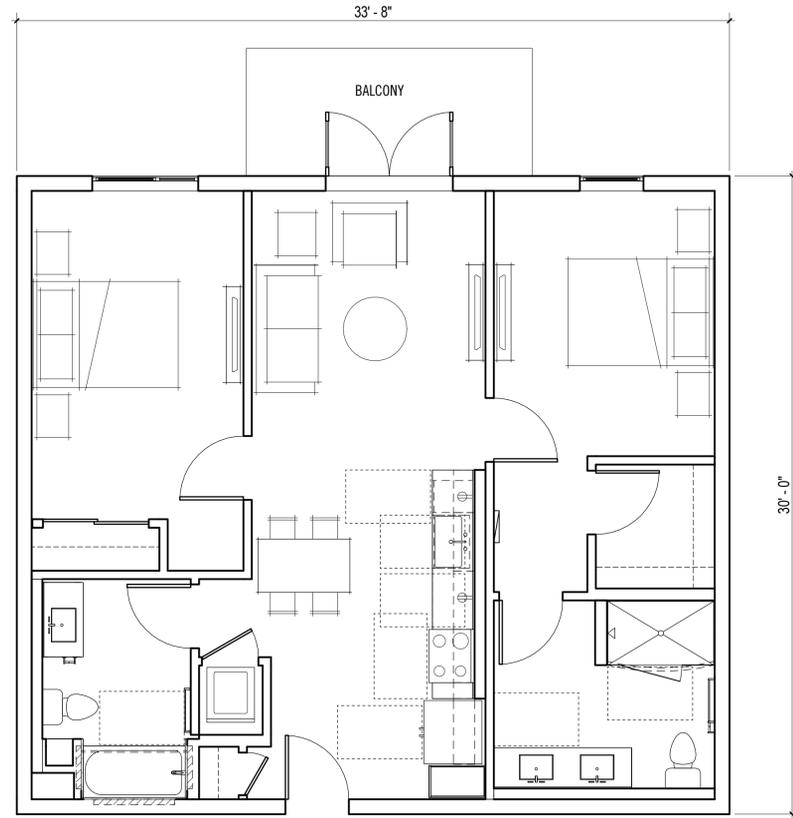
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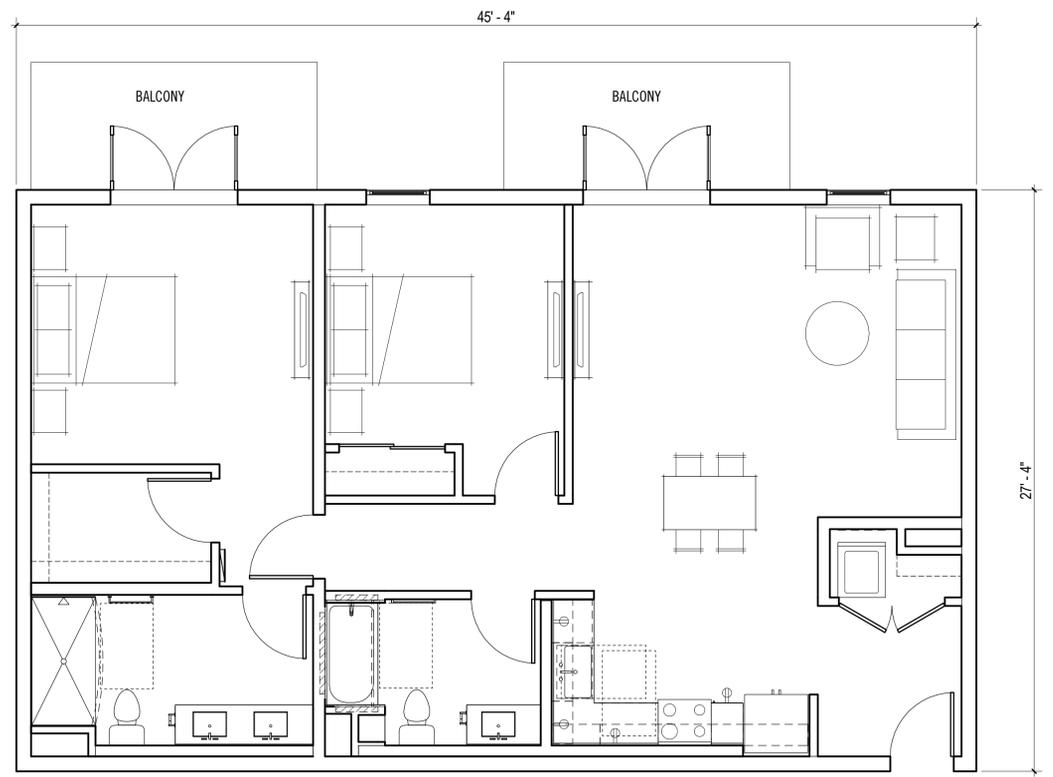
1BR - TYPE B9
1/4" = 1'-0" 765 SF 10



2BR - TYPE C1
1/4" = 1'-0" 1,001 SF 2



2BR - TYPE C2
1/4" = 1'-0" 915 SF 12



2BR - TYPE C3
1/4" = 1'-0" 1,124 SF 4

NOTES



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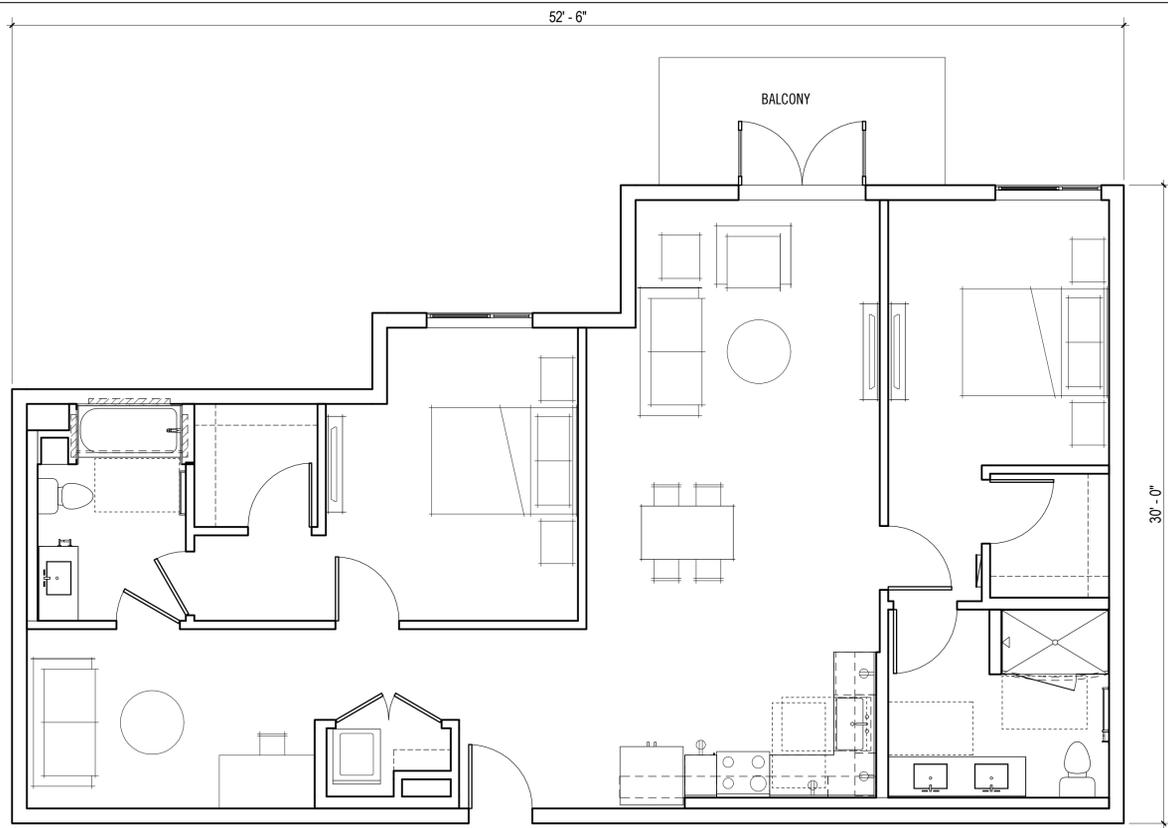
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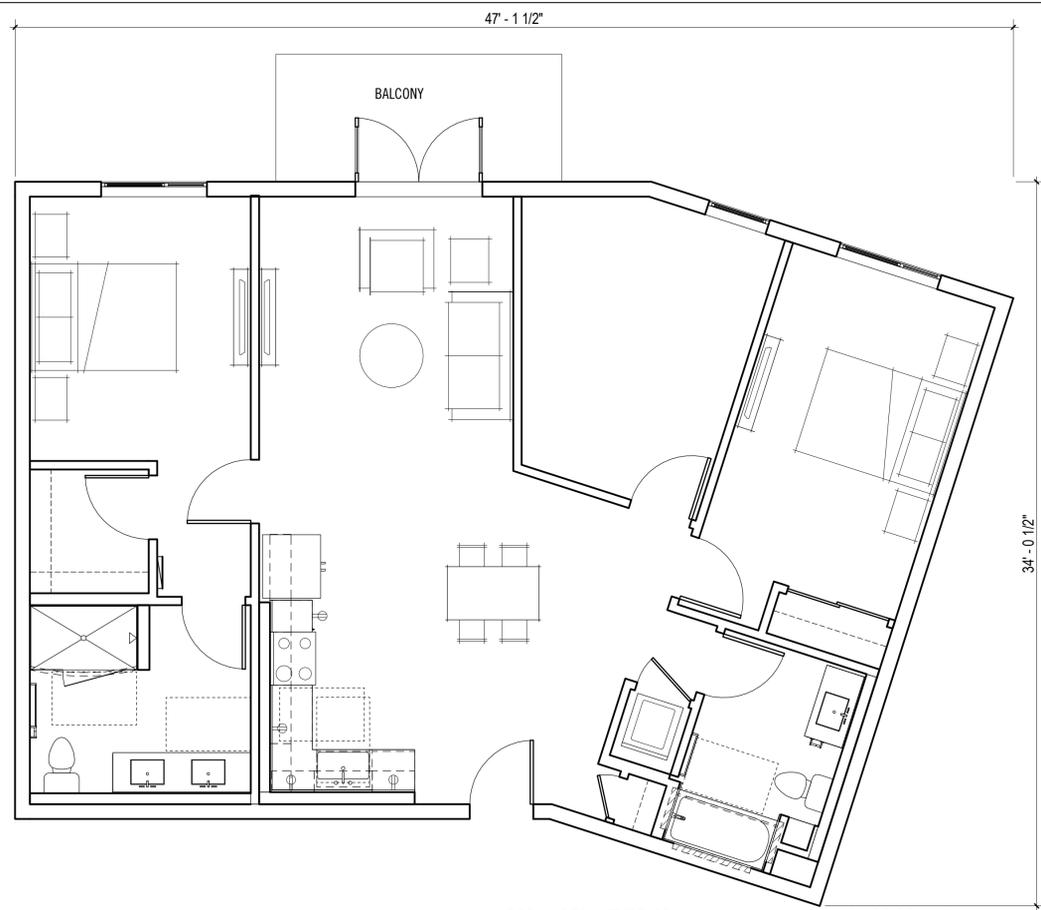
JOB NUMBER:
2022-004.00
DATE:
11.13.2023
REVISIONS:

SHEET TITLE:
ENLARGED UNIT PLANS

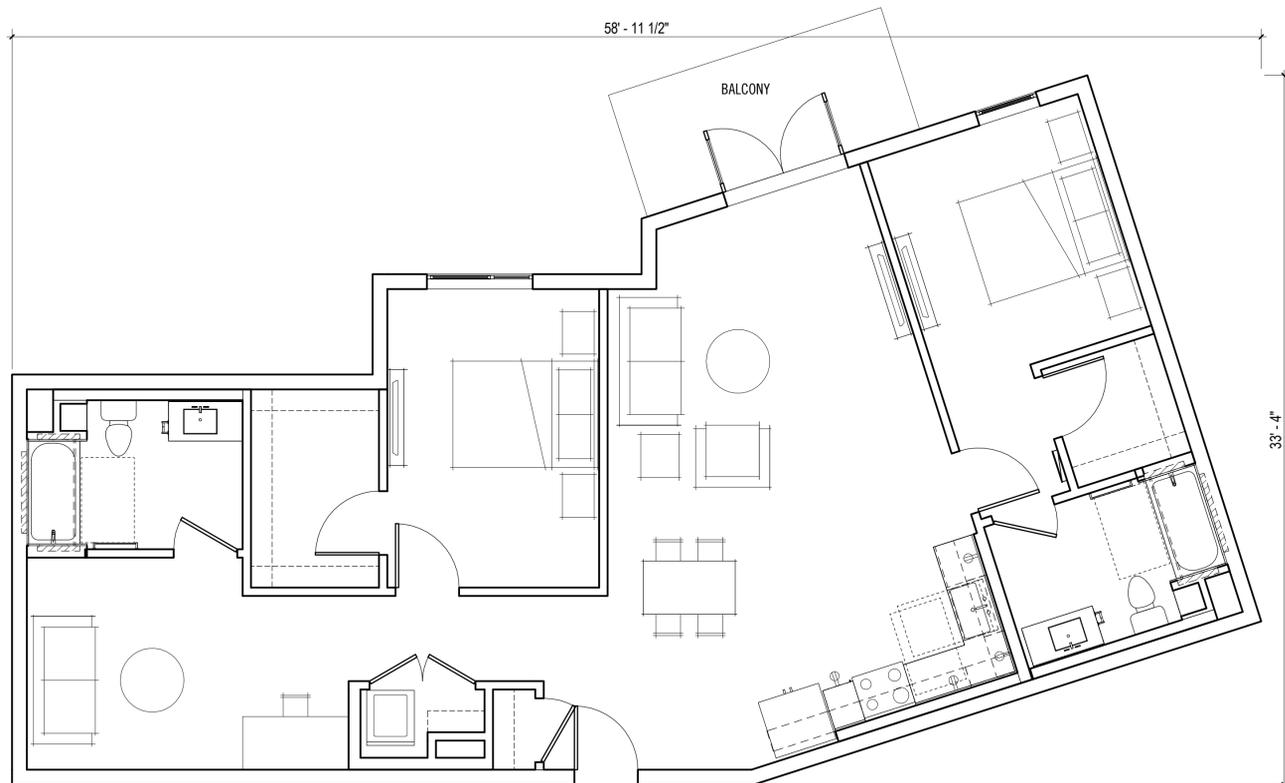
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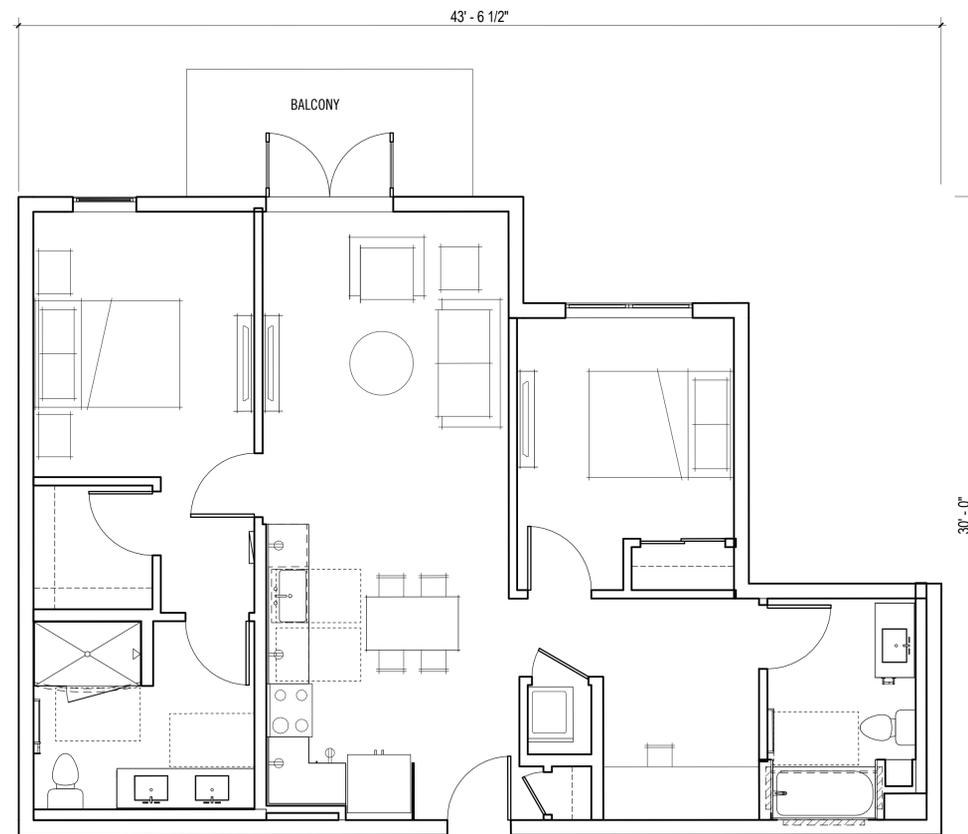
2BR + DEN - TYPE C8
1/4" = 1'-0" 1,215 SF 10



2BR + DEN - TYPE C9
1/4" = 1'-0" 1,185 SF 2



2BR + DEN - TYPE C10
1/4" = 1'-0" 1,221 SF 12



2BR + DEN - TYPE C11
1/4" = 1'-0" 980 SF 4

NOTES



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**ENLARGED UNIT
PLANS**

SHEET NUMBER:
A64



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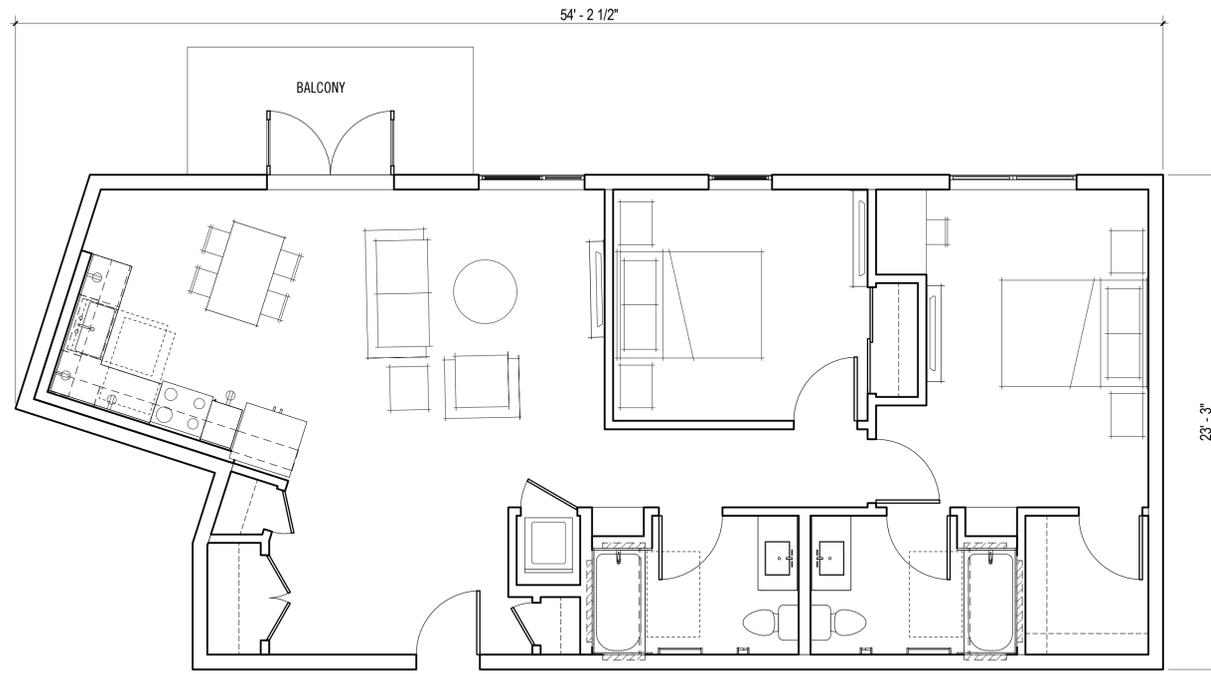
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PLANS**

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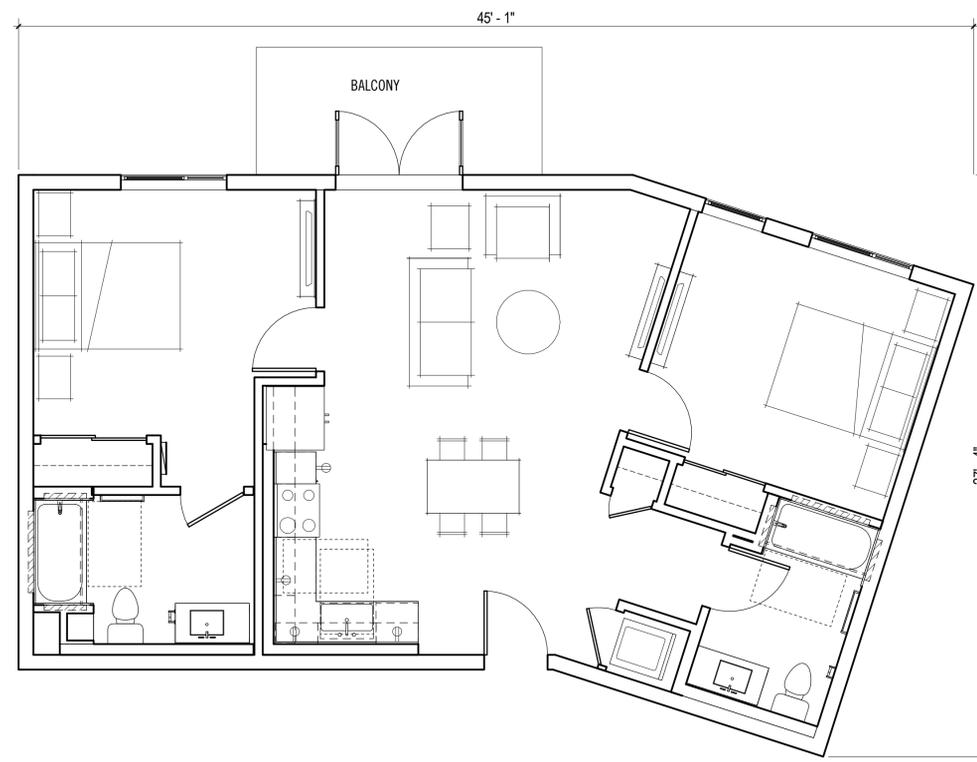
Item #1

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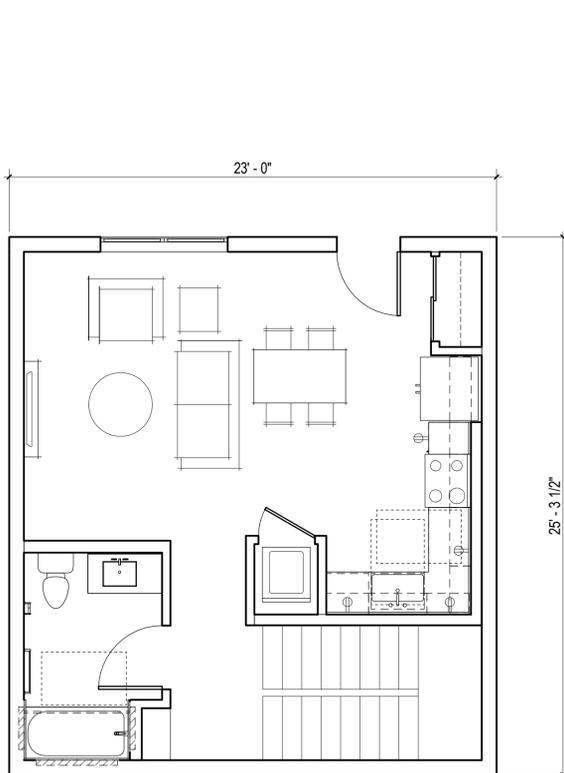
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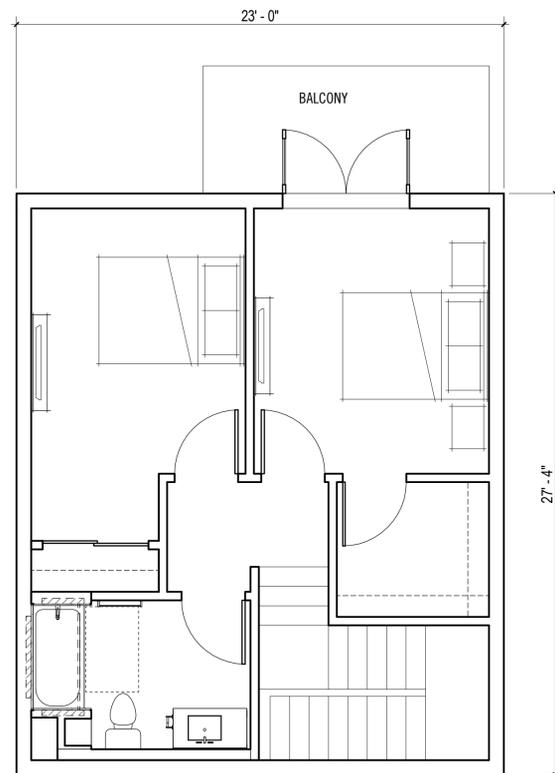
2BR - TYPE C12
1/4" = 1'-0" 1,039 SF 10



2BR - TYPE C13
1/4" = 1'-0" 879 SF 2



2BR TOWNHOUSE - TYPE C14
1/4" = 1'-0" 1,071 SF 12



3BR - TYPE D1
1/4" = 1'-0" 1,284 SF 4



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GROUNDLEVEL

LANDSCAPE ARCHITECTURE

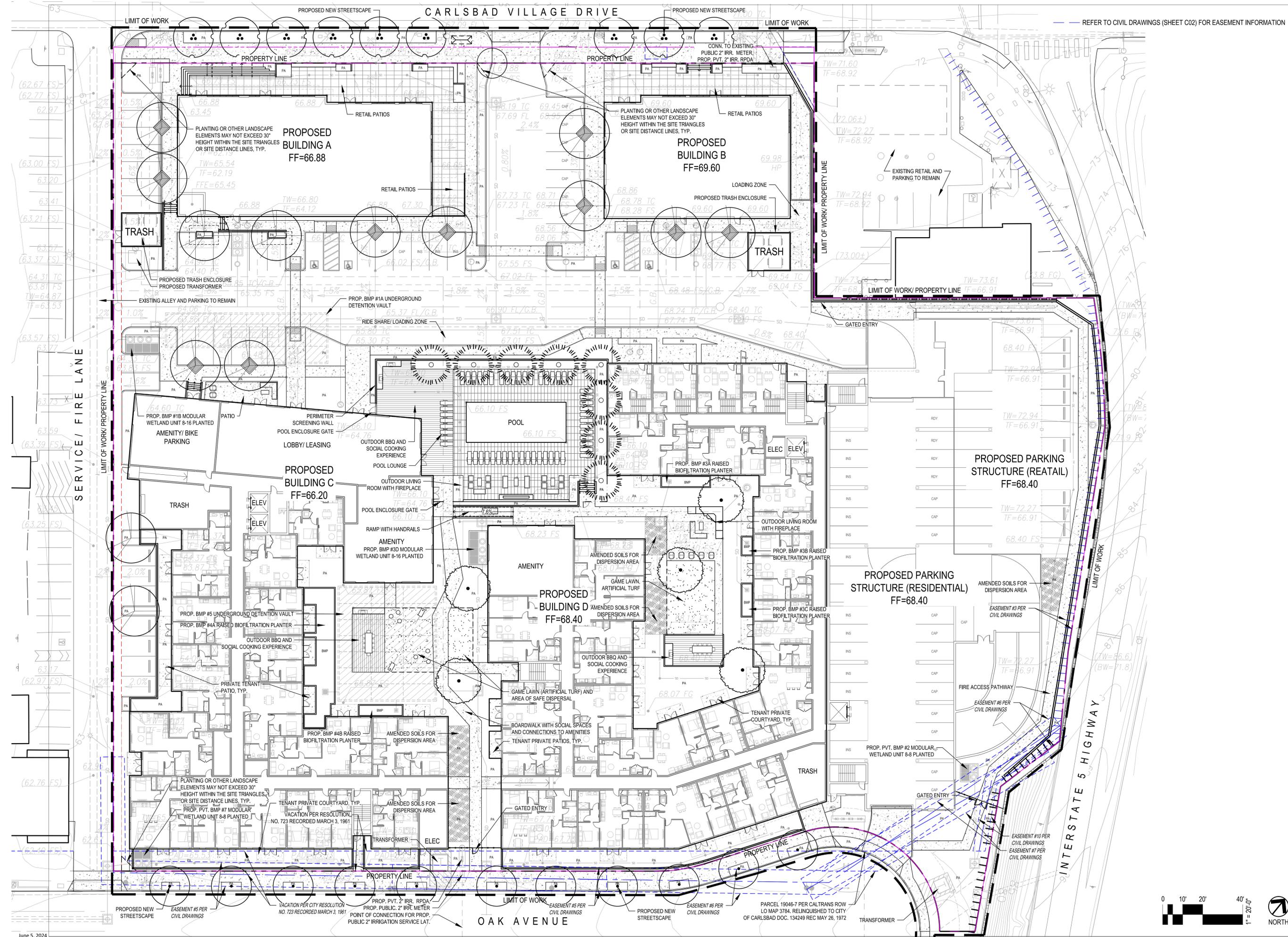
2605 State Street, Suite B
San Diego, CA 92103
619.325.1990
GroundLevelSD.com
GL Project #22139



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SHEET TITLE:
SITE PLAN

SHEET NUMBER:
L1.00
Item #1 212 of 269





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GROUNDLEVEL

LANDSCAPE ARCHITECTURE
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**CONCEPTUAL
LANDSCAPE
PLAN**

SHEET NUMBER:
L1.01
Item #1 213 of 269

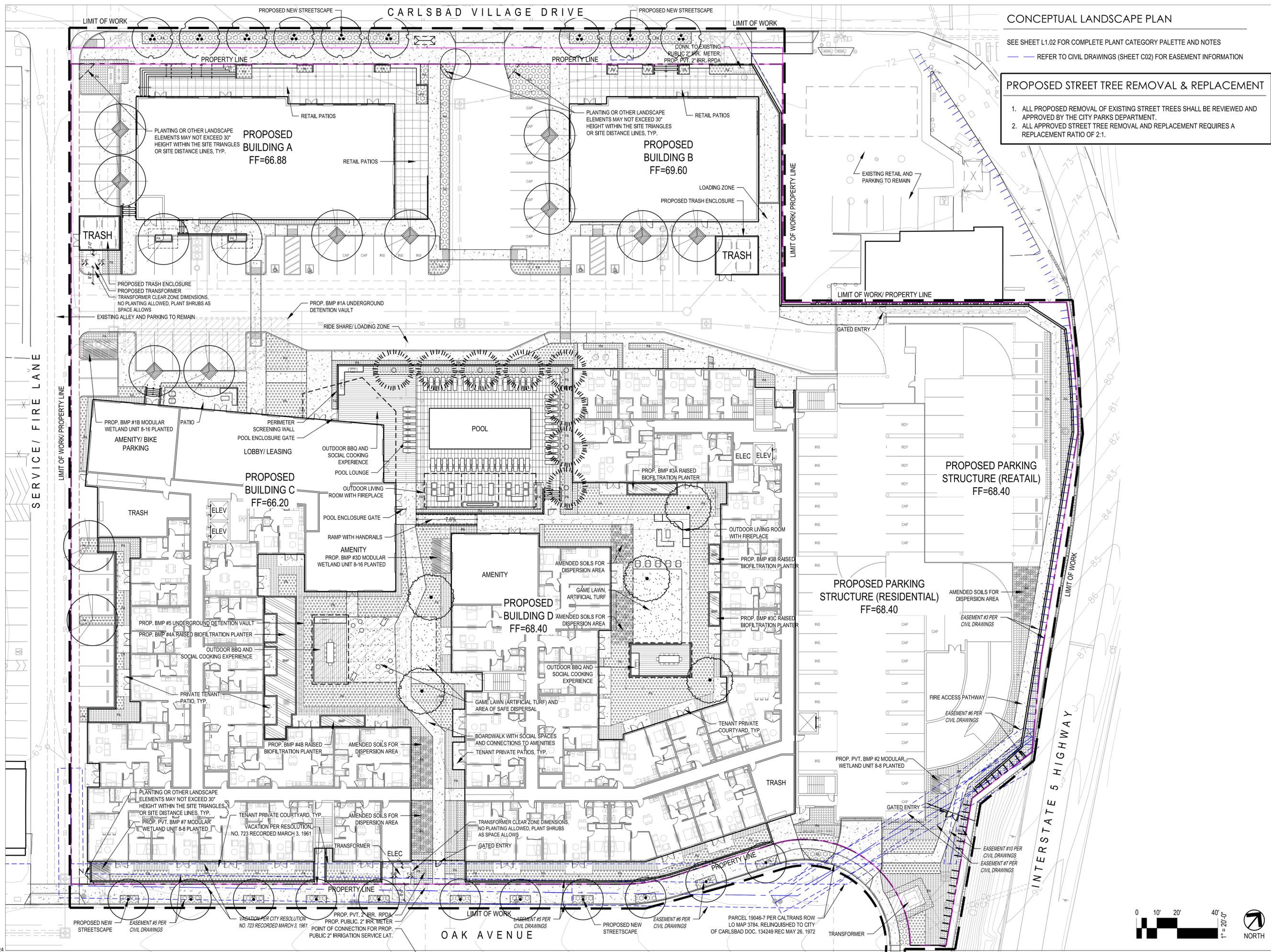
CONCEPTUAL LANDSCAPE PLAN

SEE SHEET L1.02 FOR COMPLETE PLANT CATEGORY PALETTE AND NOTES

REFER TO CIVIL DRAWINGS (SHEET C02) FOR EASEMENT INFORMATION

PROPOSED STREET TREE REMOVAL & REPLACEMENT

1. ALL PROPOSED REMOVAL OF EXISTING STREET TREES SHALL BE REVIEWED AND APPROVED BY THE CITY PARKS DEPARTMENT.
2. ALL APPROVED STREET TREE REMOVAL AND REPLACEMENT REQUIRES A REPLACEMENT RATIO OF 2:1.





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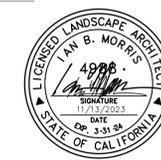
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DATE:
11.13.2023

REVISIONS:

SHEET NUMBER:

LANDSCAPE
PLANTING
PALETTE

SHEET NUMBER:

L1.02

PROPOSED PLANTING PALETTE

PROPOSED STREET TREES ALONG CARLSBAD VILLAGE DRIVE (100% 36" BOX) - SHALL BE APPROVED BY THE CITY PARKS DEPT.

BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	QUANTITY
MAGNOLIA GRANDIFLORA	SOUTHERN MAGNOLIA TREE	36" BOX	MODERATE	30'	60'	9

PROPOSED STREET TREES ALONG OAK AVENUE (100% 36" BOX) - SHALL BE APPROVED BY THE CITY PARKS DEPT.

BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	QUANTITY
CERCIS CANADENSIS 'FOREST PANSY'	EASTERN REDBUD	36" BOX	MODERATE	25'-30'	15'-20'	11

PARKING LOT TREES SUCH AS (100% 36" BOX)

BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	QUANTITY
ELAEOCARPUS DECIPIENS	JAPANESE BLUEBERRY TREE	36" BOX	MODERATE	20'-35'	20'-35'	TBD
LAGERSTROEMIA SPP.	CRAPE MYRTLE	36" BOX	MODERATE	20'-30'	15'-20'	TBD
LOPHOSTEMON CONFERTUS	BRISBANE BOX	36" BOX	MODERATE	50'	20'	TBD
PODOCARPUS GRACILIOR	FERN PINE TREE	36" BOX	MODERATE	20'-60'	10'-25'	TBD
ULMUS PARVIFILIA 'DRAKE'	DRAKE CHINESE ELM	36" BOX	MODERATE	40'	30'	TBD

POOL ENCLOSURE TREES SUCH AS (100% 36" BOX OR 15' BTH)

BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	QUANTITY
ARCHONTOPHOENIX ALEXANDRAE	KING PALM	36" BOX	MODERATE	70'	10'-15'	TBD
MAGNOLIA GRANDIFLORA	SOUTHERN MAGNOLIA TREE	36" BOX	MODERATE	30'	60'	TBD
PHOENIX RECLINATA	SENEGAL DATE PALM	15' BTH	MODERATE	35'	10'-15'	TBD

RESIDENTIAL COURTYARD TREES SUCH AS (50% 36" BOX, 30% 48" BOX, 20% 60" BOX)

BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	QUANTITY
ALOE TONGAENSIS 'MEDUSA'	MOZAMBIQUE TREE ALOES	36" BOX	LOW	12'	6'	TBD
ALOE 'HERCULES'	HERCULES TREE ALOE	36" BOX	LOW	25'-40'	12'-25'	TBD
ARBUTUS 'MARINA'	MARINA STRAWBERRY TREE	60" BOX	LOW	25'-40'	15'-20'	TBD
CERCIS CANADENSIS 'FOREST PANSY'	EASTERN REDBUD TREE	36" BOX	MODERATE	12'-20'	15'-25'	TBD
DRACAENA DRACO	DRAGON TREE	48" BOX	VERY LOW	15'-20'	15'-20'	TBD
OLEA EUROPAEA 'SWAN HILL'	SWAN HILL OLIVE TREE	18" X 18"	LOW	25'-35'	20'-30'	TBD
PLATANUS RACEMOSA	CALIFORNIA SYCAMORE TREE	36" BOX	MODERATE	50'	30'	TBD
QUERCUS VIRGINIANA	SOUTHERN LIVE OAK TREE	60" BOX	MODERATE	50'	60'	TBD
LAGERSTROMIA SPP.	CRAPE MYRTLE	36" BOX	MODERATE	20'-30'	15'-20'	TBD
ULMUS PARVIFOLIA 'DRAKE'	DRAKE CHINESE ELM TREE	36" BOX	MODERATE	40'	30'	TBD

LOW HEIGHT PARKWAY SHRUBS & GRASSES: SUCH AS (50% - 1 GAL & 50% - 5 GAL.)

BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	O.C. SPACING
CALLISTEMON VIMINALIS 'LJ1'	BETTER JOHN BOTTLEBRUSH	5 GAL	LOW	4'-5'	4'-5'	36"
CARISSA MACROCARPA 'GREEN CARPET'	GREEN CARPET NATAL PLUM	5 GAL	LOW	12"-18"	48"	30"
DIANELLA REVOLUTA 'LITTLE REV TIGHT'	LITTLE REV FLAX LILY	5 GAL	LOW	18"-24"	18"-24"	18"
LOMANDRA CONFERTIFOLIA 'LIME TIGHT'	LIME TIGHT MAT RUSH	1 GAL	LOW	2'-3'	2'-3'	24"
MUHLENBERGIA DUBIA	PINE MUHLY	1 GAL	LOW	3'	3'	30"
SESLERIA AUTUMNALIS	AUTUMN MOOR GRASS	1 GAL	MODERATE	8"-12"	8"-12"	12"
WESTRINGIA FRUTICOSA 'GREY BOX'	DWARF COAST ROSEMARY	5 GAL	LOW	24"-36"	24"-36"	24"

TALL EVERGREEN SCREENING SHRUBS: SUCH AS (100% - 15 GAL.)

BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	O.C. SPACING
DODONAEA VISCOSEA 'PURPUREA'	PURPLE-LEAFED HOP-BUSH	15 GAL	LOW	12'-16'	8'-12'	-
FEIJOA SELLOWIANA	PINEAPPLE GUAVA	15 GAL	LOW	18'-25'	18'-25'	-
LAURUS NOBILIS	BAY LAUREL	15 GAL	LOW	25'	20'	-
LIGUSTRUM JAPONICUM 'TEXANUM'	WAXLEAF PRIVET	15 GAL	MODERATE	8'	15'	-
PODOCARPUS ELONGATUS 'MONMAL'	ICEE BLUE YELLOW-WOOD	15 GAL	MODERATE	15'-25'	4'	48"
PODOCARPUS MACROPHYLLUS	YEW PINE	15 GAL	MODERATE	8'	4'	30"
PRUNUS CAROLINIANA 'MONUS'	BRIGHT 'N TIGHT CAROLINA LAUREL CHERRY	15 GAL	MODERATE	10'	6'	48"
WESTRINGIA 'WYNYABBIE GEM'	COAST ROSEMARY	15 GAL	LOW	4'-6'	36"-48"	36"

PROPOSED STREET TREE REMOVAL & REPLACEMENT

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2. ALL APPROVED STREET TREE REMOVAL AND REPLACEMENT REQUIRES A REPLACEMENT RATIO OF 2:1.

BUILDING FOUNDATION SHRUBS: SUCH AS (75% - 15 GAL & 25% - 5 GAL)

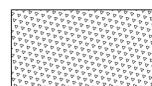
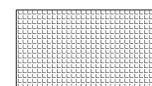
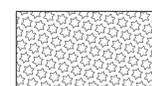
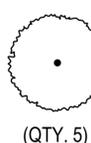
BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	O.C. SPACING
CRASSULA SPP.	CRASSULA	5 GAL	LOW	6"-8"	N/A	18"
LEUCADENDRON 'RED GEM'	RED GEM CONEBUSH	15 GAL	LOW	8'-10'	6'-8'	5'
LEUCOPHYLLUM ZYGOPHYLLUM 'CIMARRON'	BLUE RANGER SAGE	15 GAL	VERY LOW	3'	3'-4'	30"
OLEA EUROPAEA 'LITTLE OLLIE'	LITTLE OLLIE OLIVE	5 GAL	LOW	6'-8'	6'-8'	36"
PHORMIUM SPP.	NEX ZEALAND FLAX SPECIES	15 GAL	LOW	5'-7'	3'	36"
RHAPHIOLEPIS UMBELLATA 'MINOR'	DWARF INDIAN HAWTHORN	15 GAL	LOW	4'-6'	24"-36"	30"
RHAPHIOLEPIS X 'MONTIC'	MAJESTIC BEAUTY INDIAN HAWTHORN	15 GAL	LOW	4'-6'	24"-36"	30"
SENECIO DECARYI	MADAGASCAR SENECIO	5 GAL	LOW	48"	48"	36"
WESTRINGIA FRUTICOSA 'BLUE GEM'	BLUE GEM COAST ROSEMARY	5 GAL	LOW	4'-6'	36"-48"	36"

ORNAMENTAL SUCCULENTS, SHRUBS & GRASSES: SUCH AS (40% - 1 GAL, 40% - 5 GAL & 20% - 15 GAL)

BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	O.C. SPACING
ACACIA COGNATA 'ACCOG01'	COUSIN ITT LITTLE RIVER WATTLE	5 GAL	VERY LOW	24"-36"	4'-6'	42"
AGAVE SPP.	AGAVE SPECIES	15 GAL	LOW	4'-5'	4'-5'	48"
ALOE SPP.	ALOE SPECIES	5 GAL	LOW	3'-5'	2'-5'	18"-48"
CALLISTEMON VIMINALIS 'LJ1'	BETTER JOHN BOTTLEBRUSH	5 GAL	LOW	4'-5'	4'-5'	6'-8"
CEANOTHUS GRISEUS HORIZONTALIS 'YANKEE POINT'	YANKEE POINT CEANOTHUS	5 GAL	LOW	36"	60"	6'
DIANELLA SPP.	FLAX LILY SPECIES	5 GAL	LOW	24"	24"	18"
FESTUCA MAIREI	ATLAS FESCUE	1 GAL	LOW	24"	24"	18"
LANTANA X 'NEW GOLD'	NEW GOLD LANTANA	5 GAL	VERY LOW	12"-24"	4'-6'	36"
LEONOTIS LEONURUS	LION'S TAIL	5 GAL	LOW	5'-6'	4'-5'	36"
LOMANDRA SPP.	DWARF MAT RUSH	1 GAL	LOW	24"	24"	24"
MUHLENBERGIA DUBIA	PINE MUHLY	1 GAL	LOW	24"	36"	30"
OLEA EUROPAEA 'LITTLE OLLIE'	LITTLE OLLIE OLIVE	5 GAL	LOW	6'-8'	6'-8'	36"
PENNISETUM SPATHIOLATUM	SLENDER VELDT GRASS	1 GAL	LOW	24"	24"	15"
RHAPHIOLEPIS INDICA 'CLARA'	INDIAN HAWTHORN	5 GAL	LOW	4'-6'	4'-6'	42"
RHAPHIOLEPIS UMBELLATA 'MINOR'	DWARF INDIAN HAWTHORN	5 GAL	LOW	4'-6'	24"-36"	30"
ROSMARINUS OFFICINALIS 'HUNTINGTON CARPET'	HUNTINGTON CARPET ROSEMARY	5 GAL	LOW	12"-24"	8"	5"
SALVIA GREGGII 'FURMAN'S RED'	FURMAN'S RED TEXAS SAGE	5 GAL	LOW	24"-36"	24"-36"	24"
SALVIA LEUCANTHA 'SANTA BARBARA'	SANTA BARBARA SAGE	5 GAL	LOW	24"-36"	24"-36"	30"
SESLERIA AUTUMNALIS	AUTUMN MOOR GRASS	1 GAL	MODERATE	24"	24"	12"
TRACHELOSPERMUM JASMINOIDES	STAR JASMINE	1 GAL	MODERATE	2'-6"	N/A	48"

STORMWATER TREATMENT SHRUBS & GRASSES: SUCH AS (50% - 1 GAL & 50% - 5 GAL)

BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	O.C. SPACING
CAREX PRAEGRACILIS	CALIFORNIA FIELD SEDGE	1 GAL	MODERATE	8"-12"	18"-24"	15"
CHONDROPETALUM TECTORUM	SMALL CAPE RUSH	5 GAL	LOW	3'-4'	3'-4'	36"
JUNCUS PATENS	CALIFORNIA GRAY RUSH	5 GAL	LOW	24"	24"	15"
LEYMUS CONDENSATUS 'CANYON PRINCE'	CANYON PRINCE WILD RYE	1 GAL	LOW	2'-3'	2'-3'	24"



CITY OF CARLSBAD - GENERAL PLANTING NOTES

1. THE PLANTING PLAN IS DIAGRAMMATIC. ALL PLANT LOCATIONS ARE APPROXIMATE. PLANT SYMBOLS TAKE PRECEDENCE OVER PLANT QUANTITIES SPECIFIED.
 - A. QUANTITIES SHOWN ON THE PLANTING PLAN ARE APPROXIMATE AND ARE FOR THE CONVENIENCE OF THE CONTRACTOR.
 - B. CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT OF THE DISCREPANCIES BETWEEN QUANTITIES AND SYMBOLS SHOWN.
2. LANDSCAPE CONTRACTOR SHALL APPLY A CONTACT HERBICIDE, WHERE WEEDS ARE PRESENT, PER MANUFACTURERS SPECIFICATIONS A MINIMUM OF TEN (10) DAYS PRIOR TO COMMENCEMENT OF ANY PLANTING OR IRRIGATION WORK. WEEDS SHALL BE ALLOWED TO COMPLETELY DIE BACK, INCLUDING THE ROOTS BEFORE PROCEEDING WITH WORK.
3. LANDSCAPE CONTRACTOR SHALL SUBMIT A SOIL ANALYSIS REPORT FROM AN AUTHORIZED TESTING AGENCY TO THE LANDSCAPE ARCHITECT 30 DAYS BEFORE BEGINNING WORK.
4. PRIOR TO PLANTING, IRRIGATION SYSTEM SHALL BE FULLY OPERATIONAL AND PLANTING AREAS SHALL BE THOROUGHLY SOAKED.
5. ALL AREAS TO BE PLANTED, WHICH HAVE A SLOPE OF LESS THAN 10%, SHALL BE CROSS-RIPPED TO A DEPTH OF SIX (6) INCHES AND AMENDMENTS, PER THE RECOMMENDATION OF THE SOILS REPORT, SHALL BE SPREAD EVENLY AND THOROUGHLY BLENDED IN PER 1,000 SQUARE FEET (QUANTITIES SUBJECT TO CHANGE PER SOIL ANALYSIS).
 - A. SEE PLANTING SPECIFICATION FOR BID AMENDMENTS.
6. PLANT BACK FILL SHALL BE PER THE PLANTING SPECIFICATIONS.
7. PLANT PITS SHALL BE TWICE THE SIZE OF THE DESIGNATED NURSERY CONTAINER. SEE PLANTING DETAILS.
8. PLANT MATERIAL SHALL NOT BE ROOT BOUND. FIVE GALLON PLANTS AND LARGER SHALL HAVE BEEN GROWN IN CONTAINERS FOR A MINIMUM OF 6 MONTHS AND A MAXIMUM OF TWO YEARS.
9. PLANTS SHALL EXHIBIT HEALTHY GROWTH AND BE FREE OF DISEASES AND PESTS.
10. STAKE ALL TREES PER TREE STAKING DETAIL.
11. REMOVE NURSERY STAKES ON ALL VINES AND ATTACH TO ADJACENT FENCES WITH GALVANIZED WINE BUTTONS AND GREEN NURSERY TAPE.
12. REMOVE NURSERY STAKES AND TIES FROM ALL CONTAINER STOCK.
13. PLANTS SHALL NOT BE PLACED WITHIN TWELVE (12") INCHES OF SPRINKLER HEADS.
14. LANDSCAPE CONTRACTOR SHALL MAINTAIN A MINIMUM OF 2% DRAINAGE AWAY FROM ALL BUILDINGS, STRUCTURES, AND WALLS. FINISHED GRADES SHALL BE SMOOTHED TO ELIMINATE PUDDLING OR STANDING WATER.
15. FINISHED GRADES SHALL BE TWO (2) INCHES BELOW THE TOP OF CURBS, SILLS, AND WALKWAYS IN ALL AREAS. WHERE SOD IS LAID NEXT TO THESE IMPROVEMENTS-FINISH GRADE BEFORE LAYING SOD SHALL BE 2" BELOW THE TOP.
16. THE LANDSCAPE CONTRACTOR SHALL LEAVE SITE IN A CLEAN CONDITION, REMOVING ALL UNUSED MATERIAL, TRASH, AND TOOLS.
17. LANDSCAPE CONTRACTOR SHALL MAINTAIN ALL PLANTINGS FOR A PERIOD OF NINETY (90) DAYS AFTER COMPLETION. ALL AREAS SHALL BE KEPT CLEAN, WATERED, AND WEED FREE.
18. AT COMPLETION OF ALL WORK OUTLINED IN THESE PLANS, THE LANDSCAPE CONTRACTOR SHALL CONTACT OWNER AND ARRANGE FOR A WALK THROUGH TO DETERMINE THAT ALL ASPECTS OF WORK ARE COMPLETED. WORK MUST BE FULLY COMPLETED ACCORDING TO ALL PLANS AND SPECIFICATIONS AND MUST BE COMPLETED IN A GOOD WORKMANSHIP MANNER AND MUST BE ACCEPTED BY THE OWNER IN WRITING PRIOR TO THE BEGINNING OF THE MAINTENANCE PERIOD.
19. THE MAINTENANCE PERIOD SHALL INCLUDE THE FOLLOWING SCOPE OF WORK:
 - A. DAILY WATERING OF ALL PLANT MATERIAL.
 - B. WEEKLY MOWING OF ALL TURF AREAS.
 - C. WEEDING AND REMOVAL OF ALL WEEDS FROM GROUND COVER AREAS.
 - D. REPLACEMENT OF ANY DEAD, DYING, OR DAMAGED TREES, SHRUBS, OR GROUND COVERS.
 - E. FILLING AND REPLANTING OF ANY LOW AREAS WHICH MAY CAUSE STANDING WATER.
 - F. ADJUSTING OF SPRINKLER HEAD HEIGHT AND WATERING SYSTEM.
 - G. FILLING AND RECOMPACTION OF ERODED AREAS.
 - H. WEEKLY REMOVAL OF ALL TRASH, LITTER, CLIPPINGS, AND ALL FOREIGN DEBRIS.
 - I. AT 30 DAYS AFTER PLANTING, AMMONIUM SULFATE SHALL BE APPLIED TO LAWN AREAS AT THE RATE OF 5 LBS. PER 1,000 SQUARE FEET.
 - J. AT 60 DAYS AFTER PLANTING AND PRIOR TO THE END OF THE NINETY (90) DAY MAINTENANCE PERIOD, BEST FERTILIZER COMPANY 16-8-8 (OR EQUAL) SHALL BE APPLIED AT THE RATE OF 6 LBS. PER 1,000 SQUARE FEET TO LAWN AREAS AND PLANTING AREAS.
20. PRIOR TO END OF MAINTENANCE PERIOD, LANDSCAPE CONTRACTOR SHALL CONTACT OWNER AND ARRANGE FOR A FINAL WALK THROUGH. OWNER MUST ACCEPT ALL MAINTAINED AREAS IN WRITING PRIOR TO END OF MAINTENANCE PERIOD.
21. ALL GROUND COVERS SHALL BE GUARANTEED BY THE CONTRACTOR AS TO GROWTH AND HEALTH FOR A PERIOD OF NINETY (90) DAYS AFTER THE COMPLETION OF MAINTENANCE PERIOD AND FINAL ACCEPTANCE. ALL SHRUBS SHALL BE GUARANTEED BY THE CONTRACTOR AS TO GROWTH AND HEALTH FOR A PERIOD OF NINETY (90) DAYS AFTER COMPLETION OF MAINTENANCE PERIOD AND FINAL ACCEPTANCE. ALL TREES SHALL BE GUARANTEED BY THE CONTRACTOR TO LIVE AND GROW IN AN ACCEPTABLE UPRIGHT POSITION FOR A PERIOD OF ONE (1) YEAR AFTER COMPLETION OF THE SPECIFIED MAINTENANCE PERIOD AND FINAL ACCEPTANCE.
22. THE CONTRACTOR, WITHIN FIFTEEN (15) DAYS OF WRITTEN NOTIFICATION BY THE LANDSCAPE ARCHITECT, SHALL REMOVE AND REPLACE ALL GUARANTEED PLANT MATERIALS, WHICH FOR ANY REASON FAIL TO MEET THE REQUIREMENTS OF THE GUARANTEE. REPLACEMENT SHALL BE MADE WITH PLANT MATERIALS AS INDICATED OR SPECIFIED ON THE ORIGINAL PLANS, AND ALL SUCH REPLACEMENT MATERIALS SHALL BE GUARANTEED AS SPECIFIED FOR THE ORIGINAL MATERIAL GUARANTEE.
23. ALL EXISTING LANDSCAPING DESIGNATED TO REMAIN INCLUDING ALL AREAS MAINTAINED BY THE CARLSBAD RESEARCH CENTER OWNERS ASSOCIATION (CRC ASSOCIATION), SHALL BE PROTECTED IN PLACE. ANY DAMAGED LANDSCAPING (PLANTING IRRIGATION, OR HARDSCAPE) SHALL BE REPLACED IN KIND AS APPROPRIATE TO MEET LANDSCAPE MANUAL AND CRC SPECIFIC PLAN REQUIREMENTS.
24. ALL ABOVE GROUND UTILITIES ARE TO BE SCREENED WITH PLANTING MATERIAL.

CITY OF CARLSBAD SLOPE REQUIREMENTS

SECTION 7 REQUIREMENTS

A. SLOPE PLANTING

1. AREAS OF APPLICATION (PLANTING)
 - SLOPES – 6:1 OR STEEPER AND:
 - 3 FEET OR LESS IN VERTICAL HEIGHT AND ADJACENT TO PUBLIC WALKS OR STREETS REQUIRE AT A MINIMUM STANDARD #1 (COVER CROP OR EROSION CONTROL MATTING).
 - GREATER THAN 3 FEET TO 8 FEET IN VERTICAL HEIGHT REQUIRE STANDARDS #1 (EROSION CONTROL MATTING SHALL BE INSTALLED IN LIEU OF A COVER CROP), #2 AND #3.
 - IN EXCESS OF 8 FEET IN VERTICAL HEIGHT REQUIRE STANDARDS #1 (EROSION CONTROL MATTING SHALL BE INSTALLED IN LIEU OF A COVER CROP), #2, #3 AND #4.
 - AREAS GRADED FLATTER THAN 6:1 REQUIRE A COVER CROP PER STANDARD #1 WHEN THEY HAVE ONE OR MORE OF THE FOLLOWING CONDITIONS:
 - SHEET GRADED PADS NOT SCHEDULED FOR IMPROVEMENTS WITHIN SIX (6) MONTHS OF COMPLETION OF ROUGH GRADING.
 - A POTENTIAL EROSION PROBLEM AS DETERMINED BY THE CITY.
 - IDENTIFIED BY THE CITY AS HIGHLY VISIBLE AREAS TO THE PUBLIC OR HAVE SPECIAL CONDITIONS THAT WARRANT IMMEDIATE TREATMENT.

2. SLOPE PLANTING STANDARDS

SLOPES REQUIRING EROSION CONTROL MEASURES, AS SPECIFIED ABOVE, SHALL BE TREATED WITH ONE OR MORE OF THE FOLLOWING PLANTING STANDARDS:

- A. STANDARD #1 – COVER CROP/ AND EROSION CONTROL MATTING
 - COVER CROP SHALL BE A SEED MIX TYPICALLY COMPOSED OF QUICK GERMINATING AND FAST COVERING GRASSES, CLOVERS AND/OR WILD FLOWERS. SUBMIT THE SPECIFIC SEED MIX FOR CITY APPROVAL PRIOR TO APPLICATION.
 - THE COVER CROP SHALL BE APPLIED AT A RATE AND MANNER SUFFICIENT TO PROVIDE NINETY (90%) PERCENT COVERAGE WITHIN THIRTY (30) DAYS.
 - TYPE OF EROSION CONTROL MATTING SHALL BE AS APPROVED BY THE CITY AND AFFIXED TO THE SLOPE AS RECOMMENDED BY THE MANUFACTURER.
 - ON SLOPES 3 FEET OR LESS IN VERTICAL HEIGHT WHERE ADJACENT TO PUBLIC WALKS OR STREETS:
 - WHEN PLANTING OCCURS BETWEEN AUGUST 15 AND APRIL 15, EROSION CONTROL MATTING SHALL BE REQUIRED.
 - DURING THE REMAINDER OF THE YEAR, THE COVER CROP AND/OR EROSION CONTROL MATTING MAY BE USED.
 - ON SLOPES GREATER THAN 3 FEET IN HEIGHT, EROSION CONTROL MATTING SHALL BE REQUIRED AND A COVER CROP SHALL NOT BE USED, UNLESS OTHERWISE APPROVED BY THE CITY.

B. STANDARD #2 - GROUND COVER

ONE HUNDRED (100%) PERCENT OF THE AREA SHALL BE PLANTED WITH A GROUND COVER KNOWN TO HAVE EXCELLENT SOIL BINDING CHARACTERISTIC (PLANTED FROM A MINIMUM SIZE OF FLATTED MATERIAL AND SPACED TO PROVIDE FULL COVERAGE WITHIN ONE YEAR).

C. STANDARD #3 - LOW SHRUBS

LOW SPREADING WOODY SHRUBS (PLANTED FROM A MINIMUM OF 1 GALLON CONTAINERS) SHALL COVER A MINIMUM OF SEVENTY (70%) PERCENT OF THE SLOPE FACE (AT MATURE SIZE).

D. STANDARD #4 - TREES AND/OR LARGE SHRUBS

TREES AND/OR LARGE SHRUBS SHALL BE (PLANTED FROM A MINIMUM OF 1 GALLON CONTAINERS) AT A MINIMUM RATE OF ONE (1) PLANT PER TWO HUNDRED (200) SQUARE FEET.

3. PLANTS

PLANT MATERIALS USED ON SLOPES SHALL BE THOSE SPECIES THAT ARE KNOWN TO HAVE LOW WATER REQUIREMENTS AND ROOTING SYSTEMS OF VARIOUS DEPTHS THAT WILL MINIMIZE EROSION AND SOIL SLIPPAGE.



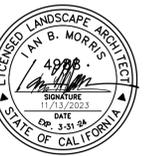
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SDP 23-0014
JOB NUMBER:
2022–004.00
DATE:
11.13.2023
REVISIONS:

SHEET TITLE:
LANDSCAPE PLANTING NOTES

SHEET NUMBER:
L1.03



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JOB NUMBER:
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11.13.2023
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SHEET TITLE:
RECYCLED WATER USE PLAN

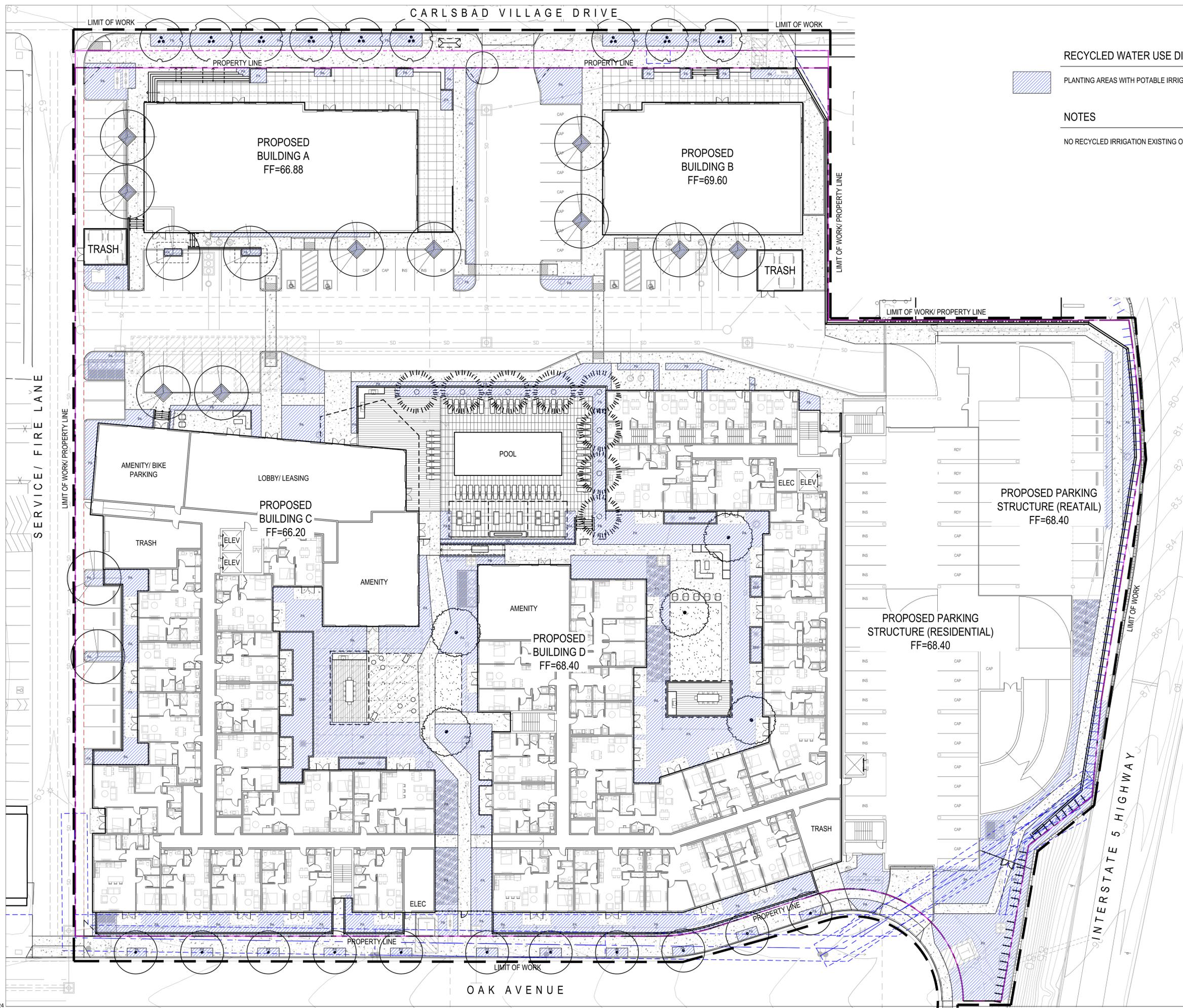
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RECYCLED WATER USE DIAGRAM

PLANTING AREAS WITH POTABLE IRRIGATION

NOTES

NO RECYCLED IRRIGATION EXISTING OR PROPOSED ON PROPERTY





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SUBMITTAL:
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JOB NUMBER:
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DATE:
11.13.2023

REVISIONS:

SHEET TITLE:
**CONCEPTUAL
WATER
CONSERVATION
PLAN**
SHEET NUMBER:
L1.05

HYDROZONE DIAGRAM

HYDROZONE	PLANT FACTOR (PF)	HYDROZONE AREA (SF)	PERCENT OF TOTAL LANDSCAPE	ZONE OR VALVE	IRRIGATION METHOD
LOW	0.3	23,323	86.2%	TBD	DRIP
MEDIUM	0.5	2,731	10.1%	TBD	DRIP
HIGH	0.8	0	0%	TBD	MP ROTATOR
HIGH	0.8	1,000	3.7%	TBD	POOL

WATER CONSERVATION FEATURES

1. PLANTING DESIGN EMPHASIZES WATER EFFICIENCY WITH USE OF NATIVE, LOW WATER USE, LOW MAINTENANCE, AND DROUGHT TOLERANT PLANTS.
2. TREES, VINES, AND SCREENING PLANTS USED ALONG BUILDING FACADE FOR NATURAL HEATING AND COOLING OPPORTUNITIES.
3. SUN ORIENTATION ACCOUNTED FOR WHEN SELECTING TREE LOCATIONS.
4. SOIL ANALYSIS WILL BE COMPLETED FOR SPECIFIC SOIL AMENDMENTS PRIOR TO PLANT AND IRRIGATION INSTALLATION.
5. TREES WILL REDUCE EVAPORATION AND SOIL AND BLOCK WIND.
6. LIMITED TURF GRASS AREAS UTILIZED AS OPEN GATHERING AND AMENITY SPACE.
7. EFFICIENT IRRIGATION SYSTEM WILL WATER DEEPLY AND INFREQUENTLY, WATERED IN EVENING OR EARLY MORNING, WILL BE MONITORED TO ACCOMMODATE CHANGING WEATHER CONDITIONS. INSTALLATION OF RAIN-SENSOR DEVICES, ELIMINATE RUNOFF.
8. ORGANIC MULCH WILL BE UTILIZED THROUGHOUT PLANTING AREAS TO KEEP PLANT ROOTS COOL, MINIMIZE EROSION, AND REDUCE WEED GROWTH.
9. REGULAR MAINTENANCE WILL BE DONE TO ENSURE ALL OF THE ABOVE.

NOTES

ONLY SUB-SURFACE IRRIGATION SHALL BE USED TO IRRIGATE ANY VEGETATION WITHIN 24" OF ANY IMPERMEABLE SURFACE UNLESS ADJACENT IMPERMEABLE SURFACES ARE DESIGNED AND CONSTRUCTED TO CAUSE WATER TO DRAIN ENTIRELY INTO A LANDSCAPED AREA.

IRRIGATION WATER BUDGET CALCULATION

MAWA WATER BUDGET:

$(ETO) (0.62) [(ETAF) (LA) + (ETAF) (SLA)]$

$[(40) (0.62)] [(0.45) (27,054) + (0.55) (1)] = 301,936 \text{ GAL/YR.}$

ETWU ESTIMATED TOTAL WATER USE:

$[(ETO) (0.62)] [(PF \times HA / IE) + SLA]$

HYDROZONE 1: $[(40) (0.62)] [(0.3 \times 23,323 / 0.81) + 1] = 214,224.9 \text{ GAL/YR.}$

HYDROZONE 2: $[(40) (0.62)] [(0.5 \times 2,731 / 0.81) + 1] = 41,807.8 \text{ GAL/YR.}$

HYDROZONE 3: $[(40) (0.62)] [(0.8 \times 0 / 0.80) + 1] = 0 \text{ GAL/YR.}$

HYDROZONE 4: $[(40) (0.62)] [(0.8 \times 1,000 / 1.00) + 1] = 19,840 \text{ GAL/YR.}$

ESTIMATED TOTAL WATER USE = 275,872.7 GAL/YR.
ETWU = 26,063 LESS THAN MAWA

MAXIMUM APPLIED WATER ALLOWANCE

MAXIMUM APPLIED WATER ALLOWANCE (MAWA)

COMMERCIAL/INDUSTRIAL:
 $MAWA = (ETO)(0.62)[(0.45 \times LA) + (0.55 \times SLA)]$

MAWA MAXIMUM APPLIED WATER ALLOWANCE IN GALLONS PER YEAR.

ETO EVAPOTRANSPIRATION IN INCHES PER YEAR.
0.62 CONVERSION FACTOR TO GALLONS PER SQUARE FOOT.

0.55/0.45 (X LA) ET ADJUSTMENT FACTOR (ETAF) FOR PLANT FACTORS AND IRRIGATION EFFICIENCY.
LA LANDSCAPED AREA INCLUDES SPECIAL LANDSCAPED AREA IN SQUARE FEET.

0.45/0.55 (X SLA) THE ADDITIONAL ET ADJUSTMENT FACTOR FOR A SPECIAL LANDSCAPED AREA (EG. 1.0 - 0.55 = 0.45 OR 1.0 - 0.45 = 0.55)

SLA SPECIAL LANDSCAPED AREA IN SQUARE FEET

ESTIMATED TOTAL WATER USE (ETWU)

$ETWU = (ETO)(0.62) [(PF \times HA) / IE + SLA]$

ETWU ESTIMATED TOTAL WATER USE IN GALLONS PER YEAR.

ETO EVAPOTRANSPIRATION IN INCHES PER YEAR.
0.62 CONVERSION FACTOR TO GALLONS PER SQUARE FOOT.

PF PLANT FACTOR FROM WUCOLS III
HA HYDROZONE AREA IN SQUARE FEET. EACH HA SHALL BE CLASSIFIED BASED UPON THE DATA INCLUDED IN THE LANDSCAPE AND IRRIGATION PLAN AS HIGH, MODERATE, LOW, OR VERY LOW WATER USE.

IE IRRIGATION EFFICIENCY OF THE IRRIGATION METHOD USED IN THE HYDROZONE.
SLA SPECIAL LANDSCAPED AREA IN SQUARE FEET.

DECLARATION OF RESPONSIBLE CHARGE

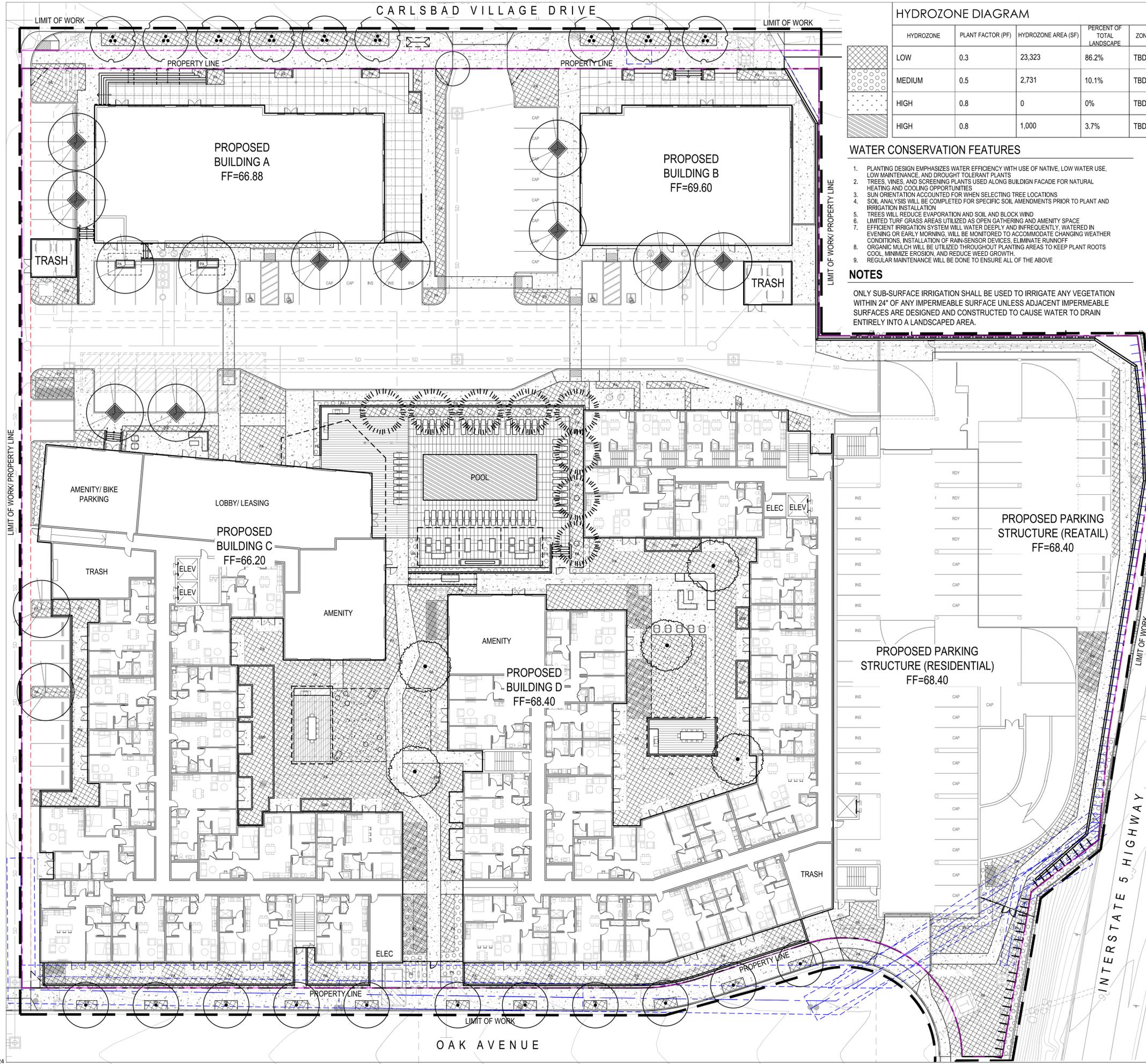
I AM FAMILIAR WITH THE REQUIREMENTS FOR LANDSCAPE AND IRRIGATION PLANS CONTAINED IN THE CITY OF CARLSBAD'S LANDSCAPE MANUAL AND WATER EFFICIENT LANDSCAPE REGULATIONS. I HAVE PREPARED THIS PLAN IN COMPLIANCE WITH THOSE REGULATIONS AND THE LANDSCAPE MANUAL. I CERTIFY THAT THE PLAN IMPLEMENTS THOSE REGULATIONS TO PROVIDE EFFICIENT USE OF WATER.

(Signature)

DATE: 08/11/2023

IAN MORRIS
GROUNDLEVEL LANDSCAPE ARCHITECTURE
2605 STATE STREET, SUITE B
SAN DIEGO, CA 92103
PH: (619) 325-1990

REGISTRATION NO: 4988
EXPIRATION DATE: 03/31/2024





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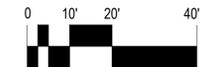
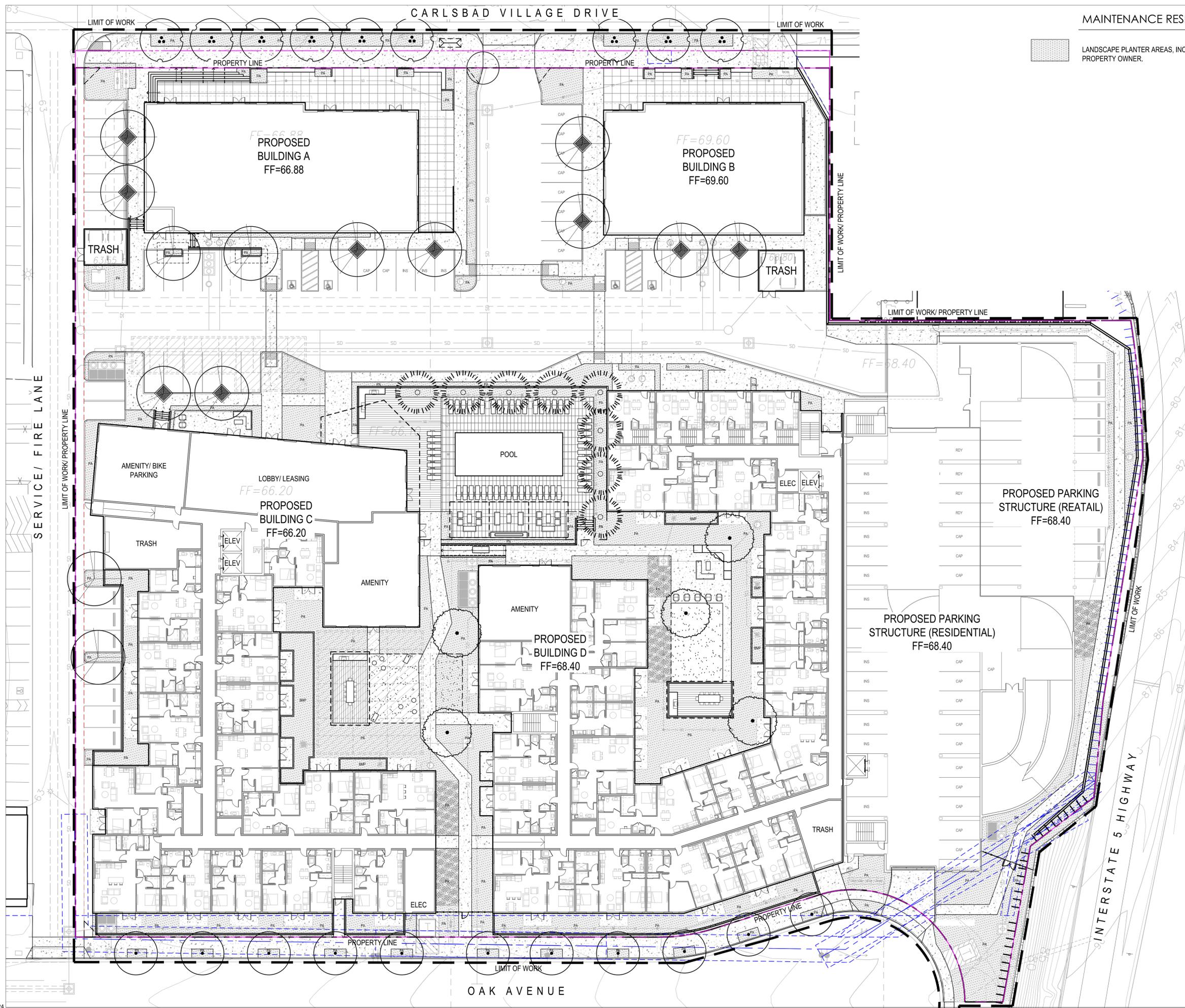
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REVISIONS:

SHEET TITLE:
CONCEPTUAL MAINTENANCE RESPONSIBILITY PLAN
SHEET NUMBER:
L1.06

MAINTENANCE RESPONSIBILITY



LANDSCAPE PLANTER AREAS, INCLUDING IRRIGATION, MAINTAINED BY THE PROPERTY OWNER.





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GROUNDLEVEL
LANDSCAPE ARCHITECTURE

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GL Project #22139



SUBMITTAL:
SDP 23-0014
JOB NUMBER:
2022-004.00
DATE:
11.13.2023
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SHEET TITLE:
EXISTING TREE SURVEY

SHEET NUMBER:
L1.07

DEMOLITION LEGEND

- EXISTING TREE, PROTECT/RELOCATE
- EXISTING TREE, TO BE REMOVED

PROPOSED STREET TREE REMOVAL & REPLACEMENT

1. ALL PROPOSED REMOVAL OF EXISTING STREET TREES SHALL BE REVIEWED AND APPROVED BY THE CITY PARKS DEPARTMENT.
2. ALL APPROVED STREET TREE REMOVAL AND REPLACEMENT REQUIRES A REPLACEMENT RATIO OF 2:1.





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SHEET TITLE:
EXISTING TREE SURVEY

SHEET NUMBER:
L1.08

TREE PROTECTION NOTES

1. PRIOR TO ANY DISTURBANCE TO THE SITE, THE OWNER/PERMITTEE SHALL MAKE ARRANGEMENTS FOR A PRE-CONSTRUCTION MEETING WITH THE CITY OF SAN DIEGO MITIGATION MONITORING COORDINATION SECTION (MMC), THE RESIDENT ENGINEER (RE) AND/OR BUILDING INSPECTOR, CERTIFIED CONSULTING ARBORIST, CONSTRUCTION MANAGER, AND LANDSCAPE PROFESSIONAL.
2. THE CONSTRUCTION MANAGER IS RESPONSIBLE FOR PREVENTING DAMAGE TO TREES.
 - a) CONSTRUCTION AND MAINTENANCE STAFF SHALL AVOID UNNECESSARY ACTIVITIES WITHIN THE DRIPLINE OF TREES.
 - b) FINES SHALL BE ASSESSED TO THOSE INDIVIDUALS FOUND TO BE RESPONSIBLE FOR THE ILLEGAL REMOVAL OR DAMAGE OF PROTECTED TREES.
3. ANY DAMAGE OR INJURY TO TREES SHALL BE REPORTED WITHIN 24 HOURS TO MMC BY THE CERTIFIED CONSULTING ARBORIST.
4. THE CERTIFIED CONSULTING ARBORIST SHALL BE RESPONSIBLE FOR ESTABLISHING, IMPLEMENTING, AND MAINTAINING THE TREE PROTECTION ZONE AS SHOWN ON THESE PLANS DURING THE ENTIRE CONSTRUCTION PERIOD:
 - a) STAKE THE COMPLETE DRIPLINE.
 - b) ROOTS SHALL BE CLEANLY CUT BACK APPROX. 6" FROM THE LIMIT OF DISTURBANCE, UNLESS OTHERWISE DIRECTED BY THE CERTIFIED CONSULTING ARBORIST.
 - c) INSTALL A TEMPORARY 6-FT HIGH CHAIN LINK FENCE AROUND THE IDENTIFIED DRIPLINE, UNLESS OTHERWISE DIRECTED BY THE CERTIFIED CONSULTING ARBORIST.
 - d) ATTACH AN ORANGE OR YELLOW PROTECTIVE FENCE (MIN. 3' HIGH_ TO THE CHAIN LINK FENCE.
 - e) WHEN AREAS UNDER THE TREE CANOPY CANNOT BE FENCED, ANOTHER METHOD SHALL BE REQUIRED, AS RECOMMENDED BY THE CERTIFIED CONSULTING ARBORIST AND APPROVED BY MMC.
 - f) POST "KEEP OUT" SIGNS IN BOTH SPANISH AND ENGLISH TO THE CHAIN LINK FENCE.
 - g) PLACE STORY POLES WHERE PROPOSED BUILDING(S) AND TREE BRANCHES INTERFACE.
 - h) SUBMIT TO MMC A TREE MAINTENANCE SCHEDULE PREPARED BY THE CERTIFIED CONSULTING ARBORIST.
 - i) IF A TREE IS ADJACENT TO OR IN THE IMMEDIATE PROXIMITY TO A GRADE SLOPE OF 8% (23 DEGREES) OR MORE, PROVIDE ADEQUATE EROSION CONTROL.
5. THE CERTIFIED CONSULTING ARBORIST SHALL VERIFY IN WRITING TO THE OWNER/PERMITTEE THAT THE TREE PROTECTION ZONE HAS BEEN ESTABLISHED.
6. THE OWNER/PERMITTEE SHALL SUBMIT THE CERTIFIED CONSULTING ARBORIST'S VERIFICATION LETTER TO MMC BEFORE SCHEDULING A SITE INSPECTION WITH MMC, THE RE AND/OR BUILDING INSPECTOR, CERTIFIED CONSULTING ARBORISTS, AND LANDSCAPE PROFESSIONAL.
7. RESTRICTIONS WITH THE TREE PROTECTION ZONE TO BE MONITORED BY THE CERTIFIED CONSULTING ARBORIST AS FOLLOWS:
 - a) TREE PRUNING OR ROOT PRUNING WILL ONLY OCCUR UNDER THE GUIDANCE OF THE CERTIFIED CONSULTING ARBORIST WITH WRITTEN APPROVAL FROM MMC.
 - b) PRUNING FOR THE VERTICAL CLEARANCE OF BUILDINGS, TRAFFIC, AND CONSTRUCTION EQUIPMENT SHALL BE PERFORMED BY THE CERTIFIED CONSULTING ARBORIST ONLY.
 - c) CONSTRUCTION EQUIPMENT AND MATERIALS SUCH AS STOCKPILING OF SOIL, VEHICLE USE, FOOT TRAFFIC, UTILITY CONTAINERS, AND STORAGE OF ANY KIND, ARE PROHIBITED.
 - d) THE EXISTING GRADE SHALL NOT BE DISTURBED UNLESS OTHERWISE APPROVED BY MMC, THE RE, AND/OR BUILDING INSPECTOR.
 - e) ROOT SYSTEMS SHALL BE PROTECTED FROM FLOODING, EROSION, CHEMICAL SPILLS, AND EXCESSIVE VARIATIONS IN SOIL MOISTURE BY CONSTRUCTION ACTIVITY.
8. ANY MODIFICATIONS AND/OR ADDITIONS TO THE APPROVED TREE PROTECTION ZONE SHALL BE SUBMITTED BY THE CERTIFIED CONSULTING ARBORIST WITH ADEQUATE INFORMATION TO MMC FOR APPROVAL.
9. THE OWNER/PERMITTEE SHALL BE RESPONSIBLE FOR A MINIMUM 3 YEAR MONITORING AND MAINTENANCE PERIOD, DESIGNED BY THE CERTIFIED CONSULTING ARBORIST AFTER CONSTRUCTION TO ENSURE THE SUCCESS OF PROTECTED AND/OR REPLACEMENT TREES.
10. THE CERTIFIED CONSULTING ARBORIST SHALL PROVIDE ANNUAL REPORTS TO AND REQUEST INSPECTIONS FROM MMC DURING THE MONITORING AND MAINTENANCE PERIOD.

PROPOSED STREET TREE REMOVAL & REPLACEMENT

1. ALL PROPOSED REMOVAL OF EXISTING STREET TREES SHALL BE REVIEWED AND APPROVED BY THE CITY PARKS DEPARTMENT.
2. ALL APPROVED STREET TREE REMOVAL AND REPLACEMENT REQUIRES A REPLACEMENT RATIO OF 2:1.

TREE SUREVY - TO REMOVE			
NUMBER	TREE SPECIES	CALIPER	ACTION
T-1	FICUS MICROCARPA	12"	PROPOSED REMOVAL
T-2	FICUS MICROCARPA	12"	PROPOSED REMOVAL
T-3	FICUS MICROCARPA	8"	PROPOSED REMOVAL
T-4	FICUS MICROCARPA	8"	PROPOSED REMOVAL
T-5	FICUS MICROCARPA	14"	PROPOSED REMOVAL
T-6	FICUS MICROCARPA	4"	PROPOSED REMOVAL
T-7	FICUS MICROCARPA	14"	PROPOSED REMOVAL
T-8	FICUS MICROCARPA	14"	PROPOSED REMOVAL
T-9	FICUS MICROCARPA	12"	PROPOSED REMOVAL
T-10	FICUS MICROCARPA	10"	PROPOSED REMOVAL
T-11	FICUS MICROCARPA	MULTI	REMOVE
T-12	FICUS MICROCARPA	14"	REMOVE
T-13	FICUS MICROCARPA	0'-10"	REMOVE
T-14	FICUS MICROCARPA	16"	REMOVE
T-15	FICUS MICROCARPA	10"	REMOVE
T-16	FICUS MICROCARPA	6"	REMOVE
T-17	FICUS MICROCARPA	10"	REMOVE
T-18	FICUS MICROCARPA	6"	REMOVE
T-19	FICUS MICROCARPA	10"	REMOVE
T-20	EUCALYPTUS SPP.	16"	REMOVE
T-21	EUCALYPTUS SPP.	16"	REMOVE
T-22	FICUS MICROCARPA	6"	REMOVE
T-23	FICUS MICROCARPA	8"	REMOVE
T-24	FICUS MICROCARPA	8"	REMOVE
T-25	FICUS MICROCARPA	12"	REMOVE
T-26	FICUS MICROCARPA	10"	REMOVE
T-27	FICUS MICROCARPA	10"	REMOVE
T-28	FICUS MICROCARPA	12"	REMOVE
T-29	FICUS MICROCARPA	12"	REMOVE
T-30	FICUS MICROCARPA	10"	PROPOSED REMOVAL
T-31	FICUS MICROCARPA	10"	PROPOSED REMOVAL
T-32	FICUS MICROCARPA	12"	PROPOSED REMOVAL
T-33	FICUS MICROCARPA	4"	PROPOSED REMOVAL
T-34	FICUS MICROCARPA	8"	PROPOSED REMOVAL

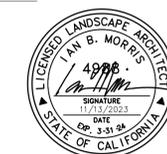


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SUBMITTAL:
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SHEET TITLE:
SHADE COVERAGE EXHIBIT
SHEET NUMBER:
L1.09

2019 CAL GREEN BUILDINGS CODE SHADE REQUIREMENTS

- SHADE TREE PLANTINGS, MINIMUM #10 CONTAINER SIZE OR EQUAL, SHALL BE INSTALLED TO PROVIDE SHADE OVER 50 PERCENT OF THE PARKING AREA WITHIN 15 YEARS. EXCEPTION: THE SURFACE PARKING AREA COVERED BY SOLAR PHOTOVOLTAIC SHADE STRUCTURES, OR SHADE STRUCTURES, WITH ROOFING MATERIALS THAT COMPLY WITH TABLE A5.106.11.2.2, IN APPENDIX A5, ARE NOT INCLUDED IN THE TOTAL AREA CALCULATION.

- SHADE TREE PLANTINGS, MINIMUM #10 CONTAINER SIZE OR EQUAL SHALL BE INSTALLED TO PROVIDE SHADE OF 20% OF THE LANDSCAPE AREA WITHIN 15 YEARS. EXCEPTION: PLAYFIELDS FOR ORGANIZED SPORT ACTIVITY ARE NOT INCLUDED IN THE TOTAL AREA CALCULATION.

- SHADE TREE PLANTINGS, MINIMUM #10 CONTAINER SIZE OR EQUAL SHALL BE INSTALLED TO PROVIDE SHADE OVER 20% OF THE HARDSCAPE AREA WITHIN 15 YEARS. EXCEPTION: WALKS, HARDSCAPE AREAS COVERED BY SOLAR PHOTOVOLTAIC SHADE STRUCTURES, AND HARDSCAPE AREAS COVERED BY SHADE STRUCTURES WITH ROOFING MATERIALS THAT COMPLY WITH TABLE A5.106.2.2 IN APPENDIX A5, ARE NOT INCLUDED IN THE TOTAL AREA CALCULATION.

SHADE COVERAGE			
	UNCOVERED LANDSCAPE AREA	19,609 SF	(25,260 TOTAL)
	LANDSCAPE COVERAGE	5,651 SF	
	LANDSCAPE PERCENTAGE	22.4%	
	UNCOVERED HARDSCAPE AREA	26,892 SF	(36,272 TOTAL)
	HARDSCAPE COVERAGE	9,380 SF	
	HARDSCAPE PERCENTAGE	25.9%	
	UNCOVERED PARKING AREA	4,239 SF	(9,175 TOTAL)
	PARKING COVERAGE	4,936 SF	
	PARKING PERCENTAGE	53.8%	





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SHEET TITLE:
LANDSCAPED PARKING LOT EXHIBIT

SHEET NUMBER:
L1.10

Item #1 222 of 269

LANDSCAPED PARKING AREA		
CATEGORY	PERCENTAGE	AREA
VEHICULAR USE AREA	95.10%	23,908 SF
LANDSCAPE AREA	4.90%	1,232 SF
TOTAL	100%	25,139 SF





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SDP 2023-0014

JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISIONS:

SHEET TITLE:
PROJECT
INFORMATION

SHEET NUMBER:
G01

Item #1 224 of 269

PROJECT INFORMATION

PROJECT NAME: CVMU (CARLSBAD VILLAGE MIXED-USE)
ADDRESS: 945-1065 CARLSBAD VILLAGE DRIVE
CARLSBAD CA 92008
OWNER: GRT CARLSBAD VILLAGE, LLC
PROJECT DESCRIPTION: THE PROJECT PROPOSES TO DEVELOP A MIXED-USE DEVELOPMENT CONSISTING OF 218 MULTI-FAMILY UNITS (27 UNITS WOULD BE RESERVED FOR VERY LOW INCOME HOUSEHOLDS), AS WELL AS 13,800 SQUARE FEET OF COMMERCIAL SPACE, INCLUDING OVER 40,000 SQUARE FEET OF OPEN SPACE ON A 4.12 ACRE SITE. THE PROPOSED COMMERCIAL USES WOULD BE CONTAINED IN TWO, ONE-STORY BUILDINGS ALONG CARLSBAD VILLAGE DRIVE, AND THE RESIDENTIAL UNITS, INCLUDING THE AFFORDABLE UNITS, WOULD BE SPREAD ACROSS TWO FIVE STORY RESIDENTIAL BUILDINGS. 340 VEHICULAR PARKING SPACES WILL BE PROVIDED AT-GRADE SURROUNDING THE COMMERCIAL AND RESIDENTIAL BUILDINGS AND WITHIN AN ABOVE-GRADE PARKING STRUCTURE. THE SITE IS CURRENTLY OCCUPIED BY A RETAIL SHOPPING CENTER WITH A TOTAL FLOOR AREA OF APPROXIMATELY 58,735 SQUARE FEET, AS WELL AS A SURFACE PARKING LOT THAT WOULD BE DEMOLISHED DURING PROJECT CONSTRUCTION.

CONSTRUCTION TYPE: 1 STORY TYPE V-A AND 5 STORY TYPE III-A, 5 STORY TYPE IA
GENERAL PLAN: VILLAGE-BARIO (V-B)
ZONING: VILLAGE-BARRIO ZONE WITHIN THE "FC" FREEWAY COMMERCIAL DISTRICT OF THE VILLAGE & BARRIO MASTER PLAN (VBMP)
FLOOD ZONE: X
GROSS SITE AREA: 179,312 SF (4.12 ACRES)
REQUIRED SETBACKS: FRONT: 10 FEET
SIDE: 0 FEET
REAR: 10 FEET
PROPOSED BLDG HEIGHT: -BLDG C: 56'-0" AND 5 STORIES
T.O. PARAPET 60'-6", T.O. STAIR 66'-0", T.O. ELEV TOWER 60'-0"
-BLDG D: 53' - 10" AND 5 STORIES
T.O. PARAPET 58'-4", T.O. STAIR 63'-10", T.O. ELEV TOWER 67'-10"
-COMMERCIAL BLDGS: 24'-6" AND 1 STORY
T.O. PARAPET 20'-0", TO EQUIP. WELL 15'-6"
-GARAGE: 53' - 10" AND 5 STORIES
T.O. PARAPET 58'-4", T.O. STAIR 63'-10", T.O. ELEV TOWER 67'-10"

OCCUPANCY TYPE: R-2 RESIDENTIAL, R-2 ACCESSORY, S-2 PARKING, A ASSEMBLY, M MERCANTILE
AREA OF SITE WHICH IS UNDEVELOPABLE: 0 SF PER ZONING ORDINANCE SECTION 21.53.230
SEWER DISTRICT: CITY OF CARLSBAD WASTEWATER DIVISION
WATER DISTRICT: CARLSBAD MUNICIPAL WATER DISTRICT
SCHOOL DISTRICT: CARLSBAD UNIFIED SCHOOL DISTRICT
CLIMATE ACTION PLAN: 0.90 ALLOWED OUTDOOR LIGHTING POWER
0.95 ENERGY BUDGET
ON-SITE RENEWABLE ENERGY PHOTOVOLTAIC: PER CEC SECTION 170.1(f) AND 141.2
HOT WATER HEATING: CEC SECTION 120.11 AND 150(n)5

VICINITY MAP



LEGAL DESCRIPTION

TRACT ONE: FEE
PARCELS A THROUGH D AS SHOWN ON "CITY OF CARLSBAD M.S. 2018-0014", IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO PARCEL MAP THEREOF NO. 21699 FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, JULY 16, 2019.
TRACT TWO: LEASEHOLD
PARCEL 1:
LOTS 1 TO 16 INCLUSIVE, IN BLOCK 74 AND THOSE PORTIONS OF BLOCK 74, TOGETHER WITH PORTIONS OF 6TH STREET AND 7TH STREET AND OAK AVENUE AS CLOSED AND VACATED TO PUBLIC USE AND THE ALLEYS ADJOINING AND LYING WITHIN THE SAID BLOCKS IN THE TOWN OF CARLSBAD, IN THE CITY OF CARLSBAD, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF NO. 535, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, THE ABOVE DESCRIBED BLOCKS AND PORTIONS THEREOF BEING ALSO A PORTION OF TRACT 115 OF THE TOWN OF CARLSBAD, ACCORDING TO AMENDED MAP THEREOF NO. 775, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, LYING WITHIN THE FOLLOWING DESCRIBED LAND: (SEE SURVEY FOR DESCRIPTION)
PARCEL 2:
ALL OF LOTS 17 TO 32 INCLUSIVE IN BLOCK 66 OF TOWN OF CARLSBAD, IN THE CITY OF CARLSBAD, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF NO. 535, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, MAY 2, 1888, AND AMENDED MAP NO. 775, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, FEBRUARY 15, 1894.
TOGETHER WITH THAT PORTION OF 6TH STREET AS SHOWN ON THE SAID MAP NOS. 535 AND 775, AND MORE PARTICULARLY DESCRIBED AS FOLLOWS: THE SOUTHWESTERLY HALF OF 6TH STREET AS SHOWN ON MAPS ABUTTING THE SAID LOTS 17 TO 32 INCLUSIVE, IN THE SAID BLOCK 66 AS SHOWN ON THE MAP NOS. 535 AND 775.
ALSO KNOWN AS:
PARCELS A THROUGH D AS SHOWN ON "CITY OF CARLSBAD M.S. 2018-0014", IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO PARCEL MAP THEREOF NO. 21699 FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, JULY 16, 1990.

REQUIRED PARKING

RETAIL:		PER VILLAGE BARRIO MASTER PLAN	
Retail: 1/1415 SF	9,800 / 415 = 23.6 = 24		
Restaurant: 1/170 SF	4,000 / 170 = 23.5 = 24		
SUBTOTAL:		48 STALLS	
RESIDENTIAL:		PER GOVERNMENT CODE SECTION 65915(p)(2)	
STUDIO: 5 / unit	15 x 5 = 8 STALLS		
1-BR: 5 / unit	87 x 5 = 44 STALLS		
2-BR: 5 / unit	90 x 5 = 45 STALLS		
3-BR: 5 / unit	26 x 5 = 13 STALLS		
SUBTOTAL:		110 STALLS	
TOTAL REQUIRED:		158 STALLS	
REQUIRED ACCESSIBLE PARKING			
RETAIL:			
ACCESSIBLE	3 OF 56 = 3 LESS 1 VAN = 2		
VAN ACCESSIBLE	1 OF 3 = 1		
RESIDENTIAL:			
ACCESSIBLE	8 OF 284 = 7 STALLS		
VAN ACCESSIBLE	1 OF 8 = 1 STALLS		
REQUIRED EV PARKING PER CALGREEN			
RETAIL:			
EV CHARGERS	3 OF 57 = 3 STALLS		
EV CAPABLE	13 OF 57 LESS 3 INSTALLED = 10 STALLS		
RESIDENTIAL:			
EV CHARGERS	5% OF 284 = 15 STALLS		
EV CAPABLE	10% OF 284 LESS 15 INSTALLED = 14 STALLS		
EV READY	25% OF 284 = 71 STALLS		

*PER AB 2097, PROJECT IS NOT SUBJECT TO MINIMUM PARKING REQUIREMENTS

PROPOSED PARKING

LOCATIONS OF PROVIDED PARKING	
PARKING STRUCTURE	SURFACE PARKING
LEVEL 1: 53 SPACES	TOTAL: 51 SPACES
LEVEL 2: 46 SPACES	
LEVEL 3: 46 SPACES	
LEVEL 4: 47 SPACES	
LEVEL 5: 48 SPACES	
ROOF: 49 SPACES	
TOTAL: 289 SPACES	

RETAIL:	56 STALLS	45 STANDARD, 11 COMPACT STALLS
RESIDENTIAL:	284 STALLS	277 STANDARD, 7 COMPACT STALLS
TOTAL PROVIDED:	340 STALLS	

OPEN SPACE REQUIRED

TOTAL OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 20% of site = 35,862 SF
PRIVATE OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 80 SF per Unit (min. dimension 6 ft) 80 SF x 218 units = 17,440 SF
COMMON OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 25 SF per Unit (min. dimension 10 ft) 25 SF x 218 units = 5,450 SF
TOTAL REQUIRED:	22,890 SF

OPEN SPACE PROVIDED

TYPE	LEVEL	QTY	
COMMON OUTDOOR SPACE: COURTYARD	LEVEL 1	1	22,517 SF
COMMON OPEN SPACE: ROOF DECK	LEVEL 5	1	593 SF
COMMON OPEN SPACE SUBTOTAL: 23,110 SF			
PRIVATE OPEN SPACE (PATIOS)	LEVEL 1	35	2,800 SF
PRIVATE OPEN SPACE	LEVEL 2	49	3,920 SF
PRIVATE OPEN SPACE	LEVEL 3	49	3,920 SF
PRIVATE OPEN SPACE	LEVEL 4	46	3,680 SF
PRIVATE OPEN SPACE	LEVEL 5	43	3,440 SF
PRIVATE OPEN SPACE SUBTOTAL: 17,760 SF			
GRAND TOTAL: 40,870 SF			
= 23% OF SITE			

BUILDING CODES USED

- 2022 CALIFORNIA BUILDING CODE (CBC), PART 2, TITLE 24 C.C.R. AND AMENDMENTS
- 2022 CALIFORNIA ELECTRICAL CODE (CEC), PART 3, TITLE 24 C.C.R. AND AMENDMENTS
- 2022 CALIFORNIA MECHANICAL CODE (CMC), PART 4, TITLE 24, C.C.R. AND AMENDMENTS
- 2022 CALIFORNIA PLUMBING CODE (CPC), PART 5, TITLE 24, C.C.R. AND AMENDMENTS
- 2022 CALIFORNIA FIRE CODE (CFC), PART 9, TITLE 24, C.C.R. AND AMENDMENTS
- 2022 CALIFORNIA REFERENCED STANDARDS CODE, PART 12, TITLE 24 C.C.R.
- 2022 CALIFORNIA GREEN BUILDING CODE, TITLE 24, PART 6

GROSS AREA

COMMERCIAL A:	8,000 SF
COMMERCIAL B:	5,800 SF
COMMERCIAL SUBTOTAL: 13,800 SF	
PARKING LEVEL 1	22,894 SF
PARKING LEVEL 2	22,894 SF
PARKING LEVEL 3	22,894 SF
PARKING LEVEL 4	22,894 SF
PARKING LEVEL 5	22,894 SF
PARKING SUBTOTAL: 114,470 SF	
RESIDENTIAL LEVEL 1	52,394 SF
RESIDENTIAL LEVEL 2	52,521 SF
RESIDENTIAL LEVEL 3	52,521 SF
RESIDENTIAL LEVEL 4	50,614 SF
RESIDENTIAL LEVEL 5	47,405 SF
RESIDENTIAL SUBTOTAL: 255,455 SF	
GRAND TOTAL: 383,725 SF	



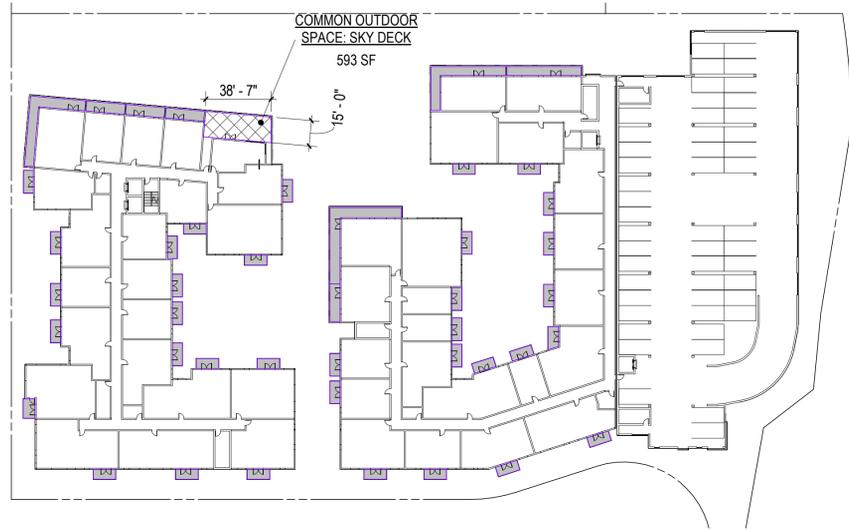
LEGEND

- PROPOSED GROUND FLOOR FOOTPRINT
- EXISTING BUILDINGS (NOT A PART)
- PROPOSED UPPER FLOORS FOOTPRINT
- EXISTING BUILDINGS TO BE DEMOLISHED

PLOT PLAN

GENERAL NOTES

- THE WORK SHALL CONFORM TO THE APPLICABLE BUILDING CODE, AND ALL OTHER ORDINANCES, CODES, AND REGULATIONS LISTED IN THE SPECIFICATIONS OR ON THE DRAWINGS, AND REQUIRED BY LOCAL BUILDING AUTHORITIES. THE GOVERNING CODES, RULES, AND REGULATIONS ARE COLLECTIVELY REFERRED TO AS "THE CODE". CONTRACTOR SHALL REPORT ANY INCONSISTENCIES, CONFLICTS OR OMISSIONS HE MAY DISCOVER TO THE ARCHITECT FOR INTERPRETATION PRIOR TO PERFORMING THE WORK.
 - CONSTRUCTION SHALL COMPLY WITH PERTINENT HEALTH AND SAFETY REGULATIONS FOR REQUIRED METHODS PROTECTING PUBLIC AND CONSTRUCTION WORKER'S HEALTH AND SAFETY DURING THE CONSTRUCTION PERIOD.
 - BEFORE ORDERING ANY MATERIAL, OR DOING ANY WORK, THE CONTRACTOR SHALL VERIFY ALL MEASUREMENTS AT THE BUILDING SITE AND SHALL BE RESPONSIBLE FOR CORRECTNESS OF SAME.
 - DIMENSIONS SHALL BE AS INDICATED ON THE DRAWINGS. CLARIFICATIONS, IF REQUIRED, SHALL BE OBTAINED FROM THE ARCHITECT. THE DRAWING SHALL NOT BE SCALED.
 - ALL ITEMS MARKED NIC ON THE SPECIFICATIONS AND DRAWINGS MEANS NOT IN CONTRACT.
 - DO NOT MAKE ANY SUBSTITUTIONS WITHOUT PERMISSION OF THE ARCHITECT OR THE OWNER.
 - WORK INDICATED ON THE DRAWINGS OR IN THE SPECIFICATIONS AS NIC, OR BY SEPARATE CONTRACTORS, IS NOT PART OF THIS CONTRACT. CONTRACTOR SHALL COOPERATE FULLY WITH ALL SEPARATE CONTRACTORS EMPLOYED BY THE OWNER.
 - THE CLIENT, ARCHITECT, CONSULTANTS, AND ALL INSPECTORS FROM PERTINENT AGENCIES SHALL BE PERMITTED ACCESS TO THE JOB SITE AT ALL TIMES DURING NORMAL WORKING HOURS.
 - WINDOW AND DOOR DIMENSIONS FOR PREFABRICATED WINDOW AND DOOR UNITS HAVE BEEN ROUNDED TO THE NEAREST INCH ON THE DRAWINGS. SPECIFIC DIMENSIONS BY MANUFACTURERS MAY VARY FROM THE DRAWINGS.
 - THE CONTRACTOR SHALL VERIFY LOCATION AND SIZE OF ALL FLOOR, ROOF, AND WALL OPENINGS WITH ALL APPLICABLE DRAWINGS.
 - DETAILS ARE INTENDED TO SHOW THE INTENT OF THE DESIGN. MINOR MODIFICATIONS MAY BE REQUIRED TO SUIT THE FIELD DIMENSIONS OR CONDITIONS, AND SUCH MODIFICATIONS SHALL BE INCLUDED AS PART OF THE WORK OF THE CONTRACT.
 - THE CONTRACT DRAWINGS AND SPECIFICATIONS REPRESENT THE FINISHED STRUCTURE AND DO NOT INDICATE THE METHODS OF CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES, AND PROCEDURES INCLUDING, BUT NOT LIMITED TO BRACING AND SHORING. OBSERVATION VISITS TO THE SITE BY FIELD REPRESENTATIVES OF THE ARCHITECT AND/OR ENGINEER SHALL NOT INCLUDE INSPECTIONS OF THE PROTECTIVE MEASURES TO THE CONSTRUCTION PROCEDURES.
- ANY SUPPORT SERVICES PERFORMED BY THE ARCHITECT AND/OR ENGINEER DURING THE CONSTRUCTION SHALL BE DISTINGUISHED FROM CONTINUOUS AND DETAILED INSPECTION SERVICES WHICH IS FURNISHED BY OTHERS. THESE SUPPORT SERVICES PERFORMED BY THE ARCHITECT AND/OR ENGINEER, WHETHER OF MATERIAL OR WORK, AND WHETHER PERFORMED PRIOR TO, DURING, OR AFTER COMPLETION OF CONSTRUCTION, ARE PERFORMED SOLELY FOR THE PURPOSE OF ASSISTING IN QUALITY CONTROL AND IN ACHIEVING GENERAL CONFORMANCE WITH CONTRACT DOCUMENTS, BUT DO NOT GUARANTEE CONTRACTOR'S PERFORMANCE AND SHALL NOT BE CONSTRUED AS SUPERVISION OF CONSTRUCTION.
- IN THE EVENT OF CONFLICTING REQUIREMENTS BETWEEN ITEMS ON THE DRAWING OR BETWEEN ITEMS IN THE SPECIFICATIONS, OR BETWEEN ITEMS ON THE DRAWINGS AND IN THE SPECIFICATIONS, THE MORE STRINGENT OR COSTLY SHALL GOVERN, UNLESS DECIDED OTHERWISE BY THE ARCHITECT.



5TH FLOOR OPEN SPACE PLAN
1" = 50'-0"

5



4TH FLOOR OPEN SPACE PLAN
1" = 50'-0"

4



3RD FLOOR OPEN SPACE PLAN
1" = 50'-0"

3



2ND FLOOR OPEN SPACE PLAN
1" = 50'-0"

2



COMMON OPEN SPACE:
COURTYARD
22517 SF

1ST FLOOR OPEN SPACE PLAN
1" = 50'-0"

1

NOTES

REQUIRED OPEN SPACE

TOTAL OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 20% of site = 35,862 SF
PRIVATE OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 80 SF per Unit (min. dimension 6 ft)
	80 SF x 218 units = 17,440 SF
COMMON OPEN SPACE:	PER VILLAGE BARRIO MASTER PLAN
REQUIRED:	Min. 25 SF per Unit (min. dimension 10 ft)
	25 SF x 218 units = 5,450 SF
TOTAL REQUIRED:	22,890 SF

PROPOSED OPEN SPACE

TYPE	LEVEL	QTY	
COMMON OUTDOOR SPACE: COURTYARD	LEVEL 1	1	22,517 SF
COMMON OPEN SPACE: SKY DECK	LEVEL 5	1	593 SF
COMMON OPEN SPACE SUBTOTAL: 23,110 SF			
PRIVATE OPEN SPACE (PATIOS)			
PRIVATE OPEN SPACE	LEVEL 1	35	2,800 SF
PRIVATE OPEN SPACE	LEVEL 2	49	3,920 SF
PRIVATE OPEN SPACE	LEVEL 3	49	3,920 SF
PRIVATE OPEN SPACE	LEVEL 4	46	3,680 SF
PRIVATE OPEN SPACE	LEVEL 5	43	3,440 SF
PRIVATE OPEN SPACE SUBTOTAL: 17,760 SF			
GRAND TOTAL: 40,870 SF			
= 23% OF SITE			

SHEET NOTES

- PROPERTY OPEN SPACE**
- A. A MINIMUM OF 20 PERCENT OF PROPERTY MUST BE MAINTAINED AS OPEN SPACE.
 - B. PUBLIC AND RESIDENTIAL PRIVATE AND COMMON OPEN SPACE MAY BE COUNTED TOWARD ACHIEVING THE PROPERTY OPEN SPACE MINIMUM REQUIREMENT.
 - C. OPEN SPACE MAY BE DEDICATED TO LANDSCAPE PLANTERS, OPEN SPACE POCKETS AND/OR CONNECTIONS, ROOF GARDENS/PATIOS, BALCONIES, OTHER PATIOS, AND/OR OUTDOOR EATING AREAS.
 - D. NO PARKING SPACES OR AISLES ARE PERMITTED IN THE OPEN SPACE.

- RESIDENTIAL PRIVATE OPEN SPACE**
- A. PRIVATE OPEN SPACE SHALL BE PROVIDED AT A MINIMUM OF 80 SQUARE FEET PER UNIT WITH A MINIMUM DIMENSION OF 6 FEET IN ANY DIRECTION. THIS REQUIREMENT MAY BE SATISFIED BY MORE THAN ONE PRIVATE OPEN SPACE AREA.

- RESIDENTIAL COMMON OPEN SPACE**
- A. RESIDENTIAL COMMON OPEN SPACE SHALL BE PROVIDED FOR PROJECTS WITH MORE THAN 10 UNITS.
 - B. COMMON OPEN SPACE SHALL BE PROVIDED AT A MINIMUM OF 25 SQUARE FEET PER UNIT WITH A MINIMUM DIMENSION OF 10 FEET IN ANY DIRECTION.
 - C. COMMON OPEN SPACE SHALL BE PURPOSEFULLY DESIGNED AS ACTIVE OR PASSIVE RECREATIONAL FACILITIES.
 - D. ROOFTOP OPEN SPACE MAY SATISFY THIS REQUIREMENT, PROVIDED IT IS AVAILABLE FOR USE BY ALL RESIDENTS.

LEGEND

- COMMON OPEN SPACE: COURTYARD
- COMMON OUTDOOR SPACE: ROOF DECK
- COMMON OUTDOOR SPACE: SKY DECK
- PRIVATE OPEN SPACE



3573 HAYDEN AVENUE
CULVER CITY, CA 90232
310.399.7975
KFALOSANGELES.COM

CVMU
945-1065 CARLSBAD VILLAGE DRIVE
CARLSBAD, CA 92008

GRT CARLSBAD VILLAGE, LLC
11661 SAN VICENTE BLVD., SUITE 850
LOS ANGELES CA 90049

THIS DRAWING AND THE INFORMATION CONTAINED HEREIN ARE THE COPYRIGHTED WORK OF KFA, LLC AND MAY NOT BE REPRODUCED WITHOUT WRITTEN PERMISSION.

SDP 2023-0014

JOB NUMBER: 2022-004.00

DATE: 11.13.2023

REVISION:

SHEET TITLE:
OPEN SPACE CALCULATIONS

SHEET NUMBER:
G03



3573 HAYDEN AVENUE
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SDP 2023-0014

JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISION:

SHEET TITLE:
BUILDING AREA

SHEET NUMBER:
G04

Item #1 226 of 269

NOTES

GROSS AREA

COMMERCIAL A:	8,000 SF
COMMERCIAL B:	5,800 SF
COMMERCIAL SUBTOTAL: 13,800 SF	
PARKING LEVEL 1	22,894 SF
PARKING LEVEL 2	22,894 SF
PARKING LEVEL 3	22,894 SF
PARKING LEVEL 4	22,894 SF
PARKING LEVEL 5	22,894 SF
PARKING SUBTOTAL: 114,470 SF	
RESIDENTIAL LEVEL 1	52,394 SF
RESIDENTIAL LEVEL 2	52,521 SF
RESIDENTIAL LEVEL 3	52,521 SF
RESIDENTIAL LEVEL 4	50,614 SF
RESIDENTIAL LEVEL 5	47,405 SF
RESIDENTIAL SUBTOTAL: 255,455 SF	
GRAND TOTAL: 383,725 SF	

SHEET NOTES

AREA IN SQUARE FEET CONFINED WITHIN THE EXTERIOR WALLS OF A BUILDING, BUT NOT INCLUDING THE AREA OF THE FOLLOWING: EXTERIOR WALLS, STAIRWAYS, SHAFTS, ROOMS HOUSING BUILDING EQUIPMENT OR MACHINERY, PARKING AREAS WITH ASSOCIATED DRIVEWAYS AND RAMPS, SPACE FOR THE LANDING AND STORAGE OF HELICOPTERS, AND BASEMENT STORAGE AREAS.

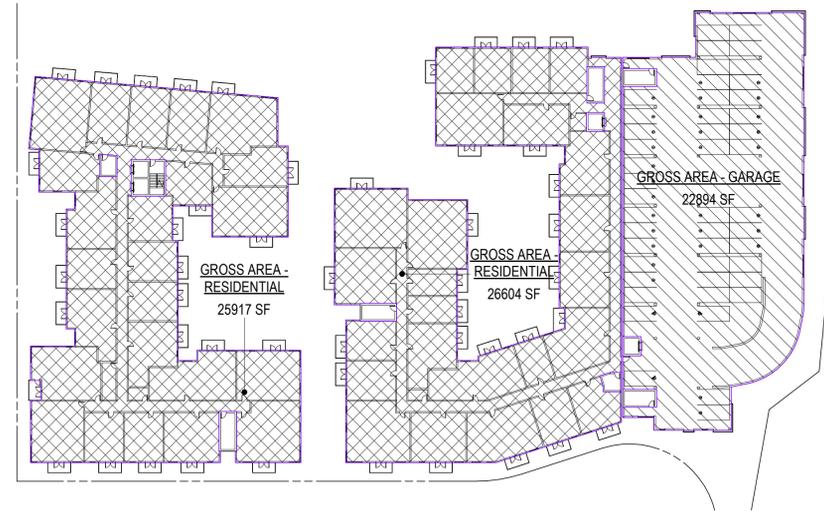
LEGEND

- GROSS AREA - COMMERCIAL A
- GROSS AREA - COMMERCIAL B
- GROSS AREA - GARAGE
- GROSS AREA - RESIDENTIAL



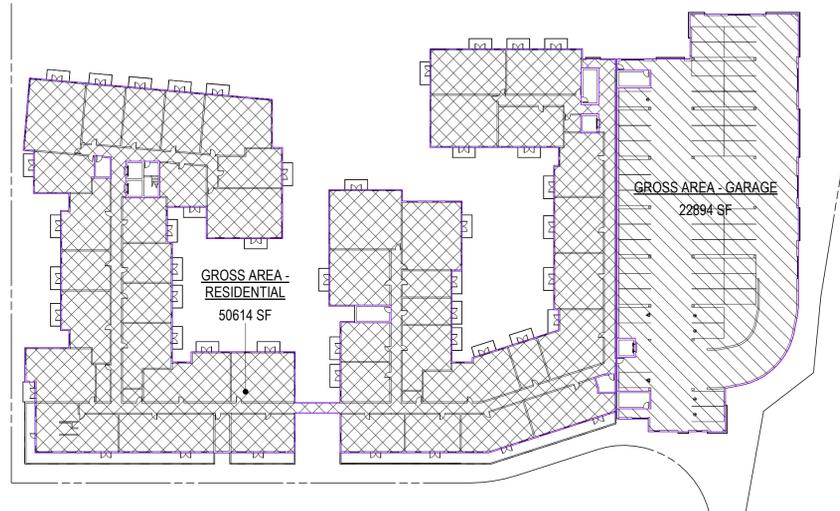
5TH FLOOR GROSS AREA PLAN
1" = 50'-0"

5



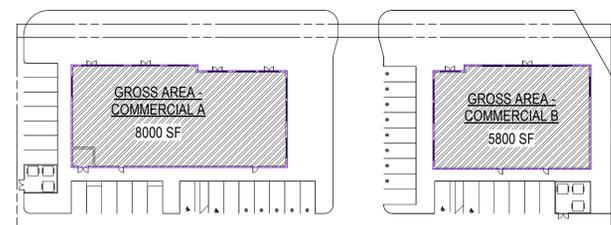
2ND FLOOR GROSS AREA PLAN
1" = 50'-0"

2



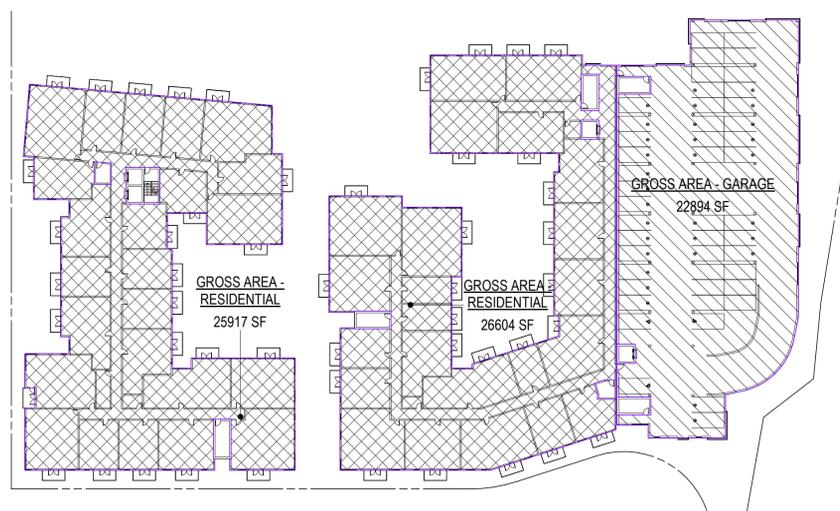
4TH FLOOR GROSS AREA PLAN
1" = 50'-0"

4



1ST FLOOR GROSS AREA PLAN
1" = 50'-0"

1



3RD FLOOR GROSS AREA PLAN
1" = 50'-0"

3

LEGAL DESCRIPTION

PARCELS A THROUGH D AS SHOWN ON "CITY OF CARLSBAD M.S. 2018-0014", IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO PARCEL MAP THEREOF NO. 21699 FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, JULY 16, 2019.

BASIS OF BEARINGS

CALIFORNIA COORDINATE SYSTEM OF 1983, ZONE 6, EPOCH 1991.35 GRID BEARING BETWEEN FIRST-ORDER G.P.S. STATION NO. 119 AND FIRST-ORDER G.P.S. STATION NO. 121 PER ROS 17271

I.E., NORTH 40°22'26" WEST
DISTANCE SHOWN HEREON ARE GROUND LEVEL DISTANCES. THE COMBINED SCALE FACTOR AT G.P.S. STATION NO. 121 IS 0.999960210.
GRID DISTANCE = GROUND DISTANCE X COMBINED SCALE FACTOR.
QUOTED BEARING FROM REFERENCE DEEDS/MAPS OR MAY NOT BE IN TERMS OF SAID SYSTEM.

BENCHMARK

CITY OF CARLSBAD CONTROL POINT NO. 121, NGVD-29 ELEVATION 80.303'
2.5" DISK IN WEST CURB OPPOSITE 3334 EUREKA STREET AT ANGLE POINT IN CURB AT NORTH END OF PARALLEL PARKING SPACES. 17' SOUTHWEST OF POWER POLE NO. 24625.

ASSESSOR'S PARCEL NO.

203-320-53 THRU 56

UTILITIES

WATER-----CARLSBAD MUNICIPAL WATER DISTRICT
SEWER-----CITY OF CARLSBAD
STORM DRAIN-----CITY OF CARLSBAD
GAS AND ELECTRIC-----SAN DIEGO GAS & ELECTRIC
TELEPHONE-----SBC TELEPHONE
CABLE T.V.-----TIME WARNER CABLE
SCHOOL-----CARLSBAD UNIFIED SCHOOL DISTRICT

GENERAL NOTES

- EXISTING GENERAL PLAN DESIGNATION: VILLAGE-BARRIO (V-B)
- PROPOSED PROJECT IS LOCATED IN LOCAL FACILITIES MANAGEMENT PLAN ZONE 1.
- TOPOGRAPHY SHOWN ON THIS SURVEY IS BASED UPON AN AERIAL SURVEY BY TERRASCRIBE INC., IN SEPTEMBER 9TH, 2022
- ALL CUT AND FILL SLOPES ARE TO BE 2:1 MAXIMUM, EXCEPT AS OTHERWISE SHOWN ON PLAN.
- GRADING SHOWN HEREON IS PRELIMINARY AND IS SUBJECT TO REVISIONS MADE DURING THE FINAL DESIGN PROCESS, IN ACCORDANCE WITH ENGINEERING POLICY NO. 30, AND APPROVAL FROM THE CITY ENGINEER AND CITY PLANNER.
- ALL LOT DIMENSIONS SHOWN ARE APPROXIMATE. ALL LOTS TO MEET MINIMUM DIMENSIONS OF UNDERLYING ZONING.
- EARTHWORK QUANTITIES (GEOMETRIC): 10,000 C.Y. CUT, 1,200 C.Y. FILL
BASED ON THE GEOTECHNICAL REPORT, ANTICIPATED REMEDIAL EXCAVATIONS WILL BE APPROXIMATELY 2FT-3FT DEEP AND EXTEND 5FT OUTSIDE HORIZONTALLY BENEATH BUILDING PADS, 2FT DEEP AND 2FT OUTSIDE HORIZONTALLY BENEATH PEDESTRIAN HARDSCAPE AND 1FT-2FT OUTSIDE HORIZONTALLY BENEATH VEHICULAR PAVEMENTS. REMEDIAL SOILS WILL BE REMOVED AND REPLACED ONSITE.

REMEDIAL QUANTITY: 12,880 C.Y. (QUANTITY WILL SHRINK APPROXIMATELY 10%)
EXPORT: 7,520 C.Y. (AFTER REMEDIAL SHRINKAGE IS ACCOUNTED FOR)

- THE BEARINGS AND DISTANCES SHOWN WERE DETERMINED FROM A FIELD SURVEY DATED SEPTEMBER 2022.

FIRE HYDRANT FLOW REQUIREMENT IS 2,500 GPM.

- FLOOD ZONE DESIGNATION: X

PROPOSED 2-LOT PARCEL MAP

LOT 1 - COMMERCIAL: 0.94 ACRES
LOT 2 - RESIDENTIAL: 3.11 ACRES
PROJECT TOTAL: 4.05 ACRES

- MASTER DRAINAGE BASIN: BASIN B
PROPOSED DRAINAGE DISCHARGE: 25.4 CFS

WATER DEMAND:

LAND USE	QUANTITY	DEMAND FACTOR	AVERAGE DEMAND
MULTI-FAMILY RESIDENTIAL	218 DUs	185 GPD/DU	40,330 GPD
COMMERCIAL	0.32 AC	1,500 GPD/AC	480 GPD
TOTAL			40,810 GPD

PER TABLE 1, AVERAGE DAY DEMAND FOR THE CARLSBAD VILLAGE MIXED USE PROJECT IS 40,810 GPD OR 28.3 GPM. THE MAXIMUM DAY DEMAND IS 65,296 GPD OR 45.3 GPM (ADD X 1.6). THE PEAK HOUR DEMAND IS 118,349 GPD OR 82.2 GPM (ADD X 2.9).

SEWER GENERATION:

LAND USE	QUANTITY	SEWER GENERATION FACTOR	AVERAGE DAILY FLOW
MULTI-FAMILY RESIDENTIAL	218 DUs	160 GPD/DU	34,880 GPD
RETAIL SPACE	13,800 SQ. FT.	200 GPD/1,800 SQ. FT.	1,533 GPD
TOTAL			36,413 GPD

PER TABLE 2, AVERAGE DAILY SEWER FLOW FOR THE CARLSBAD VILLAGE MIXED USE PROJECT IS 36,413 GPD OR 25.3 GPM. THE PEAK DAILY FLOW IS 91,033 GPD OR 63.2 GPM (AVERAGE DAILY FLOW X 2.5).

ADT TABLE

Project Trip Generation Summary															
Carlsbad Village Mixed Use															
Land Use	Quantity	Daily Volumes									PM Peak Hour				
		Rate a	Volume	Rate	Split	In	Out	Total	Rate	Split	In	Out	Total		
Proposed Project															
Apartment (Multi-family units >20 DU/acre) b	218 DU	6 /DU	1,308	8%	20%	80%	21	84	105	9%	70%	30%	83	35	118
Supermarket	5.8 KSF	150 /KSF	870	4%	70%	30%	25	10	35	10%	50%	50%	44	43	87
Retail / Strip Commercial	2 KSF	40 /KSF	80	3%	60%	40%	1	1	2	9%	50%	50%	4	3	7
Quality Restaurant	6 KSF	100 /KSF	600	1%	60%	40%	4	2	6	8%	70%	30%	34	14	48
Project Subtotal			2,858				51	97	148				165	95	260
Mixed Use Reduction c			-616				-4	-4	-8				-39	-41	-80
Final Project Trips			2,242				47	93	140				126	54	180
Existing to be replaced															
Neighborhood Shopping Center d	58.7 KSF	120 /KSF	7,044	4%	60%	40%	169	113	282	10%	50%	50%	352	352	704
Net New Trip Generation			-4,802				-122	-20	-142				-226	-298	-524

Footnotes:

- Trip generation rates from SANGAG's (Nat So) Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region, April 2002. ("SANDAG Brief Guide")
 - The site acreage is 4.12 therefore the residential density calculates to 53 DU/acre (218 / 4.12 = 53).
 - Mixed use reduction for AM/PM peak hours calculated using NCHRP 8-51 Internal Trip Capture Estimation Tool. Daily reduction estimated using relationship between peak hours and daily volumes for unadjusted project trips.
 - Per SANDAG this land use is typically less than 15 acres, less than 125,000 sq. ft., w/usually grocery & drugstore, cleaners, beauty & barber shop, & fast food services)
- The existing 4.12 acre, 58.7 KSF site contains all of these individual uses including: grocery, drugstore, cleaners, barber shop, and eating establishments.

LEGEND

DESCRIPTION	SYMBOL
PROPOSED IMPROVEMENTS	
PROJECT BOUNDARY	---
PROP. LOT LINE	---
PROP. EASEMENT	---
PROP. CONTOURS	---
PROP. CUT SLP (2:1 MAX.) (UNLESS SHOWN OTHERWISE)	Y
PROP. FILL SLP (2:1 MAX.) (UNLESS SHOWN OTHERWISE)	Y
PROP. DAYLIGHT LINE	---
PROP. DISTURBANCE LIMIT (FOR COLOR AND FINISH, REFER TO ARCHITECTURAL PLANS)	D
PROP. RETAINING WALL	---
PROP. CURB AND GUTTER	---
PROP. CURB	---
PROP. RIBBON GUTTER	---
PROP. SIDEWALK	---
PROP. TURF	---
PROP. DRIVEWAY	---
PROP. PEDESTRIAN RAMP	---
PROP. STANDARD PARKING STALL	---
PROP. VAN ACCESSIBLE PARKING STALL	---
PROP. CONC. BROW DITCH	---
PROP. SWALE	---
PROP. PROP. RIPRAP	---
PROP. SLOPE GRADE	4%
PROP. FINISH FLOOR ELEVATION	FF=XX.XX
PROP. FINISH SURFACE ELEVATION	XX.XX FS
PROP. WATER SERVICE BUILDING POC	---
PROP. WATER SERVICE	---
PROP. PRIVATE WATER	---
PROP. WATER METER	---
PROP. WATER RPDA	---
PROP. WATER METER AND BACKFLOW ABOVE GROUND ASSEMBLY	---
PROP. FIRE SERVICE BUILDING POINT OF CONNECTION (STANDPIPE WHERE NOTED)	---
PROP. FIRE SERVICE	---
PROP. PRIVATE FIRE WATER	---
PROP. FIRE RPDA	---
PROP. IRRIGATION POINT OF CONNECTION	---
PROP. SEWER MAIN	---
PROP. SEWER MANHOLE	---
PROP. SEWER SERVICE BUILDING POINT OF CONNECTION	---
PROP. SEWER GREASE INTERCEPTOR	---
PROP. PRIVATE STORM DRAIN	---
PROP. TRENCH DRAIN	---
PROP. A-4 SD CLEANOUT	---
PROP. CURB INLET	---
PROP. PERMANENT BMP MODULAR WETLANDS UNIT SIZE 8'X16'	---
PROP. PERMANENT BMP MODULAR WETLANDS UNIT SIZE 8'X8'	---
RAISED BIOFILTRATION PLANTER	---
UNDREGROUND DETENTION VAULT	---
PROP. BROOKS BOX WITH PARKWAY GRATE	---
TRANSFORMER	---

EXISTING IMPROVEMENTS

CALTRANS RIGHT-OF-WAY	---
EX. CONTOURS	---
EX. RETAINING WALL	---
EX. CURB AND GUTTER	---
EX. RIBBON GUTTER	---
EX. DRIVEWAY	---
EX. STORM DRAIN	---
EX. EASEMENT	---
EX. CONTOUR	---
EX. FENCE	---
EX. CALTRANS GUARD RAIL	---
EX. STORM DRAIN PIPE	---
EX. STORM DRAIN STRUCTURE	---
EX. WATER PIPE	---
EX. WATER SERVICE	---
EX. WATER RPDA	---
EX. FIRE WATER PIPE	---
EX. FIRE WATER SERVICE	---
EX. FIRE WATER RPDA	---
EX. GATE VALVE	---
EX. SEWER PIPE	---
EX. SEWER MANHOLE	---
EX. SEWER SERVICE	---
EX. GAS PIPE	---
EX. ELECTRICAL UNDERGROUND	---
EX. OVERHEAD ELECTRICAL	---
EX. JOINT TRENCH	---
EX. OVERHEAD ELECTRICAL POLE	---
EX. OVERHEAD ELECTRICAL POLE WITH ST. LIGHT	---
EX. ORNAMENTAL ST. LIGHT	---
EX. BOLLARDS	---



MARINA NATASHA WURST, R.C.E. C67517	DATE	REGISTRATION	DESIGNED	PM
	5/11/2023	EXPIRES 6-30-25	BY: AM/JT	REVIEW: MW

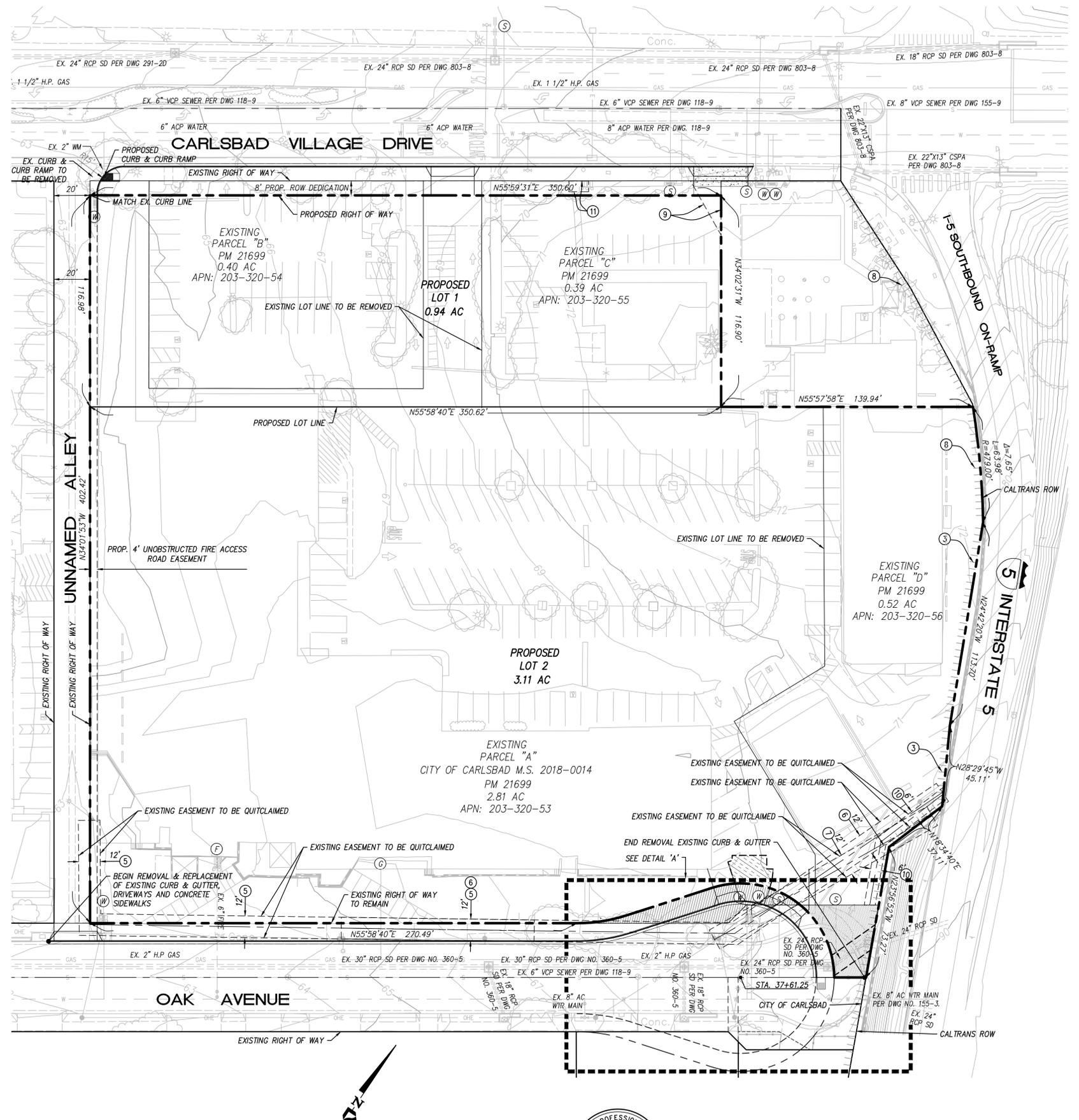
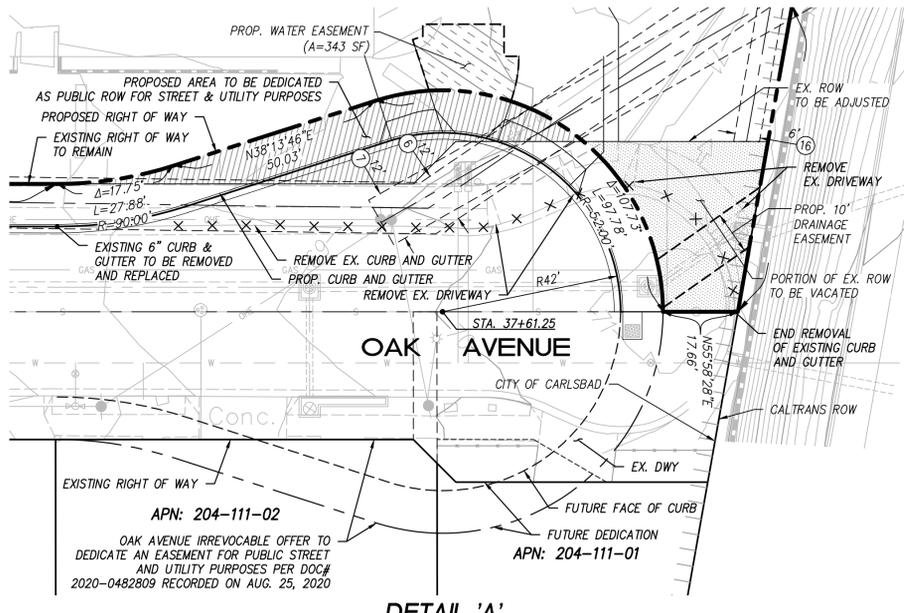
EASEMENTS/ENCUMBRANCES

NOTE: (F) INDICATES A PLOTTABLE ENCUMBRANCE.

THIS INFORMATION IS BASED ON COMMITMENT ORDER NO. NCS-1177654-ONT1 ISSUED BY FIRST AMERICAN TITLE COMPANY DATED AS OF MAY 03, 2023. ENCUMBRANCE ITEMS SUCH AS TAXES, LIENS, RIGHTS, LEASES, FACTS, AGREEMENTS AND TRANSACTIONS APPEARING IN SAID POLICY AS ITEMS NO.'S 1-13 HAVE BEEN REVIEWED AND ARE NOT CONSIDERED SURVEY RELATED, AND THEREFORE HAVE NOT BEEN SHOWN HEREON.

- 3 ABUTTER'S RIGHTS OF INGRESS AND EGRESS TO OR FROM FREEWAY HAVE BEEN RELINQUISHED IN THE DOCUMENT RECORDED APRIL 04, 1952 AS BOOK 4425, PAGE 246 OF OFFICIAL RECORDS. STATUS: TO REMAIN
- 5 AN EASEMENT FOR EITHER OR BOTH POLE LINES, FOR THE PURPOSES OF TRANSMISSION AND DISTRIBUTION OF ELECTRICITY, TOGETHER WITH THE RIGHT OF INGRESS AND EGRESS AND INCIDENTAL PURPOSES, RECORDED APRIL 13, 1964 AS INSTRUMENT NO. 66432 OF OFFICIAL RECORDS.
IN FAVOR OF: SAN DIEGO GAS AND ELECTRIC COMPANY
AFFECTS: AS DESCRIBED THEREIN
STATUS OF EASEMENT: TO BE PARTIALLY QUITCLAIMED PER SEPARATE DOCUMENT
- 6 AN EASEMENT FOR EITHER OR BOTH POLE LINES, FOR THE PURPOSES OF TRANSMISSION AND DISTRIBUTION OF ELECTRICITY, TOGETHER WITH THE RIGHT OF INGRESS AND EGRESS AND INCIDENTAL PURPOSES, RECORDED APRIL 13, 1964 AS INSTRUMENT NO. 66433 OF OFFICIAL RECORDS.
IN FAVOR OF: SAN DIEGO GAS AND ELECTRIC COMPANY
AFFECTS: AS DESCRIBED THEREIN
STATUS OF EASEMENT: TO BE QUITCLAIMED PER SEPARATE DOCUMENT
- 7 AN EASEMENT FOR EITHER OR BOTH POLE LINES, FOR THE PURPOSES OF TRANSMISSION AND DISTRIBUTION OF ELECTRICITY, TOGETHER WITH THE RIGHT OF INGRESS AND EGRESS AND INCIDENTAL PURPOSES, RECORDED DECEMBER 30, 1968 AS INSTRUMENT NO. 228130 OF OFFICIAL RECORDS.
IN FAVOR OF: SAN DIEGO GAS AND ELECTRIC COMPANY
AFFECTS: AS DESCRIBED THEREIN
STATUS OF EASEMENT: TO BE QUITCLAIMED PER SEPARATE DOCUMENT
- 8 ABUTTER'S RIGHTS OF INGRESS AND EGRESS TO OR FROM FREEWAY HAVE BEEN RELINQUISHED IN THE DOCUMENT RECORDED APRIL 01, 1969 AS INSTRUMENT NO. 54612 OF OFFICIAL RECORDS. STATUS: TO REMAIN
- 9 NON-EXCLUSIVE EASEMENT FOR INGRESS AND EGRESS DESIGNATED AS "PARCEL 2" IN AN UNRECORDED LEASE DATED OCTOBER 09, 1968, EXECUTED BY PETER D. AGUILAR AND LUISE W. AGUILAR, HIS WIFE AS LESSOR AND TEXACO INC., A DELAWARE CORPORATION AS LESSEE, AS DISCLOSED BY A MEMORANDUM OF LEASE RECORDED JUNE 25, 1969 AS INSTRUMENT NO. 114508 OF OFFICIAL RECORDS. STATUS OF EASEMENT: TO REMAIN
- 10 AN EASEMENT FOR A LINE OF PIPE AND ALL NECESSARY AND PROPER FIXTURES AND EQUIPMENT FOR USE IN CONNECTION THEREWITH, FOR THE TRANSMISSION AND DISTRIBUTION OF GAS AND FOR ALL PURPOSES CONNECTED THEREWITH TOGETHER WITH THE RIGHT OF INGRESS THERETO AND EGRESS THEREFROM AND INCIDENTAL PURPOSES, RECORDED JANUARY 19, 1970 AS INSTRUMENT NO. 9649 OF OFFICIAL RECORDS.
IN FAVOR OF: SAN DIEGO GAS AND ELECTRIC COMPANY, A CORPORATION
AFFECTS: AS DESCRIBED THEREIN
NOTE: APPROXIMATE LOCATION PLOTTED HEREON BASED UPON PM 21699
STATUS OF EASEMENT: TO BE QUITCLAIMED PER SEPARATE DOCUMENT
- 11 AN EASEMENT FOR THE TRANSMISSION AND DISTRIBUTION OF ELECTRICITY FOR ALL PURPOSES, UNDERGROUND FACILITIES CONSISTING OF CONDUITS, MANHOLES, HANDHOLES AND JUNCTION BOXES WITH WIRES AND CABLES PLACED THEREIN AND ABOVEGROUND STRUCTURES CONSISTING OF PADS, TRANSFORMERS, AND OTHER ABOVEGROUND STRUCTURES NECESSARY FOR THE OPERATION OF SAID UNDERGROUND FACILITIES AND ALL MISCELLANEOUS EQUIPMENT, AND MATERIAL CONNECTED THEREWITH, TOGETHER WITH THE RIGHT OF INGRESS THERETO AND EGRESS AND INCIDENTAL PURPOSES, RECORDED DECEMBER 27, 1971 AS INSTRUMENT NO. 301142 OF OFFICIAL RECORDS.
IN FAVOR OF: SAN DIEGO GAS AND ELECTRIC COMPANY, A CORPORATION
AFFECTS: AS DESCRIBED THEREIN
STATUS OF EASEMENT: TO BE QUITCLAIMED PER SEPARATE DOCUMENT

DETAIL 'A' NOTE:
SEE SHEET 4 FOR PROPOSED GRADING FOR NEW CURB CUL-DE-SAC.



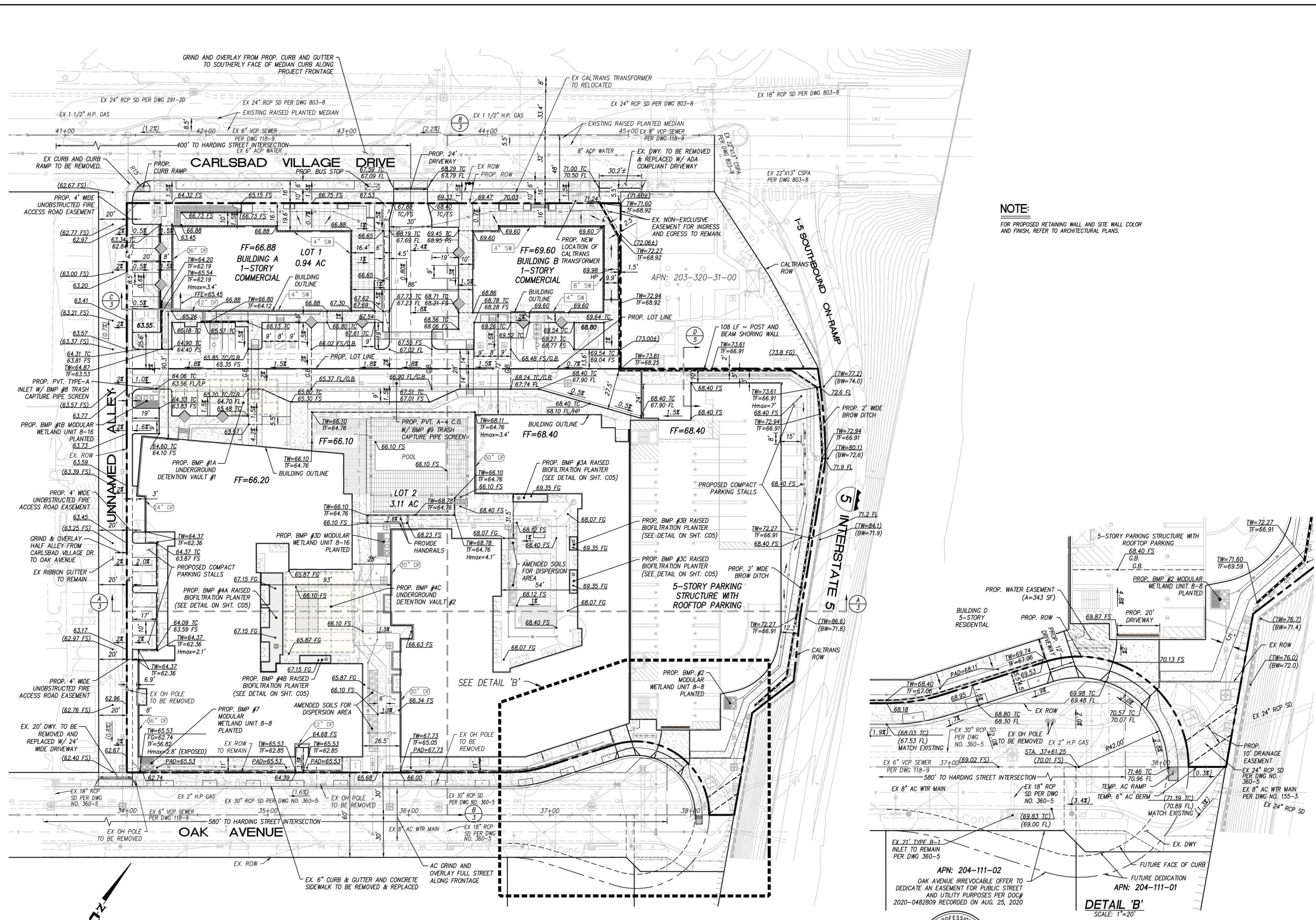
MARINA NATASHA WURST, R.C.E. C67517	DATE REGISTRATION EXPIRES 6-30-23	DESIGNED BY: AM/JT	PM REVIEW: MW
<i>M. Wurst</i>	5/11/2023		

PROJECT DESIGN CONSULTANTS
a BOWMAN company
Planning | Landscape Architecture | Engineering | Survey

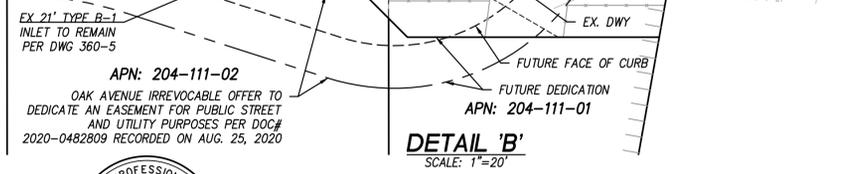
CVMU
945-1065 CARLSBAD VILLAGE DRIVE
CARLSBAD, CA, 92008
GRT CARLSBAD VILLAGE LLC
11661 SAN VICENTE BLVD, SUITE 850
LOS ANGELES CA 90049

SUBMITAL:
SDP 2023-0014
JOB NUMBER:
4488.00
DATE:
11.13.2023
REVISIONS:

SHEET TITLE:
PDC EXISTING ENCROACHMENTS, PROPOSED LOTS & DEDICATIONS
SHEET NUMBER:
C02



NOTE:
FOR PROPOSED RETAINING WALL AND SITE WALL COLOR AND FINISH, REFER TO ARCHITECTURAL PLANS.



APN: 204-111-02
OAK AVENUE IRREVOCABLE OFFER TO DEDICATE AN EASEMENT FOR PUBLIC STREET AND UTILITY PURPOSES PER DOC# 2020-0482809 RECORDED ON AUG. 25, 2020

APN: 204-111-01
FUTURE DEDICATION

APN: 204-111-01



MARINA NATASHA WURST, R.C.E. C67517	DATE	REGISTRATION	DESIGNED	PM
<i>M. Wurst</i>	5/11/2023	EXPIRES 6-30-23	AM/JT	REVIEW: MW





3573 HAYDEN AVENUE
CULVER CITY, CA 90232
310.399.7975
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CVMU
945-1065 CARLSBAD VILLAGE DRIVE
CARLSBAD, CA 92008

GRT CARLSBAD VILLAGE, LLC
11661 SAN VICENTE BLVD., SUITE 850
LOS ANGELES CA 90049

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SDP 2023-0014

JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISIONS:

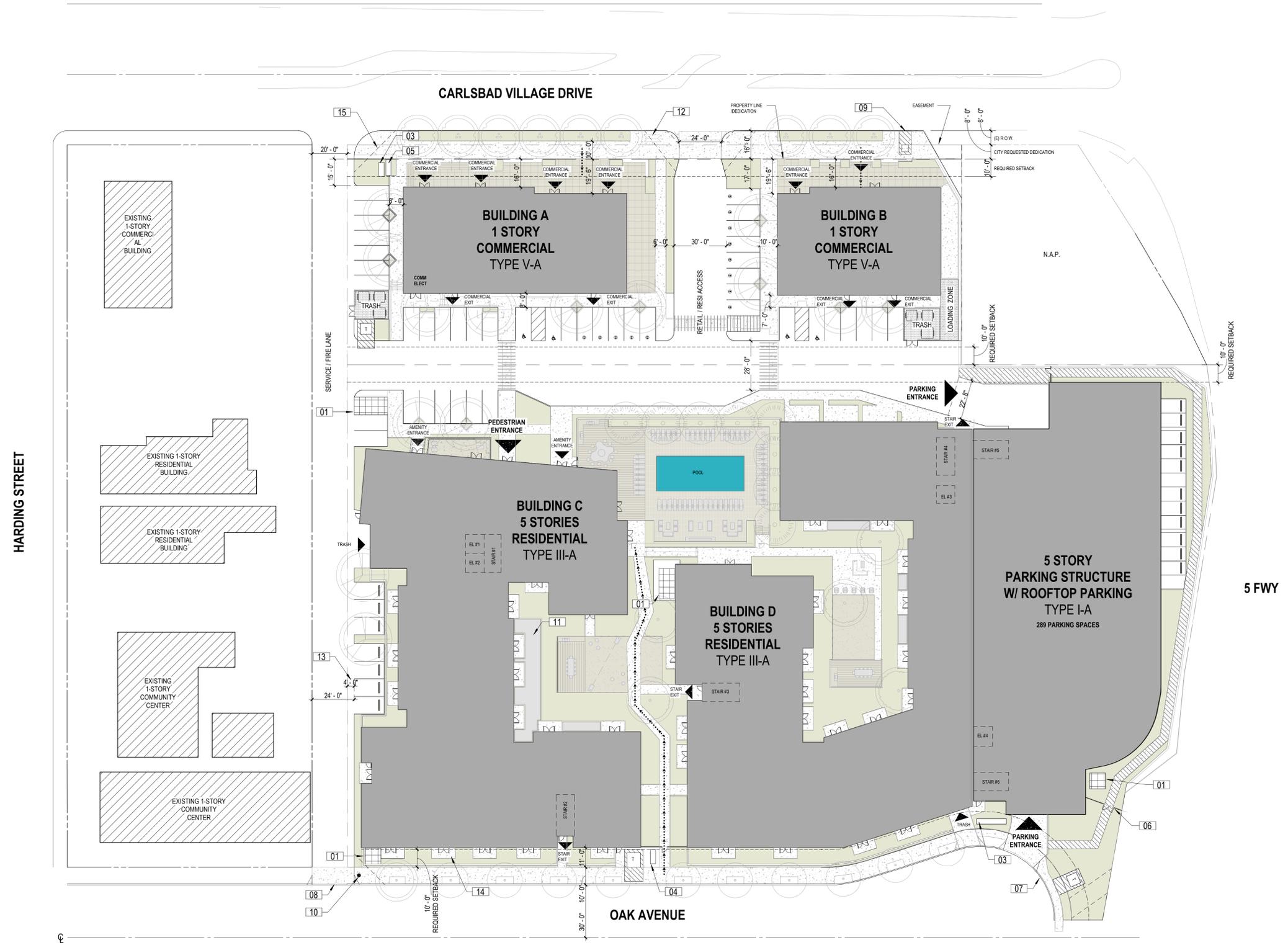
SHEET TITLE:
SITE PLAN

SHEET NUMBER:
A00

Item #1 233 of 269

NOTES

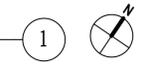
- 01 MODULAR WETLAND UNIT
- 02 STANDPIPE
- 03 DOMESTIC BACKFLOW PREVENTER
- 04 IRRIGATION BACKFLOW PREVENTER
- 05 FIRE BACKFLOW PREVENTER
- 06 KNOX BOX FOR FIRE DEPARTMENT
- 07 NEW CUL-DE-SAC CURB, SEE CIVIL DRAWINGS
- 08 (E) CURB CUT
- 09 RELOCATED CALTRANS TRANSFORMER
- 10 (E) POWER POLE TO BE REMOVED
- 11 BMP RAISED FLOW THRU PLANTER, SEE CIVIL AND LANDSCAPE DRAWINGS
- 12 (E) NCTD BUS STOP LOCATION
- 13 UNOBSTRUCTED FIRE ACCESS ROAD EASEMENT
- 14 TENANT PRIVATE COURTYARD, TYP.
- 15 VISIBILITY TRIANGLE, TYP.



LEGEND

- TRANSFORMER WITH REQUIRED CLEARANCE
- GROUND FLOOR FOOTPRINT
- EXISTING BUILDING
- ACCESSIBLE PATH OF TRAVEL
- HOSE PULL

SITE PLAN
1" = 30'-0"



CARLSBAD VILLAGE DRIVE



NOTES

- 01 MODULAR WETLAND UNIT
- 02 STANDPIPE
- 03 DOMESTIC BACKFLOW PREVENTER
- 04 IRRIGATION BACKFLOW PREVENTER
- 05 FIRE BACKFLOW PREVENTER
- 06 EXTENT OF GARAGE STRUCTURE ABOVE
- 07 TRANSFORMER
- 08 ROOF OVERHANG ABOVE
- 09 COMPOSITE TRELLIS ABOVE
- 10 PEDESTRIAN GATE AND FENCE
- 11 TRASH ENCLOSURE, SEE 1/A21
- 12 TRASH ENCLOSURE, SEE 2/A24
- 13 TENANT PRIVATE COURTYARD, TYP.
- 14 CLEAR ZONE LINE OF SIGHT PER VBMP 2.6.1(A)(4), TYP.
- 15 METAL FENCE
- 16 OVERHEAD ROLLING GATE
- 17 METAL CANOPY
- 18 PARKING BARRIER GATE

SHEET NOTES

- A. ALL PARKING STALLS TO HAVE A MAXIMUM OF 5% SLOPE PER CITY ENGINEERING STANDARDS. ADA PARKING STALLS AND STRIPING NOT TO EXCEED 2%.
- B. ALL DOORWAY CLEAR LANDINGS NOT TO EXCEED 2% SLOPE

UNIT COUNT

3	STUDIO
13	1 BR
22	2 BR
1	3 BR
TOTAL UNITS: 39	

PARKING LEGEND

RES	RESIDENTIAL
COMM	COMMERCIAL
S	STANDARD
DAS	ACCESSIBLE
C	COMPACT
INS	EV CHARGER INSTALLED
CAP	EV CAPABLE
RDY	EV READY

FIRST FLOOR PLAN
3/64" = 1'-0"



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JOB NUMBER:
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DATE:
11.13.2023

REVISIONS:

SHEET TITLE:
FIRST FLOOR PLAN

SHEET NUMBER:
A10



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JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISIONS:

SHEET TITLE:
SECOND FLOOR PLAN

SHEET NUMBER:
A11

Item #1 235 of 269

NOTES

01 FUTURE EQUIPMENT AREA

SHEET NOTES

- A. ALL PARKING STALLS TO HAVE A MAXIMUM OF 5% SLOPE PER CITY ENGINEERING STANDARDS. ADA PARKING STALLS AND STRIPING NOT TO EXCEED 2%.
- B. ALL DOORWAY CLEAR LANDINGS NOT TO EXCEED 2% SLOPE

UNIT COUNT

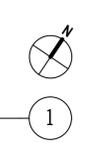
2ND FLOOR	* UNITS D101, D102, D103, D104 ARE 2-STORY UNITS THAT ARE COUNTED ON SHEET A10.
3 STUDIO	
19 1 BR	
16 2 BR	
7 3 BR	
TOTAL UNITS: 45 *	

PARKING LEGEND

RES	RESIDENTIAL
COMM	COMMERCIAL
S	STANDARD
DAS	ACCESSIBLE
C	COMPACT
INS	EV CHARGER INSTALLED
CAP	EV CAPABLE
RDY	EV READY



SECOND FLOOR PLAN
3/64" = 1'-0"





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SDP 2023-0014

JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISIONS:

SHEET TITLE:
THIRD FLOOR PLAN

SHEET NUMBER:
A12

Item #1 236 of 269

NOTES

SHEET NOTES

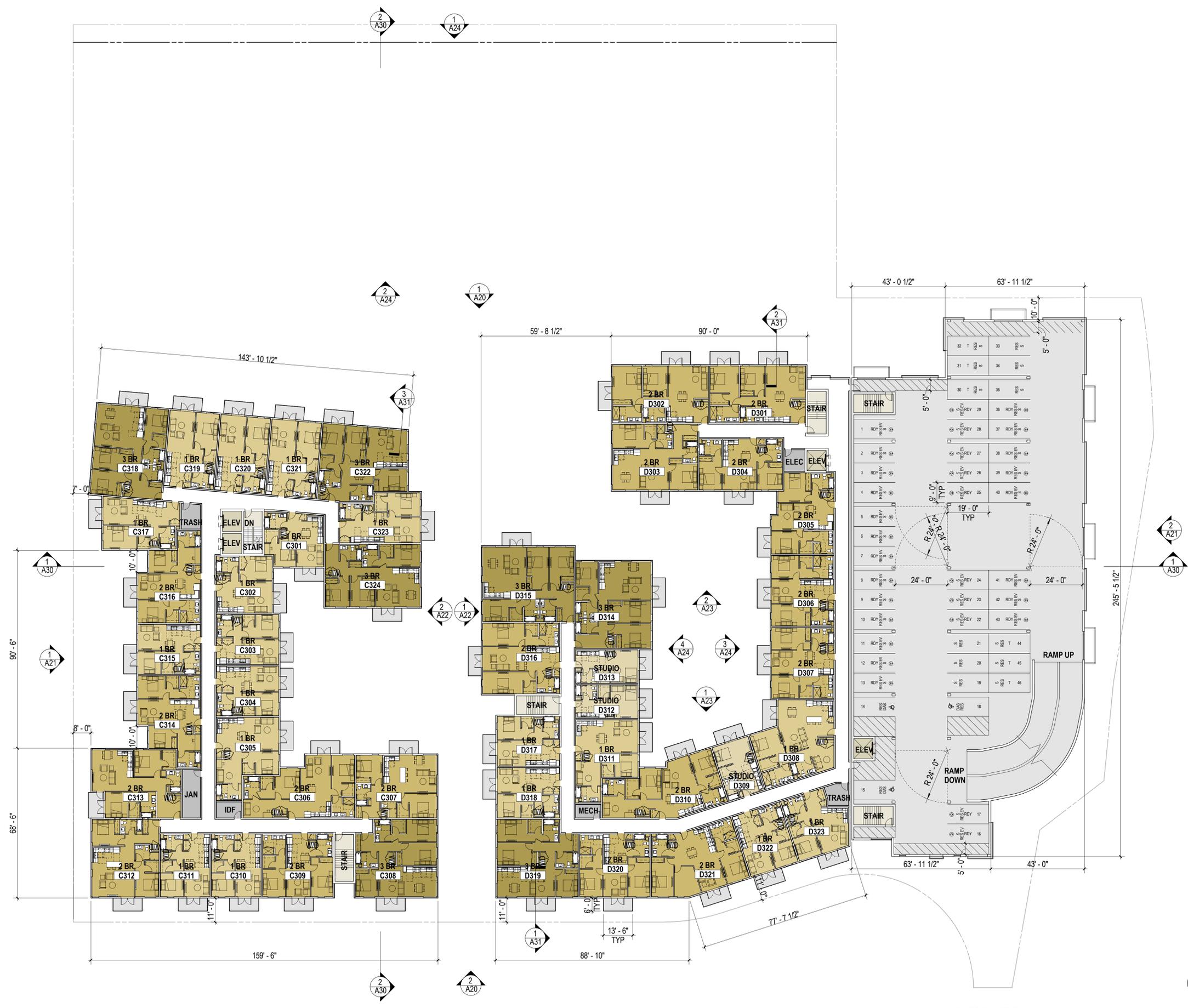
- A. ALL PARKING STALLS TO HAVE A MAXIMUM OF 5% SLOPE PER CITY ENGINEERING STANDARDS. ADA PARKING STALLS AND STRIPING NOT TO EXCEED 2%.
- B. ALL DOORWAY CLEAR LANDINGS NOT TO EXCEED 2% SLOPE

UNIT COUNT

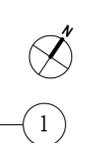
3RD FLOOR	
3 STUDIO	
19 1 BR	
18 2 BR	
7 3 BR	
TOTAL UNITS: 47	

PARKING LEGEND

RES	RESIDENTIAL
COMM	COMMERCIAL
S	STANDARD
DAS	ACCESSIBLE
C	COMPACT
INS	EV CHARGER INSTALLED
CAP	EV CAPABLE
RDY	EV READY



THIRD FLOOR PLAN
3/64" = 1'-0"





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JOB NUMBER: 2022-004.00

DATE: 11.13.2023

REVISIONS:

SHEET TITLE:
FOURTH FLOOR PLAN

SHEET NUMBER:
A13

Item #1 237 of 269

NOTES

01 PER VILLAGE BARRIO MASTER PLAN 2.7.4.G.2.a,
70% OF 4TH STORY SHALL BE SETBACK 15'-0"

SHEET NOTES

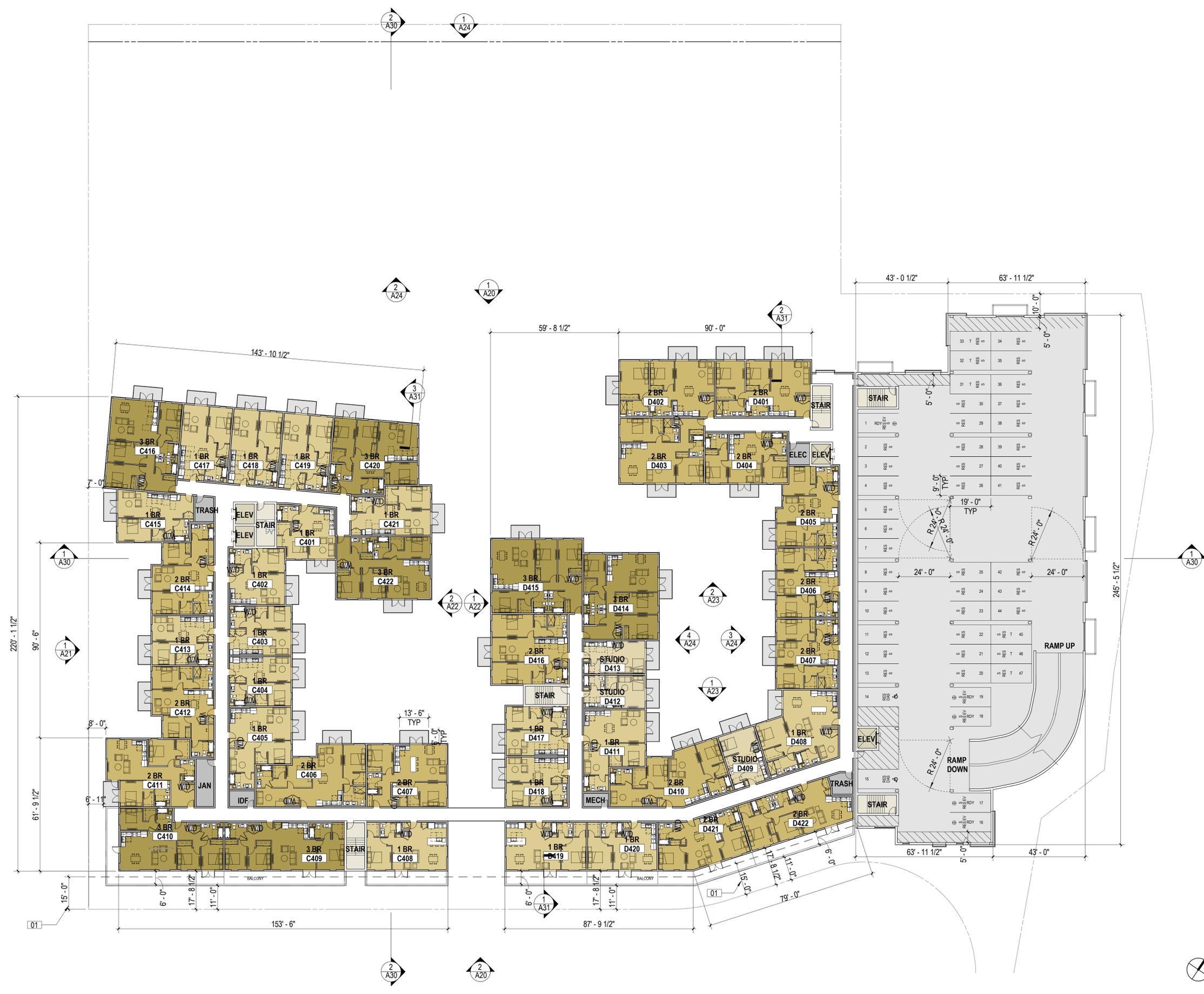
- A. ALL PARKING STALLS TO HAVE A MAXIMUM OF 5% SLOPE PER CITY ENGINEERING STANDARDS. ADA PARKING STALLS AND STRIPING NOT TO EXCEED 2%.
- B. ALL DOORWAY CLEAR LANDINGS NOT TO EXCEED 2% SLOPE

UNIT COUNT

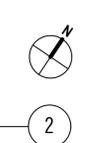
4TH FLOOR	
3 STUDIO	
18 1 BR	
16 2 BR	
7 3 BR	
TOTAL UNITS: 44	

PARKING LEGEND

RES	RESIDENTIAL
COMM	COMMERCIAL
S	STANDARD
DAS	ACCESSIBLE
C	COMPACT
INS	EV CHARGER INSTALLED
CAP	EV CAPABLE
RDY	EV READY



FOURTH FLOOR PLAN
3/64" = 1'-0"





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JOB NUMBER:
2022-004.00

DATE:
11.13.2023

REVISIONS:

SHEET TITLE:
FIFTH FLOOR PLAN

SHEET NUMBER:
A14

Item #1 238 of 269

NOTES

SHEET NOTES

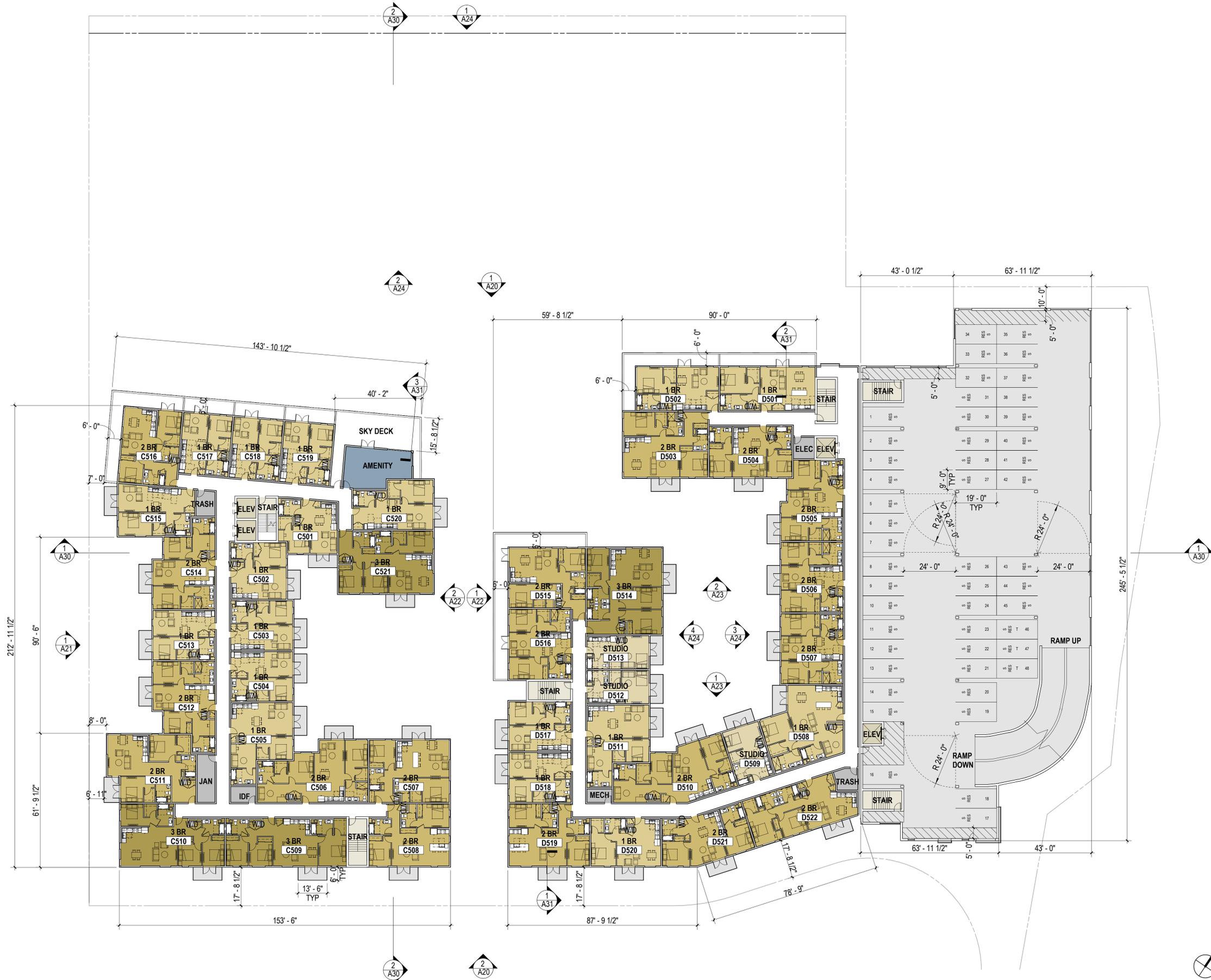
- A. ALL PARKING STALLS TO HAVE A MAXIMUM OF 5% SLOPE PER CITY ENGINEERING STANDARDS. ADA PARKING STALLS AND STRIPING NOT TO EXCEED 2%.
- B. ALL DOORWAY CLEAR LANDINGS NOT TO EXCEED 2% SLOPE

UNIT COUNT

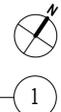
5TH FLOOR
3 STUDIO
18 1 BR
18 2 BR
4 3 BR
TOTAL UNITS: 43

PARKING LEGEND

RES	RESIDENTIAL
COMM	COMMERCIAL
S	STANDARD
DAS	ACCESSIBLE
C	COMPACT
INS	EV CHARGER INSTALLED
CAP	EV CAPABLE
RDY	EV READY

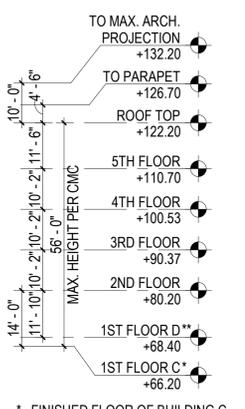


FIFTH FLOOR PLAN
3/64" = 1'-0"



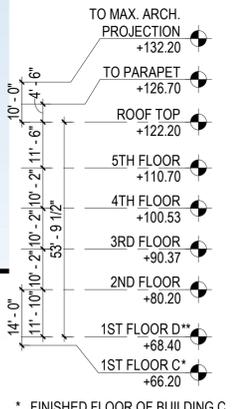


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Elevation NORTH
3/64" = 1'-0"

1



Elevation SOUTH
3/64" = 1'-0"

2

MATERIAL LEGEND

(PL1) STUCCO, COLOR WHITE	(VS1) VERTICAL SIDING BOARD, COLOR WHITE	(WN1) VINYL WINDOW COLOR DARK BRONZE
(PL2) STUCCO, COLOR LIGHT GRAY	(R1) METAL ROOF, COLOR DARK GRAY	(WN2) STOREFRONT WINDOW COLOR DARK BRONZE
(PL3) STUCCO, COLOR LIGHT BEIGE	(T1) COMPOSITE TRELLIS COLOR DARK GRAY / WHITE	
(PL4) STUCCO, COLOR DARK GRAY	(T2) METAL LOUVERS, COLOR BROWN	
(PL5) STUCCO, COLOR DARK BEIGE	(GR1) METAL GUARDRAIL, COLOR WHITE	
(HS1) HORIZONTAL SIDING BOARD, COLOR WHITE	(GR2) METAL GUARDRAIL, COLOR GRAY	
(HS2) HORIZONTAL SIDING BOARD, COLOR BEIGE	(F1) METAL FENCE, COLOR BLACK	
(HS3) HORIZONTAL SIDING BOARD, COLOR BROWN	(S1) COMPOSITE SHUTTERS COLOR LIGHT BEIGE	

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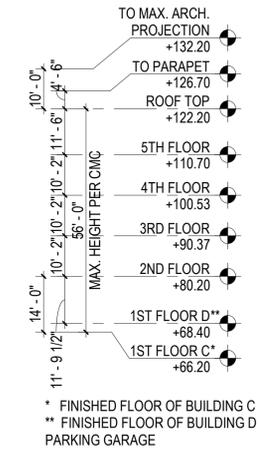
JOB NUMBER:
2022-004.00
DATE:
11.13.2023
REVISIONS:

SHEET TITLE:
ELEVATIONS

SHEET NUMBER:
A20

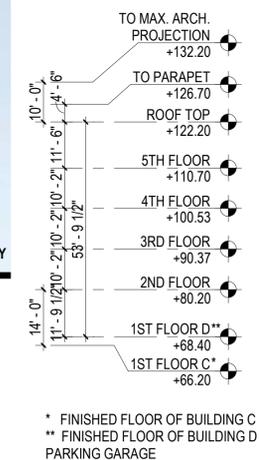


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Elevation NORTH - Courtyards
3/64" = 1'-0"

1



Elevation SOUTH - Courtyards
3/64" = 1'-0"

2

MATERIAL LEGEND

(PL1) STUCCO, COLOR WHITE	(VS1) VERTICAL SIDING BOARD, COLOR WHITE	(WN1) VINYL WINDOW COLOR DARK BRONZE
(PL2) STUCCO, COLOR LIGHT GRAY	(R1) METAL ROOF, COLOR DARK GRAY	(WN2) STOREFRONT WINDOW COLOR DARK BRONZE
(PL3) STUCCO, COLOR LIGHT BEIGE	(T1) COMPOSITE TRELLIS COLOR DARK GRAY / WHITE	
(PL4) STUCCO, COLOR DARK GRAY	(T2) METAL LOUVERS, COLOR BROWN	
(PL5) STUCCO, COLOR DARK BEIGE	(GR1) METAL GUARDRAIL, COLOR WHITE	
(HS1) HORIZONTAL SIDING BOARD, COLOR WHITE	(GR2) METAL GUARDRAIL, COLOR GRAY	
(HS2) HORIZONTAL SIDING BOARD, COLOR BEIGE	(F1) METAL FENCE, COLOR BLACK	
(HS3) HORIZONTAL SIDING BOARD, COLOR BROWN	(S1) COMPOSITE SHUTTERS COLOR LIGHT BEIGE	

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JOB NUMBER:
2022-004.00
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REVISIONS:

SHEET TITLE:
ELEVATIONS

SHEET NUMBER:
A23



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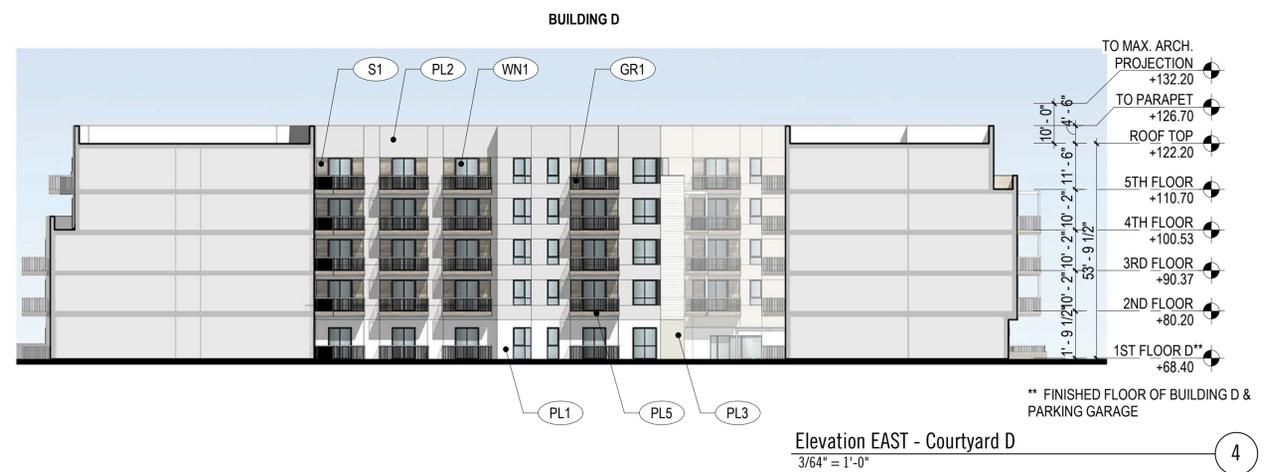
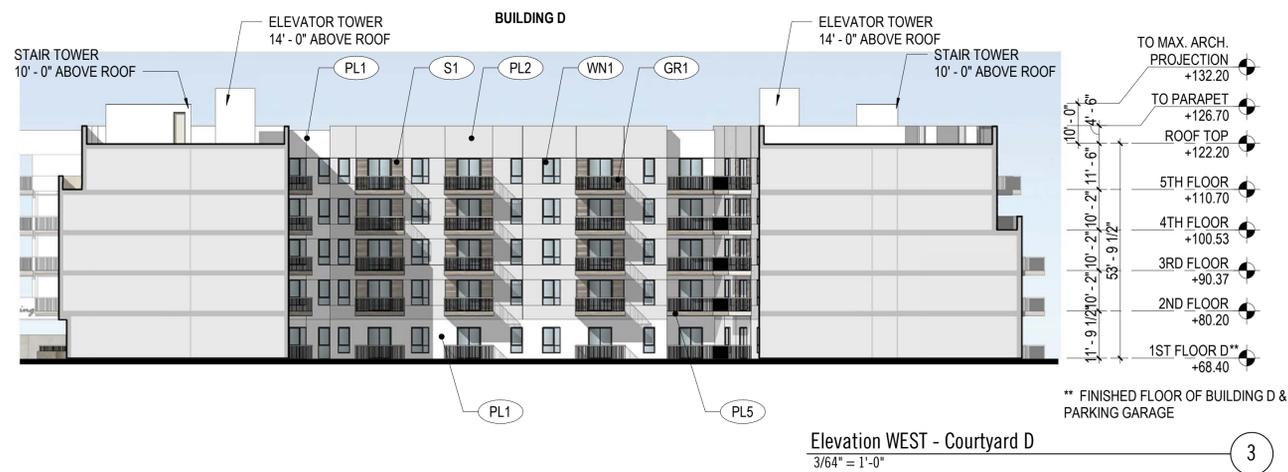
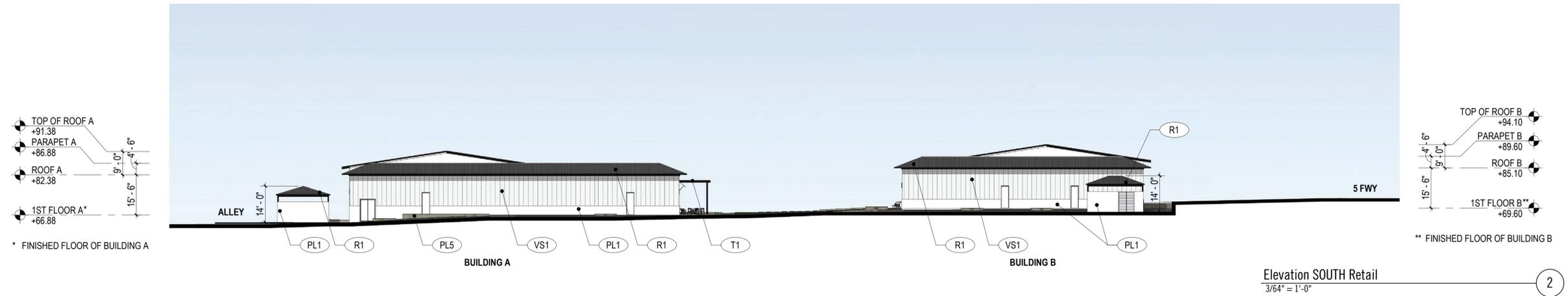
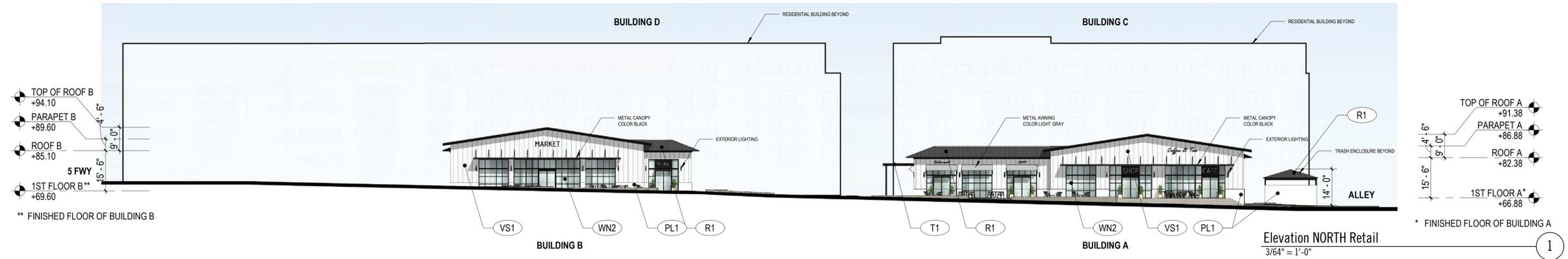
DATE:
11.13.2023

REVISION:

SHEET TITLE:
ELEVATIONS

SHEET NUMBER:
A24

Item #1 244 of 269



MATERIAL LEGEND

PL1	STUCCO, COLOR WHITE	VS1	VERTICAL SIDING BOARD, COLOR WHITE	WN1	VINYL WINDOW COLOR DARK BRONZE
PL2	STUCCO, COLOR LIGHT GRAY	R1	METAL ROOF, COLOR DARK GRAY	WN2	STOREFRONT WINDOW COLOR DARK BRONZE
PL3	STUCCO, COLOR LIGHT BEIGE	T1	COMPOSITE TRELLIS COLOR DARK GRAY / WHITE		
PL4	STUCCO, COLOR DARK GRAY	T2	METAL LOUVERS, COLOR BROWN		
PL5	STUCCO, COLOR DARK BEIGE	GR1	METAL GUARDRAIL, COLOR WHITE		
HS1	HORIZONTAL SIDING BOARD, COLOR WHITE	GR2	METAL GUARDRAIL, COLOR GRAY		
HS2	HORIZONTAL SIDING BOARD, COLOR BEIGE	F1	METAL FENCE, COLOR BLACK		
HS3	HORIZONTAL SIDING BOARD, COLOR BROWN	S1	COMPOSITE SHUTTERS COLOR LIGHT BEIGE		



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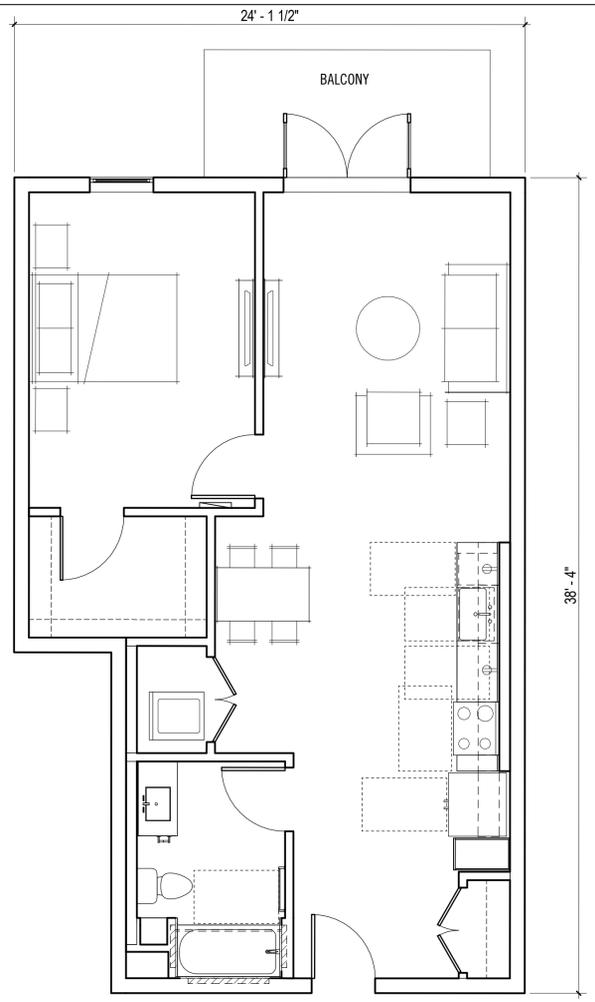
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2022-004.00

DATE:
11.13.2023

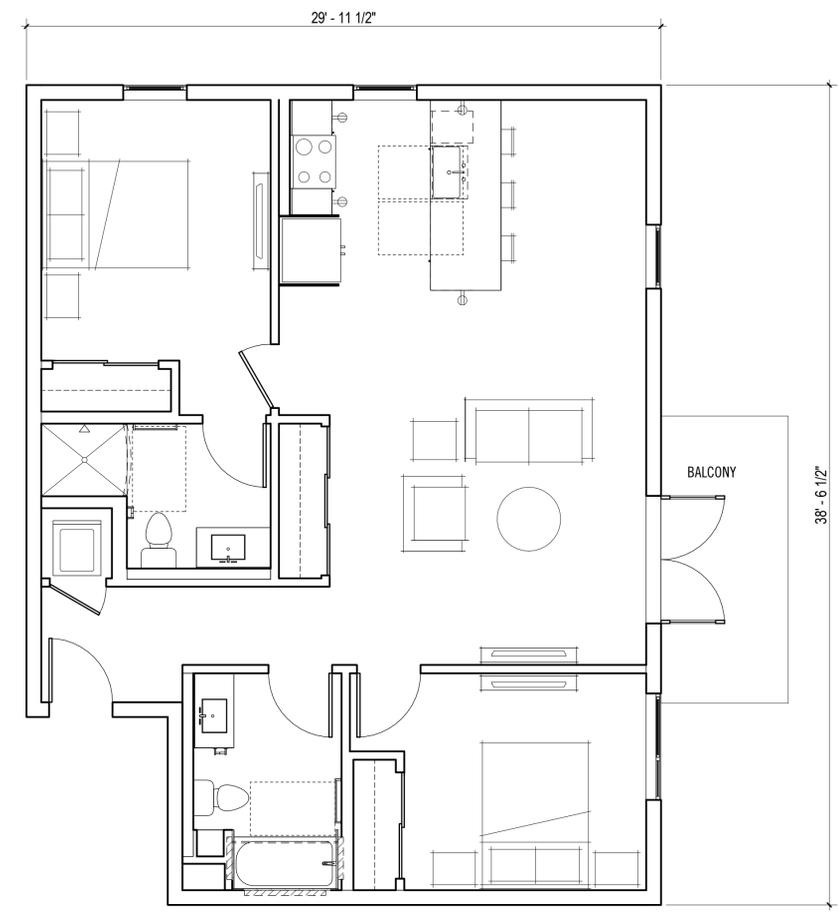
REVISION:

SHEET TITLE:
RENDERING

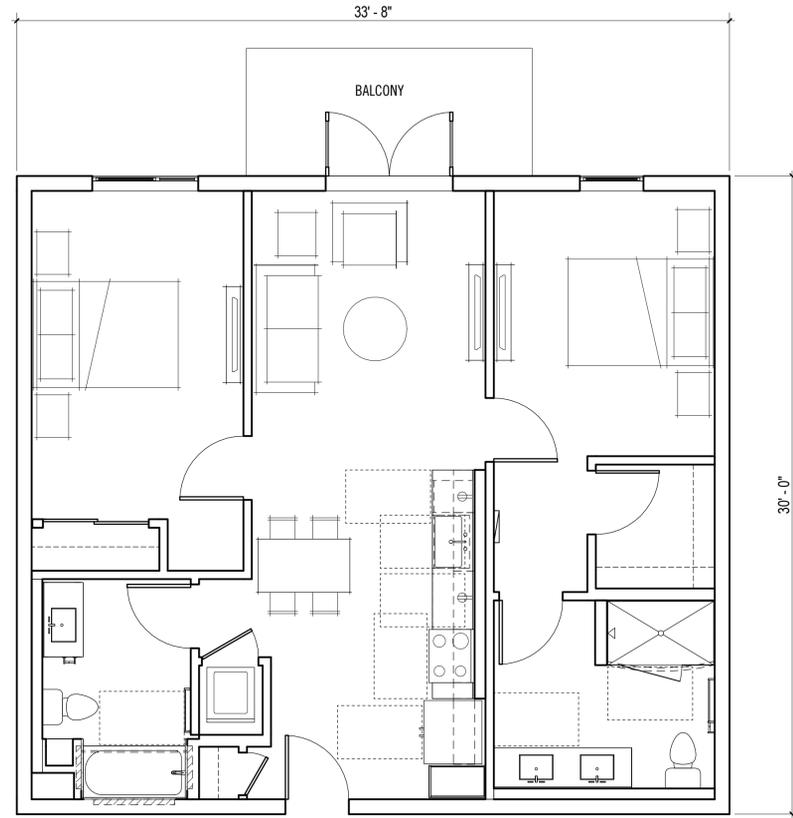
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A28



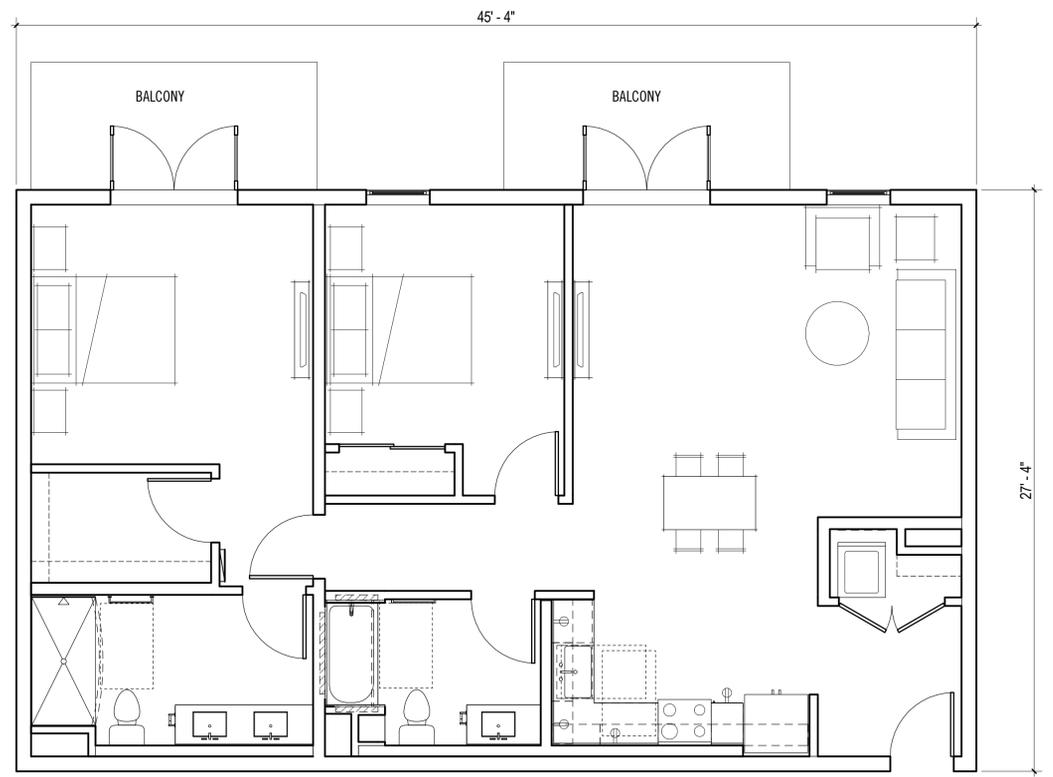
1BR - TYPE B9
1/4" = 1'-0" 765 SF 10



2BR - TYPE C1
1/4" = 1'-0" 1,001 SF 2



2BR - TYPE C2
1/4" = 1'-0" 915 SF 12



2BR - TYPE C3
1/4" = 1'-0" 1,124 SF 4

NOTES



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JOB NUMBER:
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DATE:
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REVISIONS:

SHEET TITLE:
**ENLARGED UNIT
PLANS**

SHEET NUMBER:
A62



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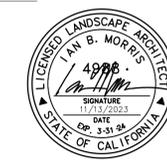
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GROUNDLEVEL

LANDSCAPE ARCHITECTURE

2605 State Street, Suite B
San Diego, CA 92103
619.325.1990
GroundLevelSD.com
GL Project #22139

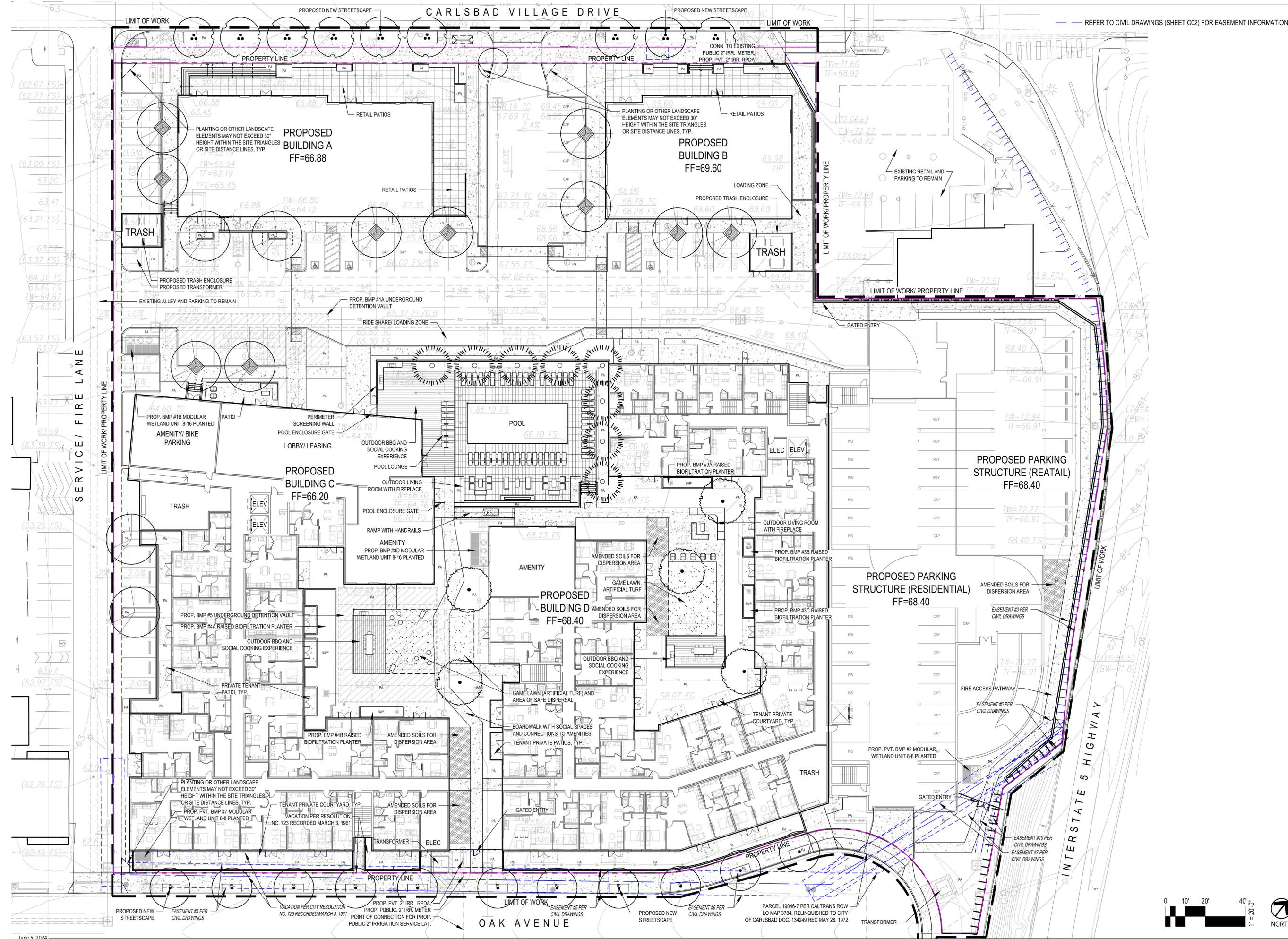


SUBMITTAL:
SDP 23-0014
JOB NUMBER:
2022-004.00
DATE:
11.13.2023
REVISIONS:

SHEET TITLE:
SITE PLAN

SHEET NUMBER:
L1.00

Item #1 259 of 269





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SUITE 850
LOS ANGELES CA 90049

GROUNDLEVEL
LANDSCAPE ARCHITECTURE
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San Diego, CA 92103
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GroundLevelSD.com
GL Project #22139



SUBMITTAL:
SDP 23-0014
JOB NUMBER:
2022-004.00
DATE:
11.13.2023
REVISIONS:

SHEET TITLE:
CONCEPTUAL LANDSCAPE PLAN

SHEET NUMBER:
L1.01
Item #1 260 of 269

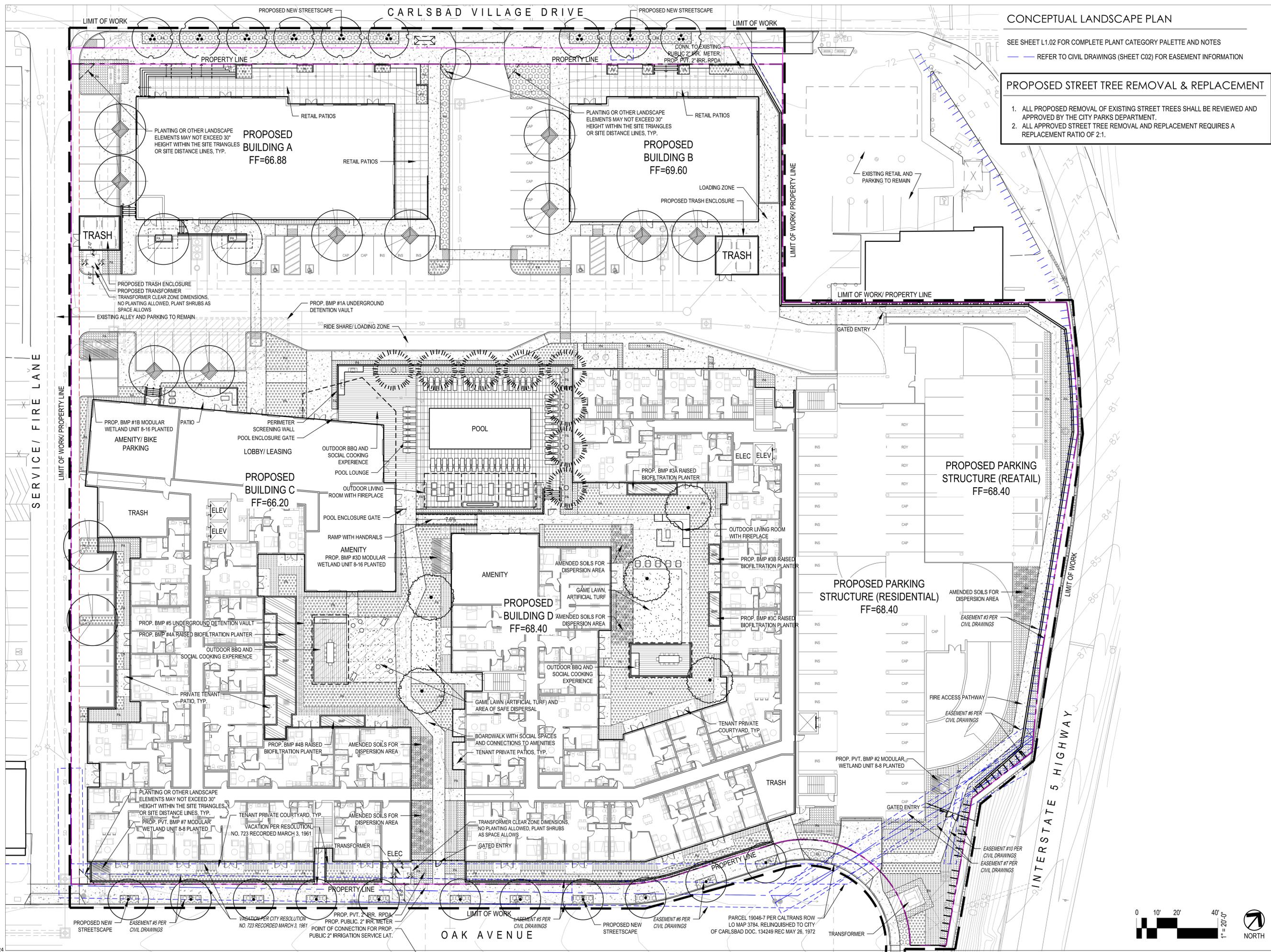
CONCEPTUAL LANDSCAPE PLAN

SEE SHEET L1.02 FOR COMPLETE PLANT CATEGORY PALETTE AND NOTES

REFER TO CIVIL DRAWINGS (SHEET C02) FOR EASEMENT INFORMATION

PROPOSED STREET TREE REMOVAL & REPLACEMENT

1. ALL PROPOSED REMOVAL OF EXISTING STREET TREES SHALL BE REVIEWED AND APPROVED BY THE CITY PARKS DEPARTMENT.
2. ALL APPROVED STREET TREE REMOVAL AND REPLACEMENT REQUIRES A REPLACEMENT RATIO OF 2:1.





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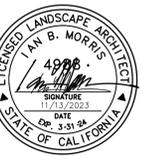
945-1065 CARLSBAD VILLAGE DRIVE
CARLSBAD, CA, 92008

CVMU

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LOS ANGELES CA 90049

GROUNDLEVEL

LANDSCAPE ARCHITECTURE
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619.225.1990
GroundLevelSD.com
GL Project #22139



SUBMITTAL:
SDP 23-0014
JOB NUMBER:
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DATE:
11.13.2023
REVISIONS:

SHEET TITLE:
LANDSCAPE PLANTING PALETTE

SHEET NUMBER:
L1.02
Item #1 261 of 269

PROPOSED PLANTING PALETTE

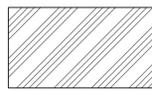
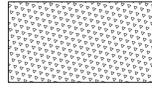
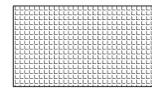
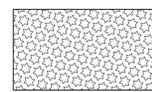
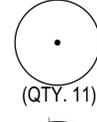
PROPOSED STREET TREES ALONG CARLSBAD VILLAGE DRIVE (100% 36" BOX) - SHALL BE APPROVED BY THE CITY PARKS DEPT.

BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	QUANTITY
MAGNOLIA GRANDIFLORA	SOUTHERN MAGNOLIA TREE	36" BOX	MODERATE	30'	60'	9
PROPOSED STREET TREES ALONG OAK AVENUE (100% 36" BOX) - SHALL BE APPROVED BY THE CITY PARKS DEPT.						
BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	QUANTITY
CERCIS CANADENSIS 'FOREST PANSY'	EASTERN REDBUD	36" BOX	MODERATE	25'-30'	15'-20'	11
PARKING LOT TREES SUCH AS (100% 36" BOX)						
BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	QUANTITY
ELAEOCARPUS DECIPIENS	JAPANESE BLUEBERRY TREE	36" BOX	MODERATE	20'-35'	20'-35'	TBD
LAGERSTROEMIA SPP.	CRAPE MYRTLE	36" BOX	MODERATE	20'-30'	15'-20'	TBD
LOPHOSTEMON CONFERTUS	BRISBANE BOX	36" BOX	MODERATE	50'	20'	TBD
PODOCARPUS GRACILIOR	FERN PINE TREE	36" BOX	MODERATE	20'-60'	10'-25'	TBD
ULMUS PARVIFOLIA 'DRAKE'	DRAKE CHINESE ELM	36" BOX	MODERATE	40'	30'	TBD
POOL ENCLOSURE TREES SUCH AS (100% 36" BOX OR 15' BTH)						
BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	QUANTITY
ARCHONTOPHOENIX ALEXANDRAE	KING PALM	36" BOX	MODERATE	70'	10'-15'	TBD
MAGNOLIA GRANDIFLORA	SOUTHERN MAGNOLIA TREE	36" BOX	MODERATE	30'	60'	TBD
PHOENIX RECLINATA	SENEGAL DATE PALM	15' BTH	MODERATE	35'	10'-15'	TBD
RESIDENTIAL COURTYARD TREES SUCH AS (50% 36" BOX, 30% 48" BOX, 20% 60" BOX)						
BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	QUANTITY
ALOE TONGAENSIS 'MEDUSA'	MOZAMBIQUE TREE ALOES	36" BOX	LOW	12'	6'	TBD
ALOE 'HERCULES'	HERCULES TREE ALOE	36" BOX	LOW	25'-40'	12'-25'	TBD
ARBUTUS 'MARINA'	MARINA STRAWBERRY TREE	60" BOX	LOW	25'-40'	15'-20'	TBD
CERCIS CANADENSIS 'FOREST PANSY'	EASTERN REDBUD TREE	36" BOX	MODERATE	12'-20'	15'-25'	TBD
DRACAENA DRACO	DRAGON TREE	48" BOX	VERY LOW	15'-20'	15'-20'	TBD
OLEA EUROPAEA 'SWAN HILL'	SWAN HILL OLIVE TREE	18" X 18"	LOW	25'-35'	20'-30'	TBD
PLATANUS RACEMOSA	CALIFORNIA SYCAMORE TREE	36" BOX	MODERATE	50'	30'	TBD
QUERCUS VIRGINIANA	SOUTHERN LIVE OAK TREE	60" BOX	MODERATE	50'	60'	TBD
LAGERSTROMIA SPP.	CRAPE MYRTLE	36" BOX	MODERATE	20'-30'	15'-20'	TBD
ULMUS PARVIFOLIA 'DRAKE'	DRAKE CHINESE ELM TREE	36" BOX	MODERATE	40'	30'	TBD
LOW HEIGHT PARKWAY SHRUBS & GRASSES: SUCH AS (50% - 1 GAL & 50% - 5 GAL.)						
BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	O.C. SPACING
CALLISTEMON VIMINALIS 'LJ1'	BETTER JOHN BOTTLEBRUSH	5 GAL	LOW	4'-5'	4'-5'	36"
CARISSA MACROCARPA 'GREEN CARPET'	GREEN CARPET NATAL PLUM	5 GAL	LOW	12"-18"	48"	30"
DIANELLA REVOLUTA 'LITTLE REV TIGHT'	LITTLE REV FLAX LILY	5 GAL	LOW	18"-24"	18"-24"	18"
LOMANDRA CONFERTIFOLIA 'LIME TIGHT'	LIME TIGHT MAT RUSH	1 GAL	LOW	2'-3'	2'-3'	24"
MUHLENBERGIA DUBIA	PINE MUHLY	1 GAL	LOW	3'	3'	30"
SESLERIA AUTUMNALIS	AUTUMN MOOR GRASS	1 GAL	MODERATE	8"-12"	8"-12"	12"
WESTRINGIA FRUTICOSA 'GREY BOX'	DWARF COAST ROSEMARY	5 GAL	LOW	24"-36"	24"-36"	24"
TALL EVERGREEN SCREENING SHRUBS: SUCH AS (100% - 15 GAL.)						
BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	O.C. SPACING
DODONAEA VISCOSEA 'PURPUREA'	PURPLE-LEAFED HOP-BUSH	15 GAL	LOW	12'-16'	8'-12'	-
FEIJOA SELLOWIANA	PINEAPPLE GUAVA	15 GAL	LOW	18'-25'	18'-25'	-
LAURUS NOBILIS	BAY LAUREL	15 GAL	LOW	25'	20'	-
LIGUSTRUM JAPONICUM 'TEXANUM'	WAXLEAF PRIVET	15 GAL	MODERATE	8'	15'	-
PODOCARPUS ELONGATUS 'MONMAL'	ICEE BLUE YELLOW-WOOD	15 GAL	MODERATE	15'-25'	4'	48"
PODOCARPUS MACROPHYLLUS	YEW PINE	15 GAL	MODERATE	8'	4'	30"
PRUNUS CAROLINIANA 'MONUS'	BRIGHT 'N TIGHT CAROLINA LAUREL CHERRY	15 GAL	MODERATE	10'	6'	48"
WESTRINGIA 'WYNYABBIE GEM'	COAST ROSEMARY	15 GAL	LOW	4'-6'	36"-48"	36"

PROPOSED STREET TREE REMOVAL & REPLACEMENT

- ALL PROPOSED REMOVAL OF EXISTING STREET TREES SHALL BE REVIEWED AND APPROVED BY THE CITY PARKS DEPARTMENT.
- ALL APPROVED STREET TREE REMOVAL AND REPLACEMENT REQUIRES A REPLACEMENT RATIO OF 2:1.

BUILDING FOUNDATION SHRUBS: SUCH AS (75% - 15 GAL & 25% - 5 GAL)						
BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	O.C. SPACING
CRASSULA SPP.	CRASSULA	5 GAL	LOW	6"-8"	N/A	18"
LEUCADENDRON 'RED GEM'	RED GEM CONEBUSH	15 GAL	LOW	8'-10'	6'-8'	5'
LEUCOPHYLLUM ZYGOPHYLLUM 'CIMARRON'	BLUE RANGER SAGE	15 GAL	VERY LOW	3'	3'-4'	30"
OLEA EUROPAEA 'LITTLE OLLIE'	LITTLE OLLIE OLIVE	5 GAL	LOW	6'-8'	6'-8'	36"
PHORMIUM SPP.	NEX ZEALAND FLAX SPECIES	15 GAL	LOW	5'-7'	3'	36"
RHAPHIOLEPIS UMBELLATA 'MINOR'	DWARF INDIAN HAWTHORN	15 GAL	LOW	4'-6'	24"-36"	30"
RHAPHIOLEPIS X 'MONTIC'	MAJESTIC BEAUTY INDIAN HAWTHORN	15 GAL	LOW	4'-6'	24"-36"	30"
SENECIO DECARYI	MADAGASCAR SENECIO	5 GAL	LOW	48"	48"	36"
WESTRINGIA FRUTICOSA 'BLUE GEM'	BLUE GEM COAST ROSEMARY	5 GAL	LOW	4'-6'	36"-48"	36"
ORNAMENTAL SUCCULENTS, SHRUBS & GRASSES: SUCH AS (40% - 1 GAL, 40% - 5 GAL & 20% - 15 GAL)						
BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	O.C. SPACING
ACACIA COGNATA 'ACCOG01'	COUSIN ITT LITTLE RIVER WATTLE	5 GAL	VERY LOW	24"-36"	4'-6'	42"
AGAVE SPP.	AGAVE SPECIES	15 GAL	LOW	4'-5'	4'-5'	48"
ALOE SPP.	ALOE SPECIES	5 GAL	LOW	3'-5'	2'-5'	18"-48"
CALLISTEMON VIMINALIS 'LJ1'	BETTER JOHN BOTTLEBRUSH	5 GAL	LOW	4'-5'	4'-5'	6'-8'
CEANOTHUS GRISEUS HORIZONTALIS 'YANKEE POINT'	YANKEE POINT CEANOTHUS	5 GAL	LOW	36"	60"	6'
DIANELLA SPP.	FLAX LILY SPECIES	5 GAL	LOW	24"	24"	18"
FESTUCA MAIREI	ATLAS FESCUE	1 GAL	LOW	24"	24"	18"
LANTANA X 'NEW GOLD'	NEW GOLD LANTANA	5 GAL	VERY LOW	12"-24"	4'-6'	36"
LEONOTIS LEONURUS	LION'S TAIL	5 GAL	LOW	5'-6'	4'-5'	36"
LOMANDRA SPP.	DWARF MAT RUSH	1 GAL	LOW	24"	24"	24"
MUHLENBERGIA DUBIA	PINE MUHLY	1 GAL	LOW	24"	36"	30"
OLEA EUROPAEA 'LITTLE OLLIE'	LITTLE OLLIE OLIVE	5 GAL	LOW	6'-8'	6'-8'	36"
PENNISETUM SPATHIOLATUM	SLENDER VELDT GRASS	1 GAL	LOW	24"	24"	15"
RHAPHIOLEPIS INDICA 'CLARA'	INDIAN HAWTHORN	5 GAL	LOW	4'-6'	4'-6'	42"
RHAPHIOLEPIS UMBELLATA 'MINOR'	DWARF INDIAN HAWTHORN	5 GAL	LOW	4'-6'	24"-36"	30"
ROSMARINUS OFFICINALIS 'HUNTINGTON CARPET'	HUNTINGTON CARPET ROSEMARY	5 GAL	LOW	12"-24"	8"	5"
SALVIA GREGGII 'FURMAN'S RED'	FURMAN'S RED TEXAS SAGE	5 GAL	LOW	24"-36"	24"-36"	24"
SALVIA LEUCANTHA 'SANTA BARBARA'	SANTA BARBARA SAGE	5 GAL	LOW	24"-36"	24"-36"	30"
SESLERIA AUTUMNALIS	AUTUMN MOOR GRASS	1 GAL	MODERATE	24"	24"	12"
TRACHELOSPERMUM JASMINOIDES	STAR JASMINE	1 GAL	MODERATE	2'-6'	N/A	48"
STORMWATER TREATMENT SHRUBS & GRASSES: SUCH AS (50% - 1 GAL & 50% - 5 GAL)						
BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	HEIGHT	SPREAD	O.C. SPACING
CAREX PRAEGRACILIS	CALIFORNIA FIELD SEDGE	1 GAL	MODERATE	8"-12"	18"-24"	15"
CHONDROPETALUM TECTORUM	SMALL CAPE RUSH	5 GAL	LOW	3'-4'	3'-4'	36"
JUNCUS PATENS	CALIFORNIA GRAY RUSH	5 GAL	LOW	24"	24"	15"
LEYMUS CONDENSATUS 'CANYON PRINCE'	CANYON PRINCE WILD RYE	1 GAL	LOW	2'-3'	2'-3'	24"



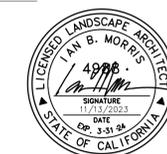


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SUBMITTAL:
SDP 23-0014
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11.13.2023
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SHEET TITLE:
RECYCLED WATER USE PLAN

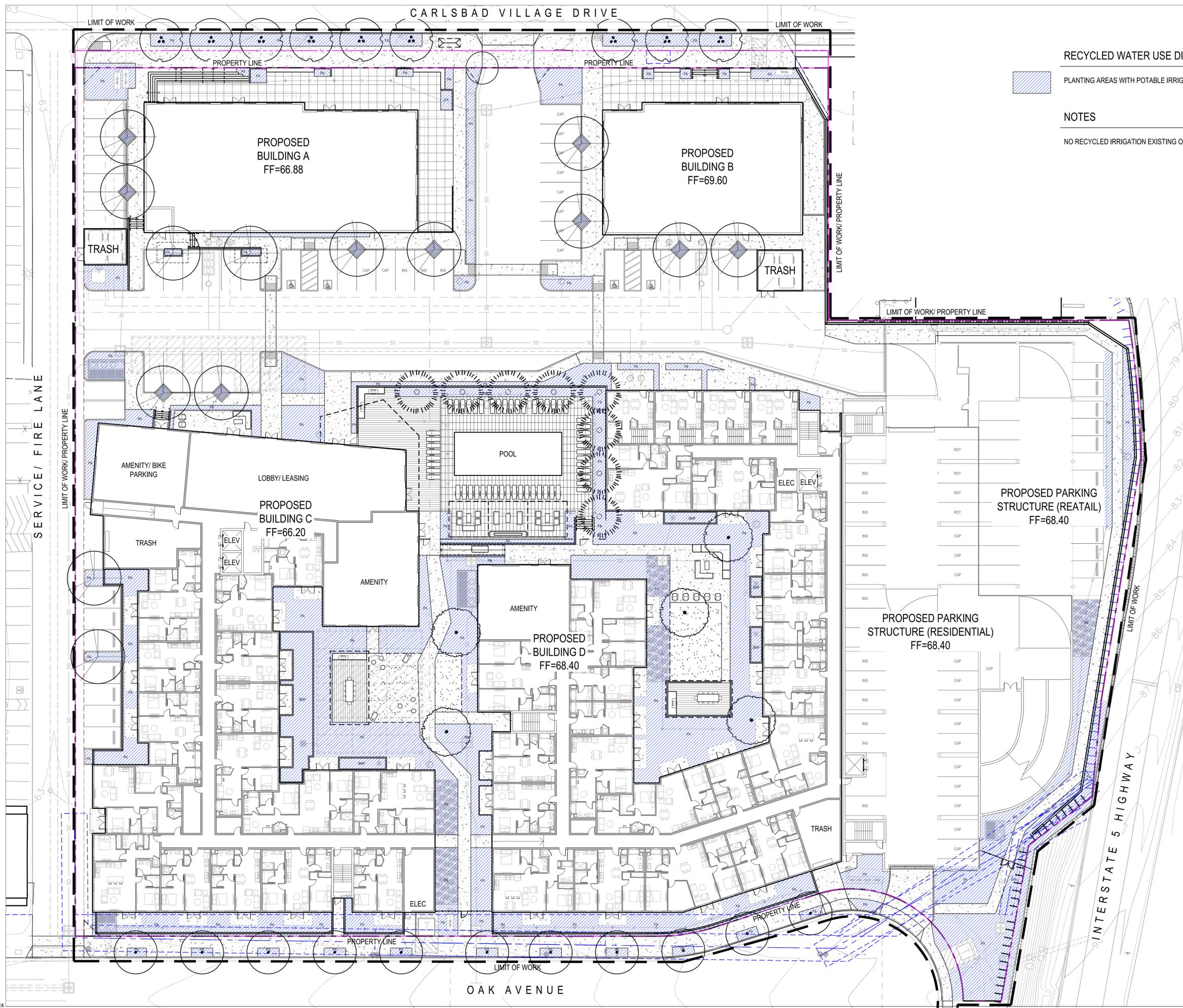
SHEET NUMBER:
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RECYCLED WATER USE DIAGRAM

PLANTING AREAS WITH POTABLE IRRIGATION

NOTES

NO RECYCLED IRRIGATION EXISTING OR PROPOSED ON PROPERTY





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SUBMITTAL: SDP 23-0014

JOB NUMBER: 2022-004.00

DATE: 11.13.2023

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SHEET TITLE:
CONCEPTUAL WATER CONSERVATION PLAN
 SHEET NUMBER:
L1.05

HYDROZONE DIAGRAM

HYDROZONE	PLANT FACTOR (PF)	HYDROZONE AREA (SF)	PERCENT OF TOTAL LANDSCAPE	ZONE OR VALVE	IRRIGATION METHOD
LOW	0.3	23,323	86.2%	TBD	DRIP
MEDIUM	0.5	2,731	10.1%	TBD	DRIP
HIGH	0.8	0	0%	TBD	MP ROTATOR
HIGH	0.8	1,000	3.7%	TBD	POOL

WATER CONSERVATION FEATURES

1. PLANTING DESIGN EMPHASIZES WATER EFFICIENCY WITH USE OF NATIVE, LOW WATER USE, LOW MAINTENANCE, AND DROUGHT TOLERANT PLANTS
2. TREES, VINES, AND SCREENING PLANTS USED ALONG BUILDING FACADE FOR NATURAL HEATING AND COOLING OPPORTUNITIES
3. SUN ORIENTATION ACCOUNTED FOR WHEN SELECTING TREE LOCATIONS
4. SOIL ANALYSIS WILL BE COMPLETED FOR SPECIFIC SOIL AMENDMENTS PRIOR TO PLANT AND IRRIGATION INSTALLATION
5. TREES WILL REDUCE EVAPORATION AND SOIL AND BLOCK WIND
6. LIMITED TURF GRASS AREAS UTILIZED AS OPEN GATHERING AND AMENITY SPACE
7. EFFICIENT IRRIGATION SYSTEM WILL WATER DEEPLY AND INFREQUENTLY, WATERED IN EVENING OR EARLY MORNING, WILL BE MONITORED TO ACCOMMODATE CHANGING WEATHER CONDITIONS. INSTALLATION OF RAIN-SENSOR DEVICES, ELIMINATE RUNOFF
8. ORGANIC MULCH WILL BE UTILIZED THROUGHOUT PLANTING AREAS TO KEEP PLANT ROOTS COOL, MINIMIZE EROSION, AND REDUCE WEED GROWTH.
9. REGULAR MAINTENANCE WILL BE DONE TO ENSURE ALL OF THE ABOVE

NOTES

ONLY SUB-SURFACE IRRIGATION SHALL BE USED TO IRRIGATE ANY VEGETATION WITHIN 24" OF ANY IMPERMEABLE SURFACE UNLESS ADJACENT IMPERMEABLE SURFACES ARE DESIGNED AND CONSTRUCTED TO CAUSE WATER TO DRAIN ENTIRELY INTO A LANDSCAPED AREA.

IRRIGATION WATER BUDGET CALCULATION

MAWA WATER BUDGET:

$(ETO) (0.62) [(ETAF) (LA) + (ETAF) (SLA)]$

$[(40) (0.62)] [(0.45) (27,054) + (0.55) (1)] = 301,936 \text{ GAL/YR.}$

ETWU ESTIMATED TOTAL WATER USE:

$[(ETO) (0.62)] [(PF \times HA / IE) + SLA]$

HYDROZONE 1: $[(40) (0.62)] [(0.3 \times 23,323 / 0.81) + 1] = 214,224.9 \text{ GAL/YR.}$

HYDROZONE 2: $[(40) (0.62)] [(0.5 \times 2,731 / 0.81) + 1] = 41,807.8 \text{ GAL/YR.}$

HYDROZONE 3: $[(40) (0.62)] [(0.8 \times 0 / 0.80) + 1] = 0 \text{ GAL/YR.}$

HYDROZONE 4: $[(40) (0.62)] [(0.8 \times 1,000 / 1.00) + 1] = 19,840 \text{ GAL/YR.}$

ESTIMATED TOTAL WATER USE = 275,872.7 GAL/YR.
ETWU = 26,063 LESS THAN MAWA

MAXIMUM APPLIED WATER ALLOWANCE

MAXIMUM APPLIED WATER ALLOWANCE (MAWA)

COMMERCIAL/INDUSTRIAL:

$MAWA = (ETO)(0.62)[(0.45 \times LA) + (0.55 \times SLA)]$

MAWA MAXIMUM APPLIED WATER ALLOWANCE IN GALLONS PER YEAR.

ETO EVAPOTRANSPIRATION IN INCHES PER YEAR.

0.62 CONVERSION FACTOR TO GALLONS PER SQUARE FOOT.

0.55/0.45 (X LA) ET ADJUSTMENT FACTOR (ETAF) FOR PLANT FACTORS AND IRRIGATION EFFICIENCY.

LA LANDSCAPED AREA INCLUDES SPECIAL LANDSCAPED AREA IN SQUARE FEET.

0.45/0.55 (X SLA) THE ADDITIONAL ET ADJUSTMENT FACTOR FOR A SPECIAL LANDSCAPED AREA (EG. 1.0 - 0.55 = 0.45 OR 1.0 - 0.45 = 0.55)

SLA SPECIAL LANDSCAPED AREA IN SQUARE FEET

ESTIMATED TOTAL WATER USE (ETWU)

$ETWU = (ETO)(0.62) [(PF \times HA) / IE + SLA]$

ETWU ESTIMATED TOTAL WATER USE IN GALLONS PER YEAR.

ETO EVAPOTRANSPIRATION IN INCHES PER YEAR.

0.62 CONVERSION FACTOR TO GALLONS PER SQUARE FOOT.

PF PLANT FACTOR FROM WUCOLS III

HA HYDROZONE AREA IN SQUARE FEET. EACH HA SHALL BE CLASSIFIED BASED UPON THE DATA INCLUDED IN THE LANDSCAPE AND IRRIGATION PLAN AS HIGH, MODERATE, LOW, OR VERY LOW WATER USE.

IE IRRIGATION EFFICIENCY OF THE IRRIGATION METHOD USED IN THE HYDROZONE.

SLA SPECIAL LANDSCAPED AREA IN SQUARE FEET.

DECLARATION OF RESPONSIBLE CHARGE

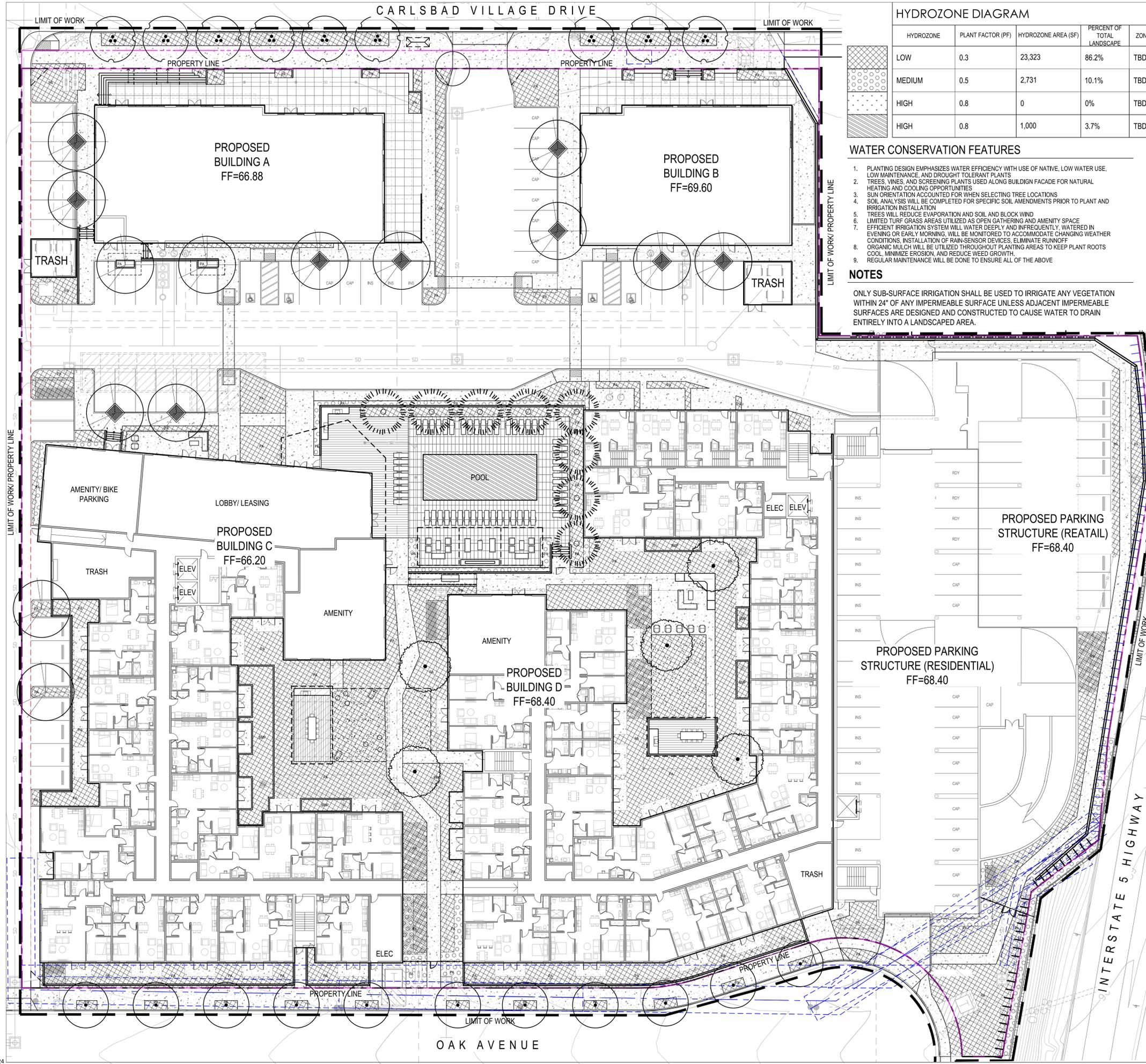
I AM FAMILIAR WITH THE REQUIREMENTS FOR LANDSCAPE AND IRRIGATION PLANS CONTAINED IN THE CITY OF CARLSBAD'S LANDSCAPE MANUAL AND WATER EFFICIENT LANDSCAPE REGULATIONS. I HAVE PREPARED THIS PLAN IN COMPLIANCE WITH THOSE REGULATIONS AND THE LANDSCAPE MANUAL. I CERTIFY THAT THE PLAN IMPLEMENTS THOSE REGULATIONS TO PROVIDE EFFICIENT USE OF WATER.

Signature of Ian Morris

DATE: 08/11/2023

IAN MORRIS
 GROUNDLEVEL LANDSCAPE ARCHITECTURE
 2605 STATE STREET, SUITE B
 SAN DIEGO, CA 92103
 PH: (619) 325-1990

REGISTRATION NO: 4988
 EXPIRATION DATE: 03/31/2024





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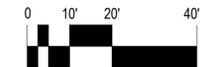
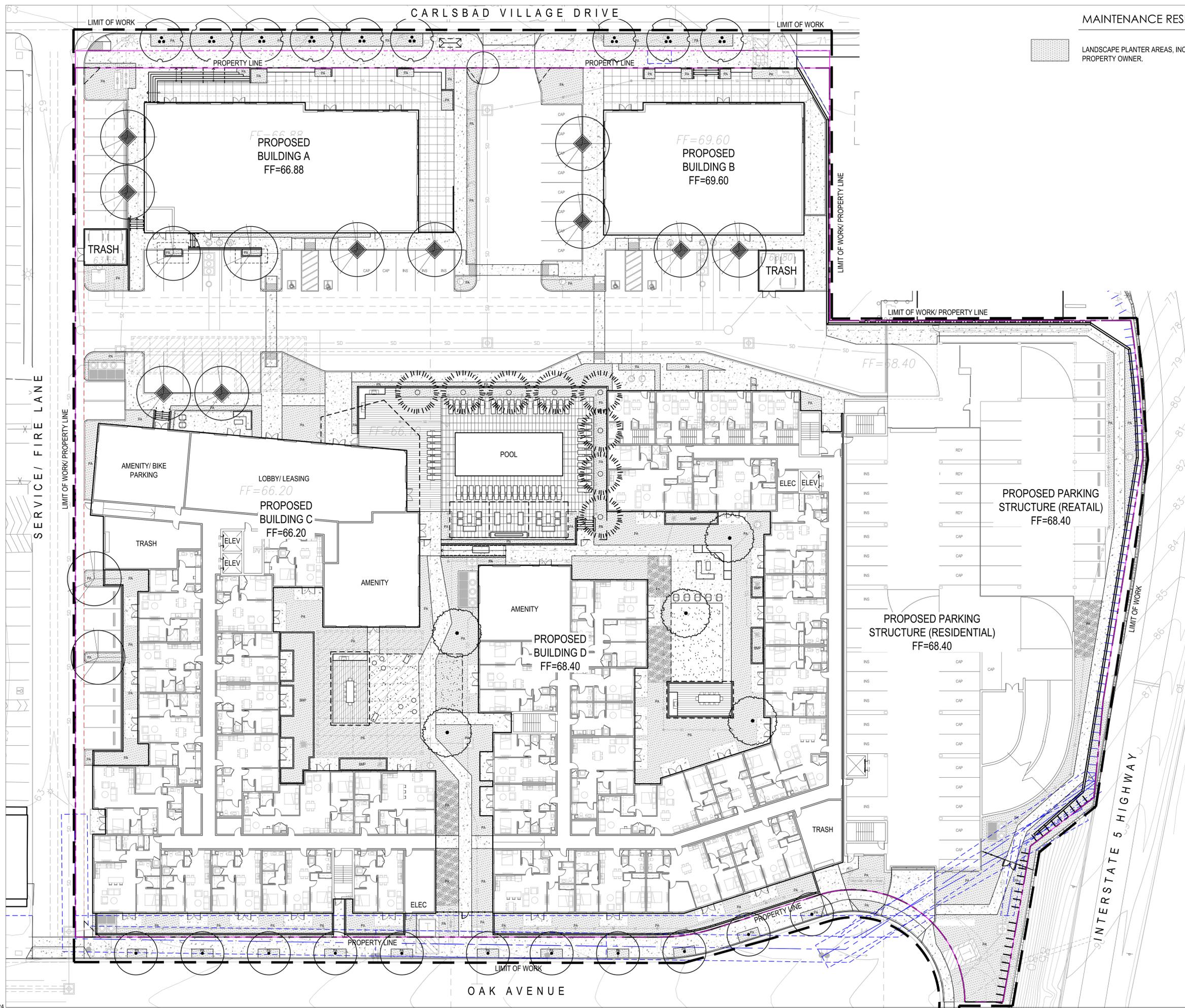
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SHEET TITLE:
CONCEPTUAL MAINTENANCE RESPONSIBILITY PLAN
SHEET NUMBER:
L1.06

MAINTENANCE RESPONSIBILITY



LANDSCAPE PLANTER AREAS, INCLUDING IRRIGATION, MAINTAINED BY THE PROPERTY OWNER.





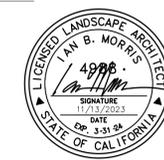
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JOB NUMBER:
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REVISIONS:

SHEET TITLE:
EXISTING TREE SURVEY

SHEET NUMBER:
L1.07

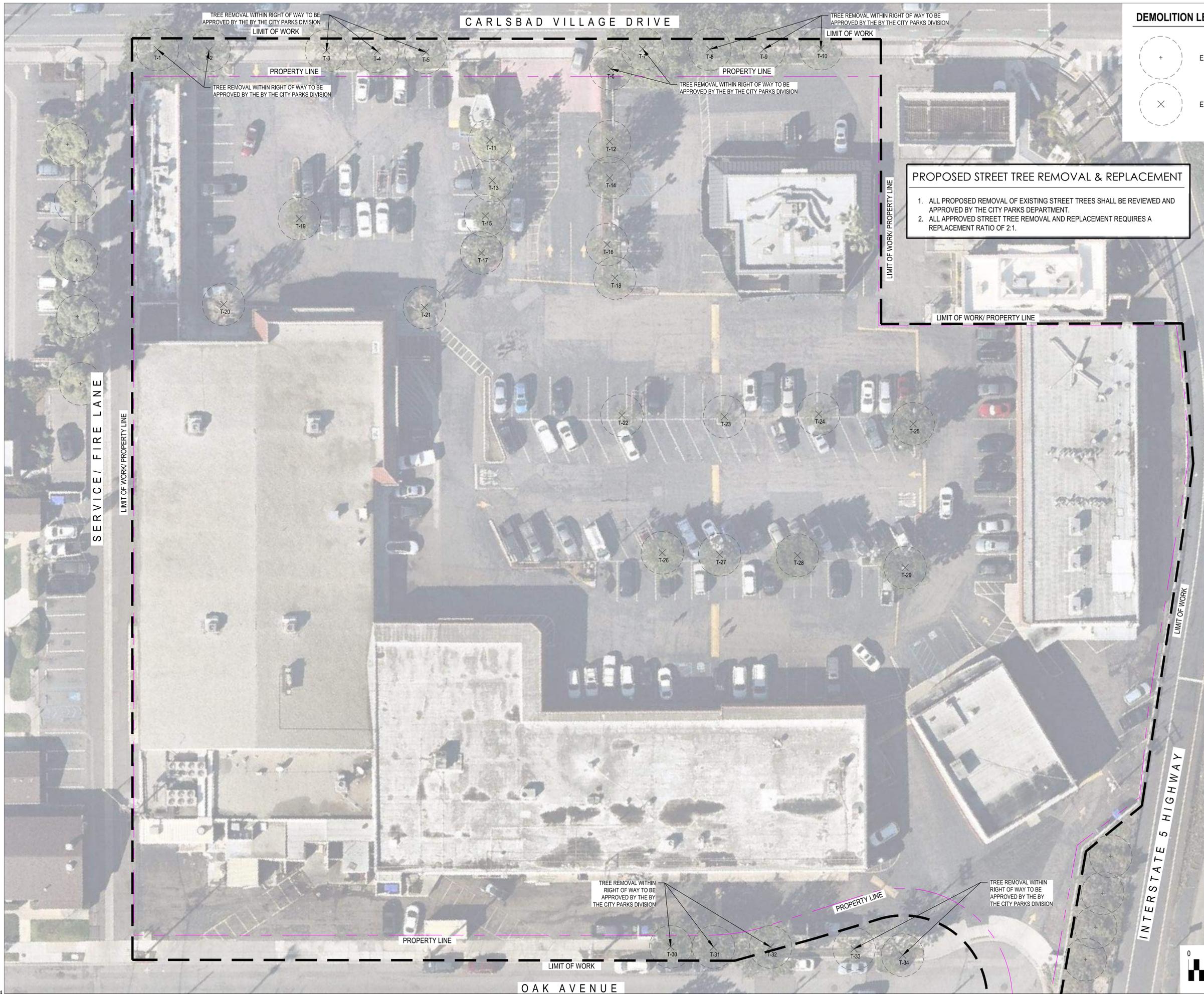
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DEMOLITION LEGEND

- EXISTING TREE, PROTECT/RELOCATE
- EXISTING TREE, TO BE REMOVED

PROPOSED STREET TREE REMOVAL & REPLACEMENT

1. ALL PROPOSED REMOVAL OF EXISTING STREET TREES SHALL BE REVIEWED AND APPROVED BY THE CITY PARKS DEPARTMENT.
2. ALL APPROVED STREET TREE REMOVAL AND REPLACEMENT REQUIRES A REPLACEMENT RATIO OF 2:1.





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SHEET TITLE:
EXISTING TREE SURVEY

SHEET NUMBER:
L1.08

TREE PROTECTION NOTES

1. PRIOR TO ANY DISTURBANCE TO THE SITE, THE OWNER/PERMITTEE SHALL MAKE ARRANGEMENTS FOR A PRE-CONSTRUCTION MEETING WITH THE CITY OF SAN DIEGO MITIGATION MONITORING COORDINATION SECTION (MMC), THE RESIDENT ENGINEER (RE) AND/OR BUILDING INSPECTOR, CERTIFIED CONSULTING ARBORIST, CONSTRUCTION MANAGER, AND LANDSCAPE PROFESSIONAL.
2. THE CONSTRUCTION MANAGER IS RESPONSIBLE FOR PREVENTING DAMAGE TO TREES.
 - a) CONSTRUCTION AND MAINTENANCE STAFF SHALL AVOID UNNECESSARY ACTIVITIES WITHIN THE DRIPLINE OF TREES.
 - b) FINES SHALL BE ASSESSED TO THOSE INDIVIDUALS FOUND TO BE RESPONSIBLE FOR THE ILLEGAL REMOVAL OR DAMAGE OF PROTECTED TREES.
3. ANY DAMAGE OR INJURY TO TREES SHALL BE REPORTED WITHIN 24 HOURS TO MMC BY THE CERTIFIED CONSULTING ARBORIST.
4. THE CERTIFIED CONSULTING ARBORIST SHALL BE RESPONSIBLE FOR ESTABLISHING, IMPLEMENTING, AND MAINTAINING THE TREE PROTECTION ZONE AS SHOWN ON THESE PLANS DURING THE ENTIRE CONSTRUCTION PERIOD:
 - a) STAKE THE COMPLETE DRIPLINE.
 - b) ROOTS SHALL BE CLEANLY CUT BACK APPROX. 6" FROM THE LIMIT OF DISTURBANCE, UNLESS OTHERWISE DIRECTED BY THE CERTIFIED CONSULTING ARBORIST.
 - c) INSTALL A TEMPORARY 6-FT HIGH CHAIN LINK FENCE AROUND THE IDENTIFIED DRIPLINE, UNLESS OTHERWISE DIRECTED BY THE CERTIFIED CONSULTING ARBORIST.
 - d) ATTACH AN ORANGE OR YELLOW PROTECTIVE FENCE (MIN. 3' HIGH_ TO THE CHAIN LINK FENCE.
 - e) WHEN AREAS UNDER THE TREE CANOPY CANNOT BE FENCED, ANOTHER METHOD SHALL BE REQUIRED, AS RECOMMENDED BY THE CERTIFIED CONSULTING ARBORIST AND APPROVED BY MMC.
 - f) POST "KEEP OUT" SIGNS IN BOTH SPANISH AND ENGLISH TO THE CHAIN LINK FENCE.
 - g) PLACE STORY POLES WHERE PROPOSED BUILDING(S) AND TREE BRANCHES INTERFACE.
 - h) SUBMIT TO MMC A TREE MAINTENANCE SCHEDULE PREPARED BY THE CERTIFIED CONSULTING ARBORIST.
 - i) IF A TREE IS ADJACENT TO OR IN THE IMMEDIATE PROXIMITY TO A GRADE SLOPE OF 8% (23 DEGREES) OR MORE, PROVIDE ADEQUATE EROSION CONTROL.
5. THE CERTIFIED CONSULTING ARBORIST SHALL VERIFY IN WRITING TO THE OWNER/PERMITTEE THAT THE TREE PROTECTION ZONE HAS BEEN ESTABLISHED.
6. THE OWNER/PERMITTEE SHALL SUBMIT THE CERTIFIED CONSULTING ARBORIST'S VERIFICATION LETTER TO MMC BEFORE SCHEDULING A SITE INSPECTION WITH MMC, THE RE AND/OR BUILDING INSPECTOR, CERTIFIED CONSULTING ARBORISTS, AND LANDSCAPE PROFESSIONAL.
7. RESTRICTIONS WITH THE TREE PROTECTION ZONE TO BE MONITORED BY THE CERTIFIED CONSULTING ARBORIST AS FOLLOWS:
 - a) TREE PRUNING OR ROOT PRUNING WILL ONLY OCCUR UNDER THE GUIDANCE OF THE CERTIFIED CONSULTING ARBORIST WITH WRITTEN APPROVAL FROM MMC.
 - b) PRUNING FOR THE VERTICAL CLEARANCE OF BUILDINGS, TRAFFIC, AND CONSTRUCTION EQUIPMENT SHALL BE PERFORMED BY THE CERTIFIED CONSULTING ARBORIST ONLY.
 - c) CONSTRUCTION EQUIPMENT AND MATERIALS SUCH AS STOCKPILING OF SOIL, VEHICLE USE, FOOT TRAFFIC, UTILITY CONTAINERS, AND STORAGE OF ANY KIND, ARE PROHIBITED.
 - d) THE EXISTING GRADE SHALL NOT BE DISTURBED UNLESS OTHERWISE APPROVED BY MMC, THE RE, AND/OR BUILDING INSPECTOR.
 - e) ROOT SYSTEMS SHALL BE PROTECTED FROM FLOODING, EROSION, CHEMICAL SPILLS, AND EXCESSIVE VARIATIONS IN SOIL MOISTURE BY CONSTRUCTION ACTIVITY.
8. ANY MODIFICATIONS AND/OR ADDITIONS TO THE APPROVED TREE PROTECTION ZONE SHALL BE SUBMITTED BY THE CERTIFIED CONSULTING ARBORIST WITH ADEQUATE INFORMATION TO MMC FOR APPROVAL.
9. THE OWNER/PERMITTEE SHALL BE RESPONSIBLE FOR A MINIMUM 3 YEAR MONITORING AND MAINTENANCE PERIOD, DESIGNED BY THE CERTIFIED CONSULTING ARBORIST AFTER CONSTRUCTION TO ENSURE THE SUCCESS OF PROTECTED AND/OR REPLACEMENT TREES.
10. THE CERTIFIED CONSULTING ARBORIST SHALL PROVIDE ANNUAL REPORTS TO AND REQUEST INSPECTIONS FROM MMC DURING THE MONITORING AND MAINTENANCE PERIOD.

PROPOSED STREET TREE REMOVAL & REPLACEMENT

1. ALL PROPOSED REMOVAL OF EXISTING STREET TREES SHALL BE REVIEWED AND APPROVED BY THE CITY PARKS DEPARTMENT.
2. ALL APPROVED STREET TREE REMOVAL AND REPLACEMENT REQUIRES A REPLACEMENT RATIO OF 2:1.

TREE SUREVY - TO REMOVE			
NUMBER	TREE SPECIES	CALIPER	ACTION
T-1	FICUS MICROCARPA	12"	PROPOSED REMOVAL
T-2	FICUS MICROCARPA	12"	PROPOSED REMOVAL
T-3	FICUS MICROCARPA	8"	PROPOSED REMOVAL
T-4	FICUS MICROCARPA	8"	PROPOSED REMOVAL
T-5	FICUS MICROCARPA	14"	PROPOSED REMOVAL
T-6	FICUS MICROCARPA	4"	PROPOSED REMOVAL
T-7	FICUS MICROCARPA	14"	PROPOSED REMOVAL
T-8	FICUS MICROCARPA	14"	PROPOSED REMOVAL
T-9	FICUS MICROCARPA	12"	PROPOSED REMOVAL
T-10	FICUS MICROCARPA	10"	PROPOSED REMOVAL
T-11	FICUS MICROCARPA	MULTI	REMOVE
T-12	FICUS MICROCARPA	14"	REMOVE
T-13	FICUS MICROCARPA	0'-10"	REMOVE
T-14	FICUS MICROCARPA	16"	REMOVE
T-15	FICUS MICROCARPA	10"	REMOVE
T-16	FICUS MICROCARPA	6"	REMOVE
T-17	FICUS MICROCARPA	10"	REMOVE
T-18	FICUS MICROCARPA	6"	REMOVE
T-19	FICUS MICROCARPA	10"	REMOVE
T-20	EUCALYPTUS SPP.	16"	REMOVE
T-21	EUCALYPTUS SPP.	16"	REMOVE
T-22	FICUS MICROCARPA	6"	REMOVE
T-23	FICUS MICROCARPA	8"	REMOVE
T-24	FICUS MICROCARPA	8"	REMOVE
T-25	FICUS MICROCARPA	12"	REMOVE
T-26	FICUS MICROCARPA	10"	REMOVE
T-27	FICUS MICROCARPA	10"	REMOVE
T-28	FICUS MICROCARPA	12"	REMOVE
T-29	FICUS MICROCARPA	12"	REMOVE
T-30	FICUS MICROCARPA	10"	PROPOSED REMOVAL
T-31	FICUS MICROCARPA	10"	PROPOSED REMOVAL
T-32	FICUS MICROCARPA	12"	PROPOSED REMOVAL
T-33	FICUS MICROCARPA	4"	PROPOSED REMOVAL
T-34	FICUS MICROCARPA	8"	PROPOSED REMOVAL



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SUBMITTAL:
SDP 23-0014
JOB NUMBER:
2022-004.00
DATE:
11.13.2023
REVISIONS:

SHEET TITLE:
SHADE COVERAGE EXHIBIT

SHEET NUMBER:
L1.09

2019 CAL GREEN BUILDINGS CODE SHADE REQUIREMENTS

- SHADE TREE PLANTINGS, MINIMUM #10 CONTAINER SIZE OR EQUAL, SHALL BE INSTALLED TO PROVIDE SHADE OVER 50 PERCENT OF THE PARKING AREA WITHIN 15 YEARS. EXCEPTION: THE SURFACE PARKING AREA COVERED BY SOLAR PHOTOVOLTAIC SHADE STRUCTURES, OR SHADE STRUCTURES, WITH ROOFING MATERIALS THAT COMPLY WITH TABLE A5.106.11.2.2, IN APPENDIX A5, ARE NOT INCLUDED IN THE TOTAL AREA CALCULATION.
- SHADE TREE PLANTINGS, MINIMUM #10 CONTAINER SIZE OR EQUAL SHALL BE INSTALLED TO PROVIDE SHADE OF 20% OF THE LANDSCAPE AREA WITHIN 15 YEARS. EXCEPTION: PLAYFIELDS FOR ORGANIZED SPORT ACTIVITY ARE NOT INCLUDED IN THE TOTAL AREA CALCULATION.
- SHADE TREE PLANTINGS, MINIMUM #10 CONTAINER SIZE OR EQUAL SHALL BE INSTALLED TO PROVIDE SHADE OVER 20% OF THE HARDSCAPE AREA WITHIN 15 YEARS. EXCEPTION: WALKS, HARDSCAPE AREAS COVERED BY SOLAR PHOTOVOLTAIC SHADE STRUCTURES, AND HARDSCAPE AREAS COVERED BY SHADE STRUCTURES WITH ROOFING MATERIALS THAT COMPLY WITH TABLE A5.106.2.2 IN APPENDIX A5, ARE NOT INCLUDED IN THE TOTAL AREA CALCULATION.

SHADE COVERAGE		
UNCOVERED LANDSCAPE AREA	19,609 SF	(25,260 TOTAL)
LANDSCAPE COVERAGE	5,651 SF	
LANDSCAPE PERCENTAGE	22.4%	
UNCOVERED HARDSCAPE AREA	26,892 SF	(36,272 TOTAL)
HARDSCAPE COVERAGE	9,380 SF	
HARDSCAPE PERCENTAGE	25.9%	
UNCOVERED PARKING AREA	4,239 SF	(9,175 TOTAL)
PARKING COVERAGE	4,936 SF	
PARKING PERCENTAGE	53.8%	





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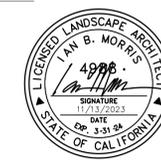
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**LANDSCAPED
PARKING LOT
EXHIBIT**

SHEET NUMBER:
L1.10

Item #1 269 of 269

LANDSCAPED PARKING AREA		
CATEGORY	PERCENTAGE	AREA
VEHICULAR USE AREA	95.10%	23,908 SF
LANDSCAPE AREA	4.90%	1,232 SF
TOTAL	100%	25,139 SF

