

*Bressi Ranch Master Plan*

**December 2016**



MP 178 (A)  
Planning Commission Resolution No. 5460  
September 17, 2003

MP 178(B)  
Planning Commission Resolution No. 5501  
November 5, 2003

MP 178(C)  
Planning Commission Resolution No. 5923  
June 15, 2005  
City Council Ordinance No. NS-768  
August 2, 2005

MP 178(D)  
Planning Commission Resolution No. 6221  
January 3, 2007

MP 178(E)  
Planning Commission Resolution No. 6310  
June 20, 2007

MP 178(F)  
Planning Commission Resolution No. 6361  
December 5, 2007  
City Council Ordinance No. NS 874  
March 12, 2008

MP 178(G)  
Planning Commission Resolution No. 6531  
February 4, 2009

MP 178(H)  
Withdrawn.







MP 178(I)  
Planning Commission Resolution No. 7175  
December 7, 2016  
City Council Ordinance CS-315  
February 14, 2017

MP 178(K)  
Planning Commission Resolution No. 7197  
October 5, 2016



**TABLE OF CONTENTS**

**I. INTRODUCTION .....I-1**

A. Purpose ..... I-1

B. Location & Physical Description ..... I-4

C. History of Bressi Ranch ..... I-4

D. Project Description ..... I-8

E. Legal Description..... I-10

F. Applications ..... I-14

    1. Applications Being Processed with the Master Plan ..... I-14

    2. Future Discretionary Applications..... I-16

**II. GOALS OF THE BRESSI RANCH MASTER PLAN .....II-1**

**III. LAND USE & ENTITLEMENT PROVISIONS .....III-1**

A. General Provisions ..... III-1

    1. Authority Granted by Approval of the Master Plan ..... III-1

    2. Entitlements ..... III-1

    3. Development Consistency ..... III-1

    4. Severability..... III-2

    5. Maximum Number of Units..... III-2

    6. Boundaries ..... III-2

    7. Recordation..... III-3



---

---

8. Growth Management.....	III-3
9. Dedications .....	III-3
10. Public Facilities .....	III-3
11. Zoning .....	III-3
12. Location of Improvements.....	III-4
13. Community Associations.....	III-4
a. Residential Areas & Open Space .....	III-4
b. Non-Residential Areas.....	III-4
c. General Provisions Applicable to all Associations .....	III-5
14. Architectural Review Board .....	III-7
15. Exterior Home Improvements .....	III-8
16. Recreational Vehicle Storage.....	III-8
B. General Plan .....	III-8
C. Zoning .....	III-9
D. Relationship to General Plan & Other Applicable Planning Documents.....	III-10
E. Land Uses .....	III-10
1. Planned Industrial .....	III-12
2. Community Facilities .....	III-12
3. Mixed Use .....	III-13
4. Residential .....	III-13
5. Open Space .....	III-14
<b>IV. STREETS.....</b>	<b>IV-1</b>

---

---

A. Introduction.....	IV-1
B. Street Cross Sections.....	IV-2
C. Traffic Calming .....	IV-24
D. Corner Sight Distance Specifications .....	IV-35
E. Maintenance.....	IV-43
F. Public Transit.....	IV-43
<b>V. LANDSCAPE .....</b>	<b>V-1</b>
A. Landscape Concept .....	V-1
B. Landscape Palette.....	V-4
C. Landscape Zones.....	V-4
D. Irrigation .....	V-13
E. Brush Management Slopes .....	V-13
<b>VI. DEVELOPMENT REVIEW PROCESS.....</b>	<b>VI-1</b>
A. All Planning Areas .....	VI-1
B. Industrial Planning Areas (PA 1-5 & 14).....	VI-1
C. Residential Planning Areas (PA 6-12) .....	VI-2
D. Community Facilities (PA 13) .....	VI-5
E. Mixed Use & Community Facilities (PA 15).....	VI-5
F. Open Space Areas (OS 1-6) .....	VI-6
G. Amendments .....	VI-6
H. Clarifications/Interpretations.....	VI-7



**VII. INDUSTRIAL/OFFICE DEVELOPMENT STANDARDS AND DESIGN**

**GUIDELINES .....VII-1**

A. Development Standards ..... VII-1

B. Industrial Driveways ..... VII-1

C. Design Criteria ..... VII-4

D. Architectural Guidelines..... VII-14

E. Sign Criteria..... VII-18

F. Individual Planning Area Standards ..... VII-18

    1. Planning Area 1..... VII-18

    2. Planning Area 2..... VII-22

    3. Planning Area 3..... VII-26

    4. Planning Area 4..... VII-30

    5. Planning Area 5..... VII-34

    6. Planning Area 14..... VII-38

**VIII. COMMERCIAL/COMMUNITY FACILITIES DEVELOPMENT STANDARDS &**

**DESIGN GUIDELINES .....VIII-1**

A. Individual Planning Area Standards .....VIII-1

    1. Planning Area 13.....VIII-1

    2. Planning Area 15.....VIII-5

    3. Planning Area 16.....VIII-17

---

---

**IX. VILLAGE GREEN (OPEN SPACE AREA 2) DEVELOPMENT STANDARDS & DESIGN GUIDELINES .....IX-1**

A. Description .....IX-1

B. Use Allocation .....IX-1

C. Development Standards .....IX-3

D. Special Design Criteria .....IX-3

**X. RESIDENTIAL DEVELOPMENT STANDARDS & DESIGN GUIDELINES .X-1**

A. Introduction.....X-1

B. Neighborhood Development Standards .....X-3

C. Neighborhood Design Criteria .....X-4

D. Architectural Design Guidelines .....X-11

E. Individual Planning Area Standards .....X-25

1. Planning Area 6.....X-25

2. Planning Area 7.....X-28

3. Planning Area 8.....X-31

4. Planning Area 9.....X-34

5. Planning Area 10.....X-37

6. Planning Area 11.....X-40

7. Planning Area 12.....X-43

7. Planning Area 12.....X-46

**XI. OPEN SPACE .....XI-1**



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A. Master Plan Open Space .....	XI-1
1. Open Space for the Preservation of Natural Resources .....	XI-2
2. Open Space for the Managed Production of Resources.....	XI-4
3. Open Space for Outdoor Recreation.....	XI-4
4. Open Space for Public Health & Safety .....	XI-5
B. Pedestrian System .....	XI-5
1. Citywide Trails.....	XI-5
C. Maintenance.....	XI-7
1. Open Space .....	XI-7
2. Citywide Trails.....	XI-7
D. Individual Open Space Areas .....	XI-8
1. Open Space Area 1 .....	XI-8
2. Open Space Area 2 .....	XI-12
3. Open Space Area 3 .....	XI-13
4. Open Space Area 4 .....	XI-16
5. Open Space Area 5 .....	XI-19
6. Open Space Area 6 .....	XI-22
<b>XII. GRADING.....</b>	<b>XII-1</b>
A. Introduction.....	XII-1
B. Conformance with City of Carlsbad Ordinances.....	XII-2
C. Advanced Grading.....	XII-4
<b>XIII. PUBLIC FACILITIES.....</b>	<b>XIII-1</b>

A. Introduction.....XIII-1

B. Facilities Based on Master Plan Phasing .....XIII-1

C. Citywide Facilities .....XIII-1

D. Quadrant Facilities.....XIII-2

E. Zone Facilities .....XIII-3

**XIV. MASTER PLAN AMENDMENT LOG .....XIV-1**

A. Planning Area Unit Count ..... XIV-1

B. Master Plan Amendments ..... XIV-2

C. Clarifications/Interpretations ..... XIV-2



**LIST OF EXHIBITS**

Exhibit I-1	Concept Master Plan .....	I-2
Exhibit I-2	Land Use Summary .....	I-3
Exhibit I-3	Vicinity Map .....	I-5
Exhibit I-4	Airport Constraints (1994) .....	I-6
Exhibit I-4a	Airport Constraints (2010) .....	I-7
Exhibit I-5	Preliminary Schematic Master Site Plan .....	I-9
Exhibit I-6	Community Features .....	I-11
Exhibit I-7	5-Minute Walk Radius Plan .....	I-12
Exhibit I-8	Conceptual Mixed-Use Site Plan .....	I-13
Exhibit I-9	Land Use Zoning .....	I-15
Exhibit III-1	General Plan Statistics by Planning Area .....	III-11
Exhibit III-2	Master Plan Open Space .....	III-16
Exhibit IV-1	Sidewalk Joint Locations .....	IV-3
Exhibit IV-2	Street Section & Plan View Location Map 1 .....	IV-4
Exhibit IV-3	Street Section 1 .....	IV-5
Exhibit IV-4	Street Section 2 .....	IV-8
Exhibit IV-5	Street Section 3 .....	IV-10
Exhibit IV-6	Street Section 4 .....	IV-11
Exhibit IV-7	Street Section 5 .....	IV-13
Exhibit IV-8	Street Section 6 .....	IV-14
Exhibit IV-9	Street Section 7 .....	IV-15
Exhibit IV-10	Street Section 8 .....	IV-16
Exhibit IV-11	Street Section 9 .....	IV-18
Exhibit IV-12	Street Section 10 .....	IV-19
Exhibit IV-13	Village Center Street Section 11 .....	IV-20
Exhibit IV-14	Village Center Street Section 12 .....	IV-22
Exhibit IV-15	Village Center Street Section 13 .....	IV-23
Exhibit IV-16	Traffic Calming Location Map .....	IV-25
Exhibit IV-17	Traffic Calming 1 .....	IV-26
Exhibit IV-18	Traffic Calming 2 .....	IV-29
Exhibit IV-19	Traffic Calming 3 .....	IV-30
Exhibit IV-20	Traffic Calming 4 .....	IV-31
Exhibit IV-21	Traffic Calming 5 .....	IV-32
Exhibit IV-22	Village Center – Traffic Calming 6 .....	IV-33
Exhibit IV-23	Village Center – Traffic Calming 7 .....	IV-34
Exhibit IV-24	Village Center – Traffic Calming 8 .....	IV-37
Exhibit IV-25	Traffic Circle & Pedestrian Scale Street Light .....	IV-38

Exhibit IV-25a	Chicanes .....	IV-39
Exhibit IV-25a	Chicanes .....	IV-40
Exhibit IV-25a	Chicanes .....	IV-41
Exhibit IV-25a	Chicanes .....	IV-42
Exhibit IV-27	Bus Stops & Bus Turnouts.....	IV-44
Exhibit V-1	Master Landscape Plan .....	V-1
Exhibit V-2	Landscape Palette for Circulation Element Roadways .....	V-5
Exhibit V-3	Bressi Ranch Recommended Plant Materials List .....	V-6
Exhibit V-4	Landscape Zones Plan.....	V-11
Exhibit V-5	Brush Management Plan .....	V-14
Exhibit V-6	Brush Management – Manufactured Slopes .....	V-16
Exhibit V-7	Brush Management – Native Slopes .....	V-17
Exhibit VII-1	Industrial Driveway Locations .....	VII-3
Exhibit VII-2	Industrial/Single Family Residential Condition.....	VII-5
Exhibit VII-3	Industrial/Apartment Condition .....	VII-6
Exhibit VII-4	Recommended Site Orientation .....	VII-7
Exhibit VII-5	Site Entry Design Treatment.....	VII-9
Exhibit VII-6	Plaza Design.....	VII-10
Exhibit VII-7	Loading & Service Area Screening.....	VII-11
Exhibit VII-8	Site Lighting.....	VII-13
Exhibit VII-9	Entry Design .....	VII-15
Exhibit VII-10	Equipment Screening .....	VII-17
Exhibit VII-11	Planning Area 1.....	VII-19
Exhibit VII-12	Planning Area 2.....	VII-23
Exhibit VII-13	Planning Area 3.....	VII-27
Exhibit VII-14	Planning Area 4.....	VII-31
Exhibit VII-15	Planning Area 5.....	VII-35
Exhibit VII-16	Planning Area 14.....	VII-39
Exhibit VIII-1	Planning Area 13.....	VIII-2
Exhibit VIII-2	Planning Area 15.....	VIII-6
Exhibit VIII-3	Village Center Plan with Adjacent Development .....	VIII-7
Exhibit VIII-4	Planning Area 16.....	VIII-18
Exhibit VIII-5	Planning Area 16 Site Plan .....	VIII-19
Exhibit IX-1	Open Space 2 .....	IX-2
Exhibit X-1	12-Foot Wide Driveway.....	X-5
Exhibit X-2	19-Foot Wide Driveway.....	X-6
Exhibit X-3	Wall Elevations .....	X-7
Exhibit X-4	Residential Entry Monumentation .....	X-8
Exhibit X-5	Variable Front Yard Setbacks .....	X-10
Exhibit X-6	Neighborhood Park .....	X-12
Exhibit X-7	Single Story Elements.....	X-14
Exhibit X-8	Corner Lot Configuration.....	X-17
Exhibit X-9	Alley Loaded Garage .....	X-18

Exhibit X-10	Split Garages .....	X-19
Exhibit X-11	Three Car Tandem Garage .....	X-21
Exhibit X-12	Deep Recessed Garages .....	X-22
Exhibit X-13	Conceptual Sign Locations .....	X-24
Exhibit X-14	Planning Area 6.....	X-26
Exhibit X-15	Planning Area 7.....	X-29
Exhibit X-16	Planning Area 8.....	X-32
Exhibit X-17	Planning Area 9.....	X-35
Exhibit X-18	Planning Area 10.....	X-38
Exhibit X-19	Planning Area 11.....	X-41
Exhibit X-20	Planning Area 12.....	X-44
Exhibit XI-1	Biological Mitigation Plan.....	XI-3
Exhibit XI-2	Village Amenities w/ Pedestrian Linkages .....	XI-6
Exhibit XI-3	Open Space 1 .....	XI-9
Exhibit XI-4	Open Space 3 .....	XI-14
Exhibit XI-5	Open Space 4 .....	XI-17
Exhibit XI-6	Open Space 5 .....	XI-20
Exhibit XI-7	Open Space 6 .....	XI-23
Exhibit XII-1	Grading Cut & Fill Areas.....	XII-3
Exhibit XIII-1	Proposed Major Drainage Facilities.....	XIII-4
Exhibit XIII-2	Proposed Major Sewer Facilities .....	XIII-8
Exhibit XIII-3	Proposed Major Water Facilities .....	XIII-9
Exhibit XIII-4	Proposed Recycled Water Facilities.....	XIII-10
Exhibit XIV-1	Master Plan Amendment Log .....	XIV-1

**APPENDICES**

- A. GENERAL PLAN CONSISTENCY ANALYSIS
- B. CONSISTENCY ANALYSIS WITH OTHER APPLICABLE PLANNING DOCUMENTS
- C. NORTH COUNTY TRANSIT DISTRICT – TRANSIT ORIENTED DESIGN PRINCIPLES
- D. AHWAHNEE PRINCIPLES



## **I. INTRODUCTION**

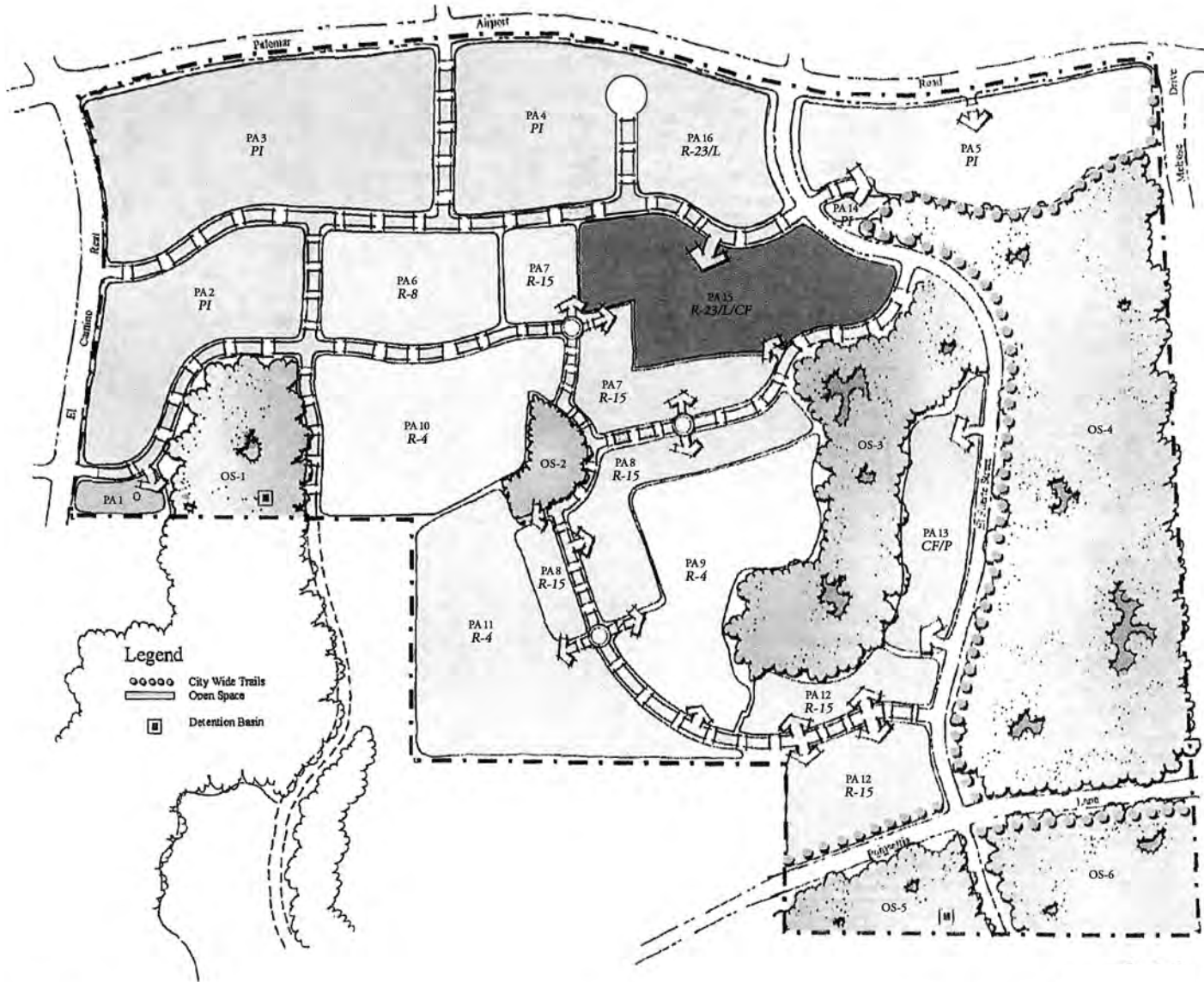
### **A. Purpose**

The purpose of the Bressi Ranch Master Plan (also referred to as Master Plan) is to provide for a comprehensive set of guidelines, regulations and implementation programs for ensuring the development of Bressi Ranch in accordance with the City's General Plan, Municipal Code and other applicable planning documents. The Bressi Ranch Master Plan has been prepared in conformance with the requirements of Chapter 21.38, Planned Community Zone, of the Carlsbad Municipal Code.

The Bressi Ranch Master Plan defines the allowable type and intensity of land uses, provides detailed development and design criteria, and describes how the Master Plan will be implemented. City Council adoption of the Bressi Ranch Master Plan will establish the zoning and development standards for this site. Adoption of the Bressi Ranch Master Plan will ensure that the subject property is developed in accordance with the following except as modified by the Master Plan, in which case the Master Plan shall prevail:

- City of Carlsbad General Plan
- Carlsbad Municipal Code
- Zone 17 Local Facilities Management Plan
- McClellan-Palomar Airport Comprehensive Land Use Plan
- McClellan-Palomar Airport Master Plan
- Carlsbad's Habitat Management Plan
- El Camino Real Scenic Corridor Development Standards
- City of Carlsbad Landscape Manual

The conceptual development scheme for the Bressi Ranch Master Plan is illustrated by Exhibits I-1 and I-2 on pages I-2 and I-3, along with the other exhibits provided throughout the Master Plan text. The purpose of Exhibit I-1 and the other exhibits is to provide sufficient information to evaluate the Master Plan concept. These exhibits are conceptual in nature and are provided solely to indicate general location, amount and type of development, potential access points, limits of grading, circulation and the proposed development scheme for each planning area. The detailed design of these planning areas will be reviewed at the time of Tentative Map or Site Development Plan submittal to ensure that they are in conformance with the design criteria of the Bressi Ranch Master Plan and all applicable City Ordinances and Policies.



*Bressi Ranch*

A planned community in the City of Carlsbad, California

Concept Master Plan

I-1



**EXHIBIT I-2 Land Use Summary**

**Bressi Ranch Master Plan Land Use Summary**

<b>Planning Area/ Open Space Area</b>	<b>General Plan Designation</b>	<b>Zoning</b>	<b>Units (residential)</b>	<b>Sq. Ft. (non-res)</b>	<b>Acreage</b>
<b>Industrial &amp; Office:</b>					
PA 1	O	O	--	**	2.5
PA 2	PI	P-M	--	**	27.5
PA 3	PI	P-M	--	**	47
PA 4	PI	P-M	--	**	26
PA 5	PI	P-M	--	**	28
PA 14	PI	P-M	--	**	1.1
subtotal				<b>1,900,000</b>	<b>132.1</b>
<b>Residential:</b>					
PA 6	RM	RD-M	63		16.2
PA 7	RMH	RD-M	95		20
PA 8	RMH	RD-M	95		19.9
PA 9	RLM	R-1	70		26.8
PA 10	RLM	R-1	84		31.5
PA 11	RLM	R-1	25		36.4
PA 12	RMH	RD-M	91		24.1
subtotal					<b>174.9</b>
<b>Community Facilities:</b>					
PA 13	CF/P	CF		138,000	13.7
subtotal				<b>138,000</b>	<b>13.7</b>
<b>Mixed Use:</b>					
PA 15	RH/L/CF	RD-M/C-L/CF	100	130,000	27.7
PA 16	R-23/L	RD-M/C-L	125	91,414	18
subtotal				<b>221,414</b>	<b>45.7</b>
<b>Open Space:</b>					
OS 1	OS	OS	--	--	16.9
OS 2	OS	OS	--	--	4
OS 3	OS	OS	--	--	39.5
OS 4	OS	OS	--	--	98.9
OS 5	OS	OS	--	--	11
OS 6	OS	OS	--	--	20.4
subtotal					<b>190.7</b>
<b>Roadways:</b>					
					<b>28</b>
<b>TOTAL:</b>			<b>748</b>	<b>2,259,414</b>	<b>585.1</b>

\*\* Total of 1,900,000 sq. ft. of non-residential space (PA 1-5 & 14)

## **B. Location & Physical Description**

The Bressi Ranch Master Plan covers 585.1 acres and is located at the southeast corner of Palomar Airport Road and El Camino Real within the southeast quadrant of the City of Carlsbad as shown by Exhibit I-3 on page I-5. The site is bordered to the east by the Rancho Carrillo Master Plan. The property to the south of Bressi Ranch is the proposed Villages of La Costa Master Plan. El Camino Real and Palomar Airport Road separate the site from industrial development to the west and open space and industrially zoned land to the north.

Elevations on site range from 125 to 460 feet above sea level. The northern portion of the site is composed of a large plateau cut by a number of steep sided gullies, dropping off steeply to the low-lying southern portion of the property. This results in an elevation differential of approximately 335 feet between the highest and lowest portions of the site. Other than the steeper slopes, which have some isolated patches of native vegetation, almost the entire site has been disturbed by agricultural operations which have occurred for more than 50 years. A single family home is located in the central portion of the site.

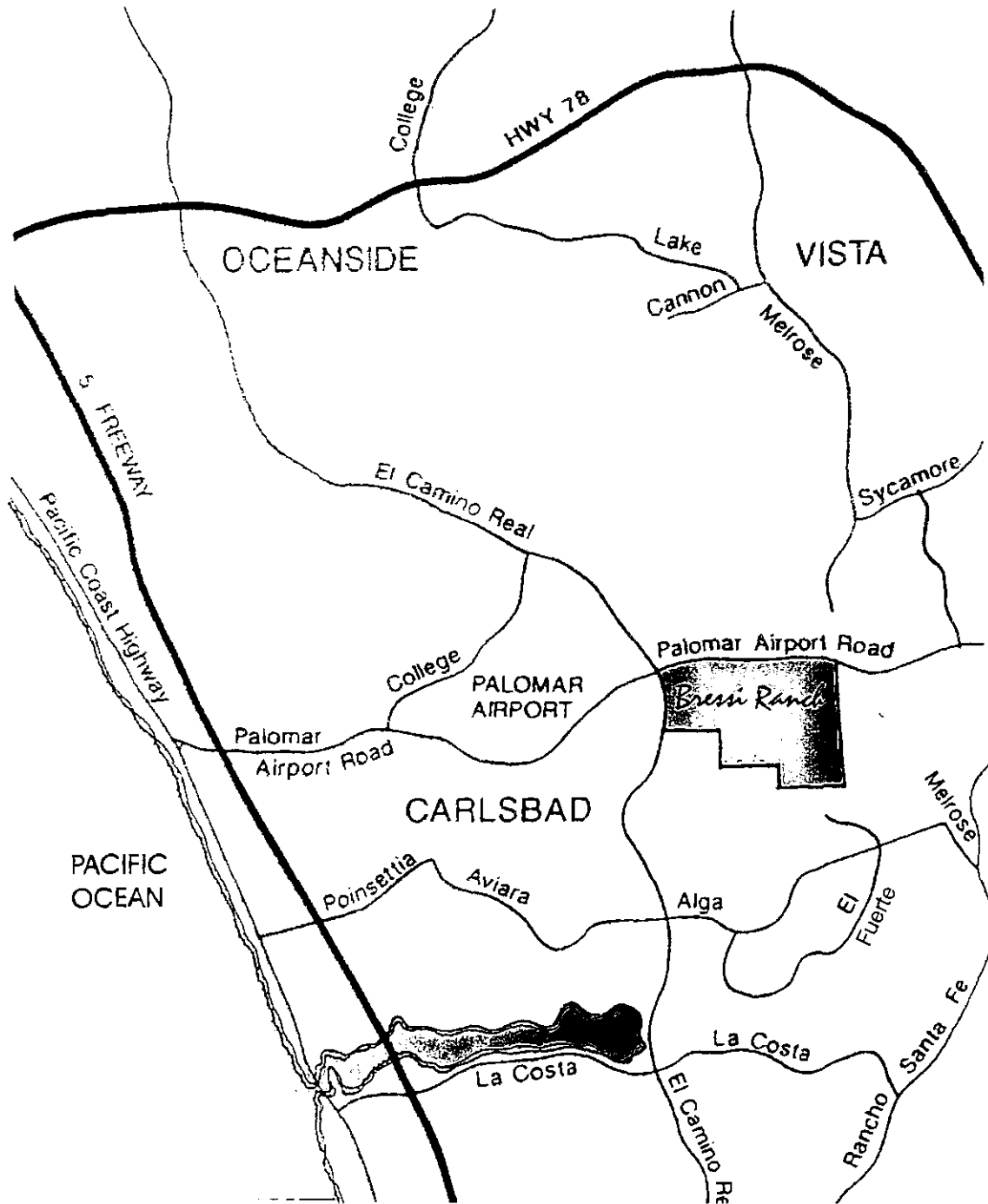
The northern and western portions of the site fall within the McClellan-Palomar Airport Influence Area as shown by Exhibit I-4 on page I-6. The westernmost portion of the site is within the Flight Activity Zone of Palomar Airport. Land uses within the Master Plan have been proposed to reflect the constraints of the Palomar Airport Influence Area.

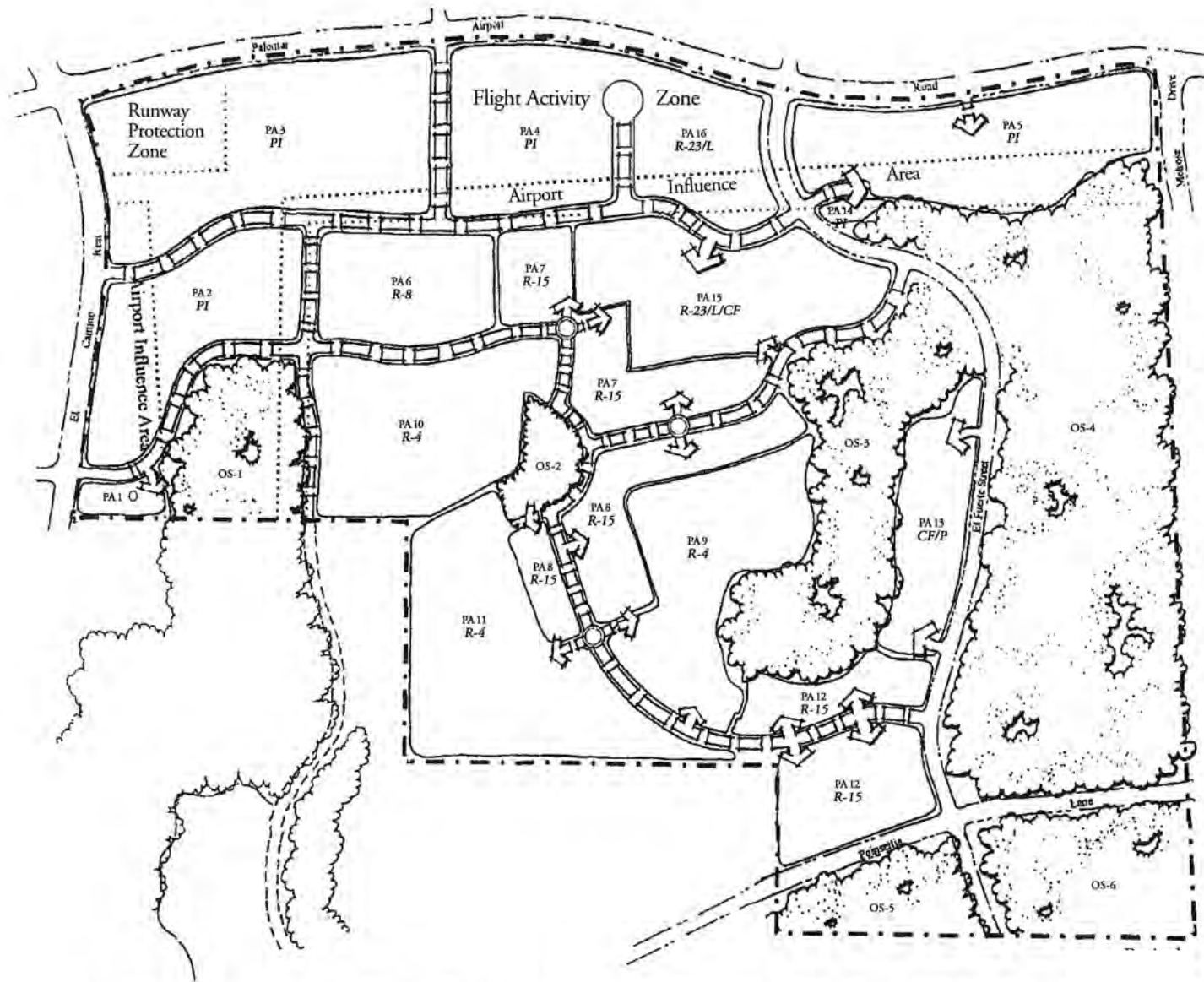
The boundaries of the Bressi Ranch Master Plan and Local Facilities Management Zone 17 are coterminous. A Local Facilities Management Plan will be adopted concurrently with this Master Plan.

## **C. History of Bressi Ranch**

Bressi Ranch was a part of the original Rancho Agua Hedionda Land Grant of the early 1800's. In 1896, the Kelly family subdivided the Rancho Agua Hedionda into 12 parcels. Bressi Ranch is made up of portions of Parcels A, B, and G of this original subdivision.

Since the early 1940's, Bressi Ranch has been used for agricultural purposes when wheat was first planted on the upland portions of the site. Up until 1961, when the first public water district was formed, the property was planted with dry-farm crops such as grains and beans. Sufficient water for irrigation allowed for more intensive agriculture and the property was planted with a variety of truck





Bressi Ranch

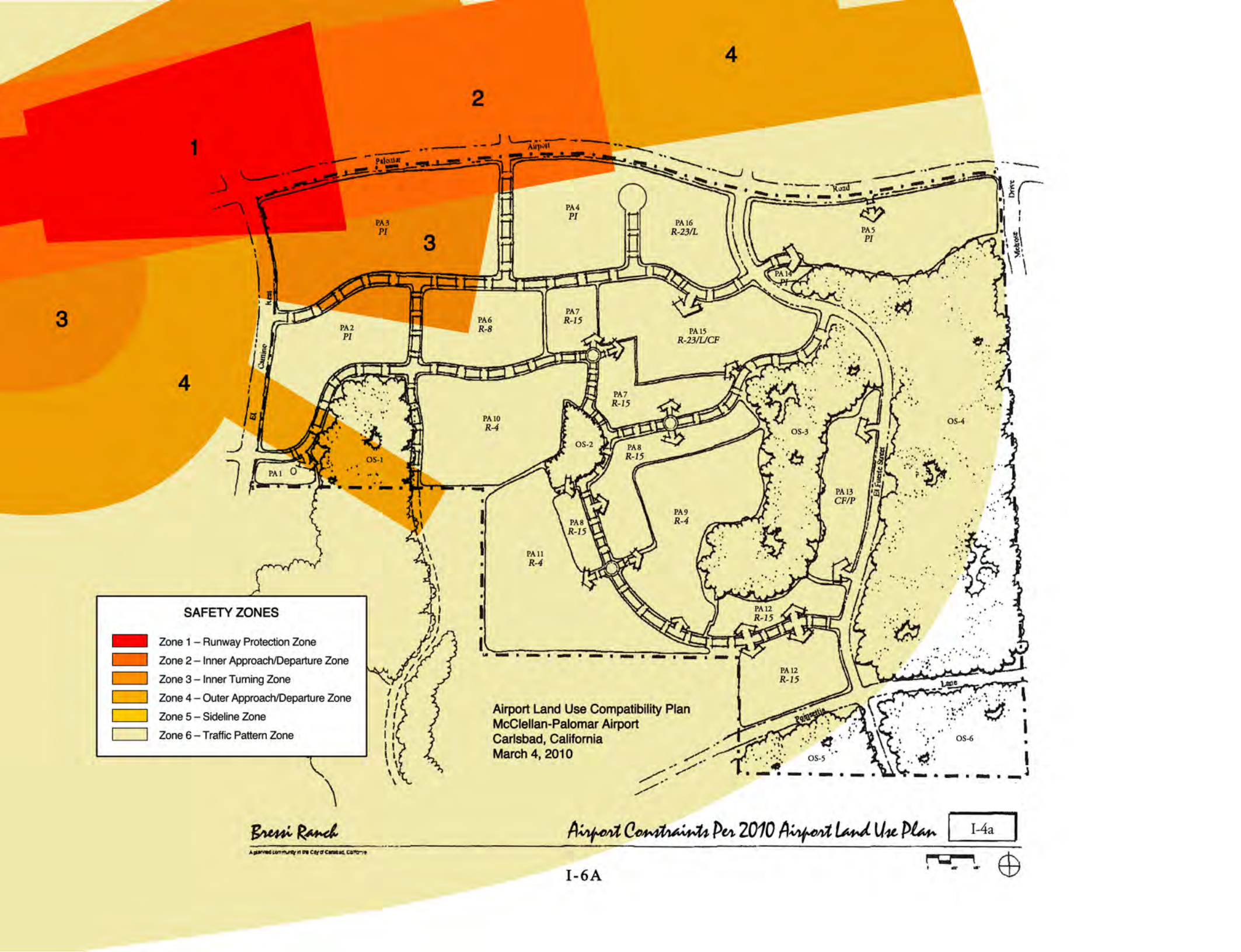
Airport Constraints Per 1994 Airport Land Use Compatibility Report

I-4

A MAPNED LANDMAP BY THE CITY OF CARLSBAD, CALIFORNIA







**SAFETY ZONES**

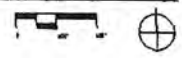
- Zone 1 – Runway Protection Zone
- Zone 2 – Inner Approach/Departure Zone
- Zone 3 – Inner Turning Zone
- Zone 4 – Outer Approach/Departure Zone
- Zone 5 – Sideline Zone
- Zone 6 – Traffic Pattern Zone

Airport Land Use Compatibility Plan  
 McClellan-Palomar Airport  
 Carlsbad, California  
 March 4, 2010

*Bressi Ranch*  
 A planned community in the City of Carlsbad, California

*Airport Constraints Per 2010 Airport Land Use Plan* I-4a

I-6A



crops including tomatoes, beans, squash and chili peppers. Bressi Ranch is currently being dry farmed.

#### **D. Project Description**

The Bressi Ranch Master Plan covers the entire 585 acre Bressi Ranch, which consists of two parcels. The northern parcel is 149.0 acres in size and is within the McClellan-Palomar Airport Influence area. This parcel will be divided into five planning areas to be developed with a combination of up to 1,818,807 square feet of office/industrial/warehouse uses. The southern parcel is 436.1 acres in size and will be divided into ten planning areas and six open space areas. Exhibit I-1 on page I-2 shows the layout of these planning areas and open space areas.

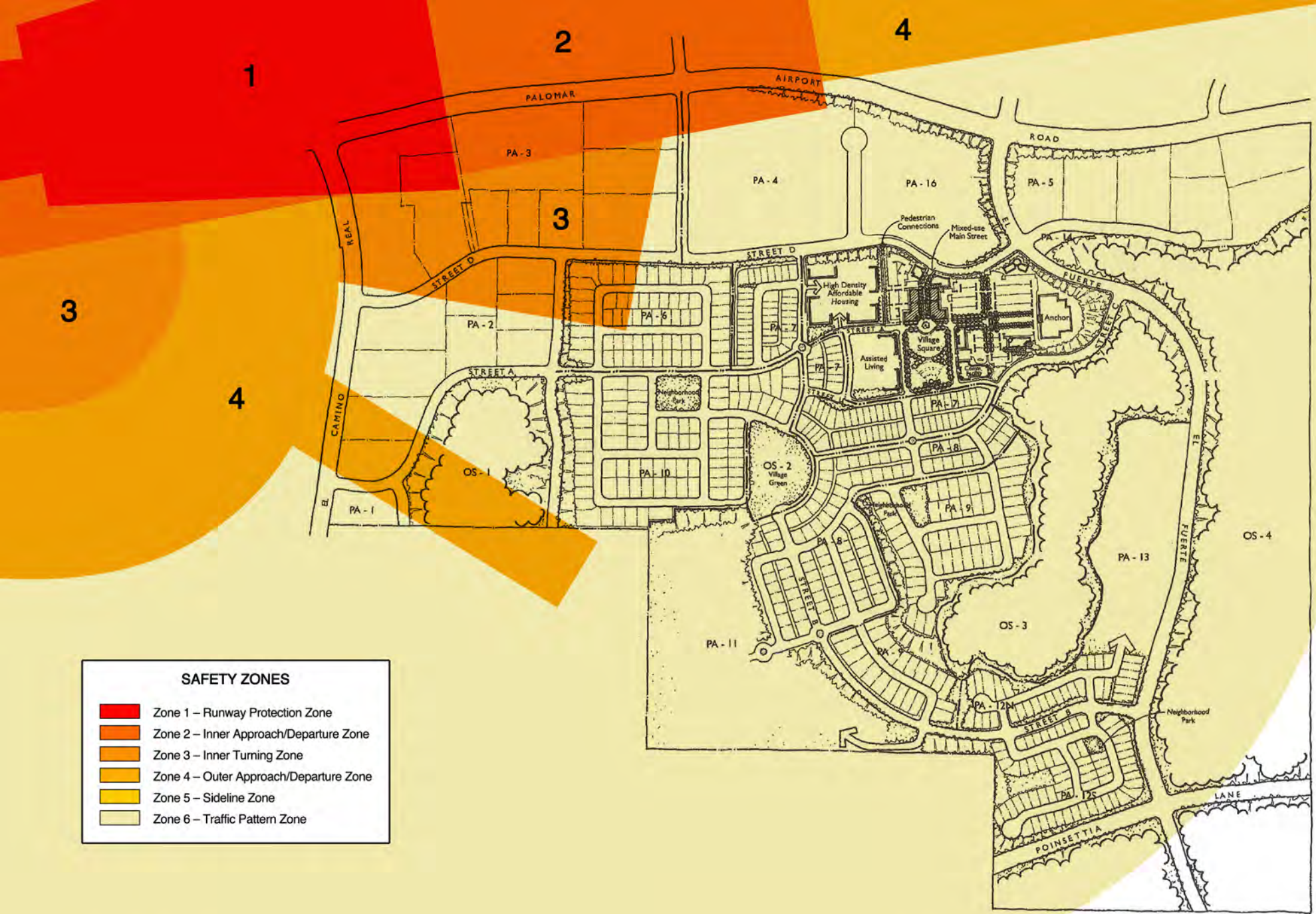
Seven of the planning areas will be developed with a wide range of single family detached homes on lots ranging in size from approximately 3,500 square feet to over 14,000 square feet in size. One hundred units of affordable multifamily apartments will be provided in Planning Area 15. Up to 20 market rate units may be located in Planning Area 15 if the total number of units approved for development in Planning Areas 6 through 12 is less than 523. Planning Area 16 proposes a maximum of 125 townhome units. There will be a maximum of 749 dwelling units in the Bressi Ranch Master Plan at buildout. Exhibit I-5 on page I-9 shows a preliminary schematic site plan of Bressi Ranch.






Planning Area 14 may be developed with a small commercial use such as a gas station/mini-mart to serve the needs of the non-residential portion of the Master Plan as well as passing motorists on El Fuerte, subject to approval of a Conditional Use Permit. Planning Area 15 will be developed as a mixed-use area with a combination of residential, commercial and community facilities to serve the needs of the residents of the Bressi Ranch Master Plan as well as the non-residential uses in the northern portion of the Master Plan. A combination of community facilities, recreational vehicle storage and possibly a private school will be developed in Planning Area 13.

Five of the six open space areas will become part of habitat lands to provide an important link in Carlsbad's Habitat Management Plan. Open Space Area 2, with its central location, will be developed as the Village Green, which will become the heart of the residential portion of the Master Plan. A combination of active and passive recreation facilities will be provided in Open Space Area 2.

A primary goal of the Bressi Ranch Master Plan is to create a pedestrian oriented community where the use of the automobile can be minimized. The Master Plan has a strong mixed-use component that includes residential, commercial and office/industrial uses in close proximity. A substantial portion of





SAFETY ZONES	
	Zone 1 – Runway Protection Zone
	Zone 2 – Inner Approach/Departure Zone
	Zone 3 – Inner Turning Zone
	Zone 4 – Outer Approach/Departure Zone
	Zone 5 – Sideline Zone
	Zone 6 – Traffic Pattern Zone



the residents, including the affordable housing, will be within walking distance of jobs in the office/industrial portion of the Master Plan. In addition, the majority of the residents of the Master Plan will be within walking distance of the Village Center in Planning Area 15, commercial uses in Planning Area 16, or the Village Green in Open Space Area 2. Both of these areas will provide a variety of opportunities for recreation and socializing. Exhibits I-6, I-7 and I-8 on pages I-11 through I-13 show the pedestrian orientation of the Master Plan.

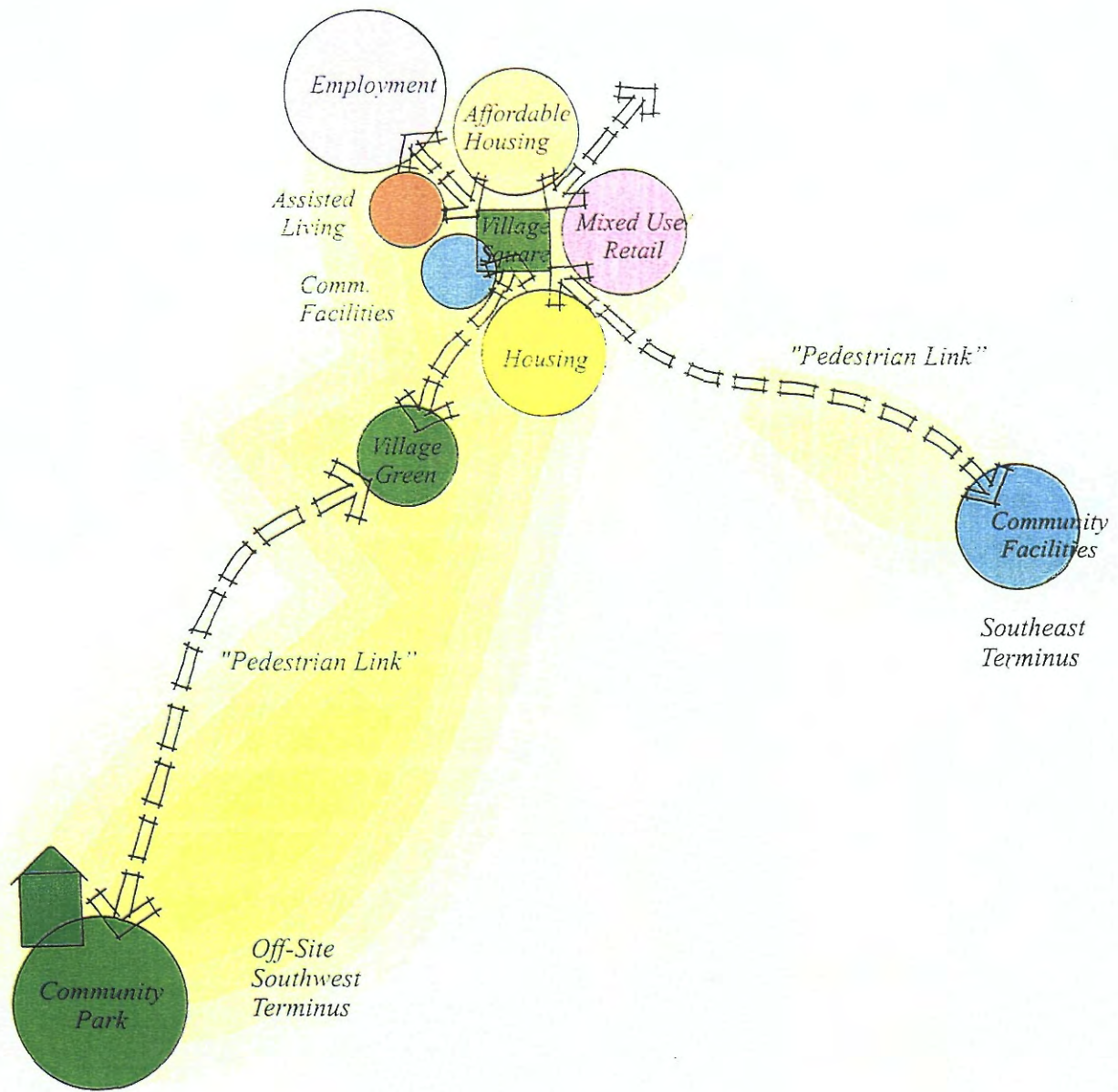
All of the local and collector residential streets will have non-contiguous sidewalks with trees adjacent to the street to encourage pedestrian traffic. Traffic calming techniques and reduced width streets, planters, enhanced pavement and special geometrics will be used to slow the flow of traffic and further encourage pedestrian activity. This goal, as well as the other goals of the Master Plan and the design criteria utilized to accomplish these goals, is discussed in detail in the following chapters of the Master Plan.

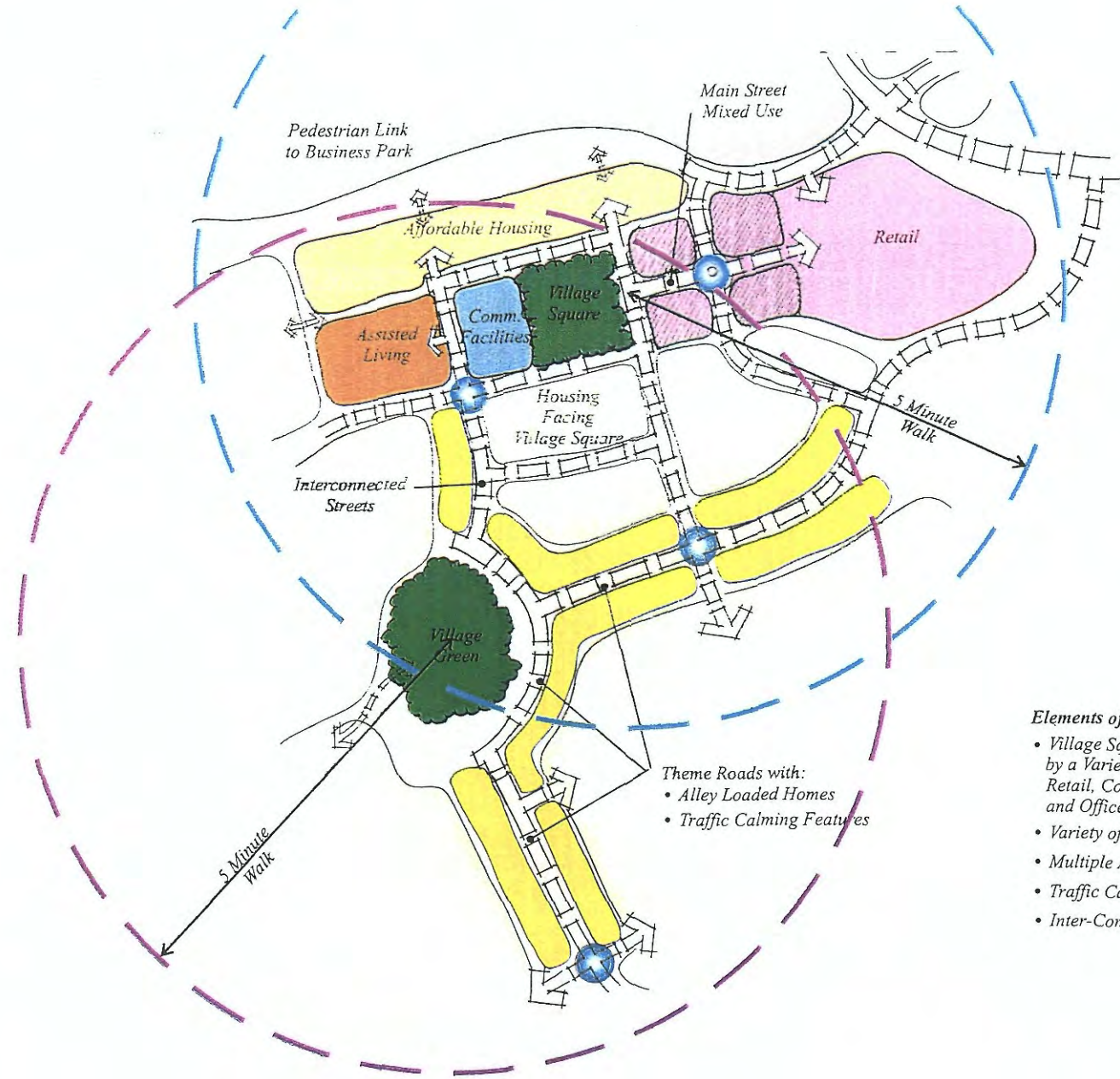
#### **E. Legal Description**

The legal description for the Bressi Ranch Master Plan is as follows:

Parcels "A" and "B" of Boundary Adjustment 543, Document No. 1999-0085753 filed in the Office of San Diego County Recorder on February 11, 1999.

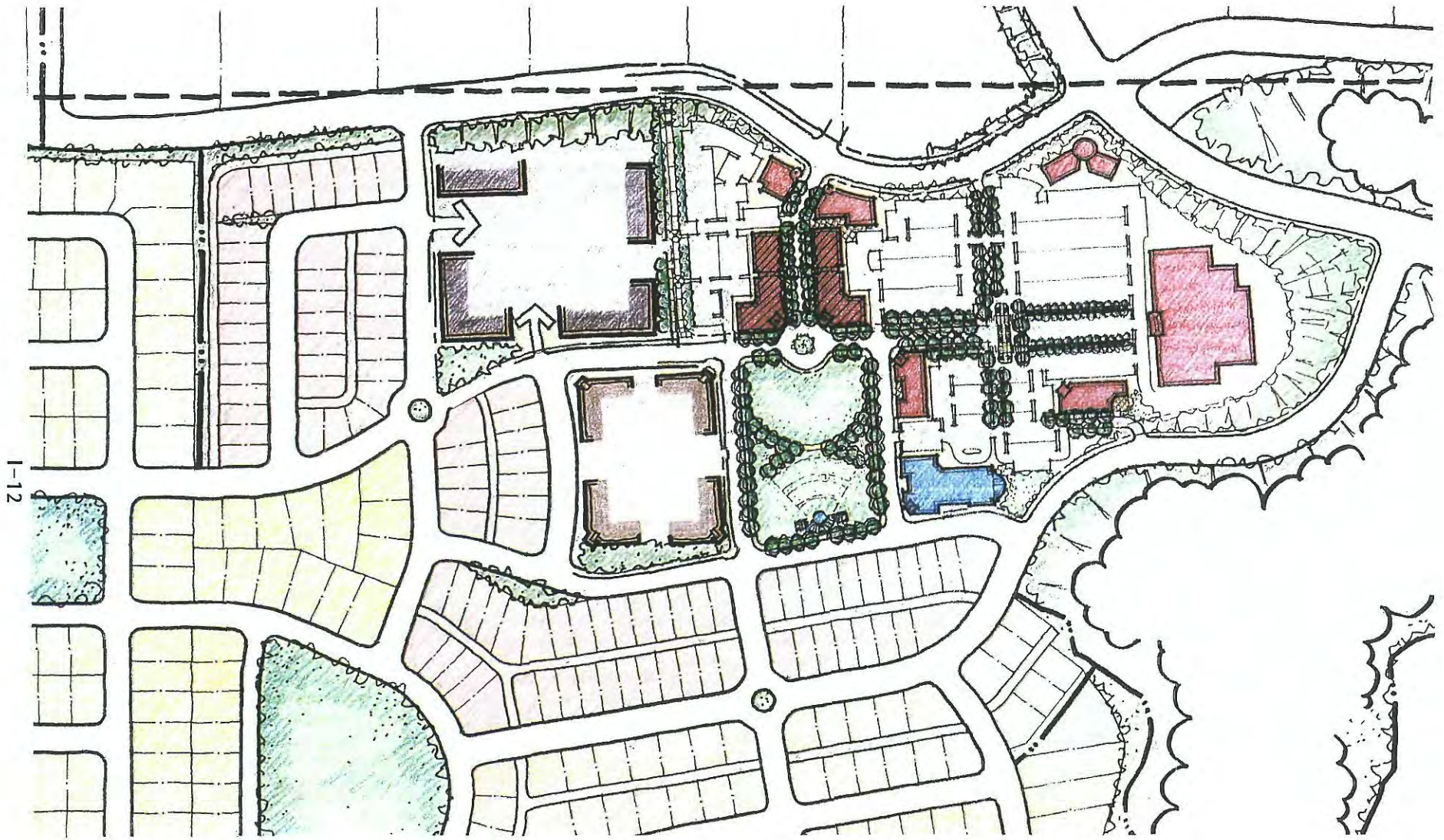






- Elements of the Plan**
- Village Square Activated by a Variety of Housing, Retail, Community Facility, and Office Space.
  - Variety of Land Uses
  - Multiple Access Points
  - Traffic Calming
  - Inter-Connected Streets





# Bressi Ranch

A planned community in the City of Carlsbad, California



# Conceptual Mixed-Use Site Plan

I-8



## **F. Applications**

### 1. Applications Processed with the Master Plan

The following applications were processed concurrently with this master plan:

General Plan Amendment – The General Plan Amendment revised land use designations within the Master Plan area. The existing General Plan designations did not accurately reflect existing topography or constraints based on up-to-date surveys and current development standards. The proposed revision resulted in a variety of residential and non-residential land uses including large natural open space areas that more accurately reflect the natural constraints of the site (see Exhibit I-9 on page I-15).

Zone Change – The zone change brought existing zoning into conformance with General Plan designations of the Master Plan (see Exhibit I-9 on page I-15). The Limited Control (L-C) zoning was replaced with Planned Community (P-C) Zoning. Since the Planned Community Zone does not have any development standards, such as setbacks or building heights, an underlying zoning of RD-M, R-1, P-M, O-S, C-2 and CF has been provided to implement the proposed General Plan designations.

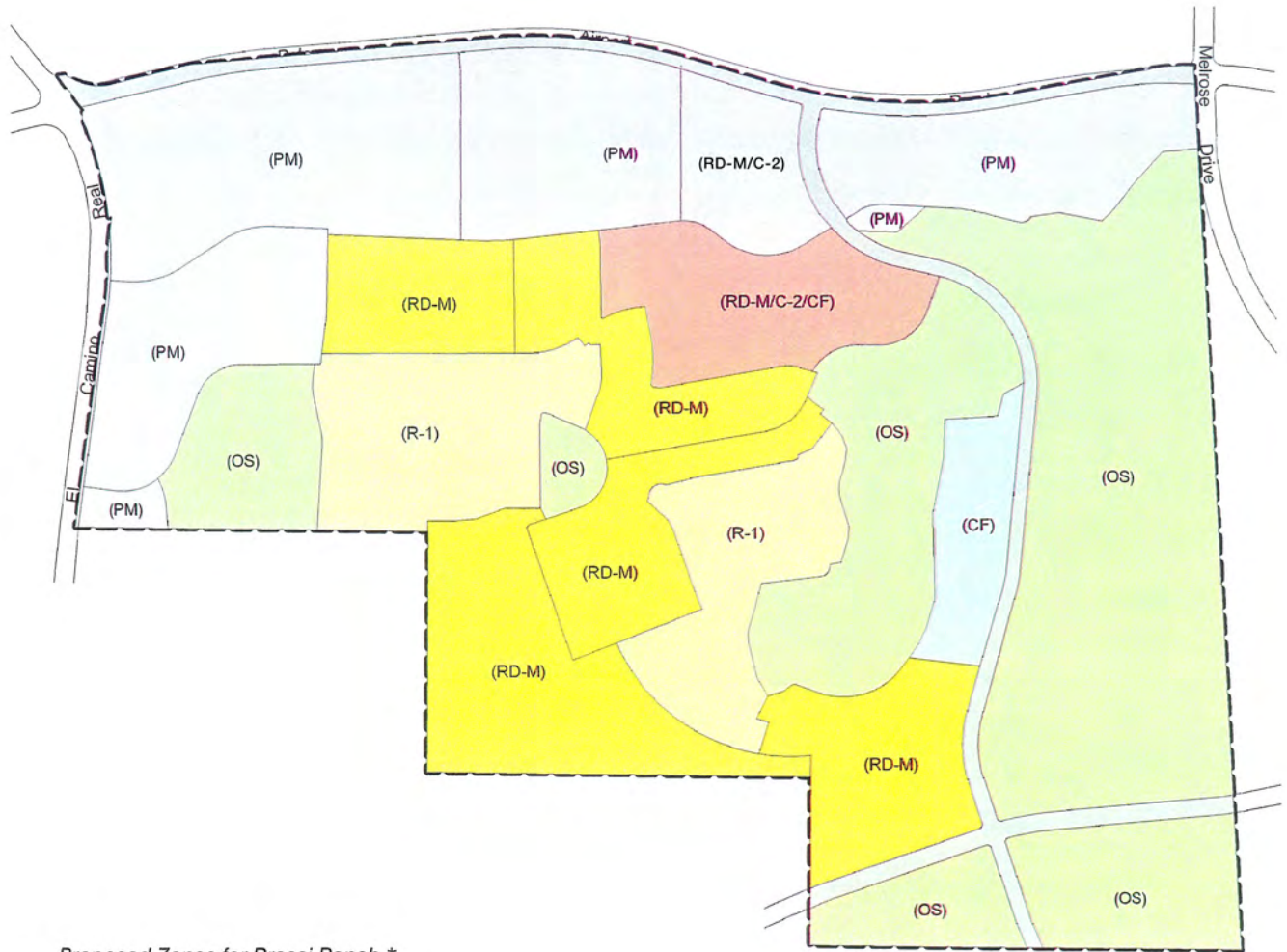
Certification of a Program Environmental Impact Report – The Program Environmental Impact Report constituted all environmental review required for approval of this Master Plan and all related discretionary approvals.

Hillside Development Permit – A Hillside Development Permit was required since the project has a slope gradient of 15% or greater and a slope height of 15 feet or greater. The approval of a Hillside Development Permit ensures that the proposed grading is in conformance with the requirements of Carlsbad's Hillside Development Ordinance.

Tentative Maps – A Master Tentative Map was processed concurrently with the Master Plan. The lots created by this map correspond to the boundaries of the Planning and Open Space Areas and establish the alignments of the major roadways within the Master Plan.

Special Use Permits – A Special Use Permit was required for grading and development of the portions of the Master Plan within the El Camino Real Scenic Corridor and within the 100-year floodplain.





**Proposed Zones for Bressi Ranch \***

- R-1 Residential Density - Single Family
- RD-M Residential Density - Multiple Zone
- C-2 Community Commercial Zone
- CF Community Facilities
- P-M Planned Industrial Zone
- O-S Open Space Zone

\* Development within Bressi Ranch will additionally be subject to the Bressi Ranch Master Plan.

Local Facilities Management Plan (LFMP) for Zone 17 – An LFMP for Zone 17 was prepared in conformance with the requirements of Chapter 21.90, Growth Management, of the Carlsbad Municipal Code. The Zone 17 LFMP demonstrates how and approximately when each public facility will be developed within Zone 17 to accommodate the Master Plan development. It also provides a financing plan for the necessary public facilities.

## 2. Future Discretionary Applications

The following applications will be processed at some future date to allow for the development of the Master Plan:

Tentative Tract Maps – Tentative Tract Maps will be required to subdivide the residential planning areas to create individual lots or ownership units. Tentative maps may also be submitted to further subdivide the commercial and industrial portions of the Master Plan at the option of the developer.

Planned Development Permits – These permits will be processed pursuant to Chapter 21.45 of the Carlsbad Municipal Code and must be submitted with any tentative map that creates lots less than 7,500 square feet in size, ownership multifamily units, or neighborhoods served by private streets. A Non-Residential Planned Development Permit pursuant to Chapter 21.47 must be submitted with any non-residential tentative map that creates industrial lots less than one acre in size, or lots that do not have frontage on a public street or have shared access or parking.

Conditional Use Permits – Pursuant to Chapter 21.42, Conditional Use Permits must be submitted for the development of a church, other community facility, private school, assisted care facility, service station or other uses allowed by Chapter 21.42 of the Carlsbad Municipal Code.

Site Development Plan – Site Development Plans will be required to establish the design of the mixed-use areas, for the development of affordable apartments, commercial areas, a daycare facility and the Village Green. In addition, pursuant to Chapter 21.34, a Site Development Plan must be processed for development in Planning Areas 1, 2, 3, 4, 5, 14 and 16.

## **II. GOALS OF THE BRESSI RANCH MASTER PLAN**

The Bressi Ranch Master Plan incorporates many “Smart Growth” techniques to create a livable community, not just a successful project. It is difficult to provide a short definition of what “Smart Growth” growth is, but its end result is to create viable long-term communities where people can live, work and play. The implementing techniques of “Smart Growth” can best be summed up by the Ahwahnee Principles.

As much as possible the Bressi Ranch Master Plan has been designed to create a community generally in keeping with guidelines established by the Ahwahnee Principles. The Ahwahnee Principles are a set of fundamental planning principles dedicated to building resource-efficient communities that serve the needs of those who live and work within them. One of the main goals of the Ahwahnee Principles is locating all things required to meet the daily needs of residents within walking distance of one another. A sense of community, the importance of open space and resource-efficient land use planning are also addressed by the Ahwahnee Principles. A number of factors such as topography, the City’s Habitat Management Plan, existing City plans for public facilities and the proximity of Palomar Airport have an impact on how closely development within Bressi Ranch can comply with the Ahwahnee Principles. However, the Bressi Ranch Master Plan has incorporated as many of the Ahwahnee Principles as is feasible.

The following presents the Ahwahnee Principles at the community level and indicates how the Master Plan endeavors to comply with these principles.

### **1. All planning should be in the form of complete and integrated communities containing housing, shops, work places, schools, parks and civic facilities essential to the daily life of the residents.**

The Master Plan has been designed so that facilities essential to the daily life of the residents are part of an integrated community. Housing, employment, commercial development, the Village Green and community facilities have all been located within the Master Plan to serve the needs of the residents.

The boundaries of the Palomar Airport Influence Area have played a significant role in the design of the Master Plan. The portion of the Master Plan within this area has been designated for development with office, warehouse or industrial uses. However, the airport influence area boundaries were altered in 2010, allowing for additional commercial and residential uses in certain portions of the Palomar Airport Influence Area. The land outside the boundaries of the airport influence area has been designated for residential, commercial or open space uses. This has created a true mixed use project that incorporates the five basic land uses allowed in the City of Carlsbad - industrial, commercial, residential, community facilities and open space.

As mentioned previously, certain uses complying with the Ahwahnee Principles cannot occur on this site due to constraints created by topography or existing city plans and programs. Based on input from the Carlsbad Unified School District, there will be no public school in Bressi Ranch due to its proximity to Palomar Airport. However, a public school will be located within reasonable walking distance of Bressi Ranch in the adjacent Villages of La Costa Master Plan. In addition, the City already has plans for a Community Park at Alga Norte to the south of Bressi Ranch. Although the park will be located outside the boundary of the Master Plan, this park will be within walking distance to many of the residents of the Master Plan. The City also does not have any plans for public civic facilities within or adjacent to Bressi Ranch.

**2. Community size should be designed so that housing, jobs, daily needs and other activities are within easy walking distance of each other.**

Housing, shops, jobs, parks and community facilities have been located within easy walking distance of each other. A mixed-use area has been strategically located south of the industrial area and north of the residential area to be within easy walking distance for both employees and residents of Bressi Ranch. In addition, the majority of the Bressi Ranch residents live within a quarter of a mile from job opportunities in the mixed use and industrial areas.

The Master Plan design provides an integrated system of sidewalks and trails accompanying the internal circulation network, allowing convenient pedestrian access to all portions of the Master Plan and to El Camino Real, Palomar Airport Road, Poinsettia Lane and El Fuerte Street. Special attention has been paid to the street and walkway design to encourage pedestrian use of these internal circulation thoroughfares. The proposed community pedestrian system will link several residential planning areas to each other while providing links to open space areas, the industrial portion of the Master Plan area, the Carlsbad citywide trail system, the Village Center and commercial and community facilities sites.

**3. As many activities as possible should be located within easy walking distance of transit stops.**

The industrial and mixed-use portions of the Bressi Ranch Master Plan will be located adjacent to transit stops (i.e. bus stops) along Palomar Airport Road, Poinsettia Lane, El Fuerte Street and El Camino Real. Based on input from North County Transit District (NCTD), a bus transfer point will be provided adjacent to Planning Area 1 or 2, or as modified by input from NCTD. The Village Center, which includes the highest density residential area, commercial and community facilities will be located near bus stops on El Fuerte.

- 4. A community should contain a diversity of housing types to enable citizens from a wide range of economic levels and age groups to live within its boundaries.**

The Bressi Ranch Master Plan will provide a wide variety of housing types serving all income levels from market rate and affordable multi-family units to small lot single family detached, standard lot homes and estate lots.

- 5. Businesses within the community should provide a range of job types for the community's residents.**

Within the planned industrial portion of the Master Plan area, manufacturing, warehouse, research and development and office uses will provide a wide variety of industrial employment opportunities for the residents of the Bressi Ranch Master Plan. Additional job opportunities will be provided in the Village Center south of the industrial portion of the Master Plan.

- 6. The location and character of the community should be consistent with a larger transit network.**

The Bressi Ranch Master Plan is adjacent to prime regional transportation corridors including Palomar Airport Road and El Camino Real. The Master Plan is designed to provide easy access to these prime regional transportation corridors as well as the major and secondary city transportation corridors of Poinsettia Lane and El Fuerte Street. Based on input from the North County Transit District (NCTD) the Master Plan has been designed to incorporate NCTD's Transit-Oriented Design Principles as contained in Appendix C.

- 7. The community should have a center focus that combines commercial, civic, cultural and recreational uses.**

The Master Plan will have one major focal point, and one secondary focal point. The major focal point of the Master Plan will be the Village Center (Planning Area 15), with the Village Square and mixed-use street. Strong pedestrian ties will link this area to the additional commercial opportunities in Planning Area 16, and recreational opportunities in the secondary focal point located at the Village Green (Open Space Area 2). Special attention has been paid to the design of this area and its variety of uses to create a sense of place for the Bressi Ranch community. People of all ages and backgrounds will be able to enjoy the commercial uses, community facilities, housing opportunities and green space offered within this area.

Attractive street sections and pedestrian pathways will connect the Village Center to the Village Green in Open Space Area 2. Special treatment along the parkways leading to this secondary focal point will create a visual and physical

sense of community. The adjacent residential areas will be designed to encourage people to take advantage of this attractive link to the Village Green.

- 8. The community should contain an ample supply of specialized open space in the form of squares, greens and parks whose frequent use is encouraged through placement and design.**

The Village Center will be the focal point of the project. This area will contain a centrally located Village Square where residents, office workers and shoppers can gather. Use of this green space is encouraged by the proximity of community facilities, multi-family housing and commercial uses within the Village Center.

The Village Green, centrally located within the residential portion of the Master Plan, also provides a major open space area for the Master Plan. This communal space, with its visual anchor and gathering places, will serve as a centralized gathering place for the residents of the Master Plan. Park-like walkways will link the Village Green with the adjacent residential areas. In addition, special overlooks, vista points or pocket parks may be located throughout the community.

- 9. Public spaces should be designed to encourage the attention and presence of people.**

The Village Center will offer a variety of uses to attract and encourage the presence of people. By locating this area between the industrial and residential portions of the Master Plan, it will be utilized by people during both daytime and evening hours. All residents within the community will be able to conveniently walk to shopping and recreational opportunities within this area.

Strong pedestrian linkages will connect the Village Center to the recreational uses in the Village Green, and to the additional commercial uses proposed for Planning Area 16. Non-contiguous sidewalks with attractive landscaping will encourage residents to stroll between the Village Center, Village Green, and Planning Area 16. Wherever feasible homes will be designed with front porches that face onto the Village Green and Village Square.

- 10. Each community or cluster of communities should have a well-defined edge, such as agricultural greenbelts or wildlife corridors, permanently protected from development.**

Large contiguous areas of open space being preserved as valuable habitat will form a distinct boundary around the eastern and portions of the southern and western edges of the residential part of the Master Plan.

Roads, slopes and landscape buffers will be provided between the industrial and residential areas of the Master Plan. These buffers will provide a defined and aesthetically pleasing edge, separating the residential areas from the non-residential portion of the Master Plan while helping to create a sense of community.

- 11. Streets, pedestrian paths and bike paths should contribute to a system of fully connected and interesting routes to all destinations. Their design should encourage pedestrian and bicycle use by being small and spatially defined by buildings, trees and lighting; and by discouraging high speed traffic.**

Other than Circulation Element roadways along the perimeter of the project, roads may have reduced right-of-way, special landscaping, and non-contiguous sidewalks to calm traffic and encourage pedestrian activity throughout the Master Plan area. Landscaped bulb outs and canopy trees are proposed along some key residential streets.

The system of sidewalks will be designed to tie into the Master Plan's community trail system. In turn, the community trail system will be linked into the trails of the Citywide Trails System along the southern and eastern edges of the Master Plan.

- 12. Wherever possible, the natural terrain, drainage, and vegetation of the communities should be preserved with superior examples contained within parks or greenbelts.**

The Master Plan has been designed to preserve large contiguous areas of open space containing native vegetation. These open space areas contain good examples of existing native habitat. Previously disturbed habitat adjacent to these areas may be revegetated to increase the amount of native habitat in accordance with the goals of Carlsbad's proposed Habitat Management Plan. The open space areas being preserved and maintained by the Bressi Ranch Master Plan will provide some of the vital linkages required by the City's Habitat Management Plan.

- 13. The community design should help conserve resources and minimize waste.**

Drought tolerant landscaping will be utilized as much as possible in accordance with the recommendations of Carlsbad's Landscape Manual. Whenever feasible, play equipment, outside furniture, etc. should be constructed from recycled materials.

### **III. LAND USE AND ENTITLEMENT PROVISIONS**

#### **A. General Provisions**

##### **1. Authority Granted by Approval of the Master Plan**

Approval of this Master Plan establishes the zoning for the entire property known as Bressi Ranch; setting forth permitted uses and regulatory criteria and guidelines. Approval of this plan does not vest any rights for future approvals, discretionary acts or other entitlements necessary for future development within the Bressi Ranch Master Plan.

No person shall use or develop any property covered by this Master Plan in a manner that is contrary to the provisions contained herein as established by the City Council. Developments within the Master Plan area shall be subject to all terms and conditions of the Bressi Ranch Master Plan.

##### **2. Entitlements**

The Master Plan sets standards for development; however, it does not provide a guarantee of approval for future discretionary projects within its boundaries. Specific development plans shall be evaluated in accordance with the provisions of the Master Plan as well as with municipal ordinances and policies in force at the time said plans are before the Planning Commission and/or the City Council for approval. Unless specifically addressed, the Master Plan requires conformance with all otherwise applicable City development standards and requirements. Where a conflict in development standards occurs, the standards of the Master Plan shall take precedence.

##### **3. Development Consistency**

All new development proposed for the Bressi Ranch Master Plan area shall be consistent with the policies and regulations of this Master Plan. The zoning set forth by Exhibit I-9 on page I-15 constitutes the zoning within the Master Plan area. All zone changes, site development plans, tentative subdivision maps and other discretionary projects within the planning areas or open space areas shall be consistent with this Master Plan. All public works and capital improvements shall also be consistent with the Master Plan. Adherence with these policies and standards will ensure the systematic implementation of the General Plan.



4. Severability

In the event that a California Court or Federal Court of competent jurisdiction holds any regulation, condition, program, or portion of this Master Plan invalid or unconstitutional, such provisions and the invalidity of such provisions shall not affect the validity of the remaining provisions.

5. Maximum Number of Units

The original base number of units within the Master Plan was determined primarily by the City of Carlsbad Growth Management Ordinance. The existing Master Plan area's net developable acreage was multiplied by the corresponding growth control point based on the original land use designation of RLM. Using this methodology, a total of 498 base dwelling units was originally anticipated within the Bressi Ranch Master Plan area prior to preparation of the Master Plan.

The original maximum number of dwelling units for the Master Plan was determined by the application of California Government Code Section 65915 (State Density Bonus Ordinance), which permits a minimum additional 25% of the base dwelling units. In return for the density bonus, 20% of the base dwelling units ( $20\% \times 498 = 100$ ) must be made affordable to lower income households. Through the use of the State ordinance, an additional 125 ( $25\% \times 498$ ) dwelling units were allocated to the Master Plan for a maximum total of 623 – 638 units when the Master Plan was first approved in 2002.

The Master Plan identifies an expected number of dwelling units for each planning area. Planning Areas 6-12 are exclusively residential and are anticipated to have 523 - 538 dwelling units. Planning Area 15 identifies 100 units as part of a mixed land use. Master Plan 178(I) revised the land use designation of 18 acres at the southwest corner of Palomar Airport Road and El Fuerte Street identified as PA-16 from Industrial to Residential 23 units per acre and Local Commercial (R-23/L) with a maximum number of 125 dwelling units. The actual number of units for any planning area may increase or decrease through the detailed review process of each subsequent development proposal. However, in no case will the maximum number of 749 dwelling units for the entire Master Plan area be exceeded.

6. Boundaries

The planning area boundaries shown in this Master Plan's small-scale exhibits are intended to show the general boundaries of each Planning Area. The large scale Master Tentative Map more clearly defines the boundaries of each Planning Area. As detailed grading plans and future tentative maps are prepared to subdivide the individual Planning Areas, slight modifications to

the exact location of these Planning Area boundaries shown on the Master Tentative Map may occur. As long as these revisions do not result in any additional density or intensity of development being allowed beyond that shown in the Master Plan they may be approved in accordance with the City's policy regarding substantial conformance for tentative maps.

7. Recordation

Notice of approval of this Master Plan for property within its boundaries shall be recorded with the County of San Diego Recorder's Office.

8. Growth Management

The applicant shall comply with all provisions of the Carlsbad Municipal Code, Section 21.90 (Growth Management Program). The applicant, by pulling building permits pursuant to the Master Plan and the Local Facilities Management Plan (LFMP) for Zone 17, agrees that all of the dedications and other requirements imposed as a condition of the Master Plan for the Zone 17 LFMP are reasonably necessary to serve the needs of the development for which the building permits are required.

9. Dedications

All land and/or easements required by this Master Plan for public streets, open space, recreational purposes and public utility purposes shall be granted to the City of Carlsbad or other public agency without cost to the City and free of all monetary liens and monetary encumbrances as conditioned by the appropriate discretionary approval.

10. Public Facilities

To ensure that all development areas of the Master Plan shall be adequately served, the developers of the Master Plan or portions thereof shall be required to provide for their share of the construction or funding of all necessary public facilities pursuant to the approved Zone 17 Local Facilities Management Plan.

11. Zoning

This Master Plan shall constitute the zoning for all lands within the Master Plan. No person shall use or develop contrary to the provisions of the Master Plan any land located within the boundaries defined by this Master Plan. All provisions of the Master Plan are imposed as a condition of zoning. Approval of this document does not excuse compliance with all other applicable City ordinances in effect at the time building permits are issued. An underlying zone for each planning area is established by Exhibit I-9 on page I-15. The

development standards of the indicated underlying zone shall apply to each planning area, except where modified by this Master Plan.

## 12. Location of Improvements

Streets, utilities, and other land use related improvements are shown at their approximate locations on the Master Plan exhibits. Precise locations of these improvements will be established through the approval of tentative maps and site development plans.

## 13. Community Associations

### a. Residential Areas & Open Space

- i. Planning Areas 6-12 and Open Space Areas 1-6 – The developer shall establish a Master Homeowners Association and corresponding covenants, conditions and restrictions (CC&Rs). Said CC&Rs shall be submitted to and approved by the Planning Director prior to Final Map approval. Prior to the first certificate of occupancy, the State will require establishment of the Master Homeowners Association.

All residentially developed properties within the Master Plan boundaries will establish sub-area Homeowner Associations and will be members of the Master Homeowners Association. The responsibilities of this Master Homeowners Association shall be as stated in the subsequent paragraphs of this chapter. Except as provided under subsection 13.a.ii below, no other residential sub-area homeowners associations shall be formed.

- ii. Planning Areas 15 & 16 (Residential) – If the affordable housing project to be located in Planning Areas 15 & 16 are built as a condominium project, then the developer shall establish a sub-area residential homeowners association and corresponding CC&Rs. Said CC&Rs shall be submitted to and approved by the City Planner prior to final Map approval. Prior to the first certificate of occupancy, the State will require establishment of the sub-area homeowners association. The sub-area homeowners association shall be responsible for the maintenance of common facilities only within the limits of this residential condominium project.

### b. Non-Residential Areas

- i. Planning Areas 1-5 and 14 – The developer shall establish a Master Community Association and corresponding covenants, conditions and restrictions (CC&Rs). Said CC&Rs shall be

submitted to and approved by the City Planner prior to the recordation of the first final map within any of these Planning Areas.

This Master Community Association shall be responsible for the maintenance of all common facilities within the boundaries of these Planning Areas. This association shall be wholly independent of any other community or homeowner association within the Master Plan boundaries.

- ii. Planning Areas 15 & 16 – The developer(s) of Planning Areas 15 & 16 shall establish a Master Community Association and corresponding covenants, conditions and restrictions (CC&Rs). Said CC&Rs shall be submitted to and approved by the City Planner prior to the recordation of the first final map. This Master Community Association shall be responsible for the maintenance of all common facilities within the boundaries of this Planning Area. This association shall be independent of any other community or homeowner association within the Master Plan boundaries.

c. General Provision Applicable to all Associations

- i. The developer of any of the aforementioned planning areas shall provide the Planning Division with a recorded copy of the official CC&Rs that have been approved by the Department of Real Estate prior to the issuance the first Certificate of Occupancy within the affected planning area.
- ii. No revisions that weaken or diminish the rights of the City shall be made to any CC&Rs without the City's prior written consent. At a minimum, the CC&Rs shall contain the applicable provisions worded as follows:
  - (a) General Enforcement by the City – The City shall have the right, but not the obligation, to enforce those Protective Covenants set forth in this Declaration in favor of, or in which the City has an interest.
  - (b) Notice and Amendment – A copy of any proposed amendment shall be provided to the City in advance. If the proposed amendment affects the City, the City shall have the right to disapprove. A copy of the final approved amendment shall be transmitted to City within 30 days for the official record.

- (c) Failure of Association to Maintain Common Area Lots and Easements – In the event that the Association fails to maintain the Common Area Lots and/or the Association’s easements, parkways and trails as provided for in the CC&Rs and this Master Plan, the City shall have the right, but not the duty, to perform the necessary maintenance. If the City elects to perform such maintenance, the City shall give written notice to the Association, with a copy thereof to the Owners in the Project, setting forth with particularity the maintenance which the City finds to be required and requesting the same be carried out by the Association within a period of thirty (30) days from the giving of such notice. In the event that the Association fails to carry out such maintenance of the Common Area lots and/or Association’s easements, parkways and trails within the period specified by the City’s notice, the City shall be entitled to cause such work to be completed and shall be entitled to reimbursement with respect thereto from the owners as provided herein.
- (d) Special Assessments Levied by the City – In the event the City has performed the necessary maintenance to either Common Area Lots, corner sight distance areas and/or Association’s Easements, the City shall submit a written invoice to the Association for all costs incurred by the City to perform such maintenance of the Common Area Lots and/or Association’s Easements. The City shall provide a copy of such invoice to each owner in the project, together with a statement that if the Association fails to pay such invoice in full within the time specified, the City will pursue collection against the owners in the Project pursuant to the provisions of the Section. Said invoice shall be due and payable by the Association within twenty (20) days of receipt by the Association. If the Association shall fail to pay such invoice in full within the period specified, payment shall be deemed delinquent and shall be subject to a late charge in an amount equal to six percent (6%) of the amount of the outstanding balance due. Thereafter the City may pursue collection from the Association by means of any remedies available at law or in equity. Without limiting the generality of the foregoing, in addition to all other rights and remedies available to the City, the City may levy a special assessment against the owners of each Lot in the Project for an equal prorata share of the invoice, plus the late charge. Such special assessment shall constitute a charge on the land and shall be a continuing lien upon each Lot against which the special assessment is levied. Each owner in the Project hereby vests the City with the right and power to levy such special assessment, to impose a lien upon their respective Lot and to bring

all legal actions and/or to pursue lien foreclosure procedures against any owner and his/her respective Lot for purposes of collecting such special assessment in accordance with the procedures set forth in the CC&Rs.

- (e) Landscape Maintenance Responsibilities – The community association and individual lot or unit owner landscape maintenance responsibilities shall be as set forth in exhibits \_\_\_\_\_.
- (f) Balconies, Patio Covers, Trellises and Decks – The individual lot or unit owner allowances and prohibitions regarding balconies, patio covers, trellises and decks shall be as set forth in exhibits \_\_\_\_\_.
- (g) Outdoor storage or outdoor parking of recreational vehicles within any of the planning areas except in approved RV storage areas is prohibited.

In addition, the Master Homeowners Association shall control the operation and maintenance of the recreational vehicle storage area, entry features, common slopes, recreation areas and other common features of the residential and open space portions of the project identified in the Master Plan. The Master Homeowners Association or other acceptable entity shall also be responsible for the maintenance of a parkway and trail system, provided, however, all public open space and trails shall be maintained by the City Open Space Maintenance District (or similar district) if such a district is formed by the City.

Common area slopes within individual residential planning areas and triangular sight distance corridors at intersections shall be maintained by the Master Homeowners Association. The developer shall submit a master maintenance plan showing all areas to be maintained by the Master Homeowners Association to be approved by the City Planner prior to sub-area final map approval of the affected planning area.

The CC&Rs shall control the use of parking areas and shall include the appropriate Community Association authority for enforcing parking regulation. That Community Association shall have the authority to cause removal of illegally parked vehicles.

#### 14. Architectural Review Board

Prior to the occupancy of the first unit within any residential planning area, the applicant shall establish an Architectural Review Board. The Architectural

Review Board shall be responsible for insuring compliance with architectural standards promulgated by the Community Association. The approval of the Architectural Review Board shall occur prior to the submittal of an application for building permits.

#### 15. Exterior Home Improvements

All proposed room additions, patio covers, patios, balconies, decks, trellises and other residential construction requiring a building permit from the City of Carlsbad shall be reviewed and approved by the Architectural Review Board. All future room additions by homeowners shall comply with the standards contained in this section. This shall be clearly stated in the Master CC&R's for this project and the architectural review manual(s) prepared for the Bressi Ranch Master Plan Architectural Review Board(s). This review shall ensure that they comply with the setback and lot coverage requirements established by the residential design guidelines as contained in Chapter X starting on page X-1. The above-mentioned information shall be clearly stated in the CC&Rs for this project.

#### 16. Recreational Vehicle Storage

The Bressi Ranch Master Plan recreational vehicle storage area is located in the northern portion of Planning Area 13. The RV storage shall be designed so that it is screened from other uses in Planning Area 13 and traffic on El Fuerte Street. Twenty square feet of storage exclusive of aisles and driveways shall be provided for each privately owned unit in the Bressi Ranch Master Plan. This RV storage area shall be operated and maintained by the Bressi Ranch Master Community Association exclusively for their use. At the option of the Master Community Association, a private operator may maintain this area.

Until the permanent RV storage area is constructed, a temporary RV storage area may be provided in another portion of the Master Plan subject to the City Planner's approval, or each residential area may provide its own temporary storage area pursuant to the standards of the Planned Development Ordinance and subject to the City Planner's approval.

### **B. General Plan**

The General Plan designations of the Bressi Ranch Master Plan are shown on Exhibit I-9 on page I-15. These designations are:

- PI - Planned Industrial
- O - Office and Related Commercial

- L - Local Shopping Center
- R-4 - Residential Low-Medium – single-family
- R-8 - Residential Medium – single-family or attached multi-family
- R-15 - Residential Medium High – small lot single-family or attached multi-family
- R-23 - Residential High – attached multi-family
- OS - Open Space
- CF - Community Facilities
- P - Private School

**C. Zoning**

Concurrent with the approval of the Bressi Ranch Master Plan, the zoning of this site was changed from Limited Control (L-C) to Planned Community (P-C). Since the P-C Zone requires the designation of underlying zoning, each of the planning areas and open space areas has been assigned an underlying zoning designation. Pursuant to the powers of Chapter 21.38 of the Carlsbad Municipal Code (Planned Community Zone), this Master Plan shall constitute the zoning of all lands within the Master Plan. No person shall use or develop contrary to the provisions of the Master Plan. All provisions of the Master Plan are imposed as a condition of zoning. Approval of this document does not excuse compliance with all other applicable City ordinances in effect at the time building permits are issued. Unless modified by this Master Plan, or approval of a Planned Development Permit or Non-Residential Planned Development Permit, all development shall comply with the requirements of the underlying zoning as shown by Exhibit I-9 on page I-15. These underlying zoning designations are provided below:

<u>Planning Areas</u>	<u>Zone</u>
1	O (Office)
2, 3, 4, 5, 14	P-M (Planned Industrial)
6, 7, 8, 12	RD-M (Residential Density - Multiple)
9, 10, 11	R-1 (One Family Residential)
13	CF (Community Facilities)
15	C-2/RD-M/CF (General Commercial/Residential Density Multiple/ Community Facilities)



16 R-23/L (R-23 Residential/Local Commercial)

Open Space Areas

1-6 O-S (Open Space)

**D. Relationship to General Plan & Other Applicable Planning Documents**

The Bressi Ranch Master Plan is consistent with and implements the Carlsbad General Plan, the Planned Community Zone, the McClellan-Palomar Airport Land Use Compatibility Plan, the Carlsbad Habitat Management Plan, the El Camino Real Scenic Corridor, the Carlsbad Landscape Manual and all other applicable planning and regulatory documents. Appendices A and B provide a consistency matrix showing how the Bressi Ranch Master Plan conforms with the Carlsbad General Plan and the other above-mentioned documents, respectively.

**E. Land Uses**

There are six major General Plan land use types within the Bressi Ranch Master Plan. The land uses are office, industrial, community facility, commercial, residential and open space. Exhibit I-1 on page I-2 shows how the land uses have been divided into sixteen planning areas and six open space areas. Exhibit III-1 on page III-11 provides the acreage and buildout potential for each planning area and open space area.

Exhibit III-1

BRESSI RANCH

GENERAL PLAN STATISTICS BY PLANNING AREA\*

Proposed General Plan

Planning Area/ Neighborhood	General Plan Designation	Zoning	Gross Acres	Constraints		Partially Constrained		Net Developable Acres	Growth Control Dwelling Units per Acre	General Plan Growth Control Dwelling Units	SF
				D	E & F	J	J/2				
PA 1	O	O	2.5	0.0	0.0	-	-	2.5			**
PA 2	PI	P-M	27.5	1.7	2.0	-	-	23.8			
PA 3	PI	P-M	47.0	0.0	0.0	-	-	47.0			
PA 4	PI	P-M	44.0	0.0	0.0	-	-	44.0			
PA 5	PI	P-M	28.0	0.3	0.0	-	-	27.7			
PA 6	R-8	RD-M	16.2	1.0	0.7	6.0	3.0	11.5	6.0	69	
PA 7	R-15	RD-M	20.0	0.2	0.6	1.8	0.9	18.3	11.5	210	
PA 8	R-15	RD-M	19.9	0.1	0.0	2.4	1.2	18.6	11.5	213	
PA 9	R-4	R-1	26.8	0.9	0.0	3.6	1.8	24.1	3.2	77	
PA 10	R-4	R-1	31.5	1.1	0.5	6.0	3.0	26.9	3.2	86	
PA 11	R-4	R-1	36.4	4.6	2.4	12.3	6.2	23.2	3.2	74	
PA 12	R-4	RD-M	24.1	1.0	0.0	2.2	1.7	21.4	11.5	246	
PA 13	CF/P	CF	13.7	0.0	0.1	-	-	13.6			138,000
<b>SUBTOTAL</b>										975	
PA 14	PI	P-M	1.1	0.0	0.0	-	-	1.1			
PA 15	R-23/L/CF	RD-M/ C-L/ CF	27.7	0.4	0.1	1.0	0.5	26.7	19	507	
PA 16	R-23/L	P-M	17.7	0.0	0.0	-	-	17.7	19	337	
OS 1	OS	OS	16.9	0.4	3.6	-	-	12.9			
OS 2	OS	OS	4.0	0.2	0.5	-	-	3.3			
OS 3	OS	OS	39.5	5.6	0.0	-	-	33.9			
OS 4	OS	OS	98.9	12.9	0.0	-	-	86.0			
OS 5	OS	OS	11.0	0.1	0.8	-	-	10.1			
OS 6	OS	OS	20.4	0.5	5.7	-	-	14.2			
Roads	Road		28.0	0.4	0.2	-	-	27.4			
<b>Total</b>			<b>585.1</b>							<b>1819</b>	

\* Acres are subject to engineering boundary verification.

\*\* Total of 2,160,500 square feet, which equates to 15,000 ADT based on an assumption of 40% Light Industrial/40% Research and Development and 20% Office. This breakdown of uses was used for traffic modeling purposes. The percentages of the various uses may vary at the time of development, as long as the traffic generated by the uses in Planning Areas 1, 2, 3, 4 and 5 do not exceed 15,000 ADT.

\*\*\* Mixed use development – Max. 130,000 sf. of commercial, church, boys and girls club, day care, assisted living, and high density residential.

\*\*\*\* Although the net acreages of the residential planning area would allow for the development of up to 4482 1819 dwelling units, the Master Plan will establish a maximum cap of 623 749 dwelling units.

Definitions: D = Slopes > 40%, E and F = Wetlands, including Tamarisk Scrub, and J = Slopes of 25% to 40%

1. Planned Industrial (PI)

Due to their geographical locations, Planning Areas 1, 2, 3, 4, 5 and 14 have a designation of Planned Industrial (P-I). These planning areas comprise part of the central industrial core of the City of Carlsbad, are bordered by regional arterials, and are located in close proximity to Palomar Airport and lie within the McClellan-Palomar Airport Influence Area. Planning Areas 1, 2, 3, 4 and 5 will be developed with a combination of corporate office, light manufacturing and research and development uses, and Planning Area 14 may be developed with an industrial serving commercial use such as a service station/convenience store/car wash. The uses within these planning areas will be developed in accordance with the recommendations of SANDAG's Land Use Compatibility Plan for McClellan-Palomar Airport and the County's Airport Master Plan.

Portions of Planning Areas 2, 3, 4 and 5 are within the Flight Activity Zone of McClellan-Palomar Airport. A small part of Planning Area 3, near the intersection of Palomar Airport Road and El Camino Real, is also within the Runway Protection Zone. Development within these areas will be limited in scope in accordance with the recommendations of SANDAG's Land Use Compatibility Plan for McClellan-Palomar Airport. Exhibit I-4 on page I-6 shows the boundaries of the Airport Influence Area, Flight Activity Zone and Runway Protection Zone.

2. Office (O)

Planning Area 1 is designated to be developed with medical offices to serve the community as well as the residents of Bressi Ranch Master Plan Area. The development of medical offices at this site would be compatible with the proposed medical office development to the south of Planning Area 1. Planning Area 1 is located within the McClellan-Palomar Airport Influence Zone; however, the subject site is not located within the Flight Activity Zone and is wholly outside the 65 dBN CNEL noise contour. Proposed development applications within Planning Area 1 will be required to process a Special Use Permit to demonstrate compliance with the El Camino Real Scenic Corridor Guidelines.

3. Community Facilities (CF)

Planning Areas 13 and 15 will be developed with community facilities to serve the residents of the Bressi Ranch Master Plan. In accordance with the requirements of Chapter 21.25 of the Carlsbad Municipal Code, a minimum of 5.09 acres of community facilities, including a 2-acre day care center, must be located in Planning Area 13 and/or 15 of the Bressi Ranch Master Plan. A portion of the community facilities, which may include a day care center, will

be provided in Planning Area 15 in or near the Village Square. The remainder of the community facilities will be provided in Planning Area 13.

Recreational vehicle storage, for residents of the Bressi Ranch Master Plan only, will be provided on roughly one acre in the northernmost portion of Planning Area 13. Subject to approval of a Conditional Use Permit; a private school may be located in a portion of Planning Area 13. Community facility requirements may be satisfied through shared facility use of private school facilities if required minimum acreage and typical hours of availability are met, subject to review and approval of a Conditional Use Permit by the Planning Commission. If a school is not developed on a portion of Planning Area 13, the entire site may be developed with community facility uses as permitted by Chapter 21.25 of the Carlsbad Municipal Code.

4. Mixed Use (R-23/L/CF and R-23/L)

Planning Area 15 is located west of El Fuerte Street and south of Planning Area 4. This site's combination designation of R-23/L/CF will allow it to be developed as a village center with a mixture of land uses in close proximity. Commercial uses, including small shops along a mixed use street, high density affordable apartments, and a senior assisted care facility may be located in close proximity to a Village Square, which may contain a community facility. The site's location between the residential and industrial portions of the Master Plan allows for residents and employees to enjoy the commercial and social activities it provides.

Planning Area 16 is located west of El Fuerte Street and east of Planning Area 4. This site will also contain a combination zoning designation of RD-M/C-L, which will allow for the development of both residential and commercial uses. A market rate, residential development is proposed for the western portion of the site, while commercial uses, including an extension of the main street along Finilla Place, will be located on the eastern portion of the site.

5. Residential (R-4, R-8, R-15)

The central portion of the Bressi Ranch Master Plan has been divided into seven residential planning areas, which will be developed with a wide variety of single-family detached homes on lots ranging in size from 3,500 square feet to over 14,000 square feet in size. Exhibit I-1 on page I-2 shows the location of the various development types. Exhibit III-1 on page III-11 shows the maximum number of potential units that have been assigned to each residential planning area. The exact number of units developed in each planning area will be determined at the time of tentative map approval.

The location of the residential portion of the Master Plan will allow for the development of a community based on the Ahwahnee Principals. Most residences will be within a 5-minute walk of either jobs, the Village Center, the Village Green or community facilities. Residents will be close to major arterial roadways, yet no arterial roads will go through the residential portion of the site. The low volume roadways within the residential portion of the Master Plan will be designed with traffic calming devices to slow traffic and encourage pedestrian activity.

One hundred affordable apartments will be developed in Planning Area 15 adjacent to the Village Square. The site was chosen for the affordable units for a number of reasons, such as its close proximity to jobs, public transit stops, store and social/recreational opportunities located in this portion of the Master Plan.

All discretionary permits for residential development in the Master Plan will be conditioned to enter into an Affordable Housing Agreement pursuant to Section 21.38.120 of the Carlsbad Municipal Code. This agreement shall be approved by the City prior to recordation of the first residential final map or issuance of residential building permits, whichever occurs first. All relevant terms and conditions of any Affordable Housing Agreement shall be filed and recorded as a deed restriction on those individual lots or units of a project, which are designated for the location of Affordable Units.

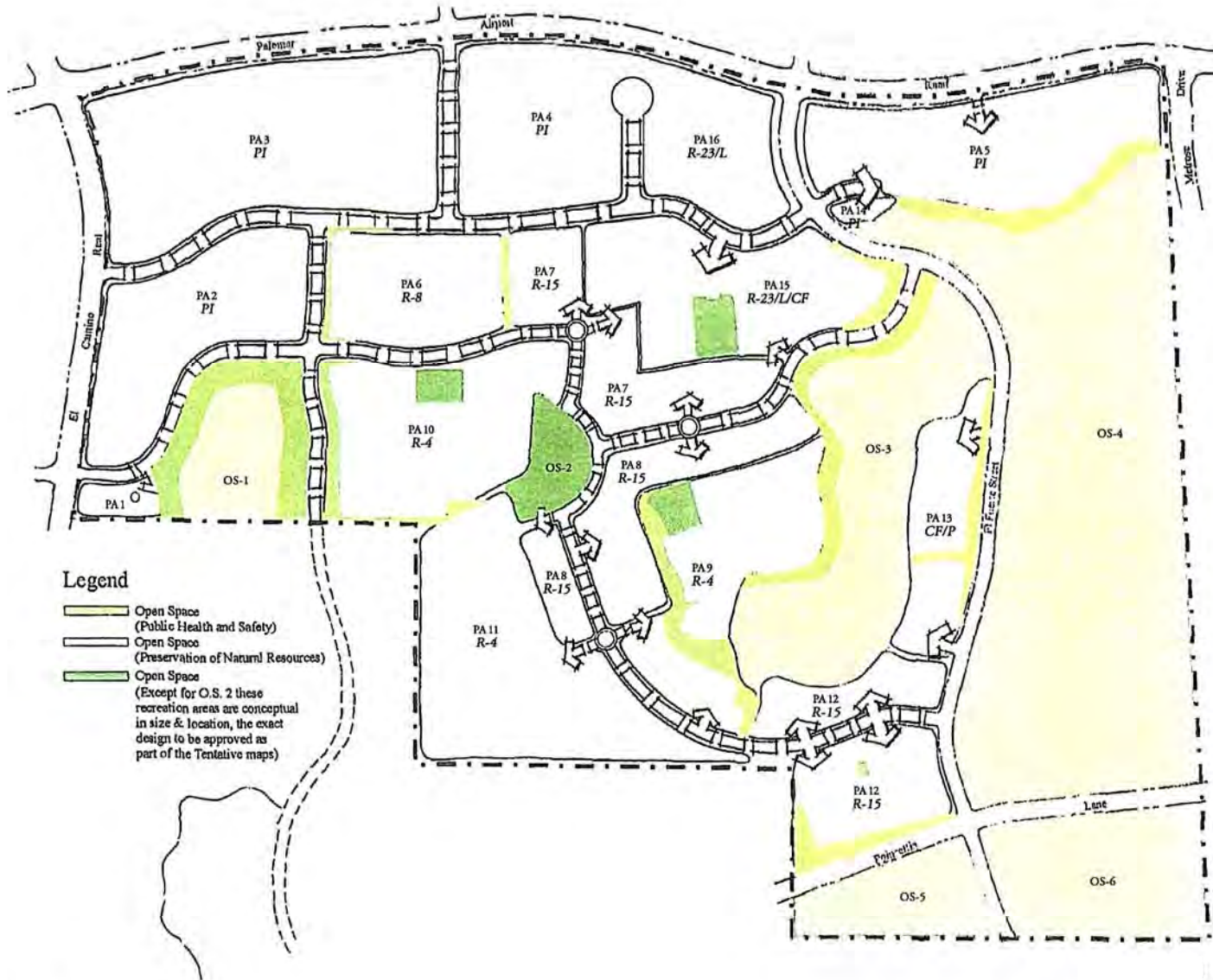
#### 6. Open Space (OS)

The Bressi Ranch Master Plan will provide approximately 191 acres or 33% of the site as natural and improved open space. Additional open space will be provided within the individual neighborhoods for recreational and landscape purposes. Open space within the Master Plan has been divided into six open space areas. Open Space Areas 1 and 3-6 will consist of native habitat. Open Space Area 2 will be developed as the Village Green (see Exhibit III-2 on page III-16).

The majority of the open space within the Master Plan is being preserved in accordance with the goals of Carlsbad's Habitat Management Plan (HMP). Natural open space within the Bressi Ranch Master Plan will provide an important link between native open space being preserved in the Rancho Carrillo Master Plan to the east and the Villages of La Costa Master Plan to the south. A more detailed discussion of open space is provided in Chapter XI.

The proposed trail system is consistent with the Citywide Trail Plan and Carlsbad's Open Space Element. The trails located in the southern and eastern portion of the Master Plan have been designed to link up with the trail

system that was approved as part of the Rancho Carrillo Master Plan. This will provide a convenient link between the Bressi Ranch Master Plan and the Carrillo Ranch Community Park.



Bressi Ranch

A proposed community in the City of Calabasas, CA, 91301

Master Plan Open Space

III-2



## **IV. STREETS**

This chapter provides the Bressi Ranch development standards and design guidelines for streets. The standards and design criteria in this chapter will establish the character and ambiance of the Bressi Ranch Master Plan, which will differentiate it from other development in Carlsbad. As discussed in Chapter V, Bressi Ranch will follow a garden district theme. Standards and guidelines that deal with residential design, office/industrial design and commercial/community facilities design are discussed in Chapters VII, VIII, IX and X of this Master Plan.

### **A. Introduction**

One of the major goals of the Bressi Ranch Master Plan is to create a pedestrian oriented community. No high speed, high volume traffic will go through the residential portion of the Master Plan; only low volume local traffic. The Bressi Ranch Master Plan does not have any Circulation Element Roadways going through its residential areas. Palomar Airport Road and El Camino Real, Prime Arterial roadways, are located along the outer edges of the Master Plan adjacent to the non-residential uses. El Fuerte Street, a Secondary Arterial, and Poinsettia Lane, a Major Arterial, are the only circulation element roadways that go through the Master Plan, and they only abut one residential planning area, Planning Area 12. They do not run through any residential areas or the Village Center. In addition, the internal street system of the Bressi Ranch Master Plan has been designed so that it will not serve as a convenient short cut between Circulation Element Roadways.

The internal roadways will be designed to create a sense of community. Since El Fuerte Street, south of Palomar Airport Road, will be one of the major entries to the Bressi Ranch Master Plan, non-contiguous sidewalks will be provided as an introduction to pedestrian oriented roadways. The sidewalks will be noncontiguous or meandering along El Fuerte Street between Palomar Airport Road and Street D. When this occurs a public access easement will be provided over the portions of the sidewalk that are outside of the right-of-way.

Per the City's Livable Streets Standards, all of the local and connector streets in the residential portion of the Master Plan will have non-contiguous sidewalks, which will allow trees to be planted between the sidewalk and the curb. When the landscape matures this will result in tree-lined streets similar to those that exist in some of the older portions of Carlsbad without the amount of noticeable disturbance to sidewalks created by an uncontrolled root system.

Trees that are planted adjacent to and in the public right-of-way will be carefully selected to ensure that they do not have an invasive root system. In addition, a chemically treated 18" bio-barrier and root guards constructed from a plastic



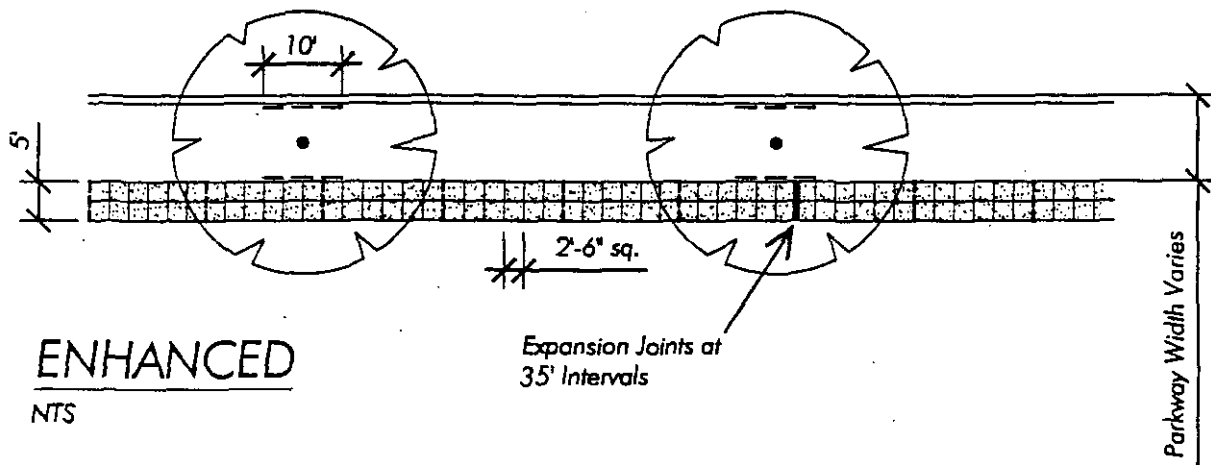
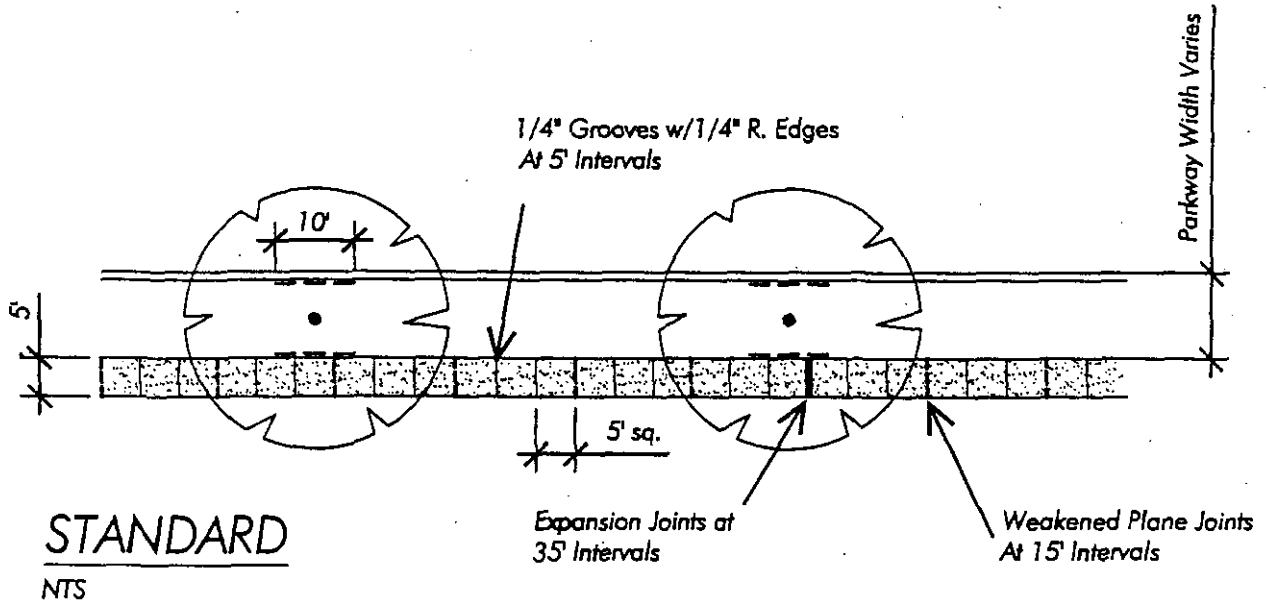
sheet will be put in place to further reduce damage to the adjacent sidewalks, curbs, streets or utilities. To ensure a common level of maintenance and to relieve the City from the responsibility of maintaining the trees that are planted within the right-of-way, the Community Association, Landscape and Lighting District or private property owner will maintain all landscaping within this area. At the option of the developer, subject to the approval of the City Engineer, in some areas scored sidewalk may be used instead of the standard sidewalk to create a more aesthetically pleasing atmosphere, as shown by Exhibit IV-1 on page IV-3.

## **B. Street Cross Sections**

Except as noted in this section of the Master Plan, all streets will be constructed in accordance with the City of Carlsbad's public street standards. No exhibits are provided for Palomar Airport Road, El Camino Real, Poinsettia Lane or Gateway Road in the industrial portion of the Master Plan since they will be constructed in accordance with the City's public street standard cross sections. The location map and street cross sections only show where the Master Plan proposes street cross sections that vary from the City's public street standards. These cross sections show the minimum curb-to-curb width for the proposed streets. In some areas the width may be slightly wider for line of sight or other safety concerns. Exhibit IV-2 on page IV-4 shows the location of the street cross sections discussed in this section of the Master Plan.

### Cross Section 1: El Fuerte Street between Palomar Airport Road & Gateway Road

This portion of El Fuerte Street will be the first entrance into the Bressi Ranch Master Plan. To create a sense of entry, enhanced landscaping and non-contiguous sidewalks will occur on both sides of this secondary arterial. A public access agreement and maintenance agreement shall be required for any portion of a public sidewalk that may meander outside of the right-of-way. In addition, this portion of El Fuerte Street to the industrial road (Gateway Road) will be constructed with an 84' curb-to-curb width and a 104' right-of-way, which is wider than required by City standards for a secondary arterial, to accommodate dual left turn lanes. This extra width only occurs along the portions of El Fuerte where dual left turn lanes are required. All other portions of El Fuerte will have a typical 64' curb-to-curb width (see Exhibit IV-3 on page IV-5).

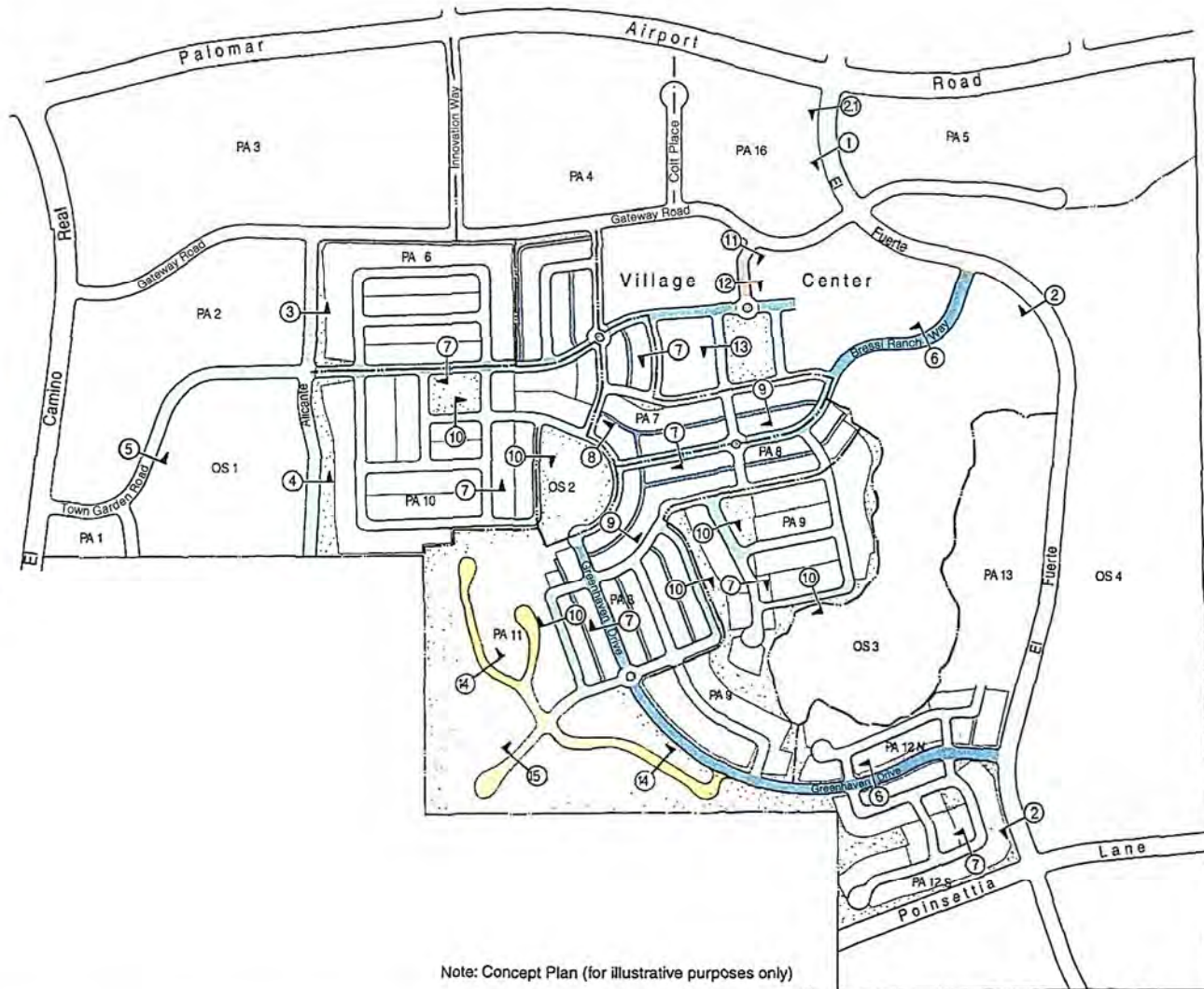


**STREET SECTIONS KEY LEGEND**

1	-	[White]
2	-	[White]
3	-	[Light Blue]
4	-	[Light Blue]
5	-	[Light Blue]
6	-	[Blue]
7	-	[Light Blue]
8	-	[Light Blue]
9	-	[Blue]
10	-	[Light Blue]
11	-	[Light Blue]
12	-	[Light Blue]
13	-	[Light Blue]
14	-	[Yellow]
15	-	[Yellow]

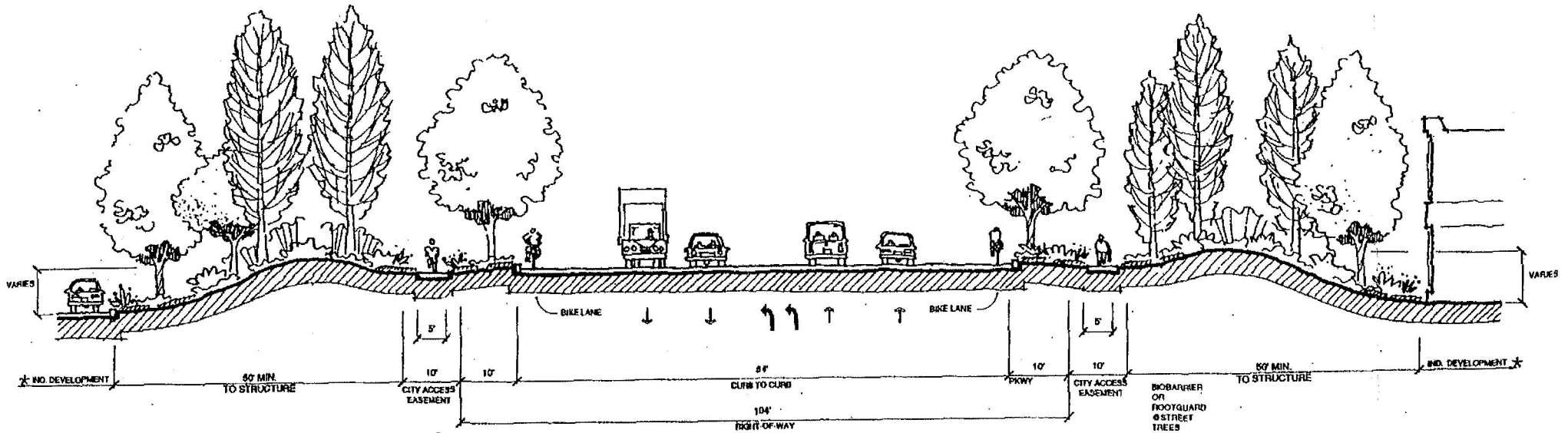
Neighborhood - [Light Blue]

Connector - [Blue]



Note: Concept Plan (for illustrative purposes only)





IV-5

**1** **EL FUERTE STREET**  
**CROSS SECTION WILL APPEAR AT REQUIRED LEFT TURN LANES**

CONCEPT SKETCH  
 (for illustrative purposes only)

\* EDGE CONDITIONS / LAND USES SUBJECT TO CHANGE  
 OUTSIDE OF ROW

*Bressi Ranch*

A planned community in the City of Carlsbad, California



*Street Section 1*

IV-3

Cross Section 2: El Fuerte between Gateway Road & Greenhaven Drive

South of the entrance to the industrial portion of the Master Plan (Gateway Road), El Fuerte will have a standard curb-to-curb width of 64'. A contiguous sidewalk will be provided along the west side of this portion of El Fuerte. Instead of a standard sidewalk, a decomposed granite trail, Segment 35 of the Citywide Trail System, will be constructed along Open Space Area 4 on the east side of El Fuerte Street. This trail will extend from the southern edge of Planning Area 5 to Poinsettia Lane, where it will connect with Segment 34 of the Citywide Trail System. Wherever feasible, the trail shall be located below the grade of El Fuerte Street so that people walking on it will be able to focus on the open space to the east instead of being distracted by traffic on El Fuerte Street (see Exhibit IV-4 on page IV-8).

Cross Section 2.1: El Fuerte between Palomar Airport Road and Planning Area 16 driveway

As part of the development of Planning Area 16, a deceleration lane will be constructed on southbound El Fuerte Street. This lane will safely allow vehicular traffic to make right hand turns from El Fuerte Street into the commercial project proposed for this portion of Planning Area 16. The construction of this deceleration lane will increase the curb-to-curb width of this portion of El Fuerte Street to 94 feet (see Exhibit IV-4.1 on page IV-9). The existing deceleration lane located south of the project driveway, and north of Gateway Road, will remain.

Cross Section 3: Alicante Road - Industrial/Residential Edge

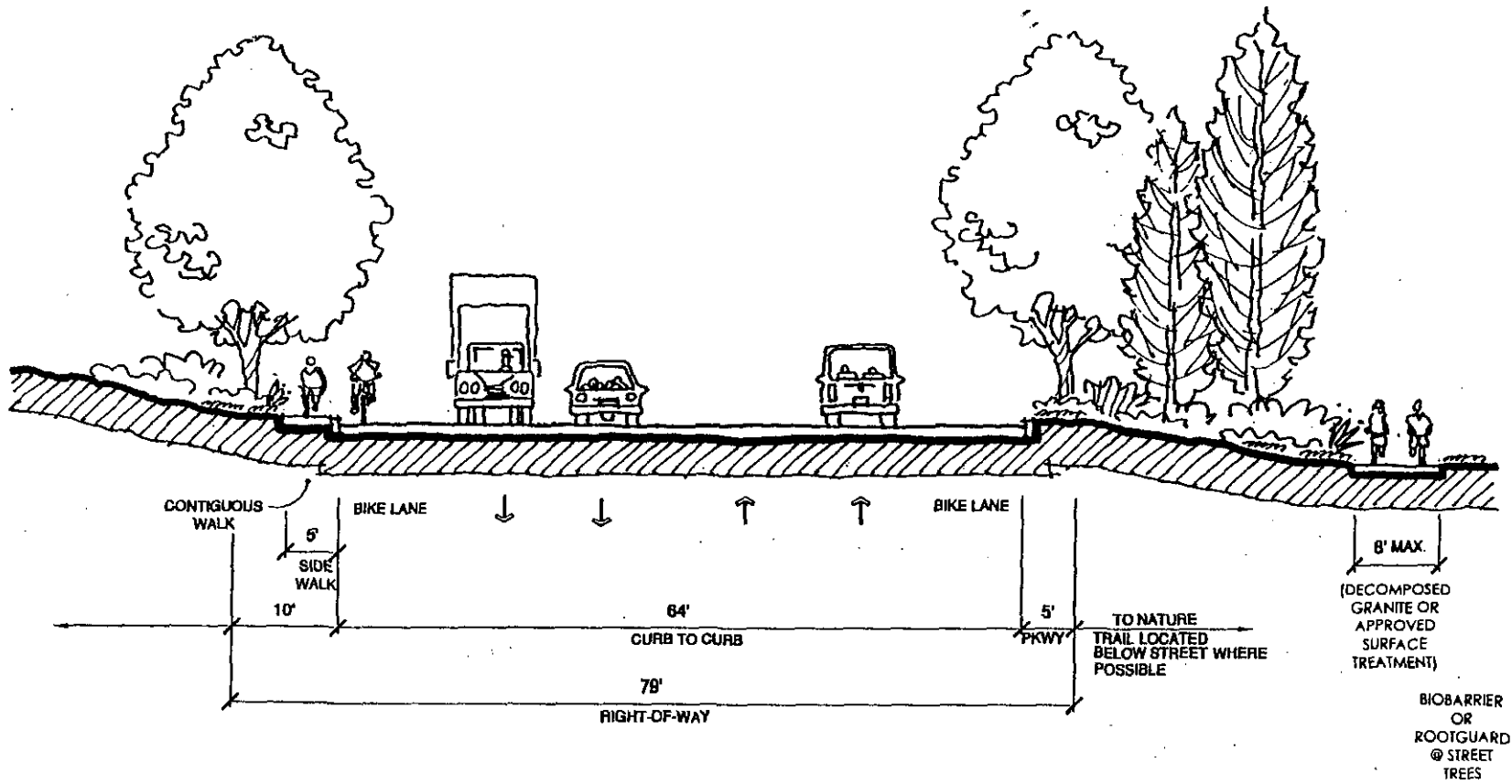
This road segment will provide access to office/industrial uses in Planning Areas 2, 3 and 4 from Alicante Road north of Street A. It will also help to serve as a buffer between the office/industrial uses in Planning Area 2 and the residential uses to the east in Planning Area 6. This street will be developed as a local collector street with a 48' curb-to-curb roadbed with no parking in a 70' right-of-way (see Exhibit IV-5 on page IV-10). Non-contiguous sidewalks with 5' wide parkways adjacent to the curb will be provided on the residential side of the street to encourage residents of the Master Plan to walk to job opportunities in the industrial area. The width of the road, along with the landscaping on each side of the road, will provide a substantial buffer between the industrial and residential portions of the Master Plan.

Cross Section 4: Alicante Road South of Town Garden Road

This road segment has been designed in conformance with the cross section of Alicante Road within the La Costa Village Master Plan to the south. A 60' right-of-way will include a 40' wide roadway with non-contiguous sidewalks and 5'

wide parkways on both sides of the street to encourage pedestrian activity (see Exhibit IV-6 on page IV-11).

IV-7



2

### EL FUERTE STREET BETWEEN RESIDENTIAL STREETS C AND B

CONCEPT SKETCH  
(for illustrative purposes only)

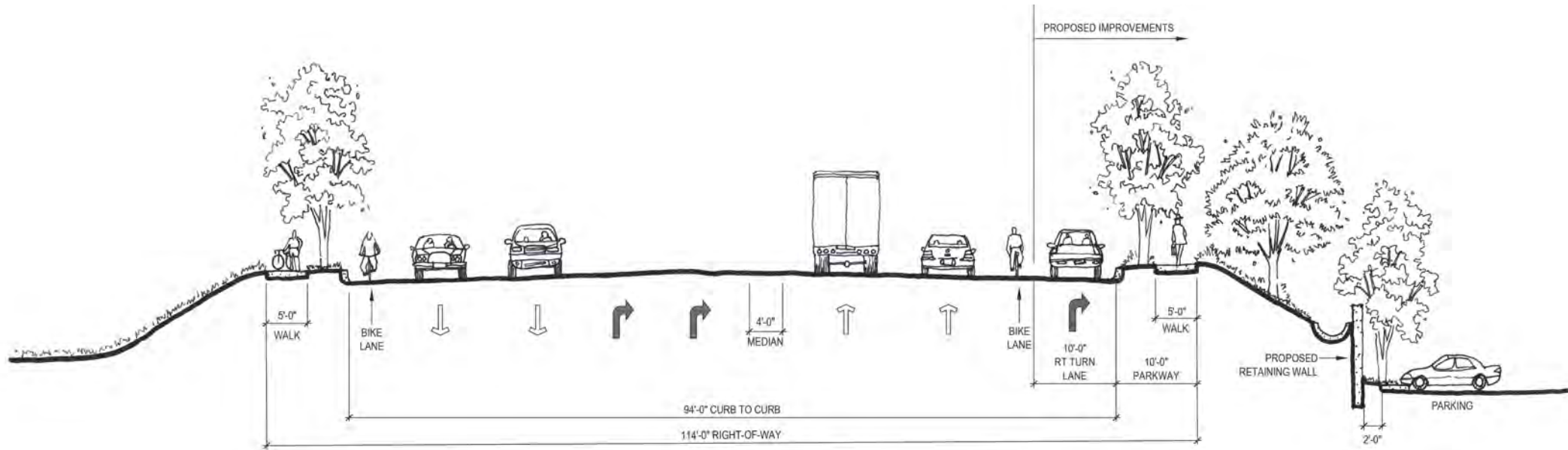
*Bressi Ranch*

A planned community in the City of Carlsbad, California

**Lenear**

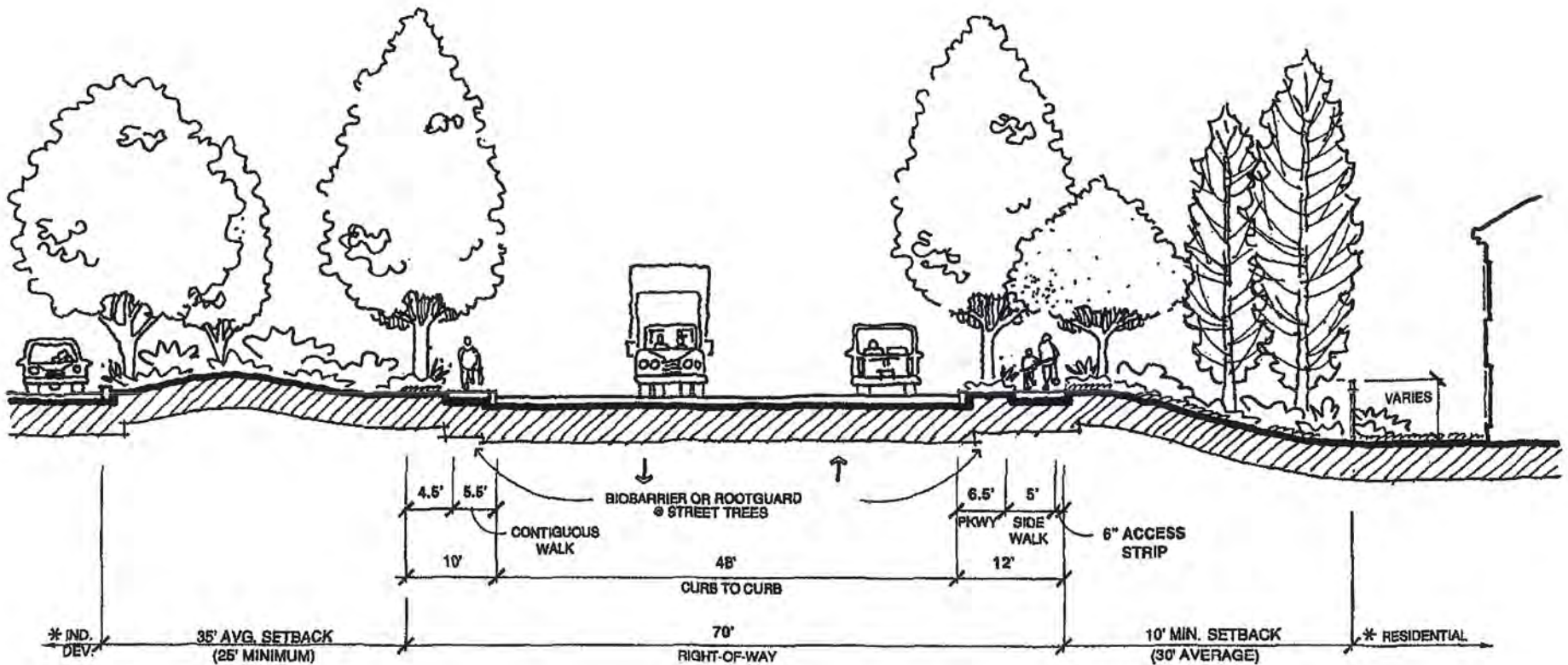
Street Section 2

IV-4



**El Fuerte Road Between Palomar Airport Road and  
Planning Area 16 driveway**





3

### INDUSTRIAL / RESIDENTIAL EDGE

@ ALCANTE ROAD (NORTH OF TOWN GARDEN ROAD)

CONCEPT SKETCH  
(for illustrative purposes only)

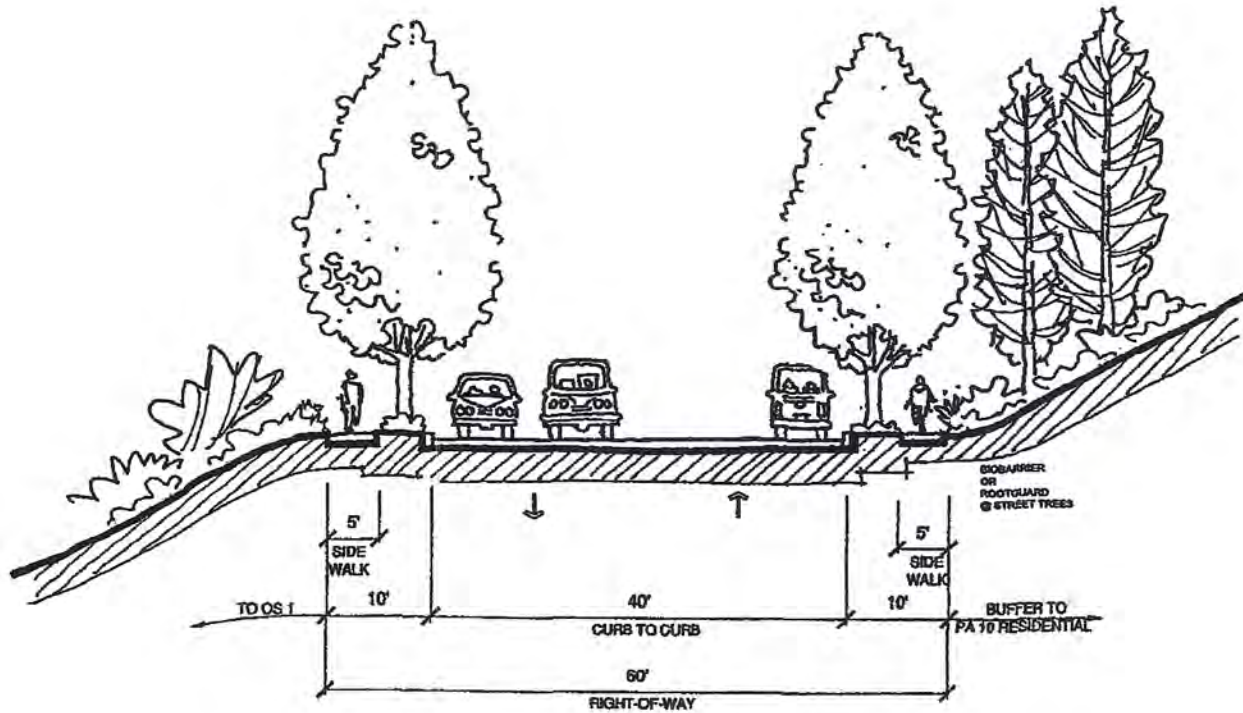
\* EDGE CONDITIONS / LAND USES SUBJECT TO CHANGE

*Bressi Ranch*

A planned community in the City of Carlsbad, California

Street Section 3

IV-5



4

**OPEN SPACE / RESIDENTIAL EDGE**

**@ ALICANTE ROAD  
(SOUTH OF TOWN GARDEN ROAD)**

CONCEPT SKETCH  
(for illustrative purposes only)

Cross Section 5: Access Road East of El Camino Real

Cross Section 5 shows the access road from El Camino Real into the Bressi Ranch Master Plan between Planning Area 2 and Open Space Area 1 (see Exhibit IV-7 on page IV-13). This section will be designed with a 52' curb-to-curb width and a 5' contiguous sidewalk on the industrial side. To reduce potential grading impacts on native habitat no sidewalk will occur on the east side of the street adjacent to Open Space 1. This will result in a 67' right-of-way instead of the standard 72' right-of-way provided for industrial streets.

Cross Section 6: Connector (Access) Road with Parkways

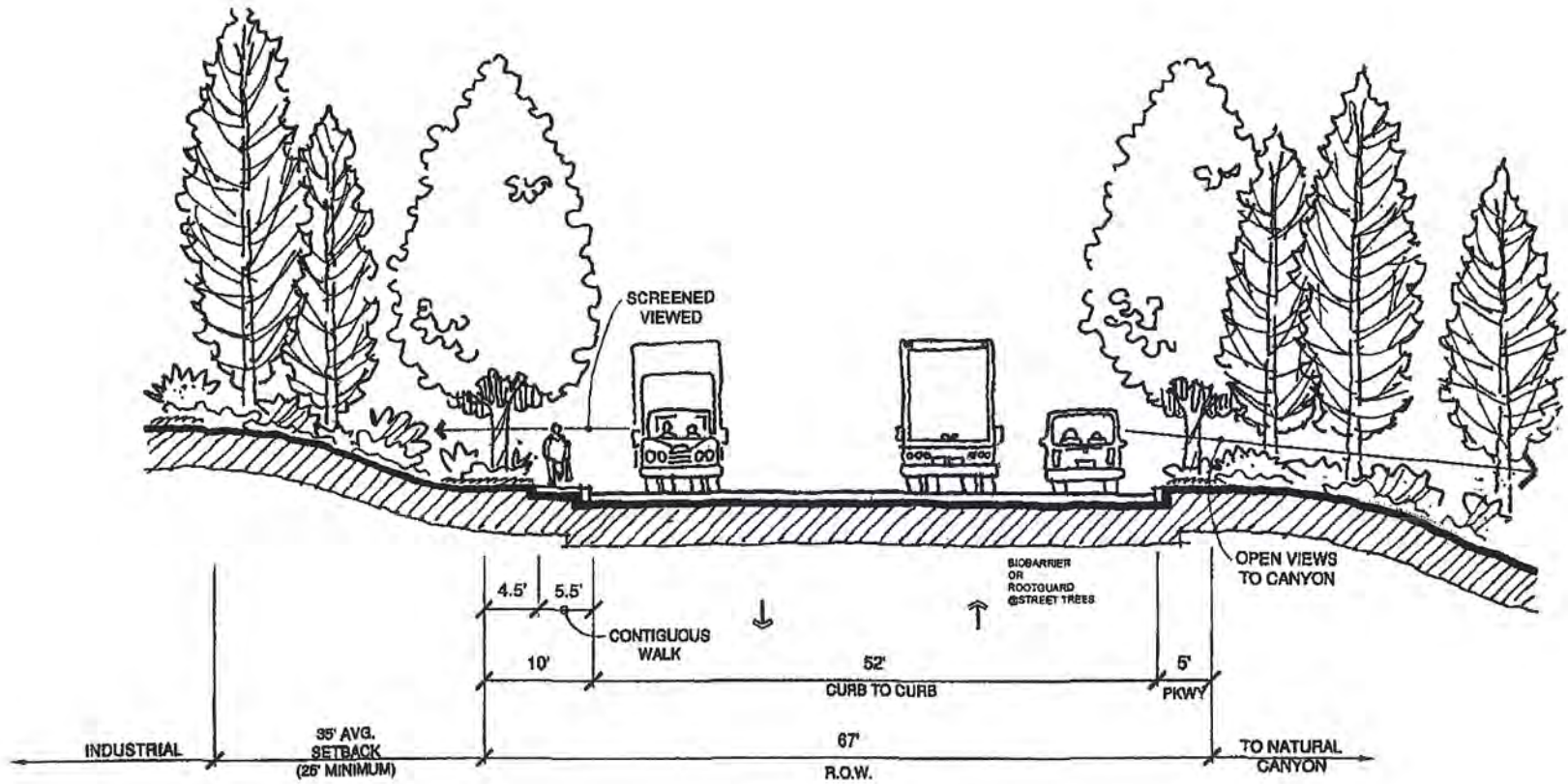
This cross section shows the access roads into the residential portion of the Master Plan, portions of Greenhaven Drive and Bressi Ranch Way closest to El Fuerte, where no homes will be fronting onto the street. There will be a curb-to-curb width of 30' with no parking to allow for two 10' travel lanes and two 5' bike lanes. Similar to other collector streets within the Master Plan the sidewalk will be setback from the curb (see Exhibit IV-8 on page IV-14). The right-of-way width shall be 56' to allow for two 5' sidewalks and a 7.5' parkway between the curb and sidewalk.

Cross Section 7: Neighborhood & Connector Streets

Neighborhood streets within the Bressi Ranch Master Plan are those with fewer than 2,000 ADT. Connector streets are those with 2,000-4,000 ADT. These streets will be developed in accordance with the City's recently adopted standards for local streets. The only situation where they will differ is where they serve homes with an alley-loaded garage (Exhibit IV-9 on page IV-15). In those areas an extended 6' parkway area 30'-40' in length will be provided adjacent to every other property line. This extended parkway will serve two purposes; it will calm traffic and it will provide a staging area for emergency vehicles since no cars will be parked in this area. Exhibit IV-17 on page IV-26 provides a detailed view of the planter areas.

Cross Section 8: Loop Road at Village Green

Similar to the local and connector streets, this street will be developed in accordance with the City's recently adopted standards for local streets. To create an open feeling in this area, parking adjacent to the Village Green will be limited by the provision of a number of 7.5' wide parkways (Exhibit IV-10 on page IV-16).



5

**ACCESS ROAD EAST OF EL CAMINO REAL  
TOWN GARDEN ROAD**

CONCEPT SKETCH  
(for illustrative purposes only)

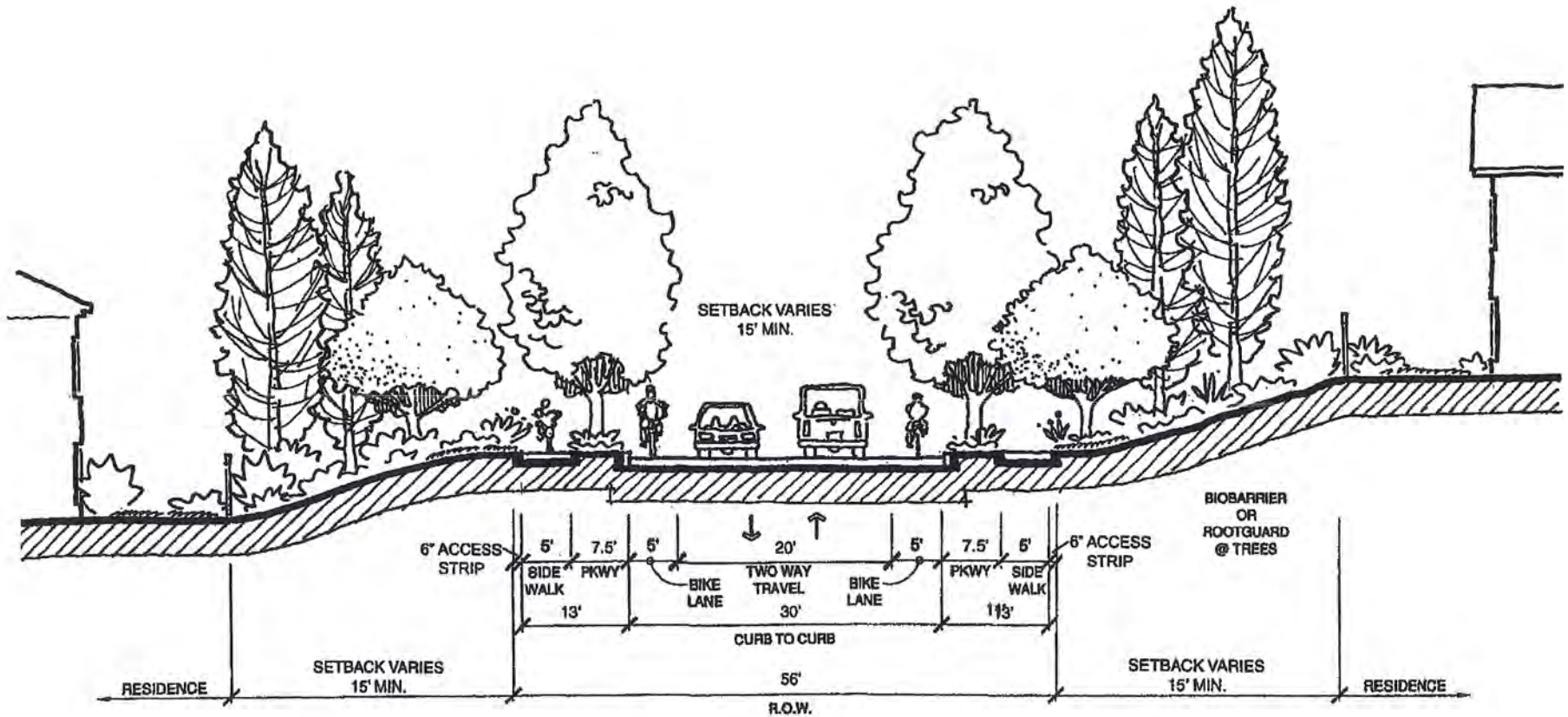
*Bressi Ranch*

A planned community in the City of Carlsbad, California

*Street Section 5*

IV-7

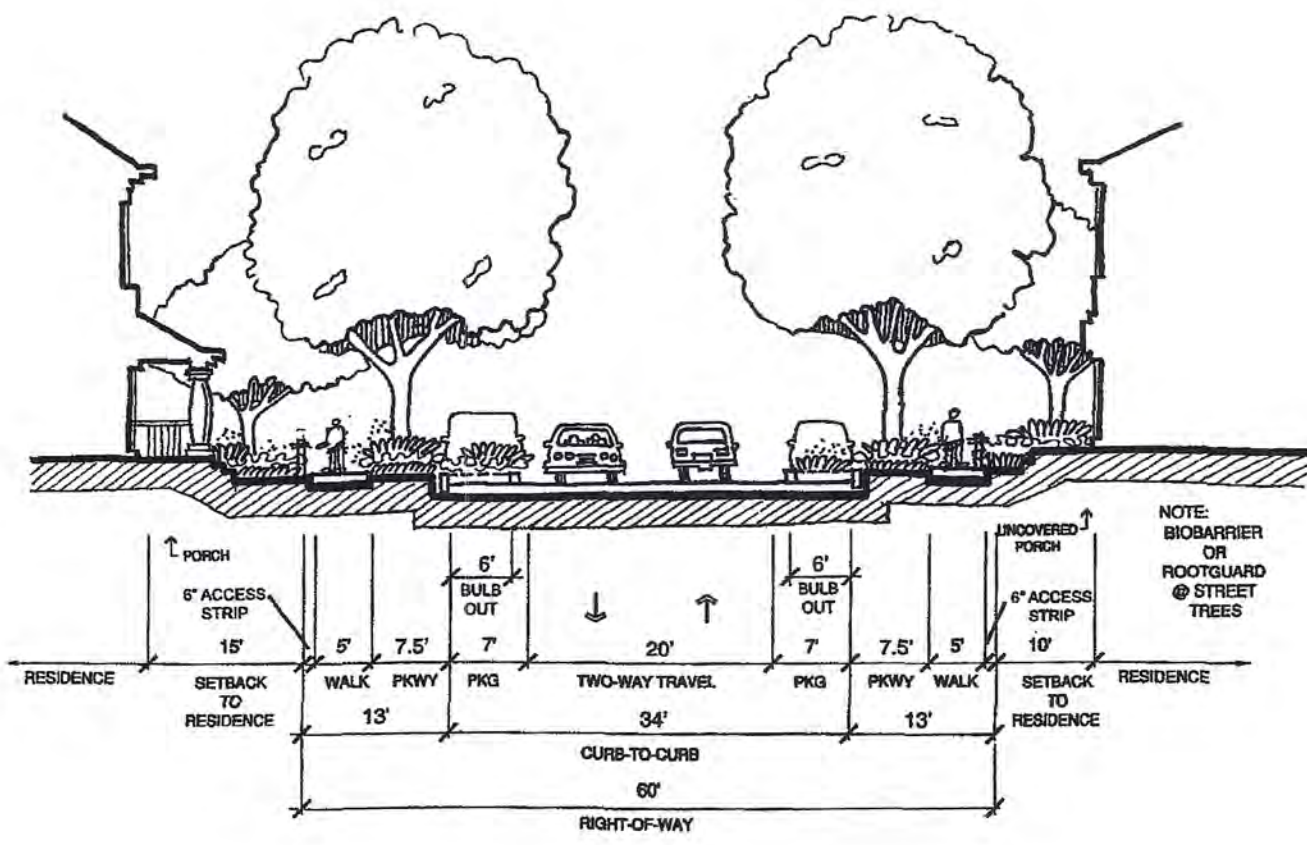




6

**CONNECTOR (ACCESS) ROAD W/ PARKWAYS**  
**GREENHAVEN DRIVE AND BRESSI RANCH WAY**  
 W/O ON-STREET PARKING AND NO LOT FRONTAGES

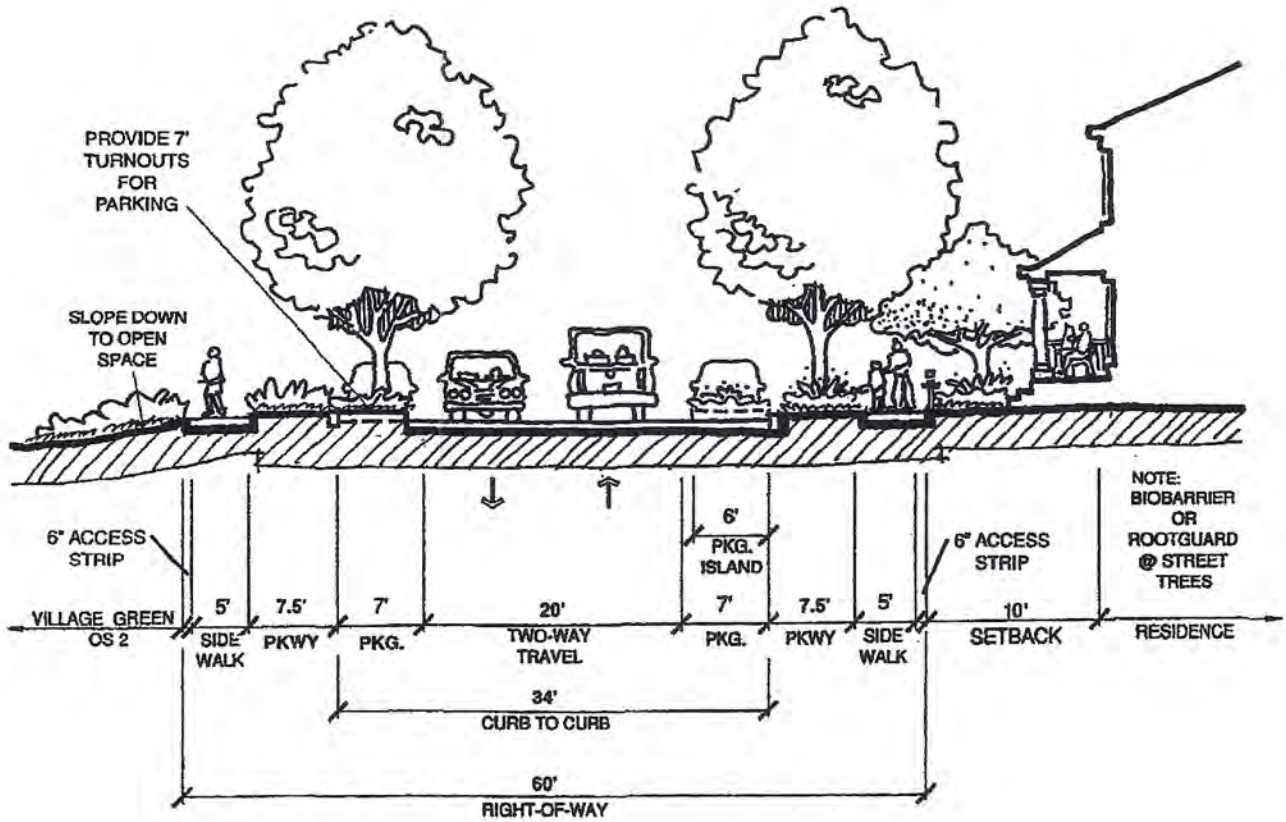
CONCEPT SKETCH  
 (for illustrative purposes only)



7

**NEIGHBORHOOD AND CONNECTOR STREET**  
**UP TO 4,000 ADT FRONT LOAD OR ALLEY TYPE**

CONCEPT SKETCH  
 (for illustrative purposes only)



8

**LOOP ROAD @ VILLAGE GREEN**

**GREENHAVEN DRIVE**

CONCEPT SKETCH  
(for illustrative purposes only)

Cross Section 9: Alleys

This cross section shows the design of the 20' public alleys that will be utilized where alley-loaded garages are provided. Garage doors shall be roll-up type doors and shall be set back a minimum of 5' from the alley (Exhibit IV-11 on page IV-18).

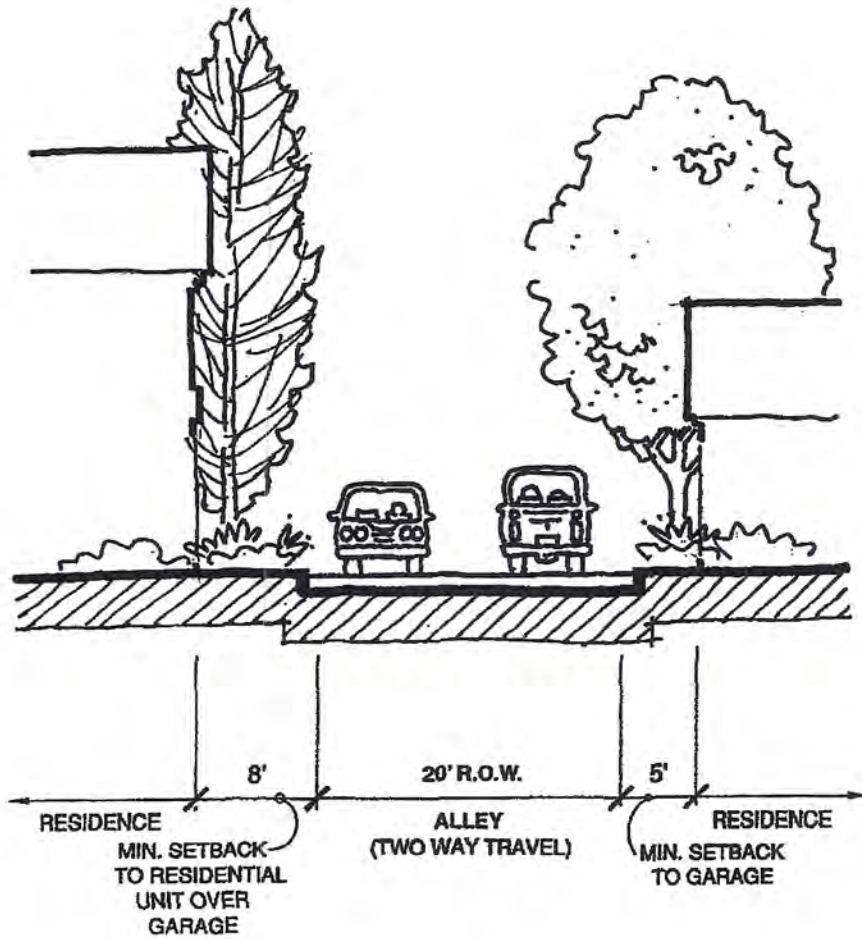
Cross Section 10: Two-Way Street Adjacent to Open Space

In several instances within the Master Plan homes may front on one side of a street while the opposite side is an open space area or slope. In this situation a reduced right-of-way will be used to provide for more open space and reduce grading impacts to slopes. A 48' right-of-way will allow for a 34' curb-to-curb roadway which will accommodate two 10' travel lanes and two 7' parking aisles. A sidewalk will be provided on the same side of the street as the residences. A seven-foot parkway will separate the sidewalk from the street. Sidewalks will not be provided on the opposite side of the street in the open space areas.

Cross Section 11: Entrance to Village Center from Gateway Road

This type of street may provide the main vehicular access into the Village Center from Gateway Road. No parking would occur at the portion of this street near the intersection. The entry road would contain two 10' travel lanes and a 10' left turn lane (Exhibit IV-13 on page IV-20). Sidewalks will be separated from the roadway by a 6' parkway on either side of the street. Sidewalks will have a minimum width of 8' to allow for sidewalk cafes and eating areas.

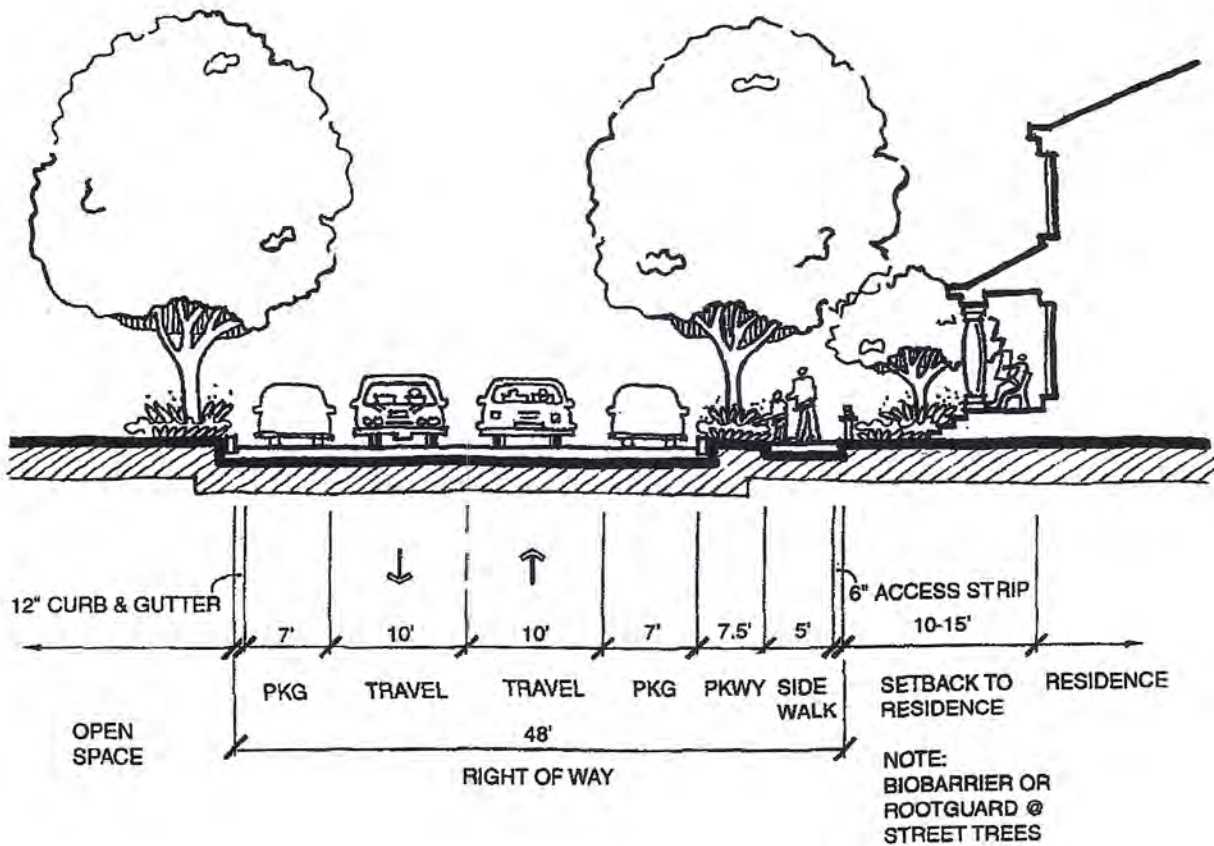




9

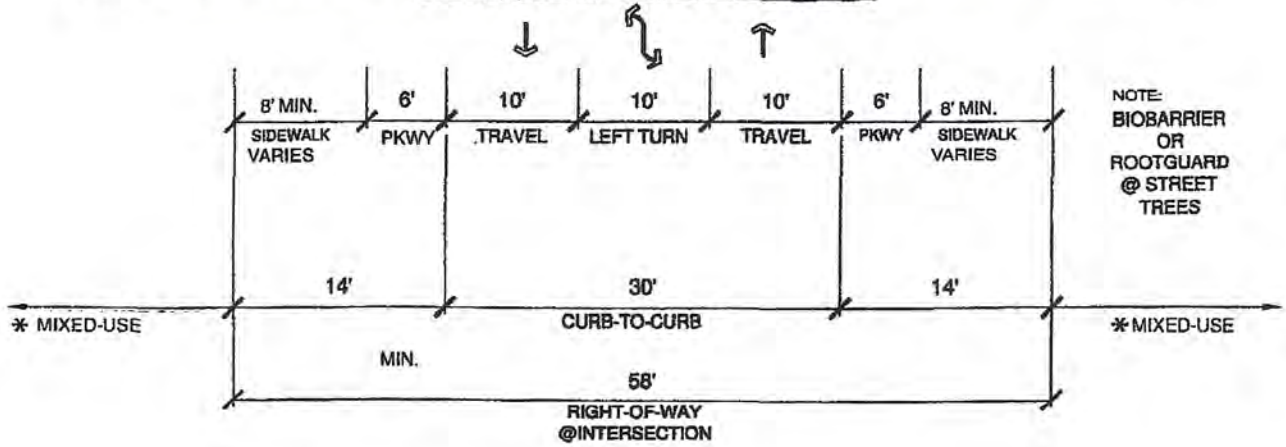
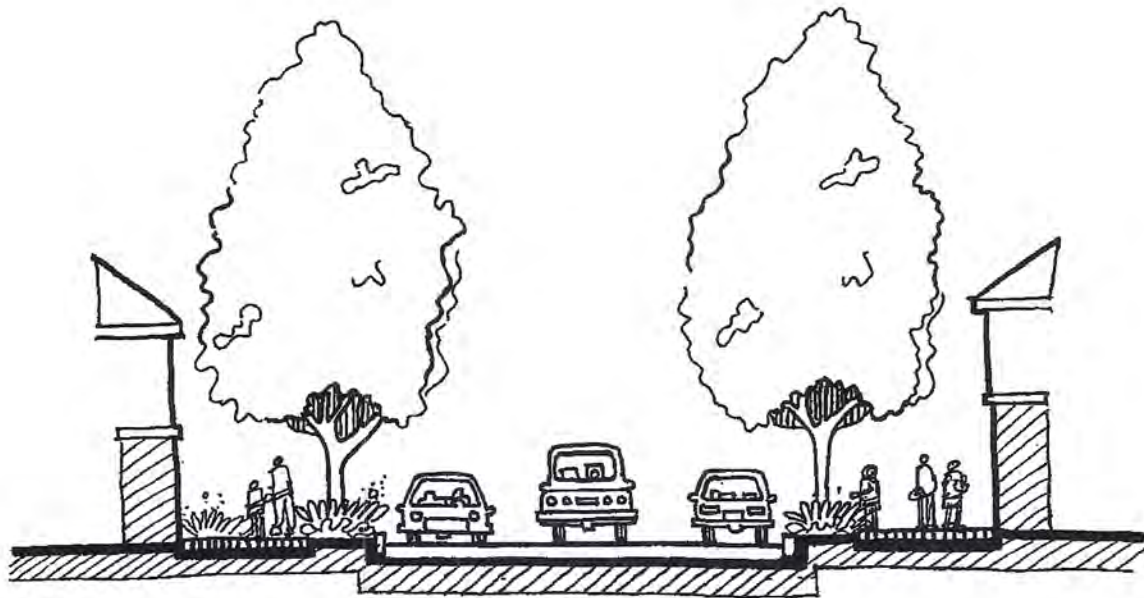
**ALLEY CROSS SECTION**

CONCEPT SKETCH  
(for illustrative purposes only)



**10 TWO-WAY RESIDENTIAL STREET @ OPEN SPACE**  
**SINGLE LOADED PARKING TWO SIDES**

CONCEPT SKETCH  
 (for illustrative purposes only)



11

**ENTRANCE TO VILLAGE CENTER  
(FROM GATEWAY ROAD)**

CONCEPT SKETCH  
(for illustrative purposes only)

\* EDGE CONDITIONS / LAND USES SUBJECT TO CHANGE

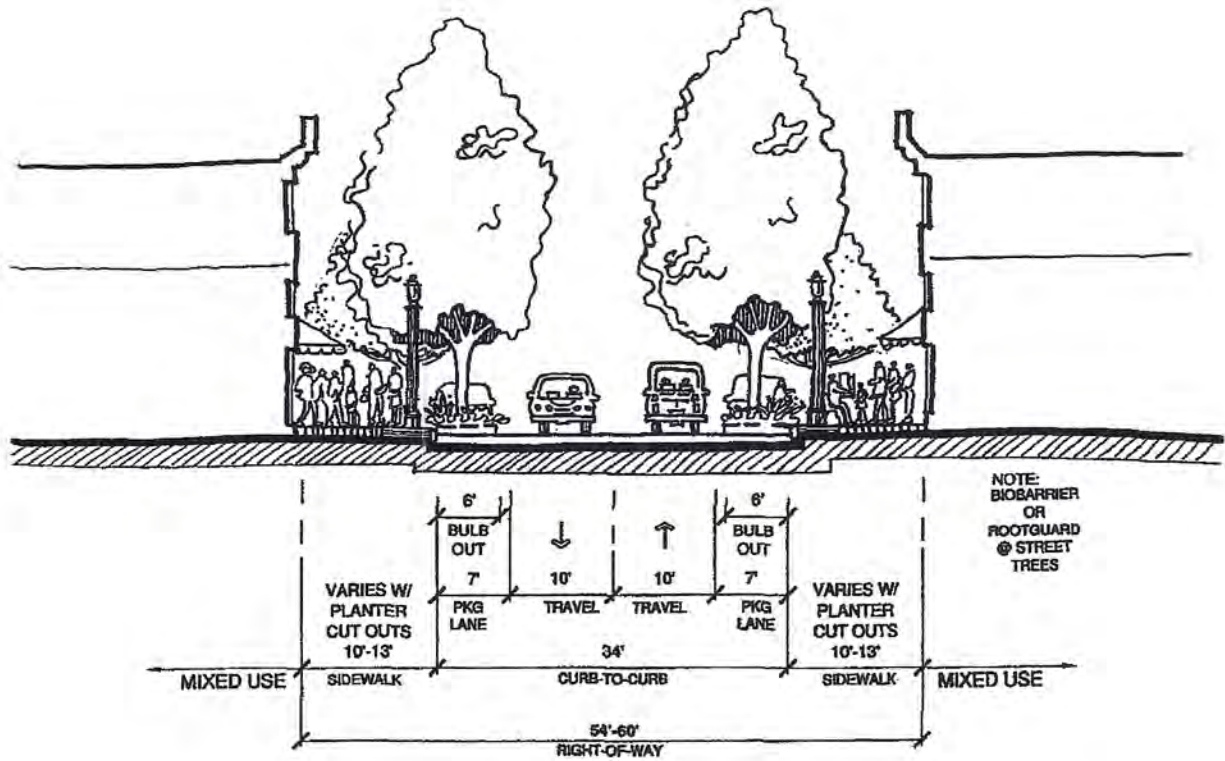
Cross Section 12: Mixed-Use Street

A mixed-use street with shops and offices can occur anywhere within the Village Center. The exact location of this type of street, as well as the Village Center street, would be determined as a part of the City's review and approval of the Site Development Plans and other discretionary permits in Planning Area 15. Wide sidewalks (10'-13' average) on either side of a mixed-use street will allow for outdoor dining areas and encourage pedestrian activity along the street. A mixed-use street would have a 34' curb-to-curb width that will allow for two 10' travel lanes with parallel parking on both sides of the street. Six-foot wide planter islands would be provided to break up the parking. The majority of parking for this area would be provided in parking lots located behind the buildings along the mixed-use street. Exhibit IV-14 on page IV-22 provides a cross section of a portion of a mixed-use street for Planning Area 15.

A mixed-use street will also be provided in Planning Area 16, as an extension of Finilla Place to the north. The exact location of this type of street, would be determined as part of the City's review and approval of the Site Development Plans and other discretionary permits in Planning Area 16. Wide sidewalks on either side of the street will allow for outdoor dining areas and encourage pedestrian activity along the street. This street will have a 28' curb-to-curb width that will allow for two 10' travel lanes with diagonal parking on both sides of the street. The majority of parking for this area would be provided in parking lots located behind the buildings along the mixed-use street, as well as in a larger parking area located to the north of this mixed use street.

Cross Section 13: Village Square Road

A Village Square may be surrounded by a 34' curb-to-curb road with parallel parking on both sides. The 5' wide sidewalks would be separated from the road by 7.5' wide landscaped strips that will include trees. Exhibit IV-15 on page IV-23 shows a cross section of a Village Square Road between the Village Square and the affordable apartments or the assisted care facility. This street is designed in accordance with the City's standards for livable streets.



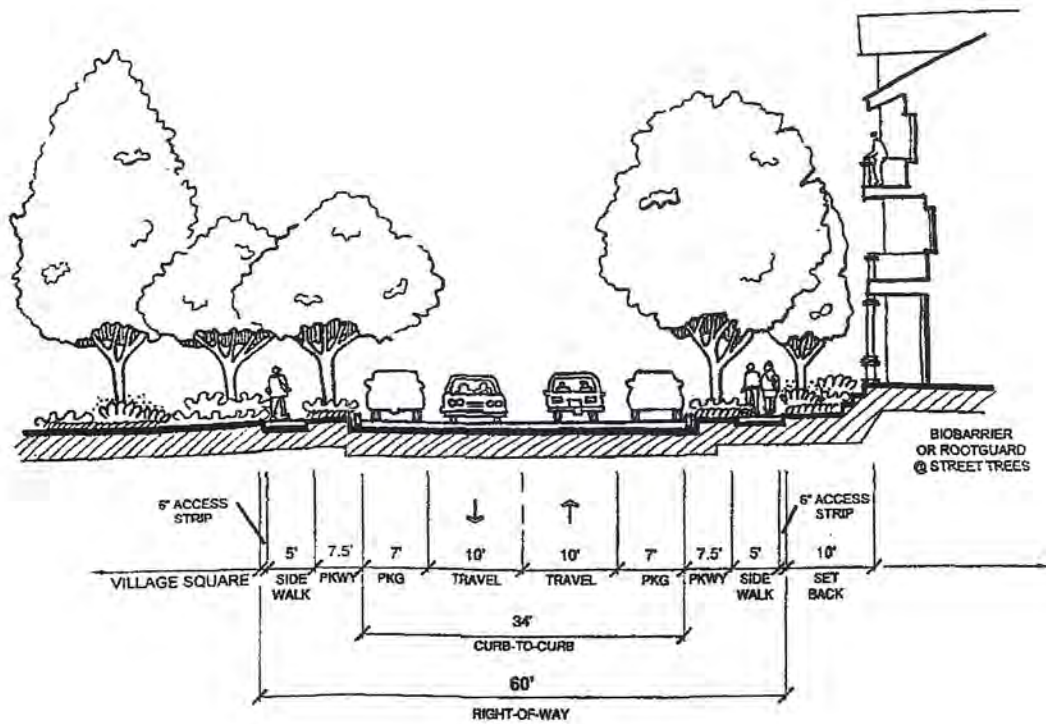
12

## MIXED USE STREET

CONCEPT SKETCH  
(for illustrative purposes only)

\* EDGE CONDITIONS / LAND USES SUBJECT TO CHANGE  
NOTE: STREET CAN OCCUR ANYWHERE WITHIN PA-15





13

VILLAGE SQUARE ROAD

CONCEPT SKETCH  
(for illustrative purposes only)

### **C. Traffic Calming**

One of the major goals of the Bressi Ranch Master Plan is to create a pedestrian friendly atmosphere that will slow traffic and encourage people to walk. As mentioned previously, there will be very little through traffic in the residential portion of the Master Plan. Many of the streets will have relatively low ADT (Average Daily Trips). A variety of traffic calming techniques will be utilized in the Village Center and residential areas to make the area even more pedestrian friendly.

Once traffic leaves El Fuerte Street and enters the residential portion of the Master Plan, special design features will be used to calm traffic and encourage pedestrian activity. These will consist of features such as narrower right-of-way widths, non-contiguous sidewalks, traffic circles, bulbouts, on street parking, enhanced paving and geometrics to encourage reduced vehicular speeds.

Bulbouts (neck downs) will be utilized at key intersections to slow traffic and make motorists aware of pedestrian crossings. Another technique that may be utilized would be to raise certain key intersections a few inches to make motorists aware of pedestrian crossings, subject to approval from the City Planner, City Engineer and Fire Marshal. A landscaped traffic circle will be used at the Village Center to slow traffic and create a focal point. Additional traffic circles will be located at strategic areas in the Master Plan to calm the flow of internal traffic in the residential areas subject to the approval of the Public Works Director, Fire Marshal and City Planner if it can be shown that they will not adversely impact the response time of emergency vehicles. Rumble strips may be utilized at certain locations to make motorists aware that they are entering a pedestrian-oriented area or busy cross walk. Exhibit IV-16 on page IV-25 shows the location of the various traffic calming features that will be provided in the Bressi Ranch Master Plan.

#### Traffic Calming Feature 1: Connector and Neighborhood Streets with Parking Islands (Bulbouts)

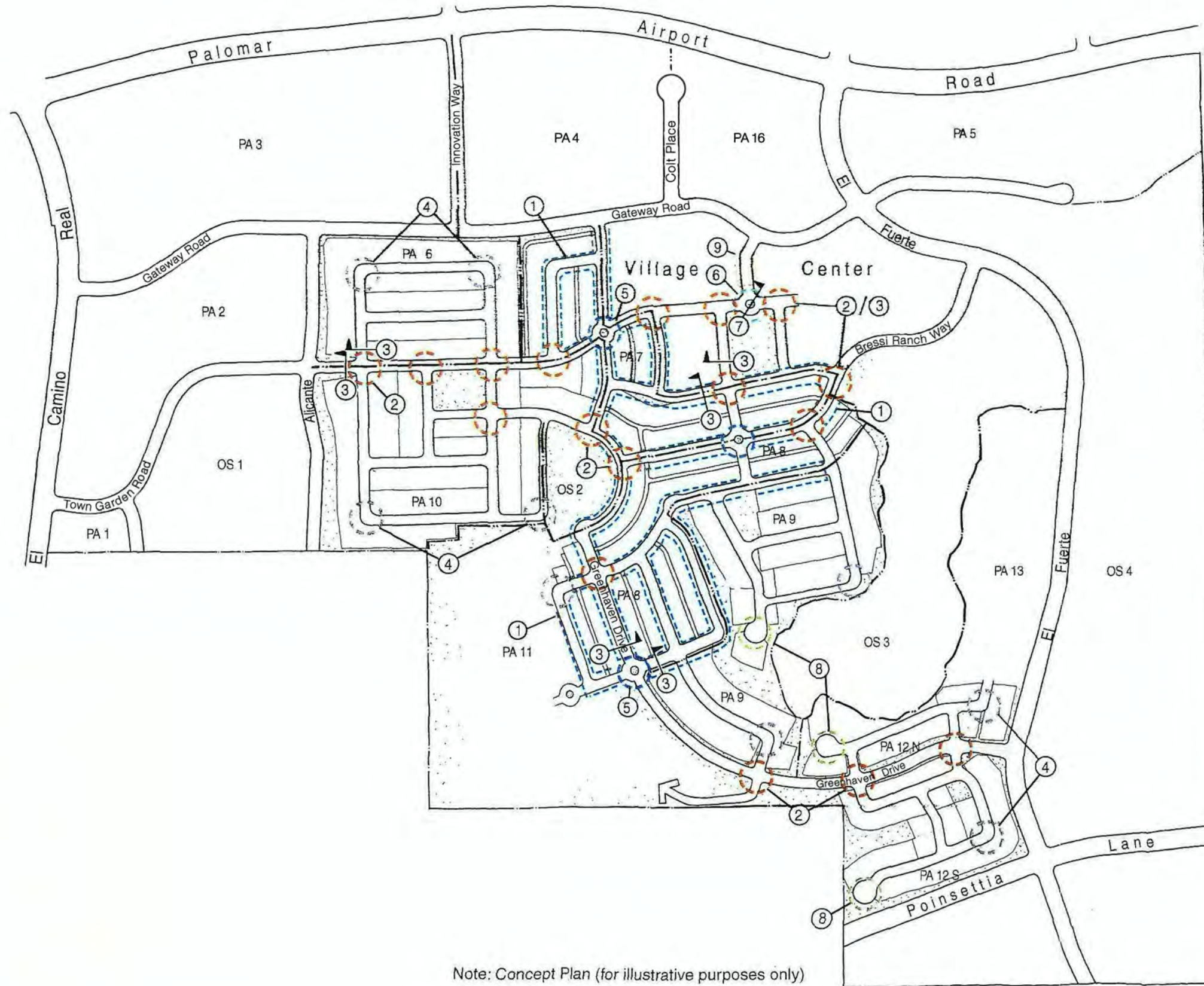
Bulbouts will be provided along connector streets and neighborhood streets (Cross Section 6) that are developed with alley loaded garages. These bulbouts will serve two purposes; they will calm traffic by providing landscaping closer to the travel lanes and they will provide a fire staging area for emergency vehicles. As shown by Exhibit IV-17 on page IV-26 the bulbouts will be staggered every other property line to allow easy emergency access to the front door of every residence. This exhibit also shows how adequate guest parking as well as room for fire staging areas can be provided for alley loaded residences.



**STREET CALMING  
KEY LEGEND**

- ①\* - [Blue dashed line]
- ②/③ - [Red dashed line]
- ④ - [Black dashed line]
- ⑤/③ - [Blue dashed line]
- ⑥/⑦ - [Green dashed line]
- ⑧ - [Yellow dashed line]
- ⑨ - [Yellow dashed line]

\* Detail ① represents streets with curb bulbouts and includes sections ⑦ ⑧ & ⑩ (Alley loaded product only) - see map 1 for sections ⑦ ⑧ & ⑩



Note: Concept Plan (for illustrative purposes only)

**Bressi Ranch**

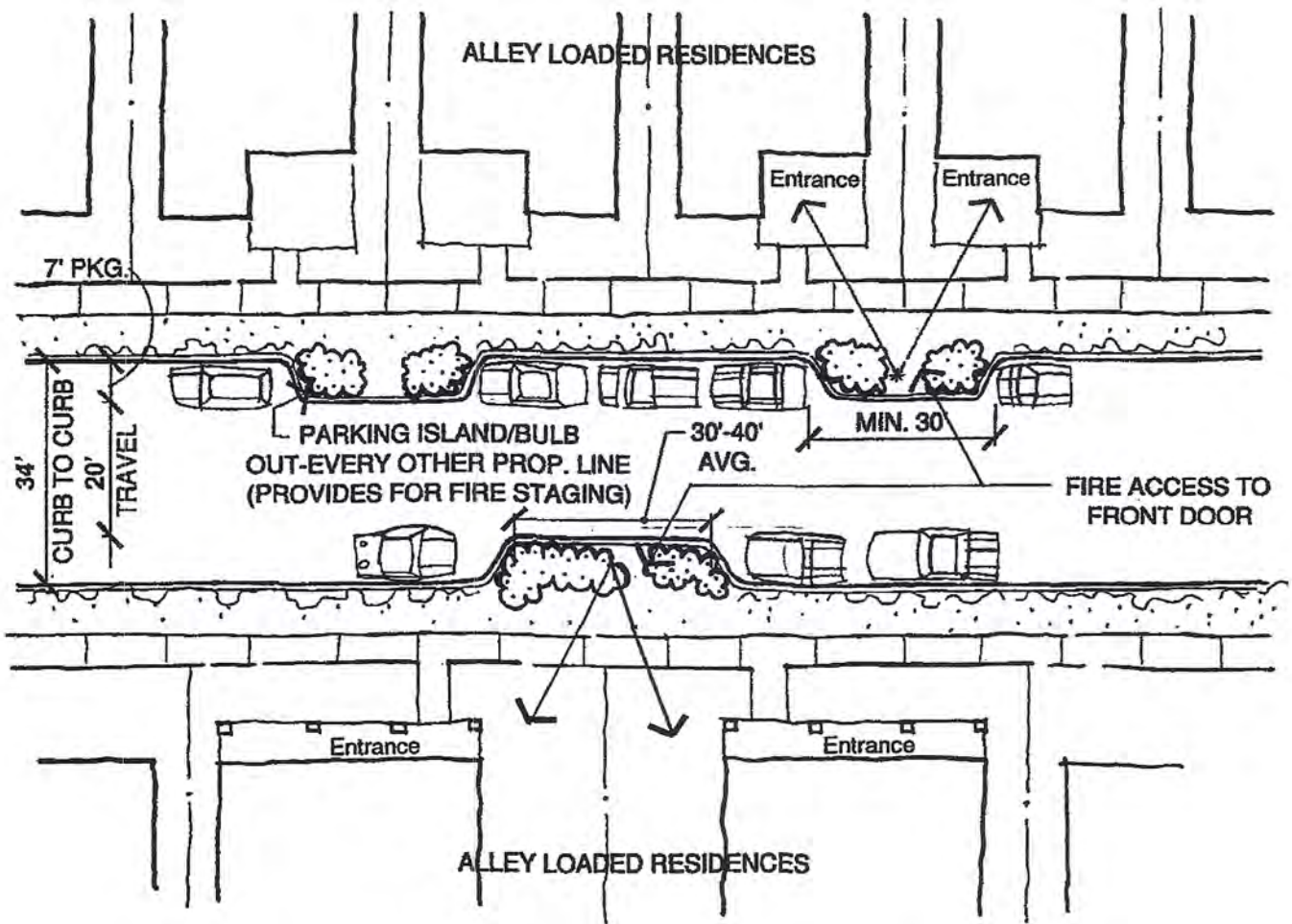
A planned community in the City of Carlsbad, California

**Traffic Calming Location Map**

IV-16







1

**CONNECTOR AND NEIGHBORHOOD STREET PLAN  
W/ PARKING ISLANDS (BULB OUTS)**

**ALLEY LOADED RESIDENCES ONLY**

CONCEPT SKETCH  
(for illustrative purposes only)



Traffic Calming Feature 2: Connector and Neighborhood Street Intersections

Where connector and neighborhood streets intersect, sidewalk bulbouts will occur at each corner (Exhibit IV-18 on page IV-29). The bulbouts will serve to calm traffic by providing a shorter crossing distance for pedestrians near intersections. However, as the street actually enters the intersection the travel lanes will be widened to a total of 24' to facilitate turn movements.

Traffic Calming Feature 3: Cross Section of Neighborhood and Collector Streets at Intersections

Exhibit IV-18 on page IV-29 provides a cross section of the neighborhood and connector streets near intersections, where they flare out to calm traffic. Exhibit IV-16 on Page IV-25 shows where these two cross sections would occur. As shown by Exhibits IV-17 & IV-18, these streets would have non-contiguous sidewalks and a 20' two-way travel lane. At the intersections the travel lane width will increase to 24' to facilitate vehicle-turning movements. These same street cross sections will be used where neighborhood and connector streets enter traffic circles.

Traffic Calming Feature 4: 90° Knuckle Detail

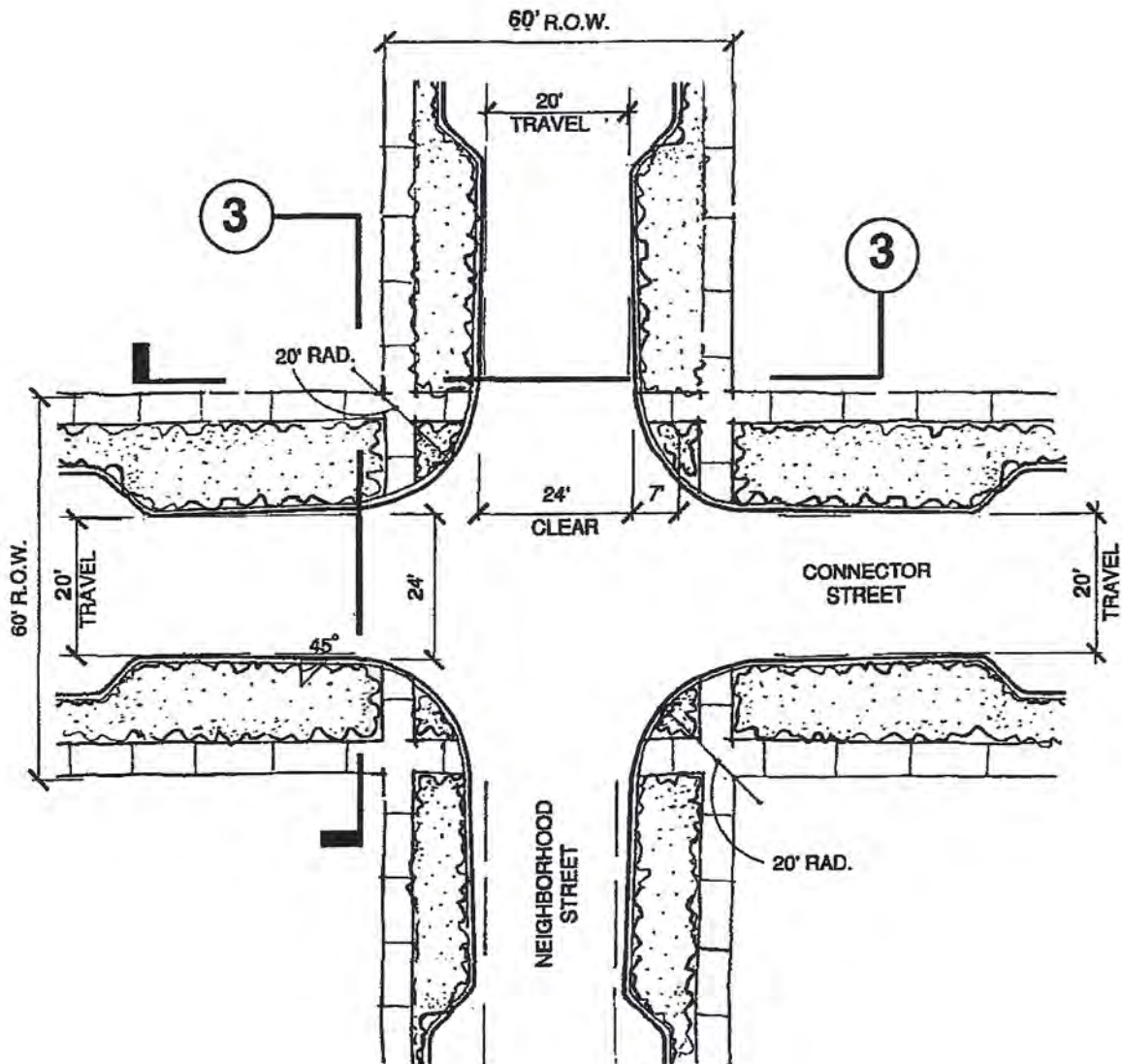
Where streets make a 90° turn, a 50' minimum turning radius will be provided and a curb bulbout will occur to help slow down traffic making the turn. No parking is allowed along the curb bulbout (see Exhibit IV-20 on page IV-31).

Traffic Calming Features 5, 6, & 7: Traffic Circles

Traffic circles will be used at several locations within the Master Plan as a traffic-calming device. Exhibit IV-16 on page IV-25 shows the location of traffic circles in the Master Plan. Traffic circles have been located at limited locations, so they will not significantly delay the arrival times for emergency services. A round 40' diameter island will be located at the center of a 50' radius traffic circle. Sidewalk bulbouts will not occur within the traffic circles, but occur outside the traffic circles before on-street parking begins. These traffic circles are designed to allow a fire truck to pass through without having to mount the curb.

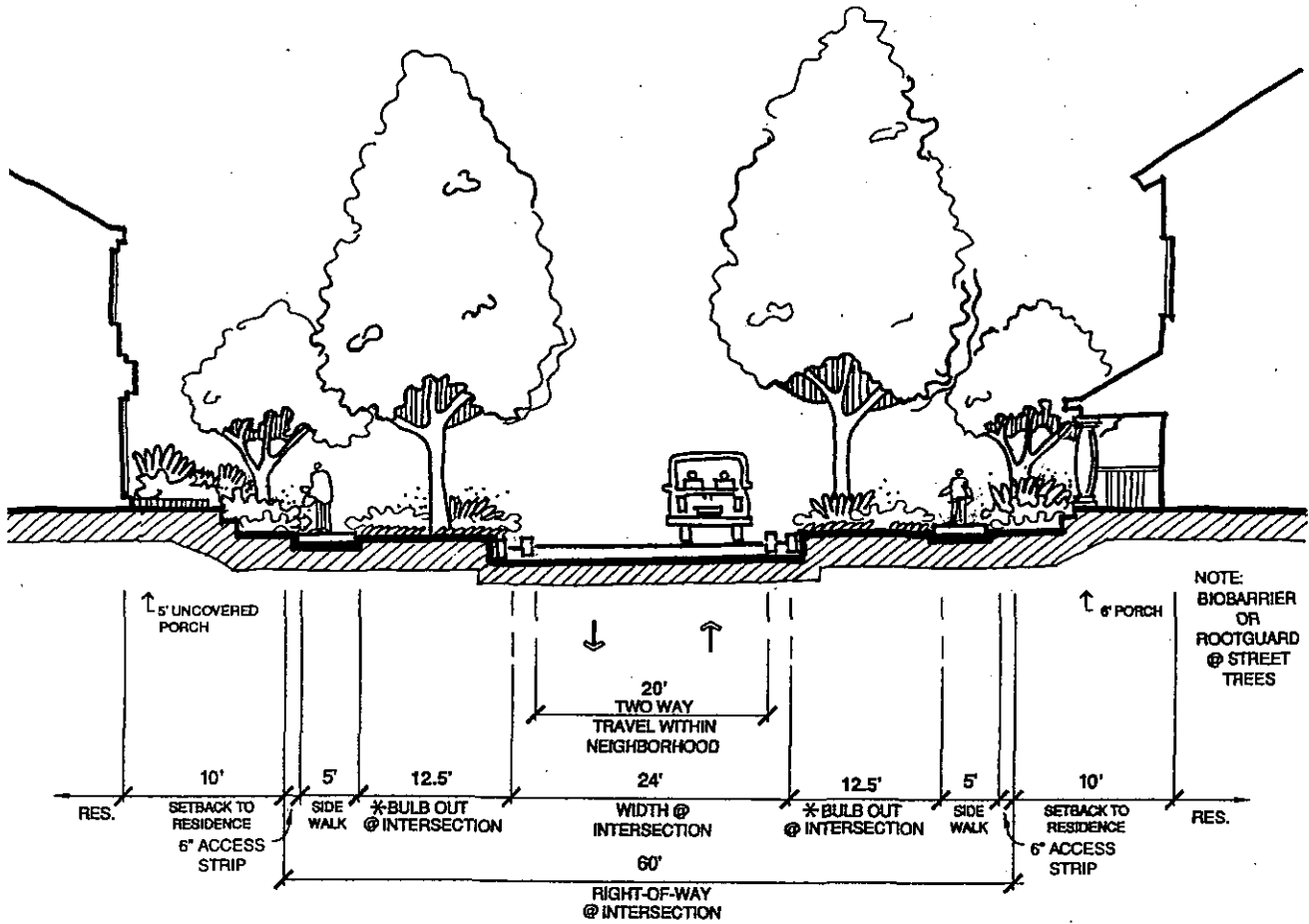
Exhibits IV-21 & IV-22 on pages IV-32 & IV-33 show two of the traffic circles that will be provided within the Bressi Ranch Master Plan. Both of these traffic circles will have the same dimensions, but one will be located in a residential area, while the other will be located in the Village Center, Planning Area 15. The traffic circle in Planning Area 15 (see Exhibit IV-22 on Page IV-33) will serve to slow traffic and create a sense of entry into the Village Center. Exhibit IV-23 on page IV-34 provides a cross section of the traffic circle that will be located in the Village Center.

The traffic circles that will be located in the residential areas will have the curb-to-curb dimensions as shown by Exhibit IV-21 on page IV-32.



**2** **CONNECTOR & NEIGHBORHOOD STREET INTERSECTION W/ SIDEWALK BULB OUTS**  
 CONCEPT SKETCH  
 (for illustrative purposes only)



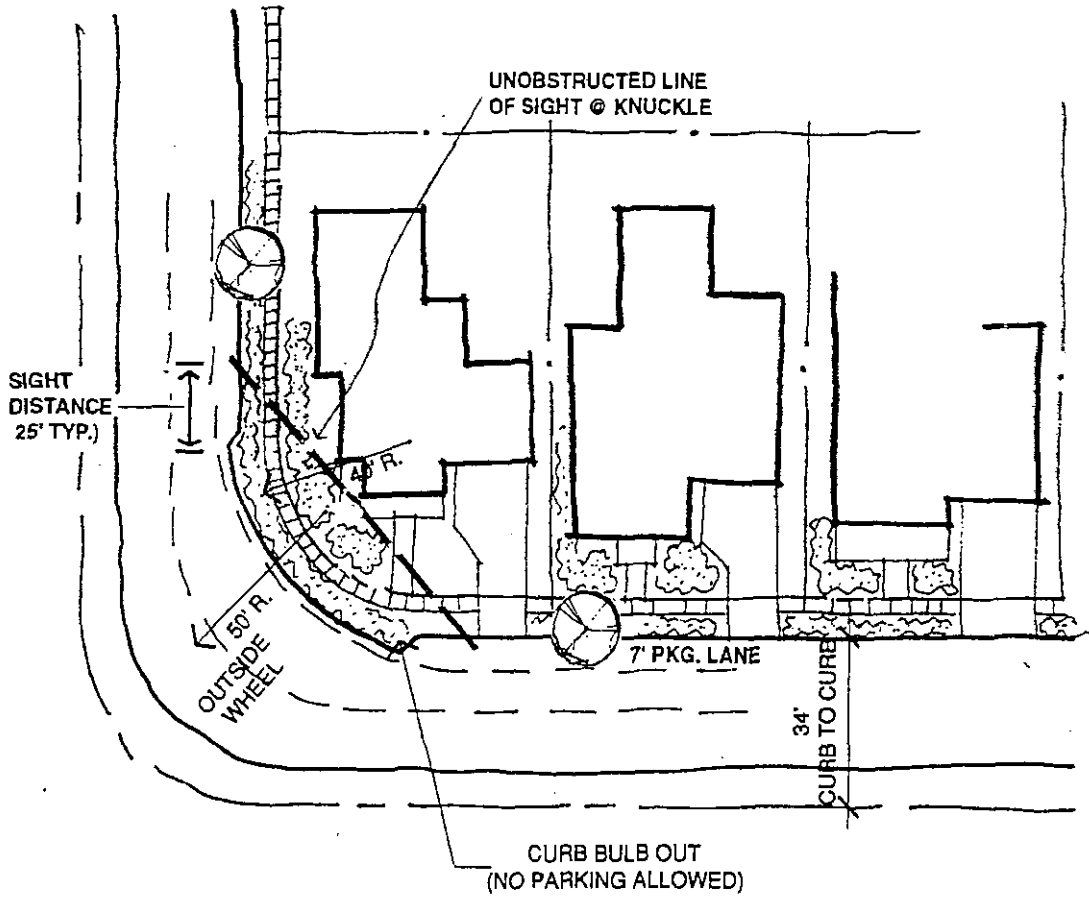


3

### NEIGHBORHOOD AND CONNECTOR STREET @ INTERSECTIONS

CONCEPT SKETCH  
(for illustrative purposes only)

\* INTERSECTION BULB-OUTS WILL OCCUR ONLY WHERE CONNECTOR STREETS AND NEIGHBORHOOD STREETS INTERSECT. SEE TRAFFIC CALMING 2 PLAN.



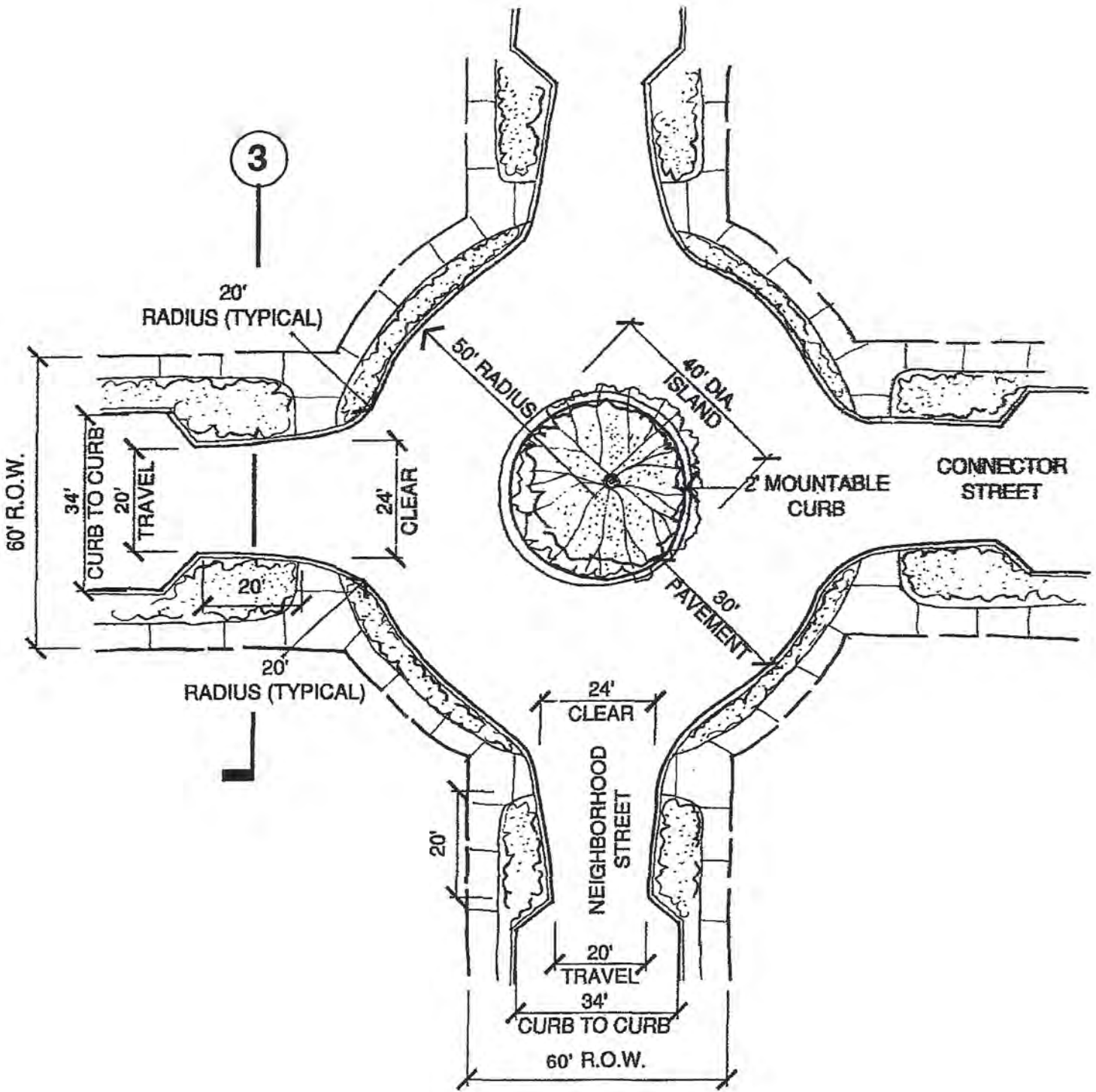
4

90° KNUCKLE DETAIL

CONCEPT SKETCH  
(for illustrative purposes only)

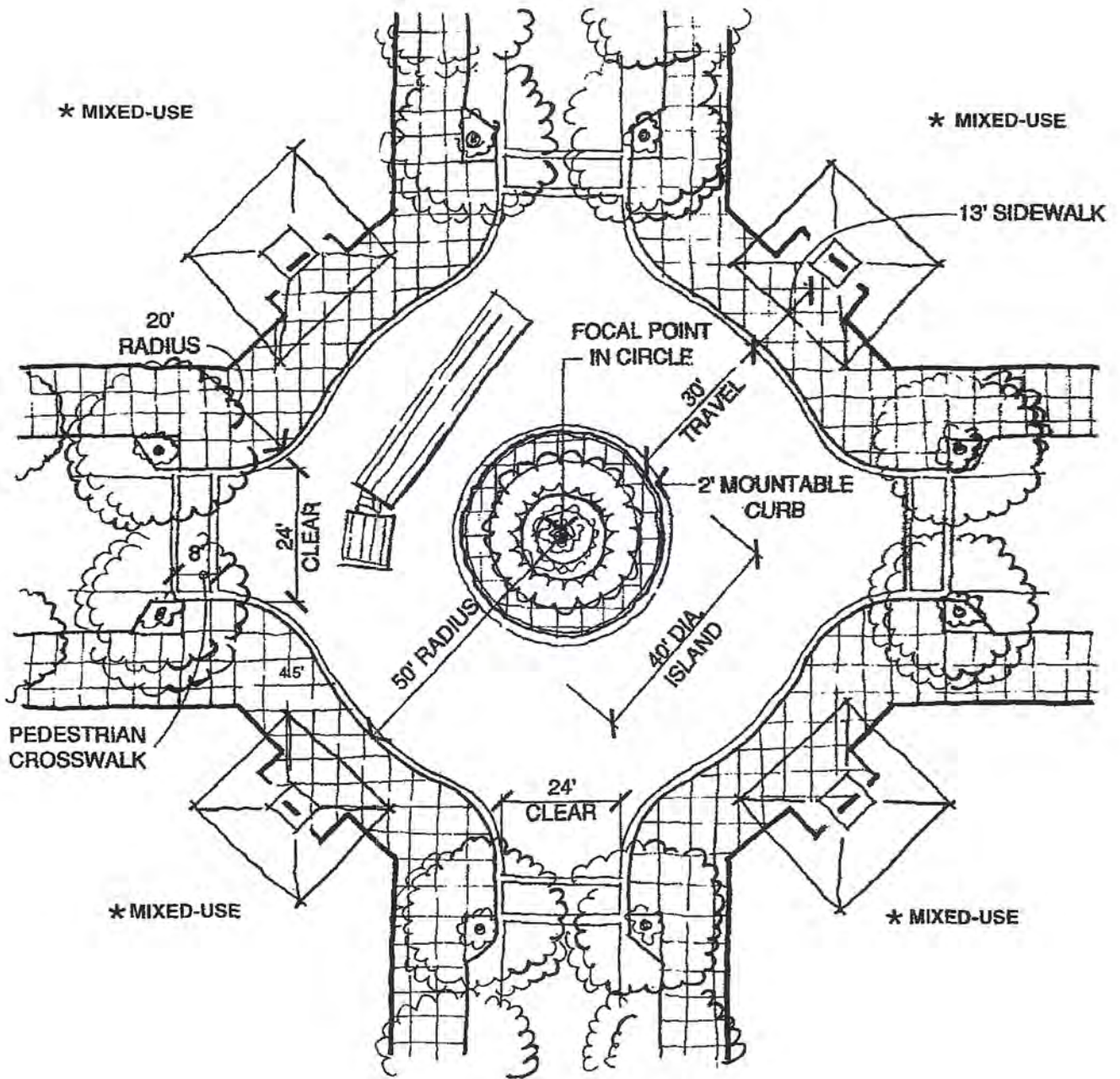






**5** **TRAFFIC CIRCLE**  
**@ CONNECTOR & NEIGHBORHOOD STREET**  
 CONCEPT SKETCH  
 (for illustrative purposes only)





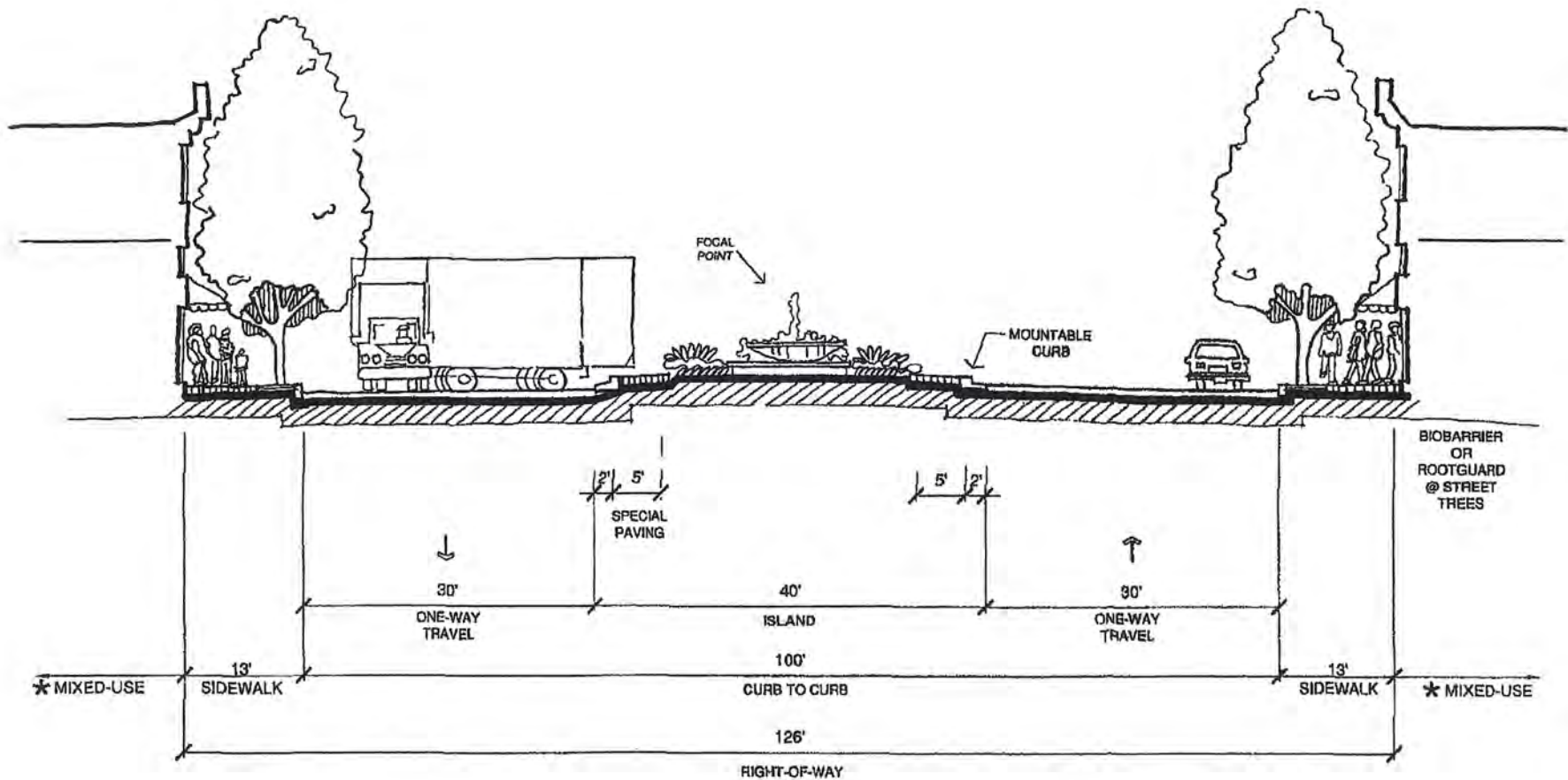
**6** TRAFFIC CIRCLE AT VILLAGE CENTER

CONCEPT SKETCH  
(for illustrative purposes only)

\* EDGE CONDITIONS / LAND USES SUBJECT TO CHANGE







7

### TRAFFIC CIRCLE @ VILLAGE CENTER

CONCEPT SKETCH  
(for illustrative purposes only)

\* EDGE CONDITIONS / LAND USES SUBJECT TO CHANGE

Traffic Calming Feature 8: Bulbouts on the Mixed Use Street

Exhibit IV-24 on page IV-37 provides a plan view of the mixed-use street, Cross Section 11. This exhibit clearly shows how bulbouts with shade trees will be used to calm traffic and provide a shady atmosphere for the mixed-use street in the Village Center, similar to that which exists along Grand Avenue in the older portion of Carlsbad. Most of the parking for this area will be provided in parking lots located behind the buildings.

Traffic Calming Feature 9 – Pedestrian Scale Street Lights

Pedestrian scale streetlights similar to those shown on Exhibit IV-25 on page IV-38 shall be provided in the residential areas, the mixed-use area and the entrance to the mixed-use area. These streetlights will create a more intimate, small-scale feeling to encourage pedestrian activity. These lights shall comply with all City standards for safety and illumination. The exact design and size of these lights is subject to approval of the Public Works Director and City Engineer.

Traffic Calming Feature 10 – Chicanes

A chicane is a series of two or more staggered curb extensions on alternating sides of the roadway, which causes motorists to reduce speed. As a traffic calming measure, concurrent with development of Planning Area 16, a chicane will be installed on Gateway Road between Innovation Way and Alicante Road, as well as on Gateway Road between Innovation Way and Village Green Drive. Exhibits IV-25.a. on pages IV-39 through IV-42 provide additional details.

**D. Corner Sight Distance Specifications**

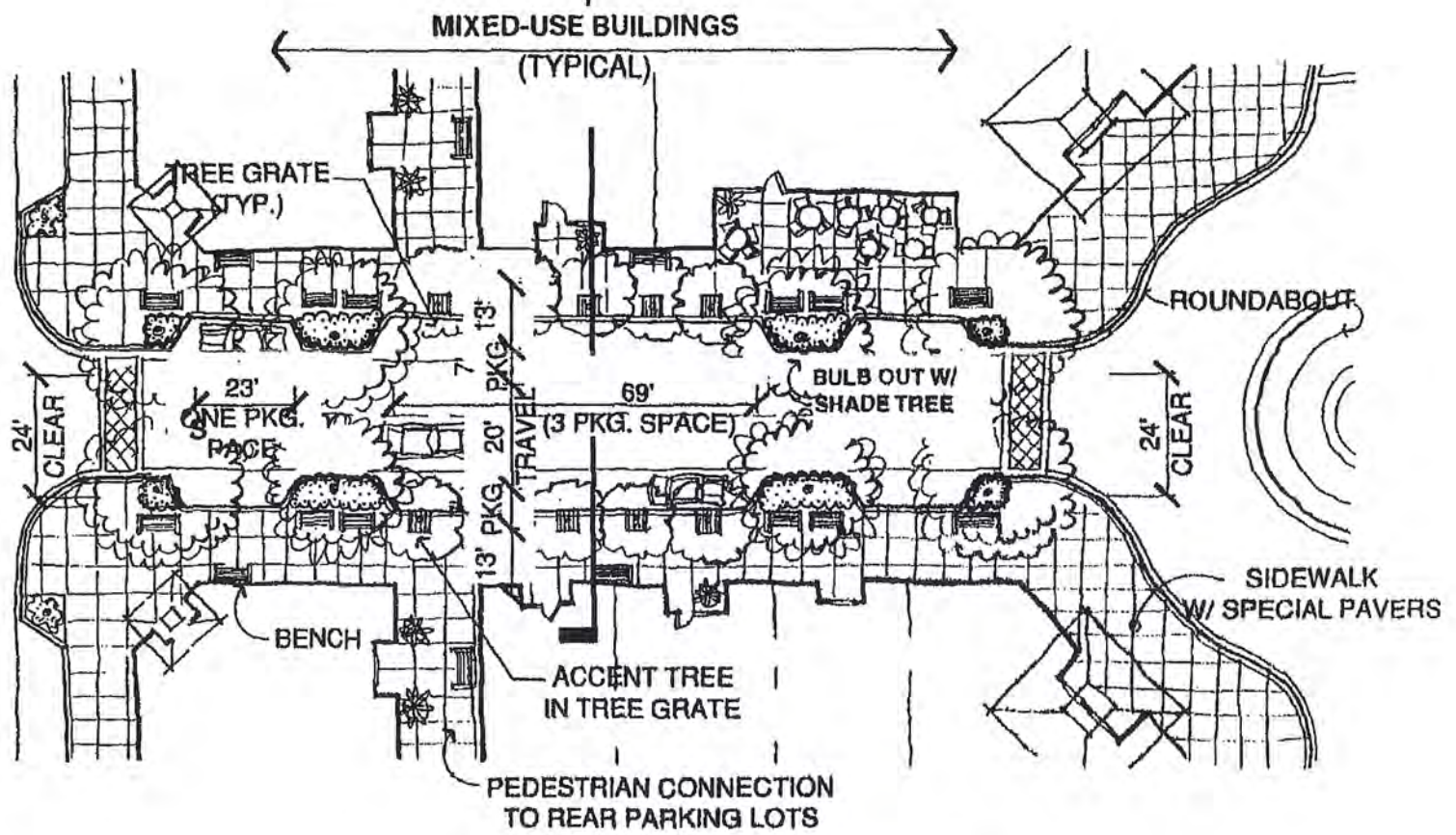
Roadway Sight Distance – Sight distances shall be in accordance with the current CalTrans Highway Design Manual.

Intersection Sight Distance – Intersection sight distance design will be governed by Topic 405 of the California Department of Transportation Highway Design Manual with the following additions and clarifications:

- For local/local and local/cul-de-sac intersections only, maintain triangular area at intersection, measured 25' back from the point of curb return (P.C.R.), to form a triangle free of any obstructions in excess of 30" in height. A sight/view corridor shall be established to restrict obstructions.
- Local/collector intersections and above follow CalTrans requirements. Signalized intersections must be designed with corner sight distance requirements.

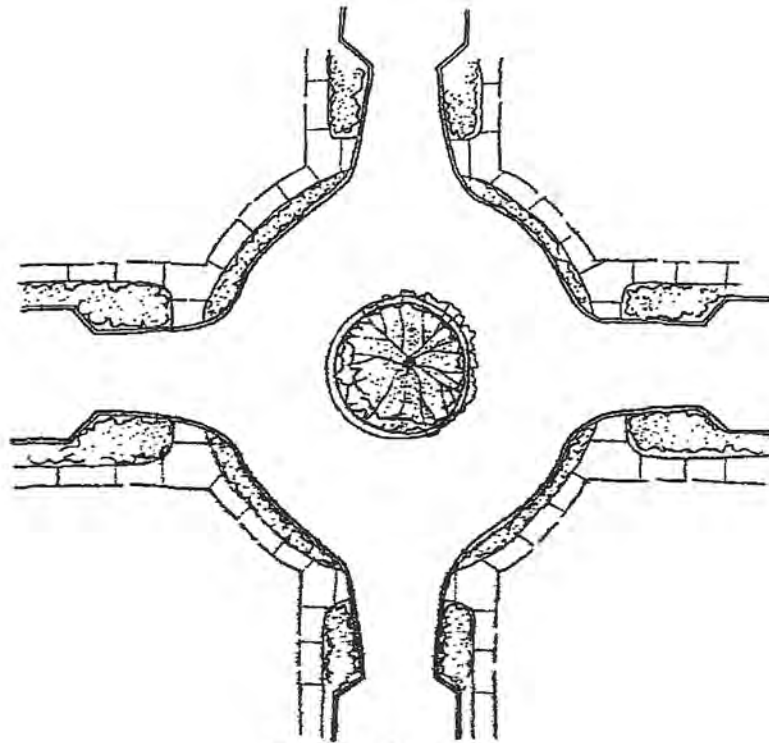
- The edge of travel way shall be considered to be the extension of the face of curb for the purposes of determining driver setbacks. Where temporary or interim connections are made to roads without curbs and gutters, the travel way shall be the edge of pavement.

**12** MIXED USE STREET SECTION - SEE MAP 1

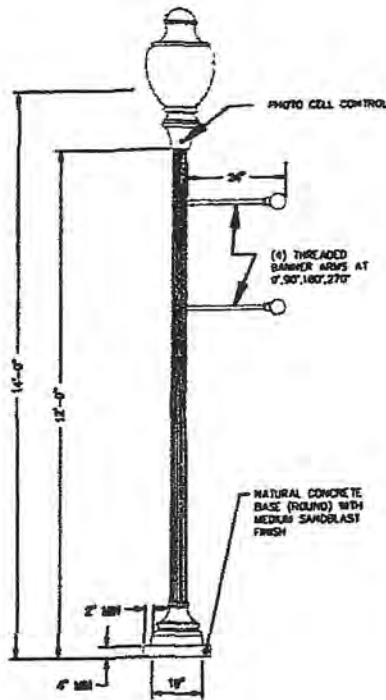


**8** MIXED USE STREET - PLAN VIEW  
 CONCEPT SKETCH  
 (for illustrative purposes only)





**Traffic Circle w/  
Island Planter**

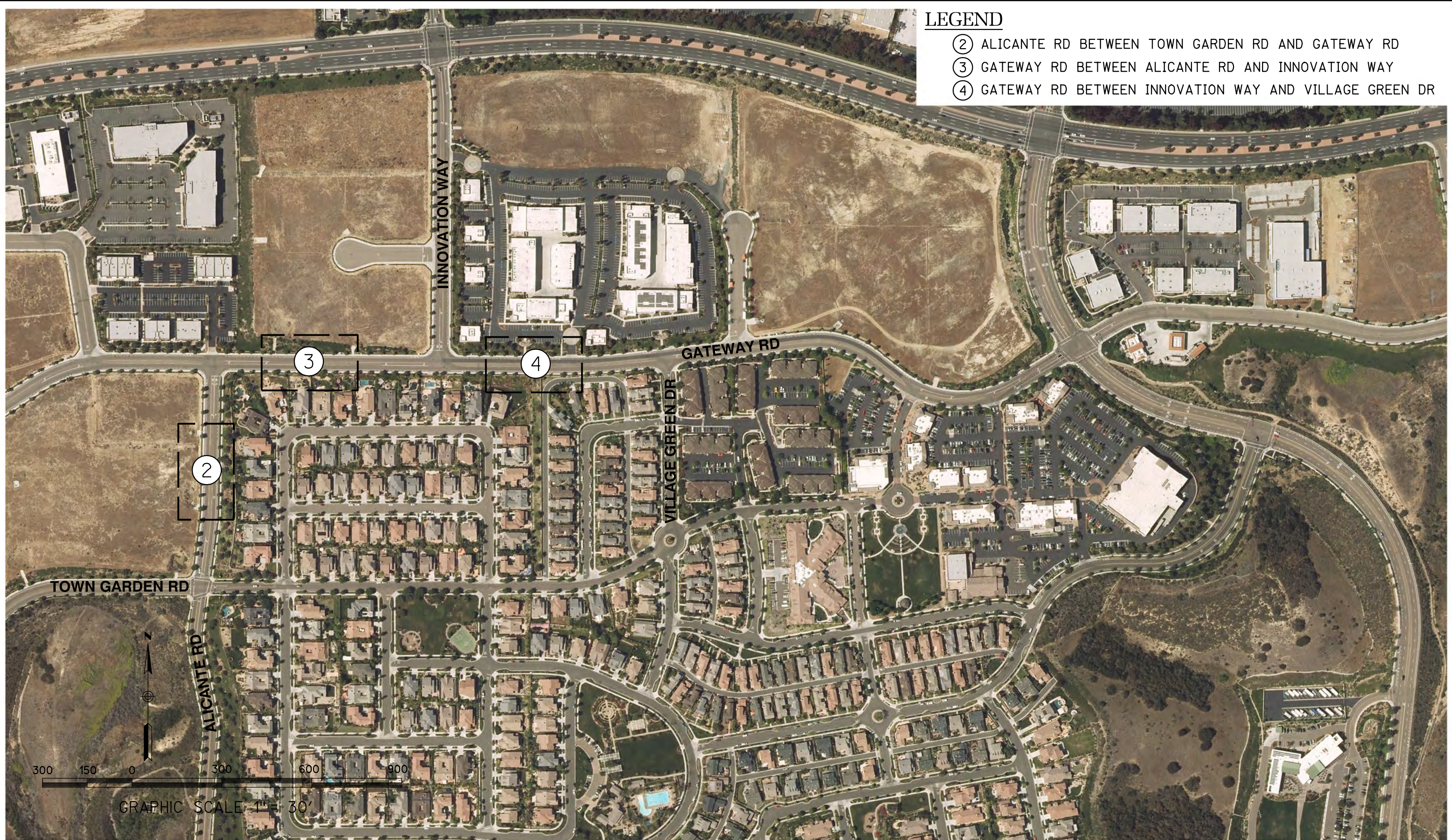


**Pedestrian Scale  
Streetlight**



**LEGEND**

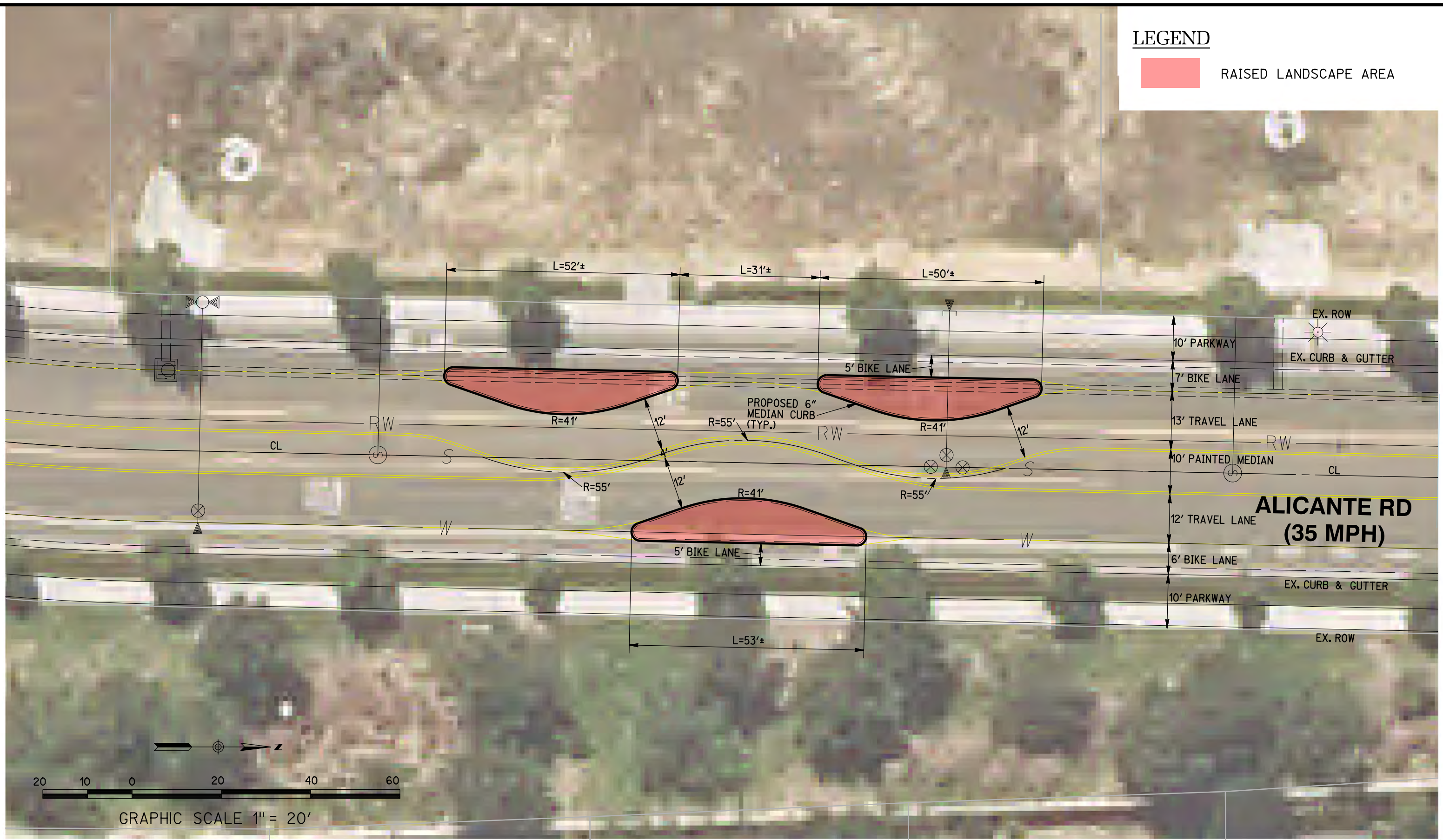
- ② ALICANTE RD BETWEEN TOWN GARDEN RD AND GATEWAY RD
- ③ GATEWAY RD BETWEEN ALICANTE RD AND INNOVATION WAY
- ④ GATEWAY RD BETWEEN INNOVATION WAY AND VILLAGE GREEN DR





**LEGEND**

 RAISED LANDSCAPE AREA



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11-25-15  
5620 FRIARS ROAD  
SAN DIEGO, CA 92110  
619-291-0707  
(FAX) 619-291-4165

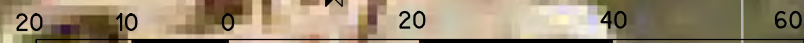
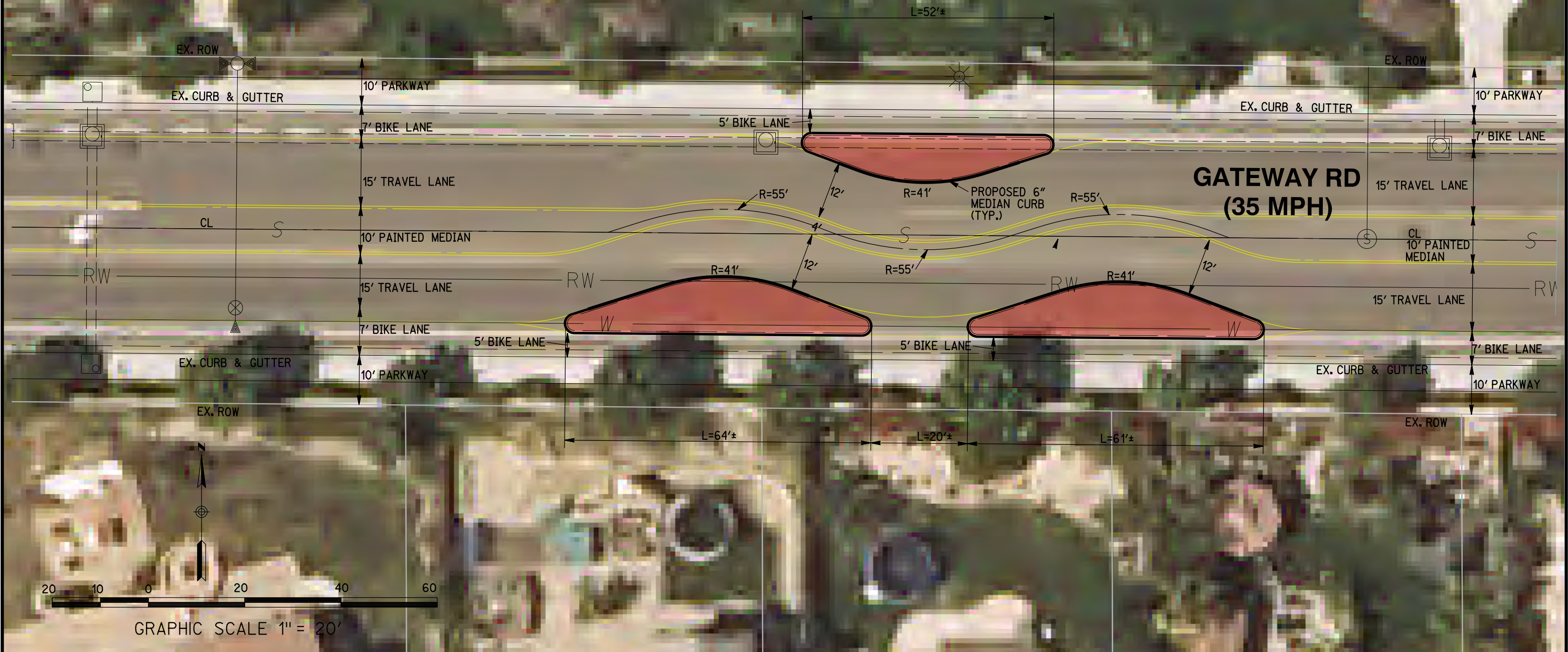
**ALICANTE RD BETWEEN  
TOWN GARDEN RD AND GATEWAY RD  
PROPOSED CHICANE**

H:\17169\CIVIL\Bressi\_Ranch\_Offsite\_Chicane\Alicante\_Gateway\_TownGarden.dgn  
H:\17169\CIVIL\Bressi\_Ranch\_Offsite\_Chicane\SD\_Corp\SDas\_2005.dscr ipt  
04-DEC-2015 11:35



**LEGEND**

 RAISED LANDSCAPE AREA



GRAPHIC SCALE 1" = 20'



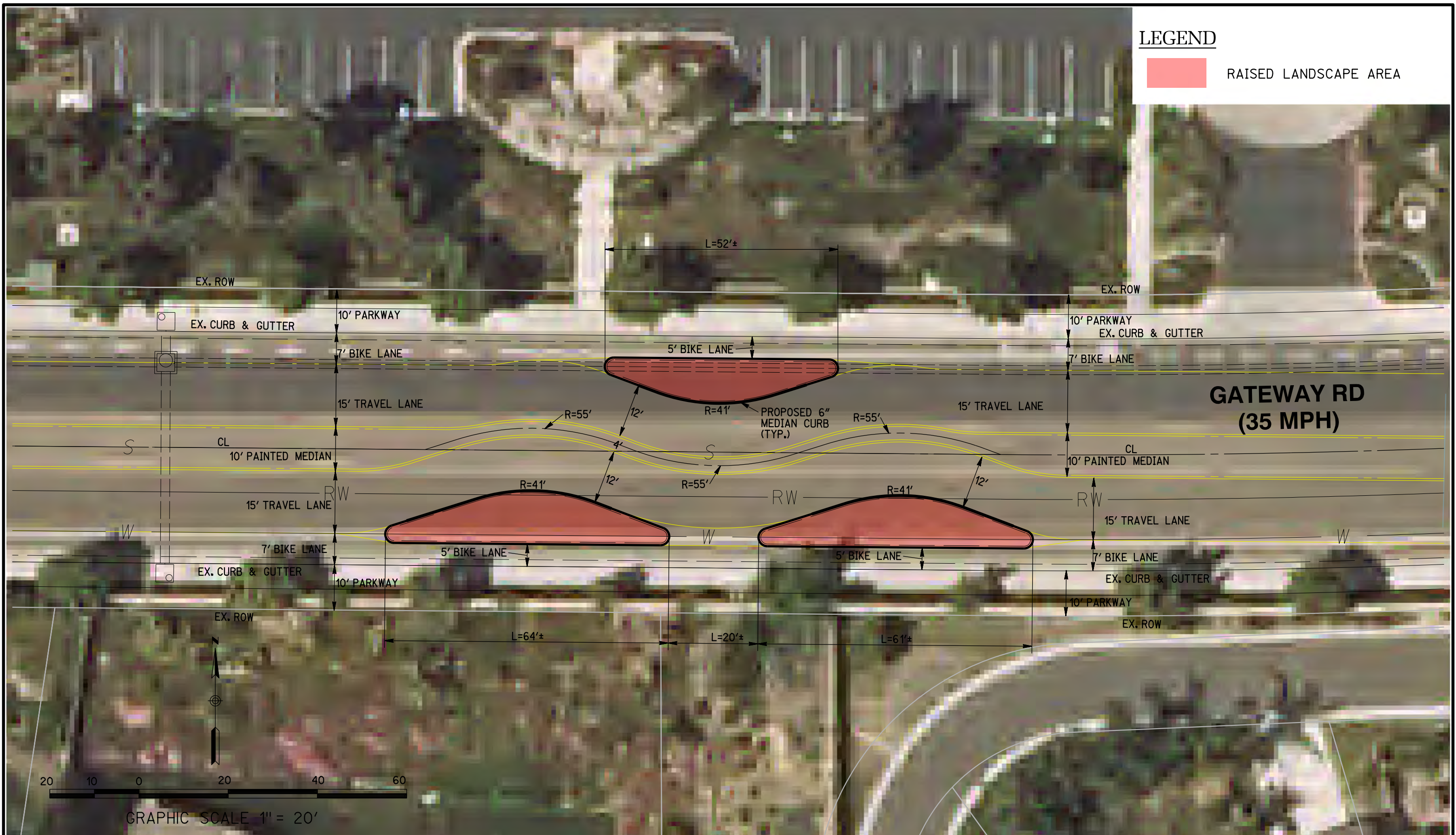
11-25-15  
 5620 FRIARS ROAD  
 SAN DIEGO, CA 92110  
 619-291-0707  
 (FAX) 619-291-4165

**GATEWAY RD BETWEEN  
 ALICANTE RD AND INNOVATION WAY  
 PROPOSED CHICANE**



**LEGEND**

 RAISED LANDSCAPE AREA



**GATEWAY RD  
(35 MPH)**

GRAPHIC SCALE 1" = 20'

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11-25-15  
5620 FRIARS ROAD  
SAN DIEGO, CA 92110  
619-291-0707  
(FAX) 619-291-4165

**GATEWAY RD BETWEEN  
INNOVATION WAY AND VILLAGE GREEN DRIVE  
PROPOSED CHICANE**

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H:\17169\CIVIL\Bressi\_Ranch\_Offsite\Chicane\SD\_Corp5\tds\_2005.dscr1.ppt  
04-DEC-2015 11:57



**E. Maintenance**

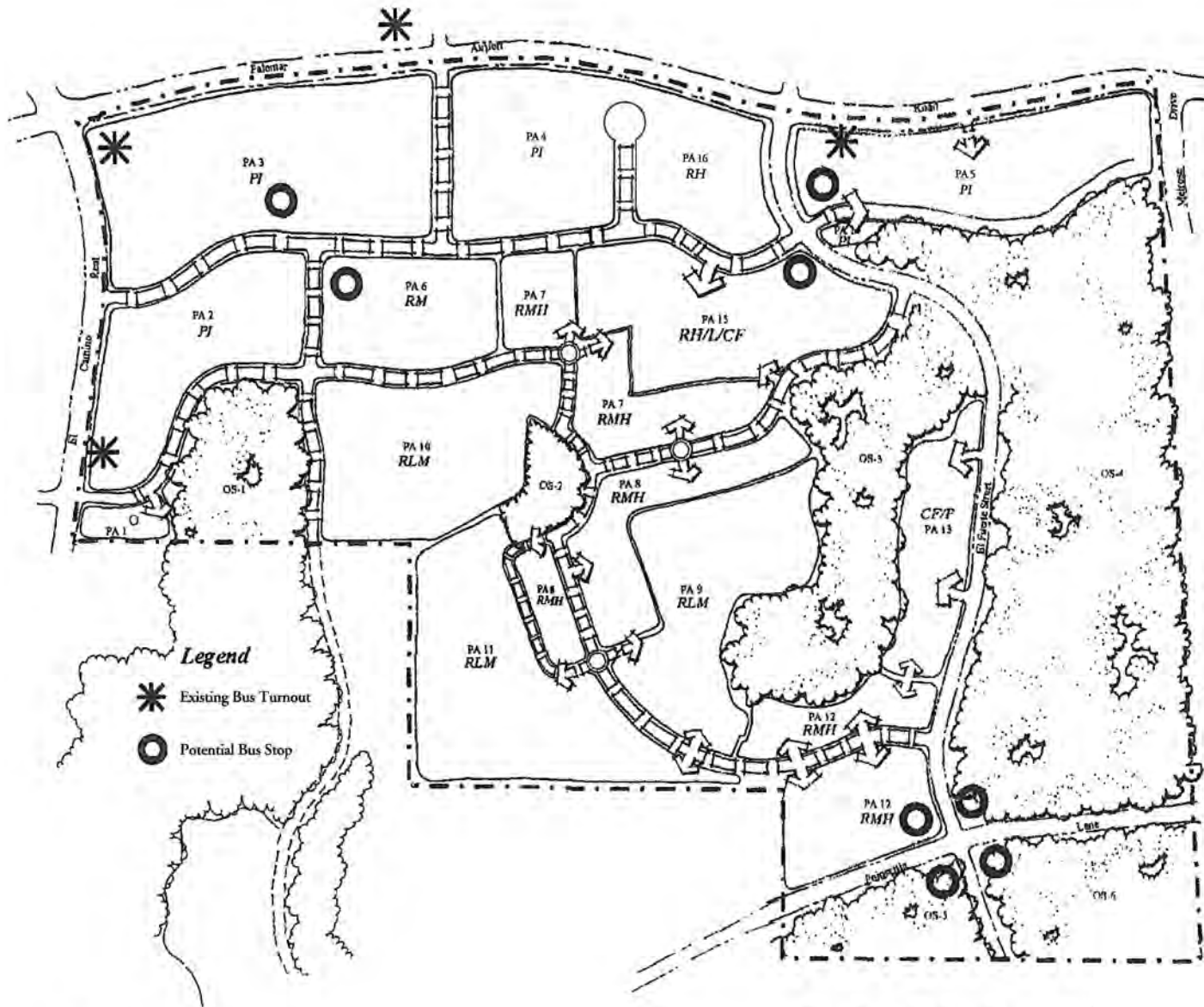
Except for Planning Area 11, which may be developed with a private street system, the streets and alleys within the Bressi Ranch Master Plan will be public. The City of Carlsbad will maintain the curb-to-curb portions of these streets and alleys and all sidewalks except for those areas with special pavement. The Homeowner's Association shall maintain all special pavement in public streets. A public access easement shall be provided for all portions of the public sidewalks that meander outside of the public right-of-way. The Bressi Ranch Master Community Association will be responsible for maintenance of all landscaping including trees within the public right-of-way of residential streets. A private landscape and maintenance agreement between the Community Association and the City shall be established for the maintenance of these areas. In the non-residential portion of the Master Plan this agreement shall be between the Owner's Association or management entity and the City. This shall be clearly delineated with text and exhibits in the Bressi Ranch CC&Rs and disclosed to all future residents of the Master Plan at the time of purchase. The private streets in Planning Area 11 will be maintained by a separate sub-association.

**F. Public Transit**



Due to its location at the intersection of two major transit corridors, Palomar Airport Road and El Camino Real, the Bressi Ranch Master Plan has been designed in accordance with the North County Transit District's (NCTD) principles for transit-oriented development. This serves to reduce traffic congestion by encouraging alternative modes of transportation such as walking and public transit. Based on input from representatives of the North County Transit District, the Bressi Ranch Master Plan provides bus turnouts and bus stops at the approximate locations shown by Exhibit IV-26 on Page IV-44.

These facilities will be located and designed in accordance with the NCTD's standards for bus turnouts and bus stops. The bus turnouts on Palomar Airport Road and El Camino Real consist of a turnout lane at least 80 feet past the adjacent intersection, as well as a bench, lighting and shelter. The bus stops are located in accordance with NCTD's standards for bus stop locations, which vary depending on the location of the stop in relation to the adjacent intersection.

The bus turnouts and facilities associated with them will be constructed concurrently with the development of the Master Plan to serve the existing bus routes along El Camino Real and Palomar Airport Road. Concrete pads to accommodate future benches, lighting and shelters will be provided at the bus stop locations as the Master Plan is developed, since it is unknown at this time



**Legend**

-  Existing Bus Turnout
-  Potential Bus Stop

*Bressi Ranch*

A planned community in the City of Carlsbad, California

*Bus Stops & Bus Turnouts*

IV-26



when bus service will be provided at these locations. Additional routes and bus stops may be provided as this Master Plan develops and may be modified without an amendment to the Master Plan.

One of the design guidelines for the industrial Planning Areas encourages future development to provide well-lighted pedestrian access to the bus turnouts and bus stops that will serve this area. Bus benches, lighting and shelters will be designed to reflect the architectural style of the Master Plan and will require the review and approval of the North County Transit District.

## **V. LANDSCAPE**

### **A. Landscape Concept**

Bressi Ranch is envisioned as a Garden District community, encompassing the entire community, including entries, streets and public spaces. The landscaping will complement and enhance the Smart Growth features of Bressi Ranch to create a pedestrian oriented community, (see Exhibit V-1 on page V-3).

#### Entries

At each main community entry, landscaping will occur at a grand scale to create the perception of a garden “room.” The streets will be bordered by canopy street trees and hedges to create a formal garden “promenade.” The central feature will be the Village Green, which will continue to be landscaped as a “formal garden” and will contain an architectural or garden feature to serve as a landmark.

#### Residential

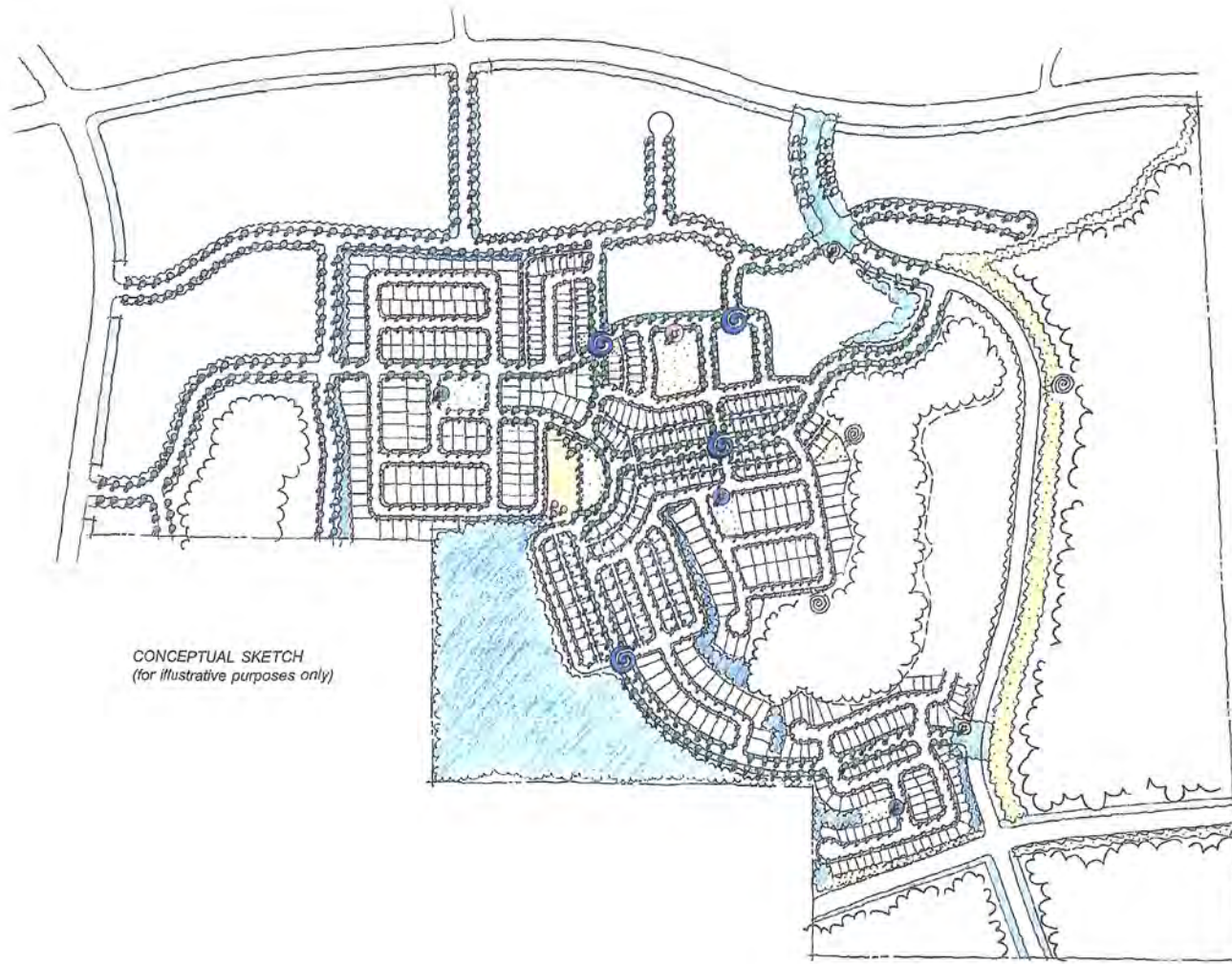
The landscape palette will transition into a less formal scheme within the residential planning areas. Streets within the neighborhoods shall continue to be lined with canopy trees, but these streets will be narrower and at a more intimate scale for the pedestrian. Some streets may lead to vistas of natural open space areas. Vertical trees, lawn parkways, terraced landscape walls and row type homes with alley garages will work to create an urban village environment. Neighborhoods may incorporate a garden theme or ornamental structure to serve as a neighborhood landmark.

#### Mixed Use Areas

The mixed-use portion of Planning Area 15 will be landscaped as a civic garden. The Village Square, with its large open space character, will be a central feature of the Village Center. A civic landmark may be located at the Village Square to serve as an identifying feature. Streets leading to the Village Square shall be lined with canopy trees to continue the “formal garden” theme from the surrounding major residential streets. The commercial uses within Planning Area 15 will contain a separate landscaping palette. The goal is to create a shopping atmosphere within a garden setting. An interactive feature such as a fountain, lookout tower or plaza may serve as a retail landmark.

The role of the landscape in Planning Area 16 is to integrate the project into the surrounding community while at the same time defining it as a unique place with its own special character. The landscape around the project’s outer edges draws from tree species established in the community master plan, which include

Sycamore and Evergreen Elms and supports those with a variety of spreading evergreen shrubs and groundcovers that blanket sloped areas and screen retaining walls. Within the project a combination of evergreen and deciduous flowering trees ensure that there will always be year-round interest. These trees are used to visually define pedestrian passages and create a consistent vocabulary between the commercial and residential portions of the project. A variety of large specimen trees will be used in the commercial area's multiple outdoor patio spaces and in the residential recreation areas to provide shade, scale and a sense of timelessness. A mixture of low and moderate water-use shrubs, groundcovers and ornamental grasses will be used in a simple combination of massings to provide visual/textural interest throughout the project and focus views where desired.



CONCEPTUAL SKETCH  
(for illustrative purposes only)

**LEGEND**

**Community Garden District**



**Community Entry**  
Perception of a garden "room"  
Grand scale but understated presence



**Community Landmark**  
Landmark as a piece of architecture or garden structure  
Memorable sense of arrival  
Meeting place that may tell a story



**Neighborhood Greens or Commons**  
Shared by individual neighborhoods  
Open lawn with special organized gardens



**Village Square**  
Open space for formal gatherings  
Grand scale framed by architecture



**City Wide Trail**  
Open space trail to be engaging



**Special Features**  
Interpretive reading areas



**Traffic Circle**  
Garden event at vehicular scale (specimen tree)



**Natural Open Space**





Industrial / Office

Entries into the industrial/office areas will have a less formal landscape scheme than those leading to the residential areas. Streets within the industrial garden shall be bordered by informal groves behind a contiguous sidewalk, with an evergreen backdrop to help screen views of industrial/office uses. The industrial/office buildings shall be designed and sited to encourage an outdoor walking experience for employees.

Natural Open Space

The streetscape adjacent to natural open space areas will be informal and provide a transition from natural vegetation to the street. Open and closed views of the open space shall occur along the length of the open space. Within the open space areas, trails and interpretive view decks shall be constructed to engage pedestrians in a “story telling experience”.

Arterial Roadways

The four major streets adjacent to and within the Master Plan (Palomar Airport, El Camino Real, El Fuerte Street and Poinsettia Lane) each have a predetermined landscape character as defined in the Carlsbad Landscape Manual. This pattern will be continued throughout Bressi Ranch on these arterials.

**B. Landscape Palette**

The following table in Exhibit V-2 on page V-5 provides the landscape palette for the Circulation Element roadways and internal roadways within the Bressi Ranch Master Plan. Palomar Airport Road, El Camino Real, El Fuerte Street, and Poinsettia Lane have established landscape patterns defined by the Carlsbad Landscape Manual. Except for Poinsettia Lane, all of these arterial roadways will be landscaped in accordance with the landscape patterns established by Carlsbad’s Landscape Guidelines Manual. Poinsettia Lane will have a different landscape palette to match the plantings within the portion of Poinsettia Lane that has already been constructed in the Rancho Carrillo Master Plan to the east. Trees used for the streetscape of individual planning areas are to be selected from the recommended plant list (see Exhibit V-3 on pages V-6 to V-8).

**C. Landscape Zones**

The general landscape zones for the Bressi Ranch community, as shown by Exhibit V-4 on page V-11, consists of four zones as specified in the City of Carlsbad Landscape Manual. These zones are based on the type of visual

**Exhibit V-2 Landscape Palette for Circulation Element Roadways**

**Palomar Airport Road**

Median Tree	Eucalyptus ficifolia – Red Flowering Gum
Theme Tree	Schinus molle – California Pepper Tree
Support Tree	Eucalyptus ficifolia – Red Flowering Gum, Oak Species

**El Camino Real**

Theme Tree	Platanus acerifolia ‘Bloodgood’ – London Plane Tree
Support Tree	Eucalyptus Species – Eucalyptus Tristania conferta – Brisbane Box

**El Fuerte**

Median Tree	Platanus racemosa – California Sycamore
Theme Tree	Platanus racemosa – California Sycamore
Support Tree	Arbutus marina - Madrone Schinus molle – California Pepper Tree Quercus agrifolia – Coast Live Oak Quercus ilex – Holly Oak

**Poinsettia Lane**

Median Tree	Melaleuca quinquenervia – Cajeput Tree
Theme Tree	Magnolia grandiflora ‘Majestic Beauty’ – Southern Magnolia
Support Tree	Brachychiton acerifolia – Flame Tree Stenocarpus sinuatus – Fire Wheel Tree

**Exhibit V-3 Bressi Ranch Recommended Plant Materials List****Bressi Ranch Recommended Trees List**

Botanical	Common
<i>Acacia melanoxylon</i>	Black Acacia
<i>Albizia julibrissin</i>	Silk Tree
<i>Alnus rhombifolia</i>	White Alder
<i>Arbutus 'Marina'</i>	Madrone
<i>Arbutus unedo</i>	Strawberry Tree
<i>Brachychiton populneus</i>	Bottle Tree
<i>Calodendrum capense</i>	Cape Chestnut
<i>Cinnamomum</i>	Camphor Tree
<i>Cassia leptophylla</i>	Gold Medallion Tree
<i>Cedrus Atlantica</i>	Atlas Cedar
<i>Cedrus Deodara</i>	Deodar Cedar
<i>Cercis Canadensis 'Forest Pansy'</i>	Forest Pansy Eastern Redbud
<i>Cercis occidentalis</i>	Western Redbud
<i>Cupaniopsis anacardioides</i>	Carrotwood
<i>Dracaena draco</i>	Dragon Tree
<i>Elaeocarpus dicipens</i>	Japanese Blueberry Tree
<i>Erythrina coralloides</i>	Naked Coral Tree
<i>Eucalyptus citriodora</i>	Lemon-scented Gum
<i>Eucalyptus cladocalyx</i>	Sugar Gum
<i>Eucalyptus leucoxylon</i>	White Ironbark
<i>Eucalyptus rudis</i>	Flooded Gum
<i>Eucalyptus viminalis</i>	Manna Gum
<i>Geijera parvifolia</i>	Australian Willow
<i>Ginkgo Biloba</i>	Maidenhair Tree
<i>Jacaranda mimosifolia</i>	Jacaranda
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree
<i>Lagerstremia indica x fauiei</i> varieties	Crape Myrtle
<i>Liriodendron tulipifera</i>	Tulip Tree
<i>Lophostemon confertus</i>	Brisbane Box
<i>Magnolia grandiflora 'Majestic Beauty'</i>	Southern Magnolia

Melaleuca quinquenervia	Cajeput Tree
Metrosideros excelsus	New Zealand Christmas Tree
Olea europaea 'Swan Hill'	Olive Tree
Pinus canariensis	Canary Island Pine
Platanus acerifolia 'Bloodgood'	London Plane Tree
Platanus racemosa	California Sycamore
Podocarpus gracilior	Fern Pine
Populus fremonti 'Nevada'	Poplar
Populus nigra 'Italica'	Lombardy Poplar
Prunus species	Rosaceae
Pyrus calleryana 'Aristocrat' and 'Bradfordii'	Ornamental Pear
Quercus agrifolia	Coast Live Oak
Quercus ilex	Holly Oak
Schinus molle	California Pepper
Sygarus romanzoffianum	Queen Palm
Tipuana tipu	Tipu Tree
Tristania conferta	Brisbane Box
Ulmus parvifolia 'True Green'	Chinese Elm
Zelkova serrata "village Green"	Village Green Zelkova

**Bressi Ranch Recommended Shrubs List**

Botanical	Common
Abelia grandiflora	Glossy Abelia
Agapanthus africanus	Lily of the Nile
Agave species	Agave
Aloe species	Aloe
Arbutus unedo	Strawberry Tree
Arctostaphylos 'Pacific Mist'	Manzanita
Arctostaphylos edmundsii	Little Sur Manzanita
Arctostaphylos hookeri	Monterey Manzanita
Arctostaphylos 'Howard McMinn'	Manzanita
Artemisia arborescens	Shrubby Worm Wood
Artemisia californica	California Sagebrush
Artemisia 'Powis Castle'	Sagebrush

Baccharis pilularis 'Pigeon Point'	Pigeon Point Coyote Bush
Bougainvillea 'La Jolla'	Bougainvillea
Buxus sempervirens	Boxwood
Calliandra inaequilatera	Pink Powder Puff
Callistemon citrinus	Bottlebrush
Ceanothus 'Concha'	Carmel Ceanothus
Ceanothus griseus	Carmel Ceanothus
Ceanothus griseus horizontalis 'Yankee Point'	Yankee Point Wild Lilac
Ceanothus 'Ray Hartman'	Wild Lilac
Cistus hybridus	White Rockrose
Cistus purpureus	Orchid Rockrose
Cordyline australis varieties	
Cotoneaster species	Cotoneaster
Dianella species	Flax Lily species
Dietes bicolor	Fortnight Lilly
Echium fastuosum	Pride of Madeira
Elaeagnus pungens	Silverberry
Elaeocarpus decipiens	Japanese Blueberry Tree
Encelia californica	California encilia
Escallonia species	Escallonia
Feijoa sellowiana	Pineapple Guava
Hemerocallis hybrids	Daylily
Lavandula species	Lavender
Leptospermum scoparium	New Zealand Tee Tree
Ligustrum Japonicum	Japanese Privet
Limonium perezii	Statice
Muhlenbergia rigens	Deer grass
Nandina domestica	Heavenly Bamboo
Nerium oleander	Oleander
Phormium tenax	New Zealand Flax
Photina species	Photinia
Pittosporum tobira	Mockorange
Prunus illicifolia	Catalina Cherry
Psidium Cattleianum	Strawberry Guava
Pyracantha species	Firethorn



Rhaphiolepis indica	India Hawthorn
Rhamnus alaternus	Italian Buckthorn
Romneya coulteri	Matilija Poppy
Rosa species	Rose
Rosemary species	Rosemary
Rhamnus californica	California Coffee Berry
Rhaphiolepis umbellate 'Minor'	Yeddo Hawthorn
Salvia apiana	White Sage
Salvia greggii	Autumn Sage
Salvia leucantha	Mexican Bush Sage
Salvia mellifera	Black Sage
Simmondsia chinensis	Bladderpod
Tecomaria capensis	Cape Honeysuckle
Viburnum species	Viburnum
Westringia fruticosa varieties	Coast Rosemary varieties
Xylosma congestum	Xylosma

**Bressi Ranch Recommended Vine List**

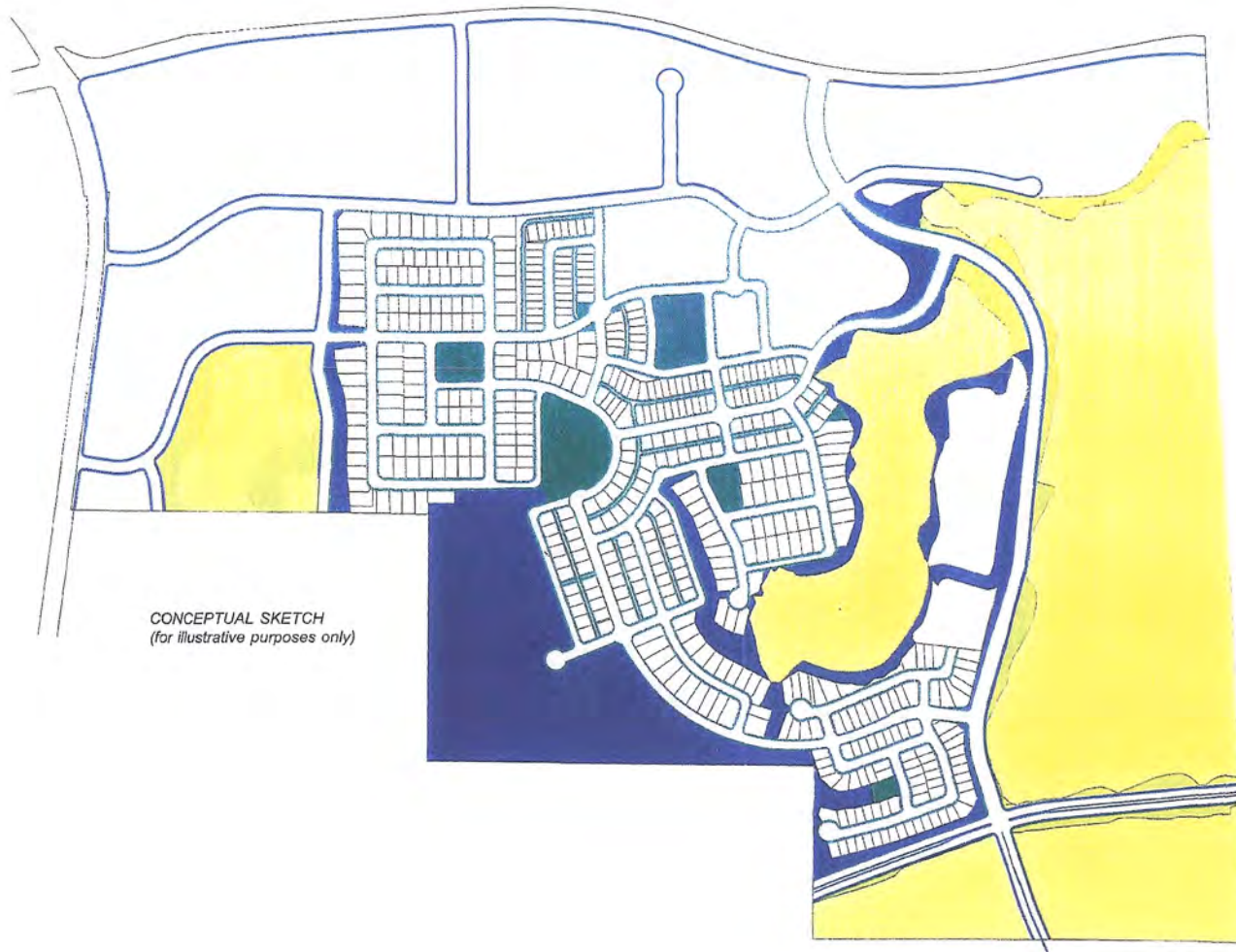
Botanical	Common
Distictus buccinatoria	Blood-red Trumpet Vine
Ficus repens	Creeping Fig
Hibbertia scandens	Guinea Gold Vine
Parthenocissus tricuspidata	Boston Ivy
Solanum jasminoides	Potato Vine
Trachelospermum jasminoides	Star Jasmine
Wisteria species	Wisteria

**Bressi Ranch Recommended Groundcovers List**

Botanical	Common
Acacia redolens	Acacia
Baccharis pilularis 'Twin Peaks' and 'Pigeon Point'	Dwarf Coyote Bush
Bougainvillea species	Bougainvillea
Coprosma kirkii	No common name

Cerastium tomentosum	Snow in summer
Gazania species	Gazania
Hedera helix	English Ivy
Hydroseed Mixes (vary)	Subject to approval
Iceplant species	Iceplant
Botanical	Common
Lantana montevidensis	Lantana
Myoporum pacificum	Myoporum
Pelargonium peltatum 'Balcan'	Ivy Geranium
Pyracantha 'Santa Cruz'	Firethorn
Rosa species	Rose
Rosmarinus officinalis 'Prostratus'	Dwarf Rosemary
Santolina species	Santolina
Verbena hybrids	Verbena
Vinca Species	Vinca

This is a recommended list for selection of plant materials. Other materials may be used subject to approval of the City Planner.



CONCEPTUAL SKETCH  
(for illustrative purposes only)

**LEGEND**

Symbol	Description
	Zone 1 - Lush
	Zone 2 - Refined
	Zone 3 - Naturalizing
	Zone 4 - Native



appearance, environmental sensitivity, and the amount of maintenance and irrigation required. The plant materials recommended for use in these landscaping zones are provided in Exhibit V-3 on pages V-6 to V-8.

#### Zone 1- Lush

This landscape type is used to attract attention with lush, colorful, and well-manicured plantings. Zone 1 landscaping is utilized at some area of high visual impact such as entries and the recreation area. Lawn areas may be included in this category. Unless the lawn area is designed for recreational use or is a focal area, such as entries or parkways, it will be limited. The Village Square and Village Green are good examples of where Zone 1 plantings can and should be utilized.

#### Zone 2- Refined

This landscape type is less manicured but still has an orderly and attractive appearance. Zone 2 plantings will be limited to areas of secondary visual importance, which promote less maintenance and lower water requirements. The plants in this zone tend to lean more to the drought-tolerant side. The water requirement (once established) is estimated to be 10 to 30 inches of supplemental water. Plant selection should be sensitive to maintenance and water conservation issues. As an example, some streetscapes and interior slopes will be of the refined type.

#### Zone 3- Naturalizing

This landscape type is directed towards little or no maintenance, low water consumption and more natural appearance. Zone 3 will be used on the edges of natural areas, in fire suppression zones and on a percentage of the large manufactured slopes. The irrigation requirements of this zone after establishment are less than 10 inches annually, giving it the ability to sustain itself through the wet/dry seasons with very little supplemental water.

#### Zone 4- Native

This landscape type is native vegetation that has little or no disturbance. These native areas are to remain intact and will become the nucleus for the expanded open space areas. The hillsides should reflect the vegetation typically found in this setting: coastal sage scrub and mixed chaparral. At the discretion of a qualified biologist, native ornamentals may be planted in areas previously disturbed by agricultural activities. These areas will be disturbed only if necessary for fire protection as directed by the Fire Marshal.

#### **D. Irrigation**

All irrigation shall conform to the City of Carlsbad Landscape Manual, Carlsbad Municipal Water District Requirements and County of San Diego Environmental Health requirements.

When a source of recycled water becomes available it will serve some of the irrigation needs in the Bressi Ranch Master Plan. Development of the Master Plan will help provide portions of the system that will provide useable reclaimed water. Because use of this important resource is imminent, selection of plant palettes, irrigation equipment and management practices will need to consider the unique requirements of watering with this resource. The irrigation system shall be designed to accept recycled water.

All systems shall be designed with “hydrozonation” techniques, grouping similar plants, similar sun and shade exposures, similar topography, and similar soils together. Runoff and over spray shall be minimized. Automatic controllers, with multiple cycle start and a flexible calendar program, shall be installed on all irrigation systems. Moisture sensors (or hydrometers) and rain shut-off devices may also be required. Irrigation should be restricted to morning and evening hours except during the initial plant establishment periods.

Irrigation systems shall be designed so that separate areas of maintenance responsibility (i.e. City, Community Association, Commercial Association, Industrial Association) are metered and controlled independently.

Equipment shall be located and installed to minimize negative visual impact. Low precipitation sprinkler heads should be utilized for optimum coverage and maximum water conservation.

#### **E. Brush Management Slopes**

Approximately a third of the area of the Bressi Ranch Master Plan will consist of native vegetation. This could create a potential fire hazard to structures within the Master Plan if not treated properly. Exhibit V-5 on page V-14 shows where there may be an interface between native vegetation and structures. Based on input from the Carlsbad Fire Department, the Bressi Ranch Master Plan proposes to provide a separation between native vegetation and residential structures that exceeds the requirements of Carlsbad’s Landscape Manual. The brush management slopes shown along the southerly and westerly edges of Planning Areas 10, 11 and 12 represent a possible interim condition that may occur if homes are constructed in these Planning Areas prior to grading and development of Neighborhoods 1.6 and 1.7 of the Villages of La Costa Master Plan. If the neighborhoods in the La Costa Master Plan are developed prior to or





CONCEPTUAL SKETCH  
(for illustrative purposes only)

### LEGEND

Symbol	Description
	<b>Manufactured Slopes</b> Downslope with Zone 1 beginning at top of slope.
	<b>Manufactured Slopes</b> Downdslope with Zone 1 beginning at face of building.
	<b>Manufactured Slopes</b> Upslope with Zone 1 beginning at bottom of slope.

### Manufactured Slopes Brush Management - (S) 20 Zones

- ZONE 1 - Irrigated.  
No trees or shrubs.  
Only very low fire retardant groundcover.
- ZONE 2 - Irrigated. No trees.  
Naturalizing plant species.  
Low fuel characteristics.
- ZONE 1 - Irrigated. Trees no closer than 20' apart.  
Naturalizing plant species known to have  
low fuel characteristics.

Fuel Modification Zones subject to change per final grading plans.



concurrent with Planning Areas 10, 11 and 12 of the Bressi Ranch Master Plan, brush management will not be required in these Planning Areas as shown on Exhibit V-5 on page V-14 because no native slopes will remain between the homes in the Rancho Carrillo Master Plan and Villages of La Costa Master Plan.

1. Residences Adjacent to Slopes with Native Vegetation

The City's Landscape Manual requires a minimum sixty-foot separation between native vegetation and residential structures. This separation is divided into three, 20-foot sections, the first of which usually starts at the edge of the structure. The sixty-foot Brush Management Zone for Bressi Ranch will start at the edge of the building pad instead of the edge of the building. Exhibits V-6 and V-7 on pages V-16 and V-17 show how these fire suppression areas will be planted on native slopes and manufactured slopes in the Bressi Ranch Master Plan.

2. Nonresidential Uses Adjacent to Slopes with Native Vegetation

Where nonresidential uses occur adjacent to a slope with native vegetation, the guidelines contained in the City's Landscape Manual will be followed. For nonresidential uses, the fire setback shall start at the edge of the building.

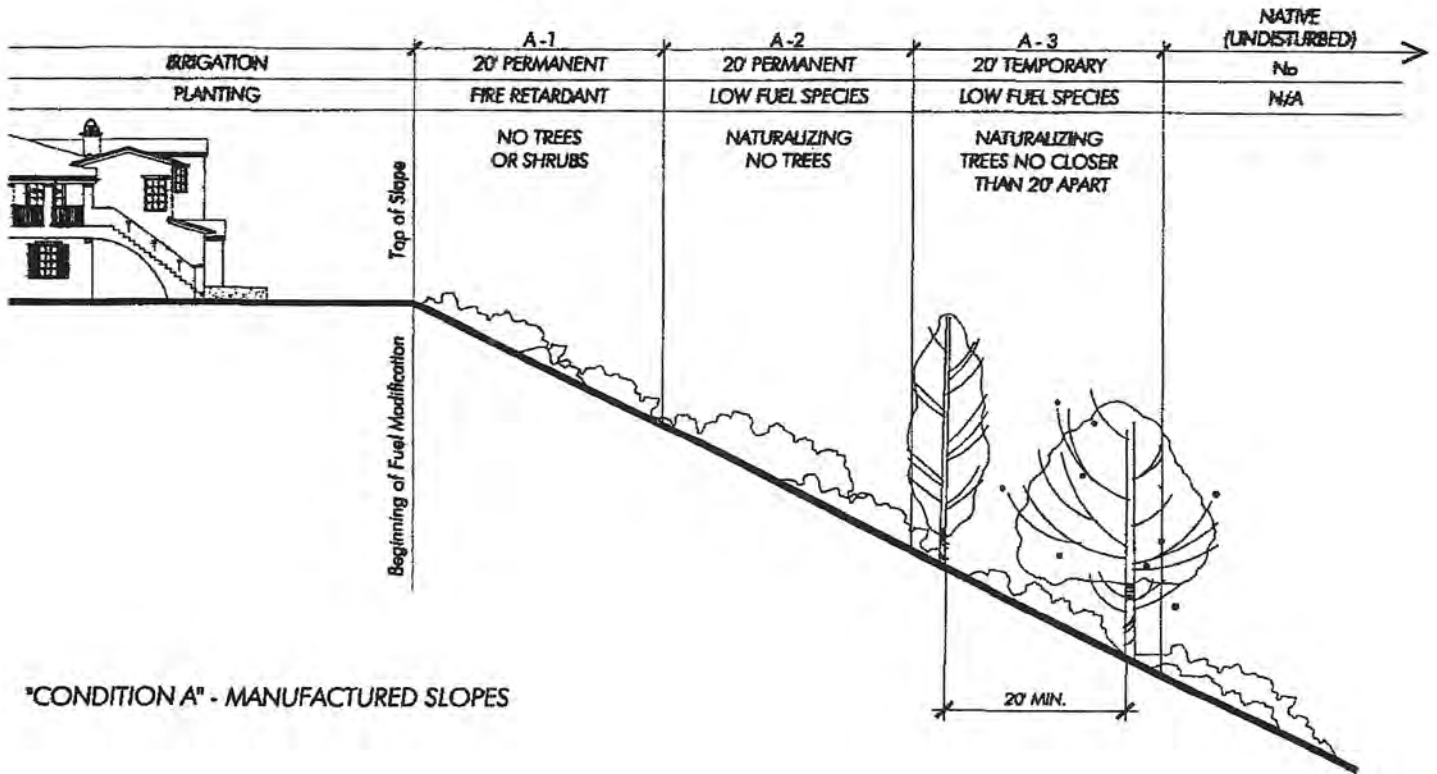
The text below provides a written summary of the planting required in the three suppression areas:

3. Manufactured Slopes

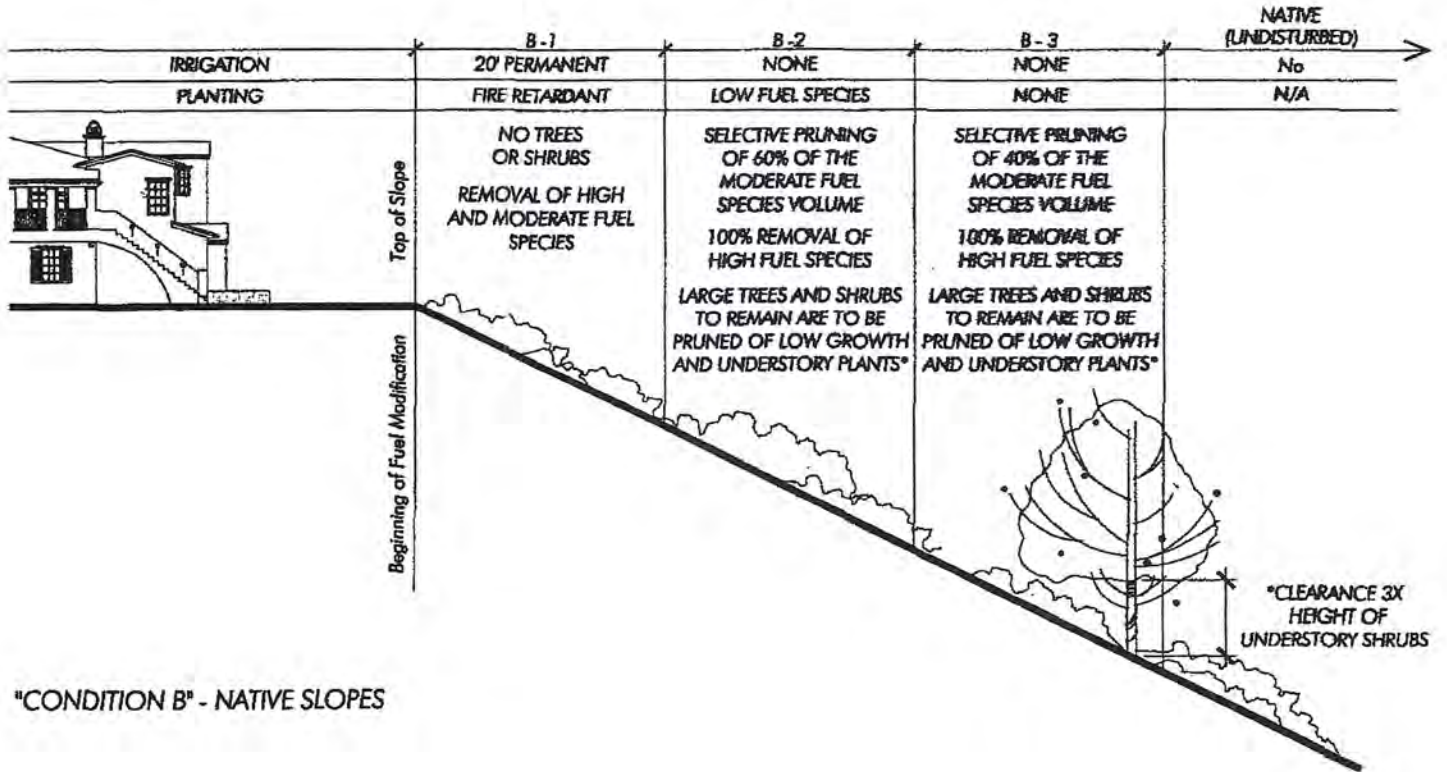
Zone A-1 is the area that starts at the edge of the pad for residences adjacent to manufactured slopes with native vegetation. This area extends from the edge of the building pad out 20 feet into the high-risk fire areas. This zone is to be planted with fire retardant groundcover and an irrigation system is required. No trees or shrubs are to be planted in this zone.

Zone A-2 is the area that extends from the outer most boundary of Zone A-1 an additional 20 feet into the high-risk area. This zone is irrigated and planted with fire retardant groundcovers and shrubs. No trees are to be planted in this zone.

Zone A-3 is the area that extends out from the boundary of Zone A-2 an additional 20 feet into the high-risk area. This zone is to be irrigated and planted with fire retardant groundcovers, shrubs, and trees. Trees should be a minimum of 20 feet apart.



"CONDITION A" - MANUFACTURED SLOPES



"CONDITION B" - NATIVE SLOPES

#### 4. Native Slopes

Zone B-1 is the area that starts at the edge of the pad for residences adjacent to slopes with native vegetation and extends into the high-risk fire area 20 feet. This area is to be cleared of all high fuel and moderate fuel species and revegetated with fire retardant groundcovers and low shrub species. This zone is to be irrigated.

Zone B-2 extends out 20 feet from the boundary of Zone B-1. This zone needs to be completely cleared of all high fuel species and selectively pruned of up to 60% of the volume of the moderate fuel species. Revegetation shall consist of fire retardant species planted by hydroseeding. Any trees and large shrubs are to be pruned of low growth and under story plants. This zone shall have one row of heads spraying into the zone.

Zone B-3 extends 20 feet from the boundary of Zone B-2. This zone needs to be completely cleared of all high fuel species and selectively pruned of up to 40% of the volume of the moderate fuel species. Remaining trees and shrubs shall be pruned of low growth and under-story plants. This zone is not revegetated or irrigated unless clearing leaves bare areas that require erosion control planting.



## **VI. DEVELOPMENT REVIEW PROCESS**

Individual planning areas within this Master Plan shall be reviewed relative to the provisions of this chapter, State Law, Carlsbad's General Plan, Municipal Codes, and City Standards, Policies and Guidelines.

### **A. All Planning Areas**

#### Tentative Maps

A tentative map or parcel map processed pursuant to Titles 20 and 21 of the Carlsbad Municipal Code must be processed to subdivide any of the Planning Areas in the Master Plan. All proposed tentative maps and parcel maps must conform to and implement the Master Plan. Any application that does not implement the Master Plan shall not be approved.

#### Residential or Non-Residential Planned Development Permits

A residential or non-residential Planned Development Permit shall be processed concurrently with a tentative map or parcel map that proposes lots smaller than that allowed by the underlying zoning, air space condominium ownership or access to lots from a private street or driveway. Residential Planned Development Permits shall be processed per the requirements of Chapter 21.45 of the Carlsbad Municipal Code. Non-residential Planned Development Permits shall be processed per the requirements of Chapter 21.47 of the Carlsbad Municipal Code.

#### Parcel Maps

Parcel maps (with or without a Planned Development Permit) that create four or fewer lots shall be approved by the City Engineer and City Planner.

#### Tentative Maps

Tentative maps (with or without a Planned Development Permit) shall be reviewed by the Planning Commission per Section 20.12.090 of the Carlsbad Municipal Code.

### **B. Office and Industrial Planning Areas (PA 1-5 & 14)**

One or more of the following administrative or discretionary applications shall be filed either concurrently with or subsequent to the application for a tentative map in Planning Areas 1-5 & 14.

#### Site Development Plan

A Minor Site Development Plan processed pursuant to Chapter 21.34, Planned Industrial Zone of the Carlsbad Municipal Code, shall be obtained for the development of any permitted use in Planning Areas 2, 3, 4, 5 & 14. Minor Site Development Plans shall be administratively reviewed and approved by the City Planner. All development proposed by a Minor Site Development Plan shall be in conformance with the requirements of the Planned Industrial Zone, the Development Standards and Design Criteria of the Bressi Ranch Master Plan and CC&Rs and Design Guidelines for the Industrial area of the Master Plan. If there is a conflict in the standards or requirements of the abovementioned ordinances or policies, the requirements of the Bressi Ranch Master Plan shall prevail.

#### Non-Residential Planned Development Permit

A Non-Residential Planned Development Permit in conformance with the requirements of Chapter 21.47 of the Carlsbad Municipal Code shall be processed concurrently with all tentative maps that propose lots that do not front on a public street, postage stamp lots, air space condominiums or lots/units that share access or parking.

#### Conditional Use Permits

A Conditional Use Permit shall be required for all uses designated as conditional uses by Chapter 21.34 and 21.42 of the Carlsbad Municipal Code. The review and approval of a Conditional Use Permit shall include building placement and design.

The approval of a Non-Residential Planned Development Permit or Conditional Use Permit, both of which address building placement and design shall supercede the need for approval of a Minor Site Development Plan.

#### El Camino Real Scenic Corridor Special Use Permit

El Camino Real has been designated as a scenic corridor and a set of standards that applies to property within certain distances of this roadway has been adopted. Planning Areas 1, 2 and 3 along the western portion of the Master Plan are subject to the El Camino Real Scenic Corridor Guidelines. Those guidelines require the review and approval of a special use permit to assure compliance with the guidelines. Development of Planning Areas 1, 2 and 3 will require approval of a special use permit as buildings are proposed to ensure that they meet the requirements of the El Camino Real Scenic Corridor Study.

### **C. Residential Planning Areas (PA 6-12)**

In addition to an application for a parcel map or tentative map, development in the residential planning areas shall process either a Planned Development

Permit, Site Development Plan or building permit addressing building placement, floor plans and elevations.

Site Development Plan – Standard Lots

Tentative maps in Planning Areas 6-10 and 12 in which all lots have a minimum size of 7,500 square feet or larger shall process a Site Development Plan addressing the building placement on the lots, floor plans and elevations. This Site Development Plan may be processed concurrently with or after the approval of the proposed tentative map, but prior to issuance of any building permits. If processed after the approval of the tentative map, the Site Development Plan shall be processed pursuant to Chapter 21.06 of the Carlsbad Municipal Code. Except as indicated below, the Planning Commission shall be the final decision making body for any Site Development Plan processed after the approval of the tentative map, regardless of the number of units in the previously approved tentative map unless appealed. The final decision making body shall make the finding that the proposed floor plans and elevations are in conformance with the Bressi Ranch Residential Development Standards and Design Guidelines and the appropriate Planning Area Development Standards and Design Guidelines. Any conditions placed on the Site Development Plan shall be limited to dealing with architecture and floor plans only.

If Planning Area 11 is developed under a custom lot sales program, individual site plans for each lot in that planning area shall be submitted to the City Planner for approval. The City Planner's review shall determine if the proposed structures are consistent with the Master Plans architectural design guidelines. The review of five or more homes may be processed under a single Site Development Plan as described in the previous paragraph.

Planned Development Permits – Lots Less Than 7,500 Square Feet and Lots Gaining Access From a Private Street

All tentative maps that will create residential lots less than 7,500 square feet in size, create condominiums or have homes gaining access from a private street must process a Planned Development Permit pursuant to Chapter 21.45 of the Carlsbad Municipal Code concurrently with the Tentative Map.

Tentative Maps creating duplex projects or single family detached projects on lots less than 7,500 square feet but greater than 5,000 square feet in size may use either of the following review processes subject to the desire of the applicant.

1. Standard Procedure

Use of the standard procedure where floor plans and elevations are processed concurrently with the Tentative Map and Planned Unit Development or Condominium Permit.

OR

2. Delayed Architectural Review

Tentative Maps and Planned Unit Development Permits may be processed through Planning Commission and City Council without floor plans or elevations.

Prior to issuance of a building permit, the applicant shall submit floor plans and elevations for Planning Commission review and approval as a Major Planned Unit Development Permit Amendment in conformance with the requirements of Chapter 21.45 of the Carlsbad Municipal Code.

Except as indicated below, the Planning Commission shall be the final decision making body for any Site Development Plan processed after the approval of the tentative map, regardless of the number of units in the previously approved tentative map unless appealed. The Planning Commission's review shall focus on architecture only, and the previously approved tentative map shall not be opened for review and no new conditions shall be added to the tentative map. Any new conditions added to the resolution approving the Planned Unit Development Permit shall be limited to dealing with architecture and floor plans only.

Tentative maps accompanied by Planned Development Permits that create lots less than 5,000 square feet in size shall only be processed per the Standard Procedure which requires that floor plans and elevations be processed and reviewed concurrently with the tentative map and Planned Development Permit.

Conditional Use Permit

A Conditional Use Permit shall be required for all uses designated as conditional uses by Chapters 21.24 and 21.42 of the Carlsbad Municipal Code. The approval of a Conditional Use Permit, which also addresses building placement and architecture, supercedes the need to process a Site Development Plan.

**D. Community Facilities (PA 13)**

Conditional Use Permits

A Conditional Use Permit shall be required for community facilities per Chapter 21.25 of the Carlsbad Municipal Code. The Conditional Use Permit shall also address building placement, architecture, parking and access.

Site Development Plan

A Site Development Plan shall be required for a stand-alone day care center in Planning Area 13 per the requirements of Chapter 21.25 of the Carlsbad Municipal Code.

**E. Mixed Use and Community Facilities (PA 15 & 16)**

Planned Development Permits

Subdivision of non-residential units for the purpose of separate ownership shall be processed in accordance with Chapter 21.47 “Non-Residential Planned Developments” of the Carlsbad Municipal Code and is subject to the Design Guidelines and Development Standards specified for Planning Areas 15 & 16.

Subdivision of residential units for the purpose of separate ownership shall be processed in accordance with Chapter 21.45 “Planned Developments” of the Carlsbad Municipal Code and are subject to the Design Guidelines and Development Standards specified for Planning Areas 15 & 16.

Conditional Use Permits

A Conditional Use Permit shall be required for churches, assisted care facilities, service stations, drive thru facilities and community facilities as well as all other uses designated as allowed by conditional use permit in Chapters 21.24, 21.25, 21.28 and 21.42. The Conditional Use Permit shall also address building placement, architecture, parking and access and will supersede the need for a Site Development Plan.

Site Development Plan

A Site Development Plan shall be required for all uses that do not require a Conditional Use Permit, Planned Development Permit or non-residential Planned Development Permit and are allowed by Chapters 21.24, 21.25 and 21.28 including, but not limited to, apartments, daycare centers, commercial development and a Village Square.



**F. Open Space Areas (OS 1-6)**

Conditional Use Permits

A Conditional Use Permit approved by the Planning Commission shall be required for the site design and any structures proposed in Open Space Area 2 (Village Green).

A Conditional Use Permit shall be required for all uses that are not prohibited by deed restriction in Open Space Areas 1 and 3-6 designated as conditional uses by Chapter 21.33 of the Carlsbad Municipal Code. The Conditional Use Permit shall address building placement, architecture, parking and access.

Site Development Plan

Any use or structure that is not prohibited by deed restriction and designated as a permitted use by Section 21.33 of the Carlsbad Municipal Code shall require approval of a Site Development Plan, unless approved as part of a Tentative Map or Conditional Use Permit, in which case a Site Development Plan will not be required.

Special Use Permit

A small portion of Open Space Area 5 falls within the Flood Plain Overlay Zone. Any grading or development that occurs in that area shall require the approval of a Special Use Permit in accordance with the requirements of Chapter 21.110 of Carlsbad's Municipal Code.

**G. Amendments**

Master Plan Amendments

Master Plan Amendments shall be processed pursuant to the requirements of Chapter 21.38.120 (P-C Zone) of the Carlsbad Municipal Code and all applicable City and Planning Division Policies. Minor Master Plan Amendments shall be reviewed and approved by the Planning Commission. Major Master Plan Amendments shall be reviewed and approved by the Planning Commission and City Council. A Minor Master Plan Amendment shall not change the densities of a planning area, the boundaries of the subject property, or involve an addition of a new use or group of uses not shown in the original Master Plan or rearrangement of uses within the Master Plan.

The Master Plan provides an estimate of the approximate number of units that will be developed within each Planning Area. The exact number of units that will be developed within each Planning Area will be determined during the subsequent review of the tentative maps that divide each Planning Area into

individual residential lots. A Minor Master Plan Amendment shall be processed concurrently with each tentative map requesting 10% (or less) above or below the anticipated number of dwelling units stated for the Planning Areas. This amendment will revise Exhibit XIV-1 on page XIV-1 of the Master Plan to indicate exactly how many units have been approved in each Planning Area and how many remaining units exist in the Master Plan. If the number of units requested in an individual Planning Area varies by more than 10%, a Major Master Plan Amendment shall be processed concurrently with the proposed Tentative Map. In no case shall the number of dwelling units approved in an individual Planning Area exceed that allowed by its General Plan designation (see Exhibit III-1 on page III-11), nor shall the total number of residential units in the Master Plan exceed 749.

#### Tentative Map/Planned Development Permit Amendments

Amendments to Tentative Maps and Planned Development Permits may be permitted in accordance with the requirements of Chapter 20 and Section 21.45.100 of the Carlsbad Municipal Code.

#### Site Development Plan Amendments

Amendments to Site Development Plans shall be processed in accordance with the requirements of Chapter 21.06 of the Carlsbad Municipal Code.

#### Substantial Conformance

Minor revisions to Tentative Maps, Parcel Maps, Planned Development Permits, Site Development Plans and Conditional Use Permits may be administratively approved by the City Planner subject to the requirements of the Planning Division's Substantial Conformance Policy in effect at the time of the proposed minor revision.

### **H. Clarifications/Interpretations**

How a development standard applies to a proposal may be unclear on occasion. In such cases, clarifications and/or interpretations of the Bressi Ranch development standards is within the purview of the City Planner. Clarifications and Interpretations shall be recorded in the Master Plan Amendment Log (Exhibit XIV-1 on page XIV-1).

## **VII. INDUSTRIAL/OFFICE DEVELOPMENT STANDARDS & DESIGN GUIDELINES**

The following design guidelines will promote high quality design including landscaping, architecture and overall construction for all office and industrial development in Planning Areas 1-5 and 14 of the Bressi Ranch Master Plan. In accordance with the Land Use Element of the Carlsbad General Plan and the El Camino Real Scenic Corridor Development Standards, development in these planning areas will be designed to provide opportunities for research and development industries in a campus setting.

The design guidelines are general and may be interpreted with some flexibility in their application to specific projects. Variations may be considered for projects with special design characteristics that still meet the general design objectives of these guidelines. All development in Planning Areas 1-5 and 14 of the Bressi Ranch Master Plan, subject to review by the City, shall adhere to these guidelines where applicable.

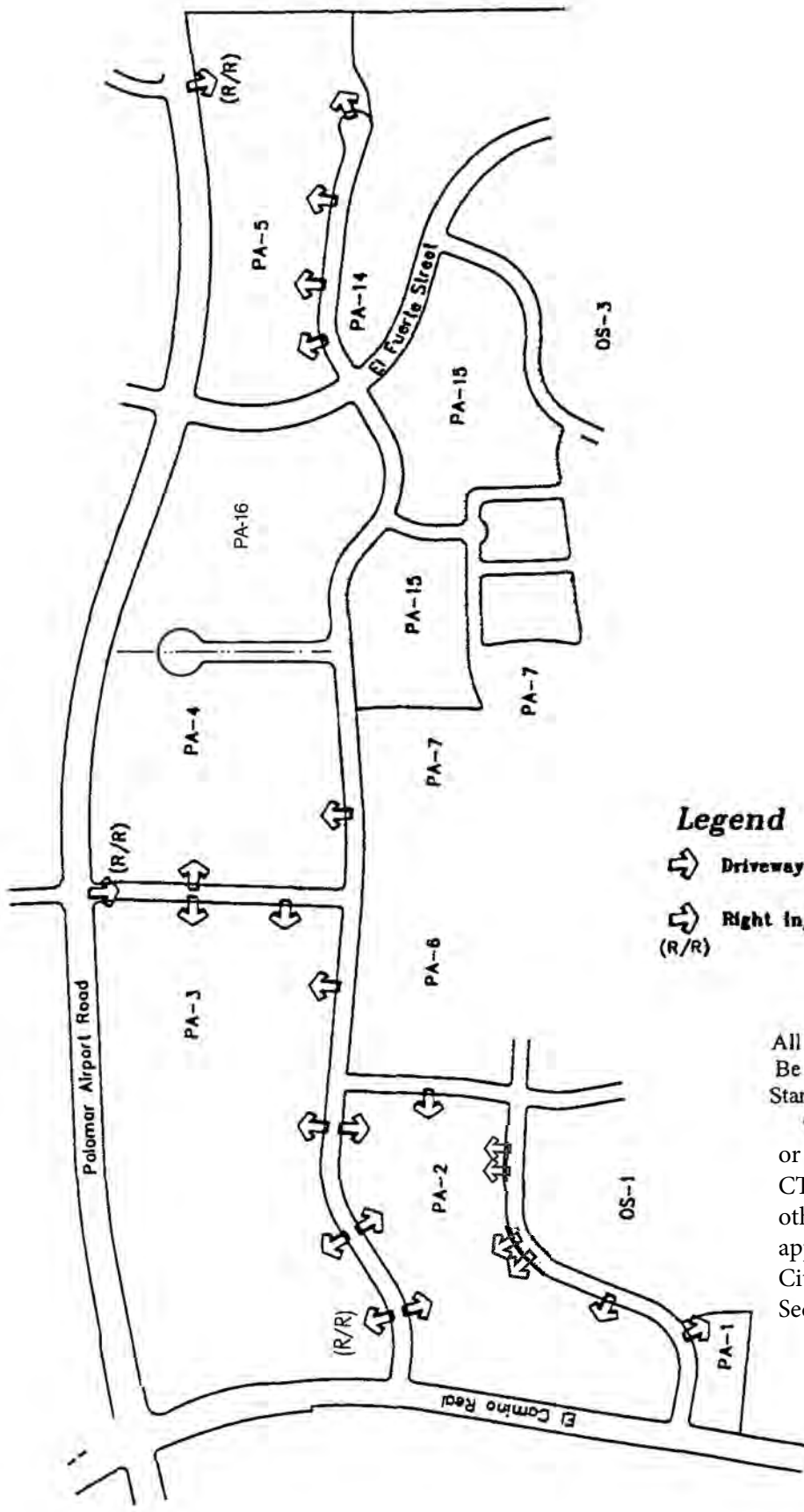
### **A. Development Standards**

Unless modified in the development standards of the individual Planning Areas, all development in Planning Areas 2-5 and 14 shall comply with the development standards of the P-M Zone, Chapter 21.34, of the Carlsbad Municipal Code.

### **B. Industrial Driveways**

Exhibit VII-1 on page VII-3 shows the location of potential driveways serving future office/industrial development in Planning Areas 1-5. The individual exhibits for Planning Areas 1-5 also show these driveway locations. A minimum guideline spacing of 300 feet will be provided between driveways serving Planning Areas 1-5, measured from the centerlines of the driveways or street intersections. Sight distance lines will be provided in accordance with the current edition of the CalTrans Highway Design Manual and all applicable City standards and policies. Obstructions in excess of 30" in height will not be permitted within the sight distance lines. None of the Planning Areas shall gain direct driveway access from El Camino Real or Palomar Airport Road except for the right turn in and out shown in Planning Area 5. The maximum number of driveways which could serve each Planning Area may be located at a spacing closer than the 300 feet spacing guideline, measures from the centerlines of the driveways or street intersections, provided that adequate sight distance, vehicle queuing, and vehicle turning movements can be demonstrated subject to the satisfaction of the City Engineer.

Exhibit VII-1 shows the maximum number of driveways that could be developed in accordance with City standards and policies dealing driveway locations. Subject to the approval of the City Engineer the exact number and location of driveways serving Planning Areas 1-5 may be less than shown on this exhibit.



**Legend**

- Driveway Location
- Right in/Right Out (R/R)

All Driveways Shall Be Spaced Per City Standards As Shown On CT 00-06 or as modified per CT 02-15, unless otherwise approved by the City Engineer per Section VIIIB.





depending on the type of development that occurs in these Planning Areas. However, any relocation of the driveways shown on Exhibit VII-1 on Page VII-3 or the exhibits for Planning Areas 1-5 shall be in accordance with the standards described in the preceding paragraph.

### C. Design Criteria

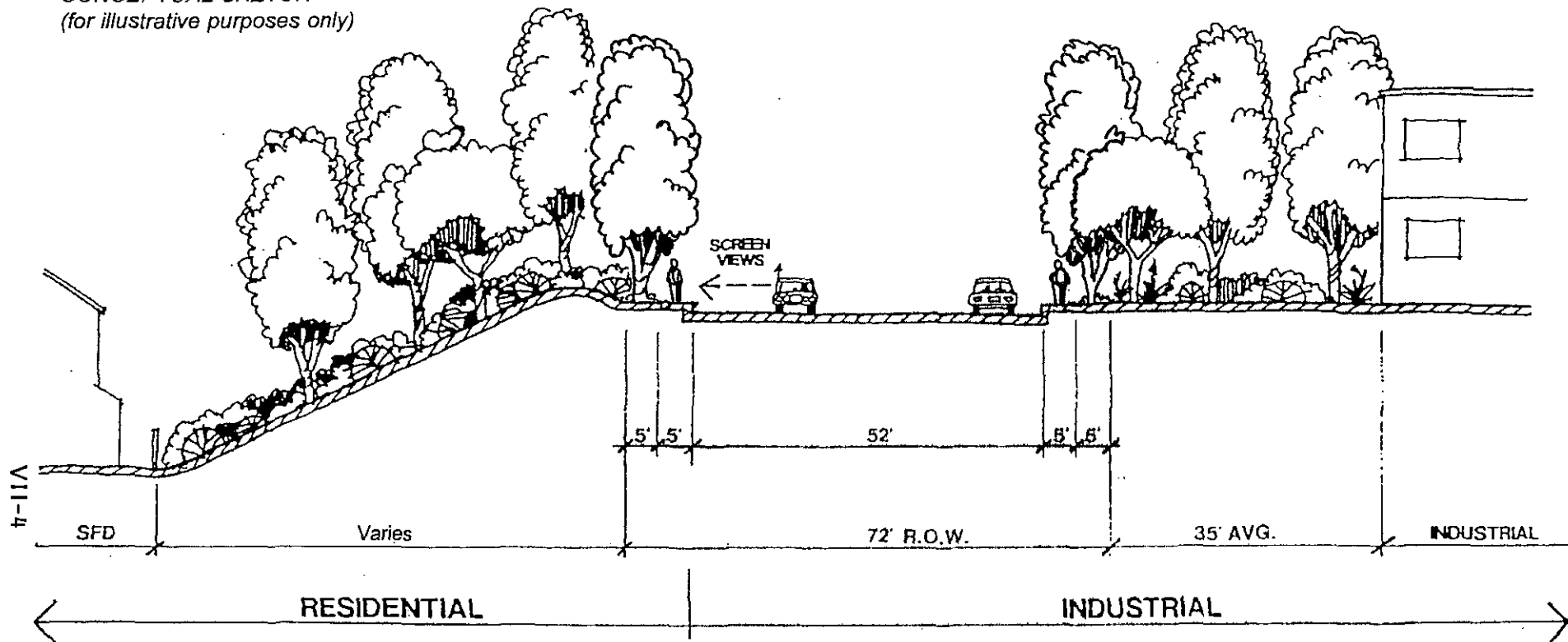
Buffer from Residential – Appropriate buffering techniques such as roads, setbacks, screening and landscaping should be provided where office/industrial uses in Planning Areas 2, 3 and 4 are adjacent to residential land uses. Exhibits VII-2 and VII-3 on pages VII-5 and VII-6 provide cross sections between the industrial and residential development in the northern portion of the Master Plan. These cross sections show how, through the use of a street, slopes, berming and landscaping, the residences will be buffered from the non-residential development to the north. In addition, special conditions have been added to the six office/industrial planning areas to limit the amounts of toxic, highly toxic and regulated substances that can be used or stored in these planning areas. These regulations, along with the separation shown by Exhibits VII-2 and VII-3 on pages VII-5 and VII-6, will buffer the residential uses from the non-residential uses to the north and west.

Basic Orientation – Building entries, office areas and windows should front onto the street whenever feasible. Attention shall be provided to areas visible from public streets and public access on-site and to adjacent properties (see Exhibit VII-4 on page VII-7). Loading and parking should generally be located to the side and rear of buildings with the following exceptions:

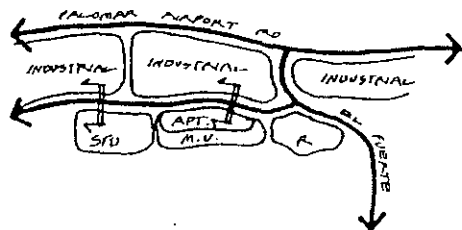
- Parking lots may front onto streets but must conform to the guidelines contained within *Street Frontage and Parking Lots*, as addressed in these design guidelines.
- Where rear or side loading areas are not practical because of site constraints, loading and service areas may front onto streets but must conform to guidelines contained within *Loading and Storage Area Orientation & Design*, as addressed in these design guidelines.

Street Frontage and Parking Lots – Visitor and short-term parking lots may be sited between the street and building entrances. The parking lot should not be the dominant visual element of the site. Parking shall be designed to minimize conflicts with loading areas. Where parking lots occur adjacent to streets, a landscape buffer adjacent to the street should be provided to minimize views of parked cars from the street. The landscape buffer should include a screening feature that is no more than 36 inches in height and consists of a hedge, berm or

CONCEPTUAL SKETCH  
 (for illustrative purposes only)



(PARKING ON INDUSTRIAL SIDE OF STREET ONLY)

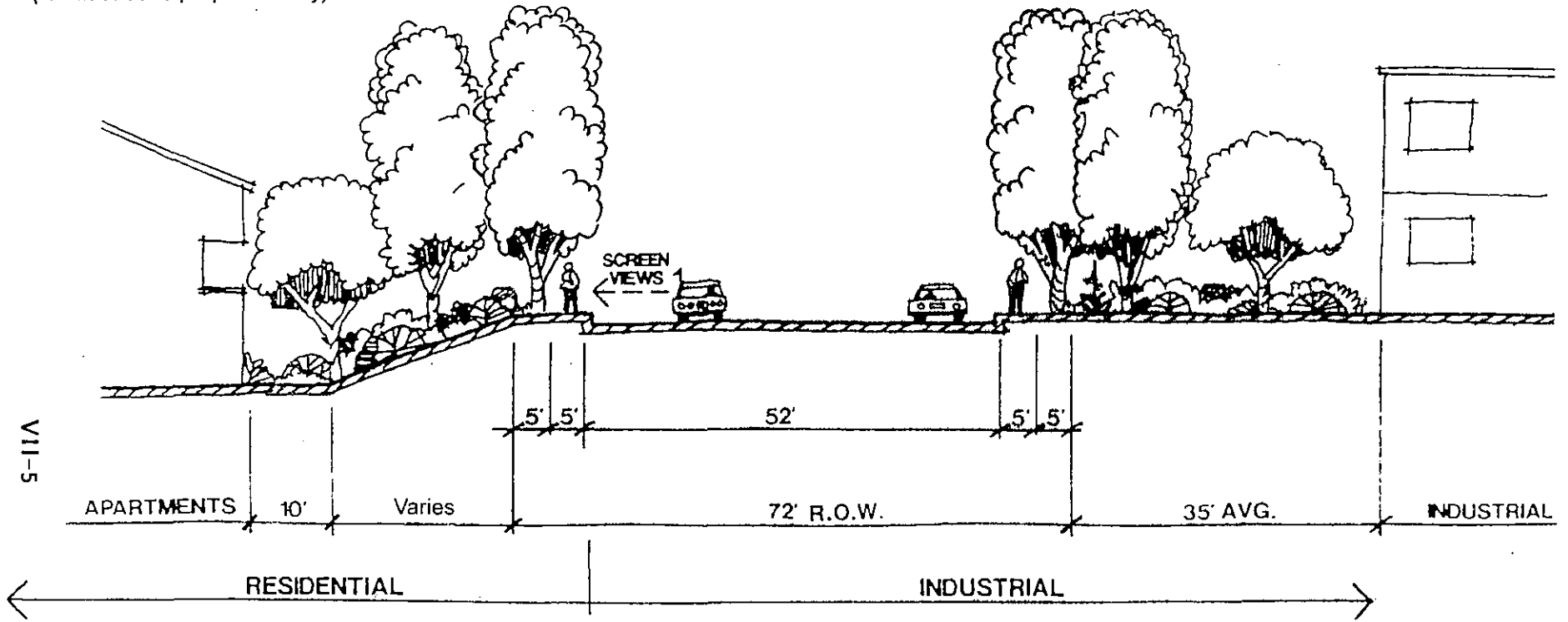


Bressi Ranch

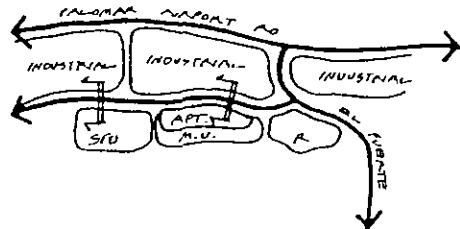
Industrial/Single Family Residential Condition

VII-2

CONCEPTUAL SKETCH  
(for illustrative purposes only)



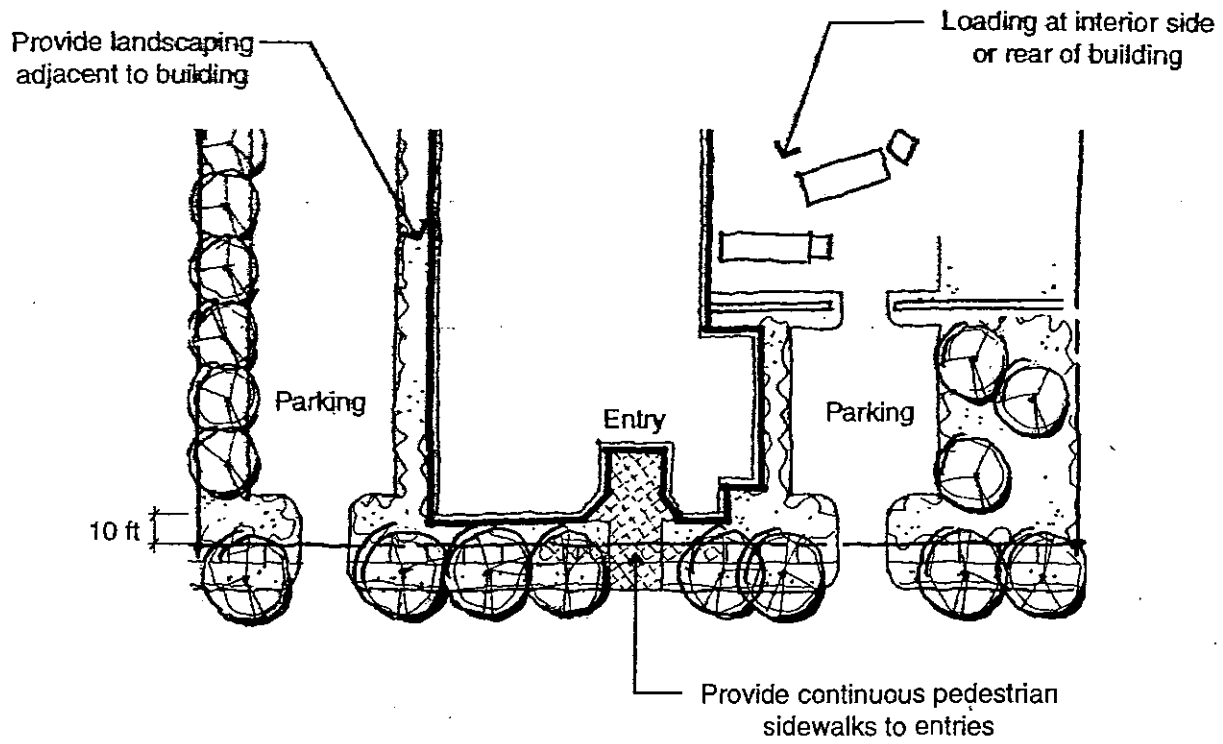
(PARKING ON INDUSTRIAL SIDE OF STREET ONLY)



Bressi Ranch

Industrial/Apartment Condition

VII-3



CONCEPT SKETCH  
(for illustrative purposes only)

combination hedge and berm. Earth berms shall be rounded and densely landscaped to have a natural appearance.

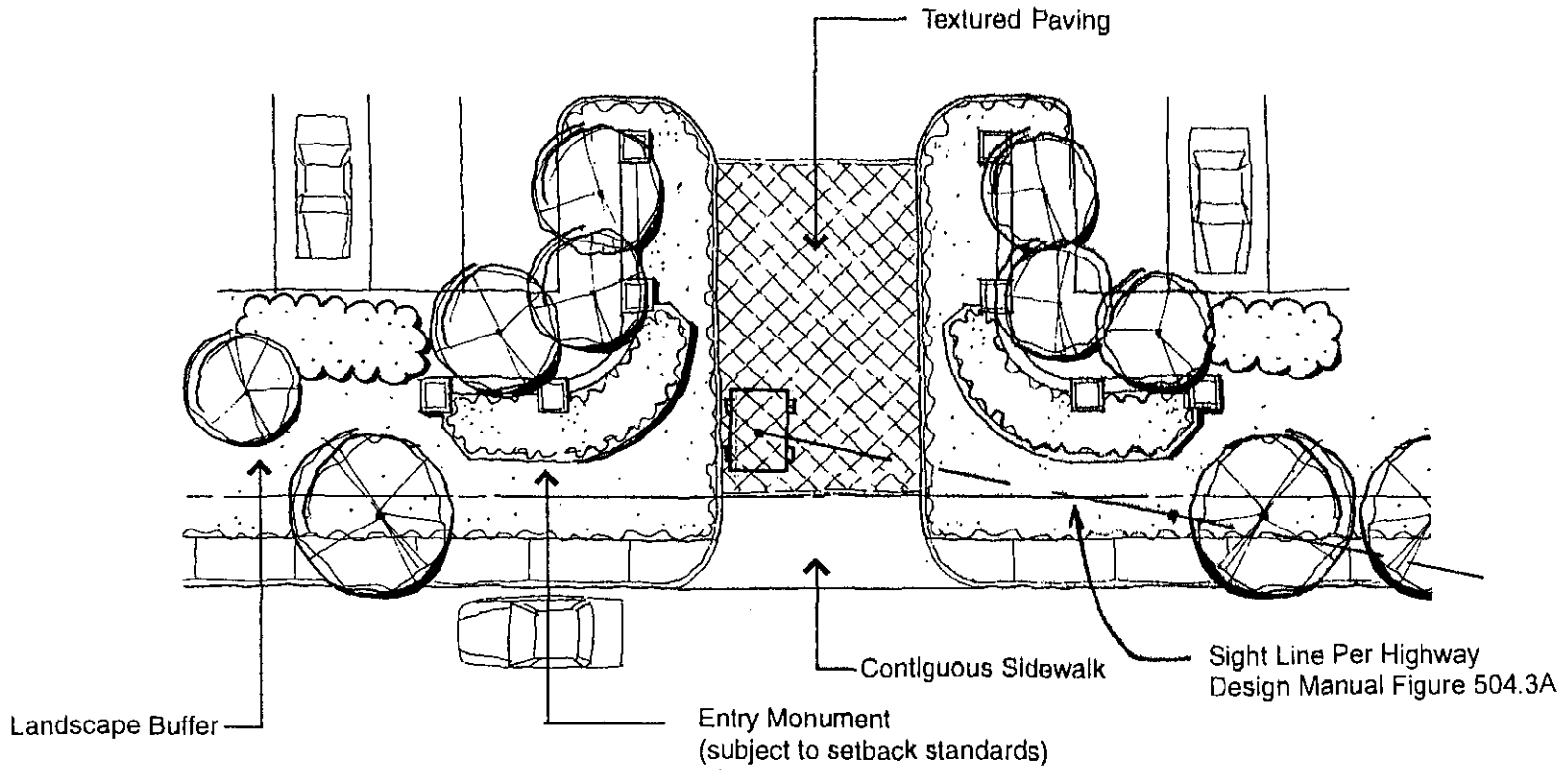
Site Entries – Major vehicular and pedestrian entries should be enhanced with special design features such as enhanced pavement with accent trees and other landscape features, and monumentation. Where site entries are adjacent to a building entry, pedestrian plazas are strongly encouraged (see Exhibit VII-5 on page VII-9).

Plazas – Plazas are encouraged as a site amenity and design detail. Buildings should be arranged to include opportunities for plazas, courts or gardens, and outdoor eating areas for employees with amenities such as outdoor seating, landscaping, water elements, arbors, special lighting and other ‘place-making’ features. Locations where high levels of pedestrian activity are expected, such as adjacent to building entrances, food service areas or between building clusters, provide opportunities for plazas, (see Exhibit VII-6 on page VII-10).

Pedestrian Access to Bus Stops – Buildings and walkways should be designed to provide convenient, well lighted pedestrian access to bus stops on El Camino Real, Palomar Airport Road and Gateway Road.

Loading and Storage Area Orientation & Design – Buildings should be designed so that loading and storage areas do not face onto public rights-of-way. When these features must face a street due to site constraints, they should be screened with a solid decorative wall or berm. Where oblique views of these features are possible from streets, connecting walkways or residences, the features should be screened through the use of walls, trellises, tall landscaping, or equivalent features. Outdoor storage should be located so it is not visible from residential areas. Adequate room shall be provided for trucks maneuvering or waiting to unload. Also, loading and storage areas shall not conflict with connecting walkways or required parking areas (see Exhibit VII-7 on page VII-11).

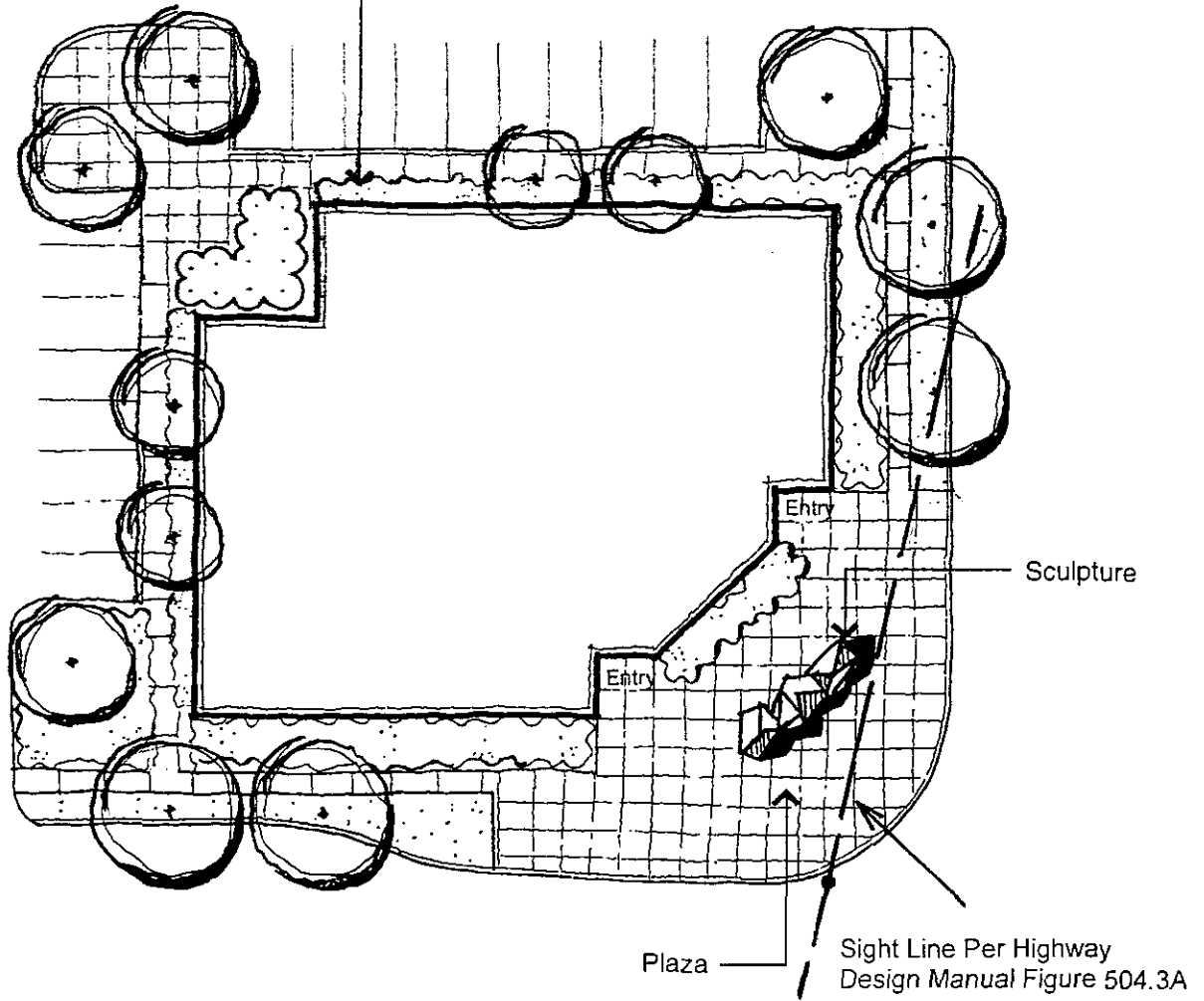




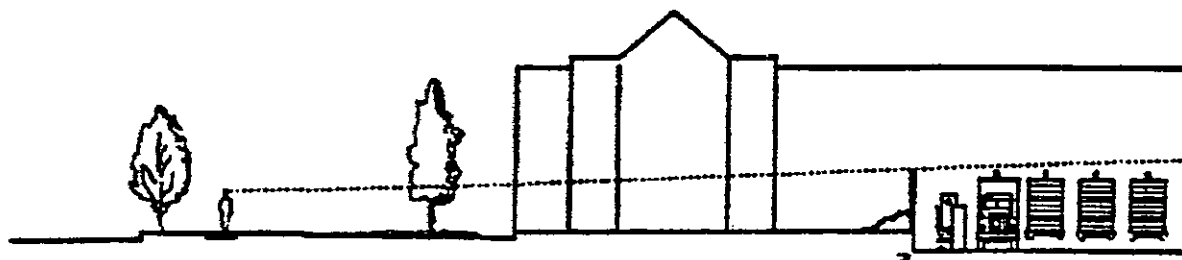
Entries to be enhanced with textured paving and special landscape treatment.

CONCEPT SKETCH  
(for illustrative purposes only)

Provide landscaping adjacent to building

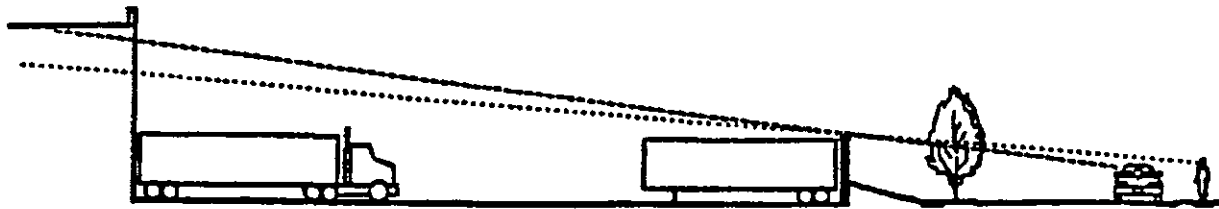


# Loading and Service Area Screening



Service areas should be screened from streets, freeways, and adjacent properties.

Lowering loading docks reduces the wall height needed to block views.



Screen walls should be setback a minimum of 30' from the street curb.

30' min.  
from curb



Fence and Wall Design – Fences and walls in public view should be built with attractive durable materials, such as wrought iron, textured concrete block or formed concrete with reveals. Fences or walls shall be consistent with materials and designs used throughout the project. Gates to loading areas visible from the street should be constructed with tubular steel and high-density perforated metal screening, painted to match or complement adjacent walls. Site entries requiring gates should be non-contiguous from direct view to loading areas where possible to minimize the extent of screening and to avoid direct views into loading areas when the gates are open.

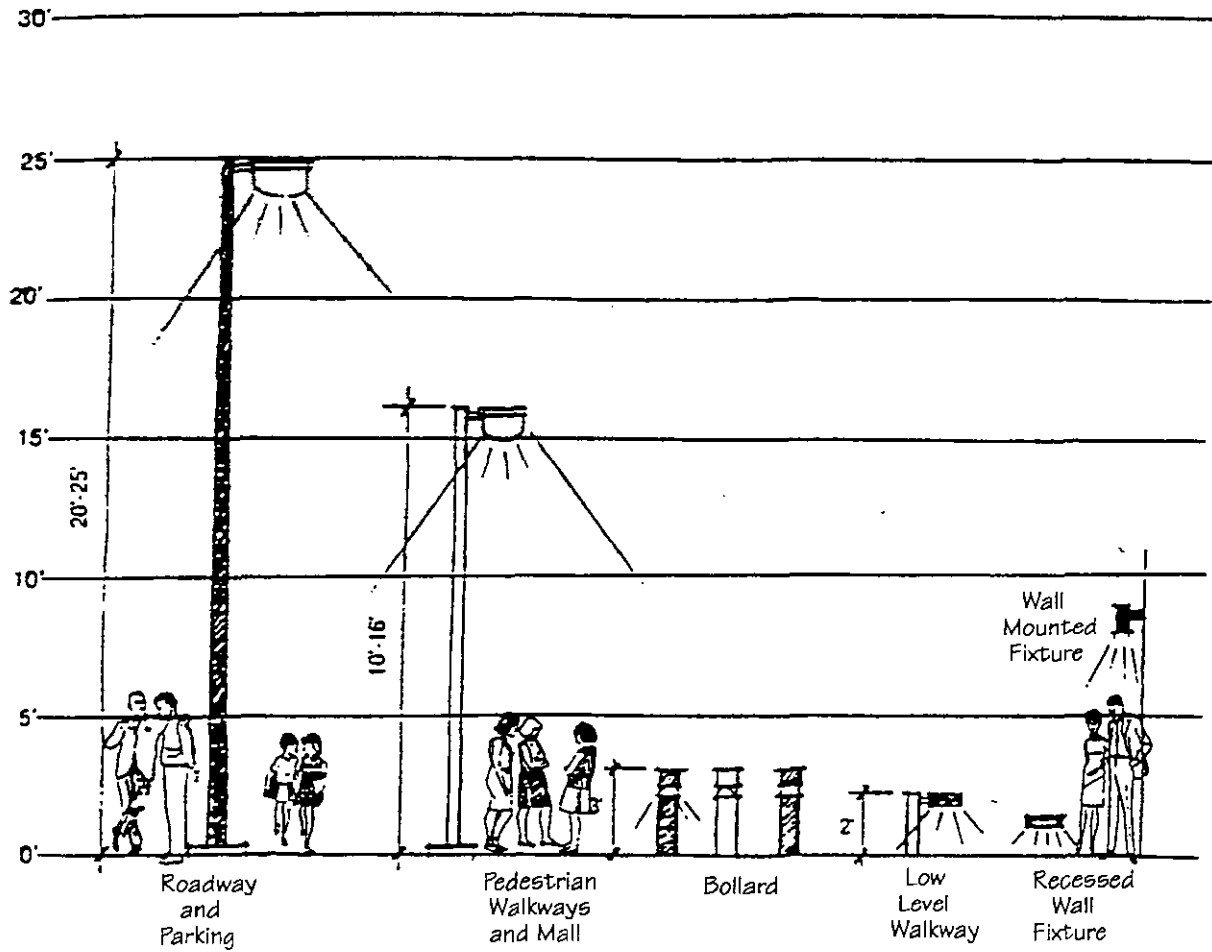
Street side fences or walls should adequately screen views to the top of loading bays and parked vehicles and/or trailers and meet building setback requirements if taller than 42 inches. The height of screen walls along a setback area shall not exceed 8' from the highest finish grade, from the public-view side of the wall. This requirement can be met by berming up to within 8' of the top of the wall. The area in front of screen walls and fences shall be landscaped with shrubs and trees, which will reach a mature height that exceeds the height of the wall or fence.

Along street frontages, long expanses of uninterrupted fences and walls should be avoided through the use of offset surfaces and architectural treatment to provide visual relief. Techniques to accomplish this treatment will include items such as raised planters, openings, material change, texture change, offset sections, and pilasters or posts. Openings in the fences and walls should be provided to connect walkways directly to the street. These pedestrian entry points shall be enhanced with treatments such as special landscaping, trellises, pilasters or other special features. Landscape berms should be provided to minimize the height impact of screen walls.

Lighting – Light standards shall be located and designed to minimize direct glare beyond the parking lot or service area. Light standards under 20 feet in height, as well as bollards, are encouraged throughout the project to illuminate all sidewalks and connecting walkways. Taller standards may be used only if they are designed to prevent light and glare onto adjacent properties and trees are planted along streets and property lines at a spacing of not more than 25 feet on center. Exhibit VII-8 on page VII-13 provides examples of the types of lighting proposed for development in Planning Areas 1, 2, 3, 4 and 5. Standard City streetlights shall be installed on all public streets.

All light standards shall be consistent with respect to design, materials, color and color of light and with the overall architectural style of the project. Additional light features are encouraged to add interest in the site:

# Site Lighting



- Concealing light features within buildings and landscaping
- Lighting at entries, plazas and other areas where evening activity is expected
- Indirect illumination of buildings and landscaping

Site Accessories – Site features such as recycling bins, bicycle racks, litter cans, planters, benches and transit shelters should be designed as an integral part of the project. Architectural character and use of materials should be consistent with the overall project design. Features should be designed to be graffiti and vandal resistant by using materials that are easily cleaned or painted. These features should not interrupt, but complement, connecting walkways.

Screening of Refuse Containers and Utilities – Refuse containers and utility equipment shall be easily accessible by service vehicles but screened from public view. Refuse containers and equipment should be located within a building's facade or within a screened enclosure. The architectural style of the building should be reflected in the design of the enclosures. Landscaping or trelliswork shall be provided on each side of any screened enclosures that are located within parking areas or when visible from a street or connecting walkway.

#### **D. Architectural Guidelines**

Entry Design – Entries and windows are encouraged to face streets, pedestrian walkways and plazas. Special materials, color, detailing or equivalent architectural treatment shall be used to highlight major entries into buildings (see Exhibit VII-9 on page VII-15).

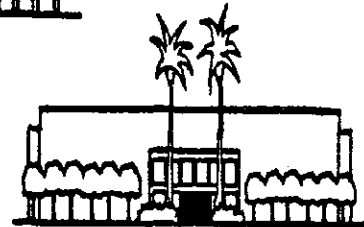
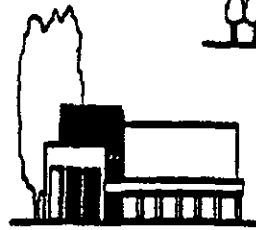
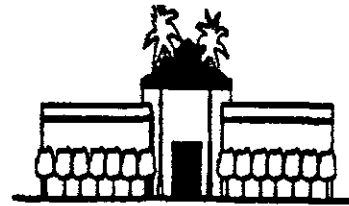
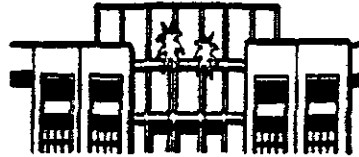
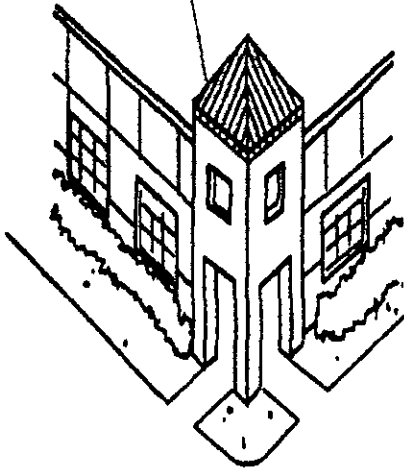
Primary building entries should be highlighted through the massing of the building. Greater height can be used to highlight and accentuate entries in the form of corner tower elements, tall voids, or a central mass meeting an entry plaza consistent with the height limitations of Section 21.34.070(1) of the Carlsbad Municipal Code. Conversely, smaller building masses can also communicate the location of entries.

General Massing – A single dominant building mass should be avoided. Substantial variations in massing should include changes in height and horizontal plane. The extent of massing breaks and building projections should relate visually to the overall scale of the building.



# Concept Entry Design

Greater height can be used to highlight and accentuate entries in the form of corner tower elements.



Vertical variations to the roof line should incorporate roof projections to avoid false/unfinished appearance.



Building Wall Treatment – To avoid blank walls between massing breaks, especially along facades immediately visible from adjacent streets or walkways, one or more of the following techniques shall be employed:

- Change in texture
- Revealed pilaster
- Change in plane (2-foot minimum)
- Vertical variation of roof line
- Windows
- Lattice, accent tree or equivalent

Roofs – Vertical variations to the roofline should incorporate roof projections or returns to avoid a false front/unfinished appearance. Roof forms should be simple, avoid a massive appearance and reflect the internal organization of buildings. Roofing materials shall be durable. Where visible from the street, acceptable roofing materials include metal, standing seam and concrete tile.

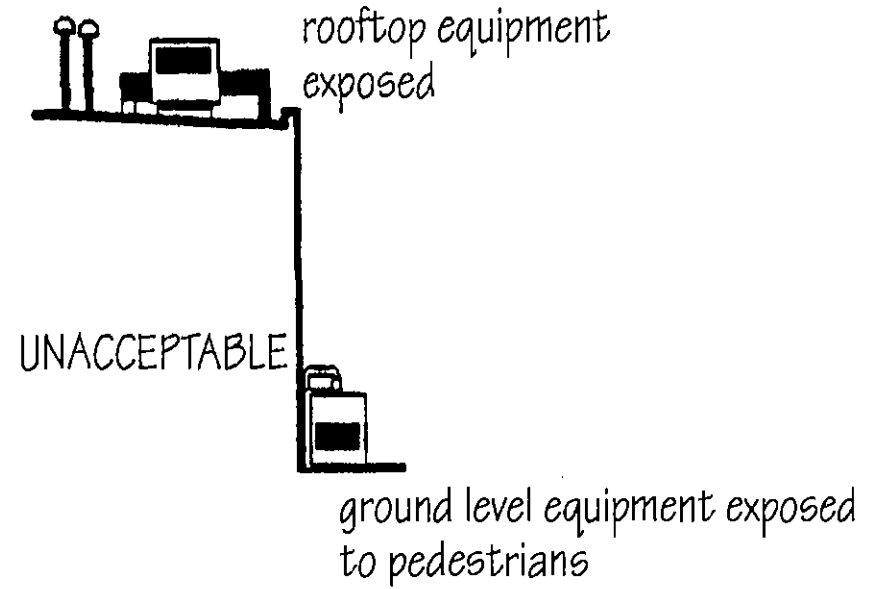
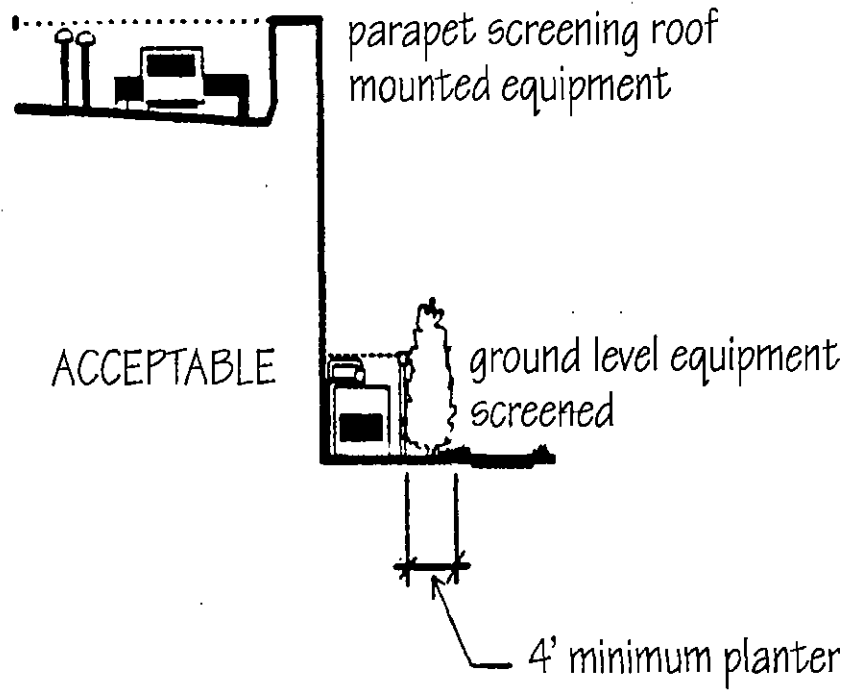
Colors – For larger building surfaces, colors should be muted and subdued. Accent colors may include brighter and darker colors.

Quality of Construction – An attractive appearance to all facades shall be provided through careful detailing especially, at the base of buildings, along cornices, eaves, parapets or ridge tops, and around entries and windows. Appearance may also be enhanced through the use of materials, expansion joints and reveals.

Mechanical Equipment Screening – Mechanical equipment screening shall be integrated as part of a project's site and building design. Rooftop and ground-mounted equipment should be screened from view of all streets, parking lots, connecting walkways and adjacent properties (see Exhibit VII-10 on page VII-17).

Where possible, rooftop equipment should be integrated into the overall mass of a building. At a minimum, screen roof mounted equipment through the use of parapets, screen walls, equipment wells, mechanical room enclosures and similar design features. Screening devices other than parapet walls should be designed as an integral element of the building mass. Picket fencing, chain-link fencing and metal boxes are not acceptable. The top of screens should be at least as high as the top of the equipment, with additional height provided where topography creates possible views to the equipment.

# Conceptual Equipment Screening



VII-16

Typical ground-mounted equipment such as transformers, cooling and heating units shall be screened with walls and/or landscaping.

Downspouts – Downspouts shall be concealed on facades.

#### **E. Sign Criteria**

All signage shall be compatible in appearance and scale with the adjacent buildings and shall comply with the requirements of Chapter 21.41 of the Carlsbad Municipal Code.

#### **F. Individual Planning Area Standards**

##### 1. Planning Area 1

###### Description

Planning Area 1 (see Exhibit VII-11 on page VII-19) is located in the western portion of the project, south of Palomar Airport Road, and is bordered on the west by El Camino Real. Open Space Area 1 is on the eastern side of Planning Area 1, and a more extensive industrial area, Planning Area 2, is located to the north. The eastern side is also the location of a possible frontage road connecting to The Villages of La Costa. The small size of Planning Area 1 is created by the fixed location of the access road off of El Camino Real, the open space to the east and the property boundary to the south of the area. Planning Area 1 has a gross area of 2.5 acres and a net developable area of 2.5 acres. The exact developable acreage of this site may be less if it is determined that a frontage road must go through Planning Area 1.

###### Implementation of Master Plan Vision and Goals

Planning Area 1 is designated as Office due to its location within the Palomar Airport Influence Area, and its location adjacent to other major industrial development on the west side of El Camino Real and within the Master Plan area (Goal 1). Residents of the Master Plan may access Planning Area 1 using the sidewalks along the public streets within the Master Plan area, which meets Goal 2 of this plan. In addition, a variety of Office uses are allowed in Planning Area 1, which could provide a range of job types available (Goal 5).

# Legend



Access



Detention Basin



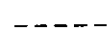
Open Space



Fire Protection Zone



City Wide Trail



Setbacks(Width in Feet)



Bus Stop/Bus Turnout



Runway Protection Zone



Airport Influence Area



Flight Activity Zone

General Plan Land Use:

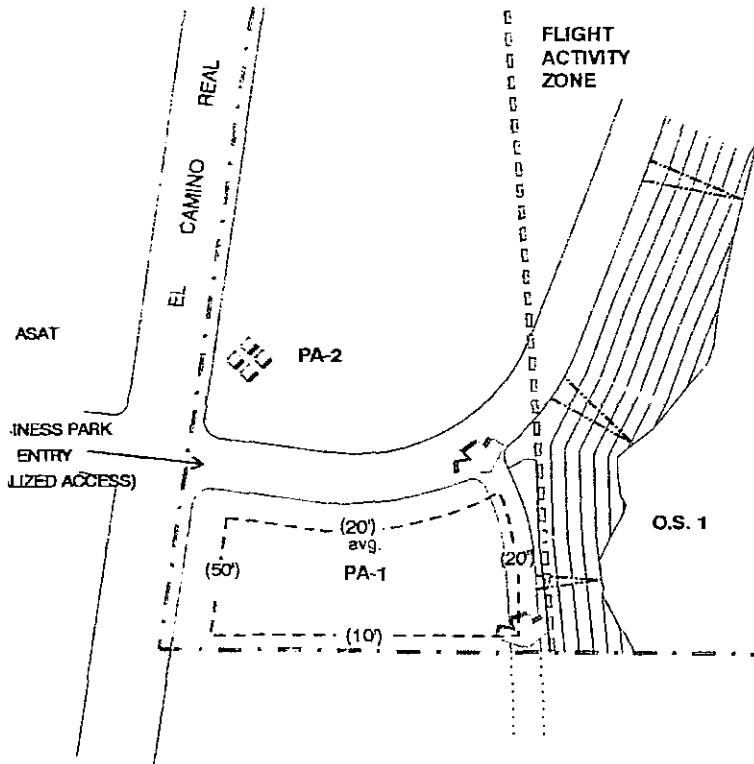
O

Zoning:

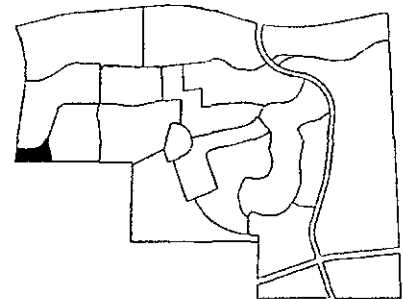
O

Product:

*Office*



Concept Sketch  
(for illustrative purposes only)



*Bressi Ranch*

A planned community in the City of Carlsbad, California



3-26-02 Bressi PA1.cdr

*Planning Area 1*

VII-11



General Plan & Zoning

General Plan Land Use: Office (O)

Zoning: Office (O)

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

Uses Allowed

Office and all uses identified in Section 21.27 of the Carlsbad Municipal Code (O Zone) are allowed in Planning Area 1.

Use Restrictions

No facilities in the development shall store, handle, or use toxic, or highly toxic gases as defined in the most recent Uniform Fire Code (UFC) at quantities that exceed exempt amounts as defined in the most recent UFC.

Facilities which store, handle, or use regulated substances as defined in the California Health and Safety Code §25532(g) in excess of threshold quantities shall prepare risk management plans for determination of risk to the community.

Facilities which store, handle, or use any quantity of a toxic or highly toxic gas as defined in the most recent Uniform Fire Code which are also regulated substances as defined in the California Health and Safety Code §25532(g) shall prepare an offsite consequence analysis (OCA). This analysis shall be performed in accordance with Title 19 of the California Code of Regulations §2750.2 through §2750.3. If the OCA shows the release could impact the residential community, the facility will not store, handle, or use the material in those quantities. If a decrease in the quantity of material reduces the distance to toxic endpoint to where the community is not impacted, the facility shall be able to utilize the material in that quantity.



Design Criteria

In addition to complying with the design criteria contained in Sections VII.A, B and C of this Master Plan, development in Planning Area 1 shall comply with the following design criteria:

- A frontage road may be constructed along the easterly edge of Planning Area 1 from Street A to the southern boundary of Planning Area 1 to connect to the frontage road in Neighborhood 1.1 of the Villages of La Costa Master Plan. The exact location and need for this frontage road will be determined as a part of the review of detailed development plans for Planning Area 1 and the adjacent property to the south in the Villages of La Costa Master Plan.
- 
- Planning Area 1 may be developed in conjunction with the Office designated property to the south in the Villages of La Costa Master Plan.

Development Standards

Planning Area 1 shall be developed in accordance with the Office (O) Zone standards set forth in Section 21.27 of the Carlsbad Municipal Code, which include the following setbacks.

All development in Planning Area 1 shall comply with the setback requirements of Section 21.27.040 of the Office Zone. All setbacks are measured from the right of way line. The following paragraphs summarize these setback requirements.

- Front or Side Yard facing El Camino Real, Palomar Airport Road or El Fuerte Street – Minimum setback of fifty (50) feet fully landscaped and irrigated shall be provided.
- Front or Side Yard on a Collector, Local or Private Street –The setback of twenty (20) feet —shall be fully landscaped.
- Side Yard Interior – Ten (10) feet fully landscaped and irrigated.
- Rear Yard –Ten (10) feet shall be fully landscaped and irrigated.
- Projects in Planning Area 1 shall process a Special Use Permit per the requirements of the El Camino Real Scenic Corridor study concurrent with any other discretionary permit.
- Planning Area 2

## 2. Planning Area 2

### Description

Planning Area 2 (see Exhibit VII-12 on page VII-23) is located in the western edge portion of the Master Plan, bordered by El Camino Real to the west. Planning Area 3, an industrial area, is located to the north of Planning Area 2. To the south of Planning Area 2 is Planning Area 1. Planning Area 2 has a gross area of 27.5 acres and a net developable area of 23.8 acres.

### Implementation of Master Plan Vision and Goals

The purpose of this planning area, including its design standards and allowed uses, is to provide for office or industrial/warehouse uses (Goal 5). Planning Area 2 is designated as Planned Industrial due to its location along El Camino Real (Goal 6). Palomar Airport Road and El Camino Real provide convenient vehicular access to Planning Area 2. Residents of the Master Plan may conveniently access this area using the sidewalks along public streets or the internal pedestrian circulation system of the Master Plan (Goal 2).

### General Plan & Zoning

General Plan Land Use:           Planned Industrial (PI)

Zoning:                            Planned Industrial (P-M)

### Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.




### Uses Allowed

Office, Research & Development, Light Manufacturing and Warehousing, and all uses identified in Section 21.34 of the Carlsbad Municipal Code (P-M Zone) are allowed in Planning Area 2.


### Use Restrictions

No facilities in the development shall store, handle, or use toxic or highly toxic gases as defined in the most recent Uniform Fire Code (UFC) at quantities that exceed exempt amounts as defined in the most recent UFC.


# Legend


-  Access
-  Detention Basin
-  Open Space

 Fire Protection Zone


 City Wide Trail

 Setbacks (Width in Feet)

 Bus Stop/ Bus Turnout

 Runway Protection Zone

 Airport Influence Area

 Flight Activity Zone

General Plan Land Use:

*PI*

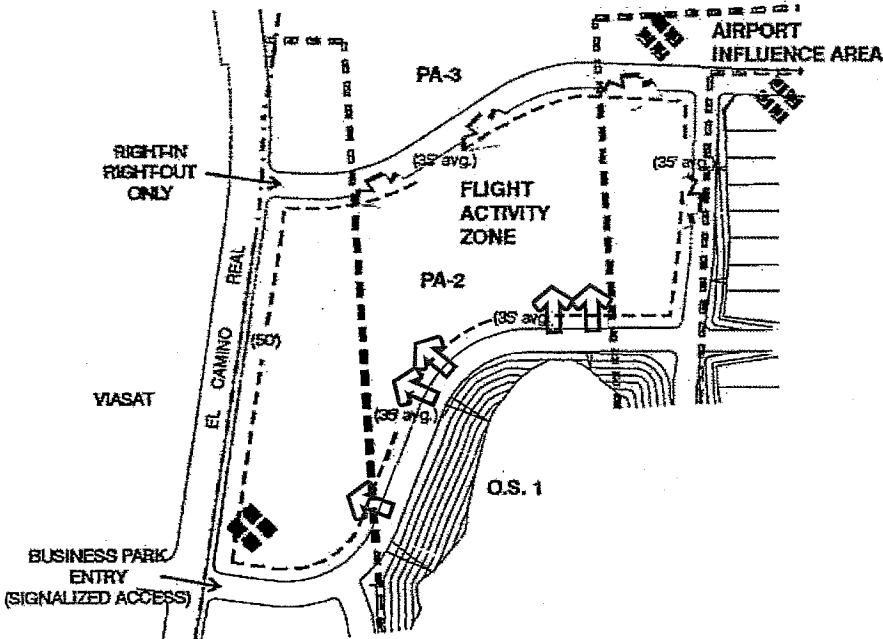
Zoning:

*PM*

Product:

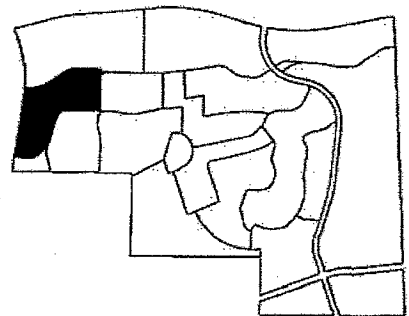
*Industrial/Office/Warehouse*

*Per P-M Zone as further restricted by Master Plan*



**Concept Sketch**  
*(for illustrative purposes only)*

*Note: Industrial access points shall provide intersection spacing per Section VII.B.*



**Bressi Ranch**

A planned community in the City of Carlsbad, California

*Planning Area 2*

VII-12



Facilities which store, handle, or use regulated substances as defined in the California Health and Safety Code §25532(g) in excess of threshold quantities shall prepare risk management plans for determination of risk to the community.

Facilities which store, handle, or use any quantity of a toxic or highly toxic gas as defined in the most recent Uniform Fire Code which are also regulated substances as defined in the California Health and Safety Code §25532(g) shall prepare an offsite consequence analysis (OCA). This analysis shall be performed in accordance with Title 19 of the California Code of Regulations §2750.2 through §2750.3. If the OCA shows the release could impact the residential community, the facility will not store, handle, or use the material in those quantities. If a decrease in the quantity of material reduces the distance to toxic endpoint to where the community is not impacted, the facility shall be able to utilize the material in that quantity.

#### Design Criteria

In addition to complying with the Design Criteria contained in Sections VII.A, B and C of this Master Plan, development in Planning Area 2 shall comply with the following Design Criteria:

- Planning Area 2 is located within the boundaries of the Airport's Flight Activity Zone. Development within this portion of Planning Area 2 shall be restricted in accordance with the requirements of the Comprehensive Land Use Plan for McClellan-Palomar Airport.
- Enhanced landscaping shall be provided along the easterly edge of Planning Area 2 for screening purposes. The landscape treatment of this road along with its location along the eastern portion of Planning Area 2 will create a buffer between the non-residential uses and the residences to the east.
- Strong pedestrian linkage shall be provided between Planning Area 2 and the residential development in Planning Areas 6 and 10.
- All loading docks, outdoor storage areas or refuse collection visible from El Camino Real, Palomar Airport Road, El Fuerte Street or any adjacent residential area shall be appropriately screened.

#### Development Standards

Planning Area 2 shall be developed in accordance with the Planned Industrial (P-M) Zone standards set forth in Chapter 21.34 of the Carlsbad Municipal Code, which include the following setbacks that are measured from the right-of-way line:

- Front or Side Yard facing El Camino Real, Palomar Airport Road or El Fuerte Street – Minimum setback of fifty (50) feet fully

landscaped and irrigated shall be provided. Subject to approval of the City Planner the landscaped portion of the setback may be reduced to thirty-five (35) feet to accommodate a driveway along the portion of the setback farthest from the right-of-way.

- Front or Side Yard on a Collector, Local or Private Street – Average setback of thirty-five (35) feet, with a minimum of twenty-five (25) feet. This setback shall be fully landscaped.
- Side Yard Interior – Ten (10) feet fully landscaped and irrigated.
- Rear Yard – Twenty (20) feet, of which ten (10) feet adjacent to the rear property line shall be fully landscaped and irrigated.
- Projects in the portion of Planning Area 2 within the El Camino Real Scenic Corridor Study shall process a Special Use Permit per the requirements of that study concurrent with any other discretionary permit or Minor Site Development Plan.

### 3. Planning Area 3

#### Description

Planning Area 3 (see Exhibit VII-13 on page VII-27) is located at the intersection of Palomar Airport Road and El Camino Real. Planning Area 4, an industrial area, is located to the east of Planning Area 3. To the south of Planning Area 3 is Planning Area 2 and a portion of residential Planning Area 6. Planning Area 3 is the largest of the five industrial planning areas located within the Bressi Ranch Master Plan with a gross area of 47.0 acres and a net developable area of 47.0 acres.

#### Implementation of Master Plan Vision and Goals

The purpose of this planning area, including its design standards and allowed uses, is to provide for a wide variety of job opportunities in the office or industrial/warehouse uses that will be developed in this area (Goal 5). Planning Area 3 is designated as Planned Industrial due to its location at the intersection of Palomar Airport Road and El Camino Real (Goal 6) and its location within the Airport Influence Zone. Palomar Airport Road and El Camino Real provide convenient vehicular access to Planning Area 3 and offer transit stops for workers of this area (Goal 3). Residents of the Master Plan may conveniently access this area using the sidewalks along public streets or the internal pedestrian circulation system of the Master Plan (Goal 2).

#### General Plan & Zoning

General Plan Land Use:           Planned Industrial (PI)

Zoning:                            Planned Industrial (P-M)

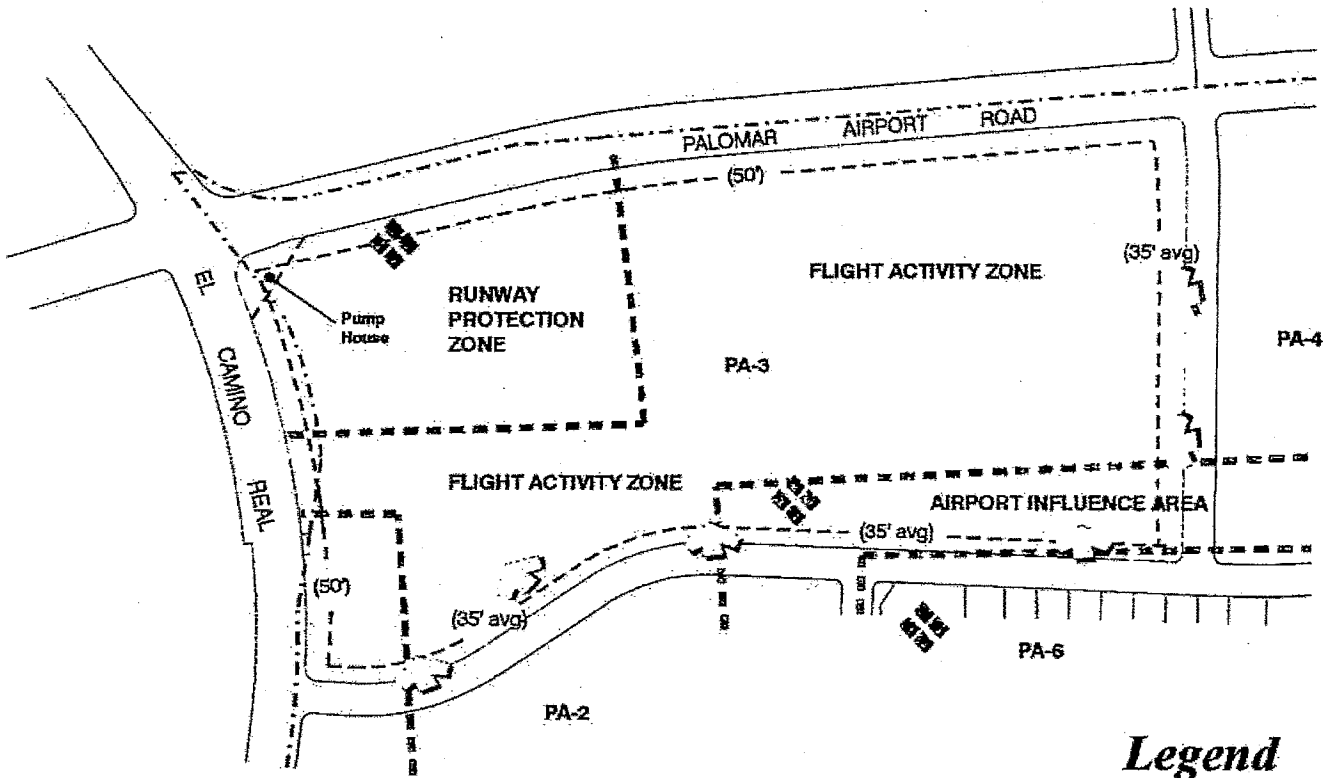
#### Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

#### Uses Allowed

Office, Research & Development, Light Manufacturing and Warehousing, and all uses identified in Section 21.34 of the Carlsbad Municipal Code (P-M Zone) are allowed in Planning Area 3.






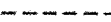








Concept Sketch  
(for illustrative purposes only)

Note: Industrial access points shall provide intersection spacing per Section VII.B.

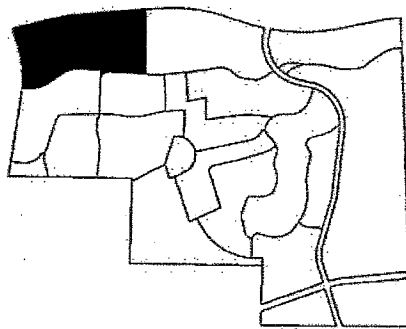
### Legend

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Wide Trail
-  Setbacks(Width in Feet)
-  Bus Stop/Bus Turnout
-  Runway Protection Zone
-  Airport Influence Area
-  Flight Activity Zone

General Plan Land Use:  
PI

Zoning:  
PM

Product:  
Industrial/Office/Warehouse  
Per P-M Zone as further  
restricted by Master Plan



Breni Ranch

A planned community in the City of Carlsbad, California

Planning Area 3

VII-13



Use Restrictions

No facilities in the development shall store, handle, or use toxic or highly toxic gases as defined in the most recent Uniform Fire Code (UFC) at quantities that exceed exempt amounts as defined in the most recent UFC.

Facilities which store, handle, or use regulated substances as defined in the California Health and Safety Code §25532(g) in excess of threshold quantities shall prepare risk management plans for determination of risk to the community.

Facilities which store, handle, or use any quantity of a toxic or highly toxic gas as defined in the most recent Uniform Fire Code which are also regulated substances as defined in the California Health and Safety Code §25532(g) shall prepare an offsite consequence analysis (OCA). This analysis shall be performed in accordance with Title 19 of the California Code of Regulations §2750.2 through §2750.3. If the OCA shows the release could impact the residential community, the facility will not store, handle, or use the material in those quantities. If a decrease in the quantity of material reduces the distance to toxic endpoint to where the community is not impacted, the facility shall be able to utilize the material in that quantity.

Design Criteria

In addition to complying with the Design Criteria contained in Sections VII.A, B and C of this Master Plan, development in Planning Area 3 shall comply with the following Design Criteria:

- Most of Planning Area 3 is located within the boundaries of the airport's Flight Activity Zone. Development within this portion of Planning Area 3 should be restricted in accordance with the requirements of the Comprehensive Land Use Plan for McClellan-Palomar Airport.
- A small portion of the northwest corner of Planning Area 3 falls within the Runway Protection Zone of the McClellan-Palomar Airport. This area, consisting of approximately 11.7 acres, is restricted to low intensity land uses with a low number of employees and customers.
- Strong pedestrian linkages shall be provided between Planning Area 3 and the residential development in Planning Area 6.
- Special attention shall be given to the design of projects and the type of uses in the portions of Planning Area 3 that are adjacent to the boundaries of the residential planning areas to the south. Buildings shall be setback from the top of the slopes a minimum of 15 feet so they do not loom over the residential units to the south.

- Special attention shall be provided to the landscaping along the road located on the southern edge of Planning Area 3. The landscape treatment of this road, with its location along the southern portion of Planning Area 3, will create a buffer between the non-residential uses and the residences to the south.
- All loading docks, outdoor storage areas or refuse collection visible from El Camino Real, Palomar Airport Road, El Fuerte Street or any adjacent residential area shall be appropriately screened.

#### Development Standards

Planning Area 3 shall be developed in accordance with the P-M Zone standards set forth in Chapter 21.34 of the Carlsbad Municipal Code, which include the following setbacks that are measured from the right-of-way line:

- Front or Side Yard facing El Camino Real, Palomar Airport Road or El Fuerte Street – Minimum setback of fifty (50) feet fully landscaped and irrigated shall be provided. Subject to approval of the City Planner the landscaped portion of the setback may be reduced to thirty-five (35) feet to accommodate a driveway along the portion of the setback farthest from the right-of-way.
- Front or Side Yard on a Collector, Local or Private Street – Average setback of thirty-five (35) feet, with a minimum of twenty-five (25) feet. This setback shall be fully landscaped.
- Side Yard Interior – Ten (10) feet fully landscaped and irrigated.
- Rear Yard – Twenty (20) feet, of which ten (10) feet adjacent to the rear property line shall be fully landscaped and irrigated.
- Projects in the portion of Planning Area 3 within the El Camino Real Scenic Corridor Study shall process a Special Use Permit per the requirements of the study concurrent with any other discretionary permit or Minor Site Development Plan.
- As discussed in Chapter XIII of this Master Plan, a large reclaimed water pump station will be located near the intersection of Palomar Airport Road and El Camino Real in Planning Area 3. Due to its highly visible location it is important that the pump station have an attractive design and that it conforms to the development standards and design criteria of this chapter. The pump station and the screening structure shall be located so that they do not do not impact line of site near the intersection.

4. Planning Area 4

Description

Planning Area 4 (see Exhibit VII-14 on page VII-31) is located at the southeast intersection of Palomar Airport Road and Innovation Way. Planning Area 16, a mixed multi-family residential and commercial area, is located east of Planning Area 4 and Planning Area 3, an industrial area, is located to the west. To the south of Planning Area 4 is a portion of residential Planning Area 6 and mixed-use Planning Area 15. Planning Area 4 has a gross area of 26.0 acres and a net developable area of 26.0 acres.

Implementation of Master Plan Vision and Goals

The purpose of this planning area, including its design standards and allowed uses, is to provide for office or industrial/warehouse uses (Goal 5). Planning Area 4 is designated as Planned Industrial due to its location at the intersection of Palomar Airport Road and Innovation Way (Goal 6) and its location within the Airport Influence Zone. Palomar Airport Road and Innovation Way provide convenient vehicular access to Planning Area 4, and transit stops located along Palomar Airport Road will be within easy walking distance to this planning area (Goal 3). Residents of the Master Plan may also access this area using the sidewalks along public streets or the internal pedestrian circulation system of the Master Plan (Goal 2). In addition, employees of Planning Area 4 can conveniently utilize the Village Center for a variety of activities due to the close proximity of the two areas.

General Plan & Zoning

General Plan Land Use:           Planned Industrial (PI)

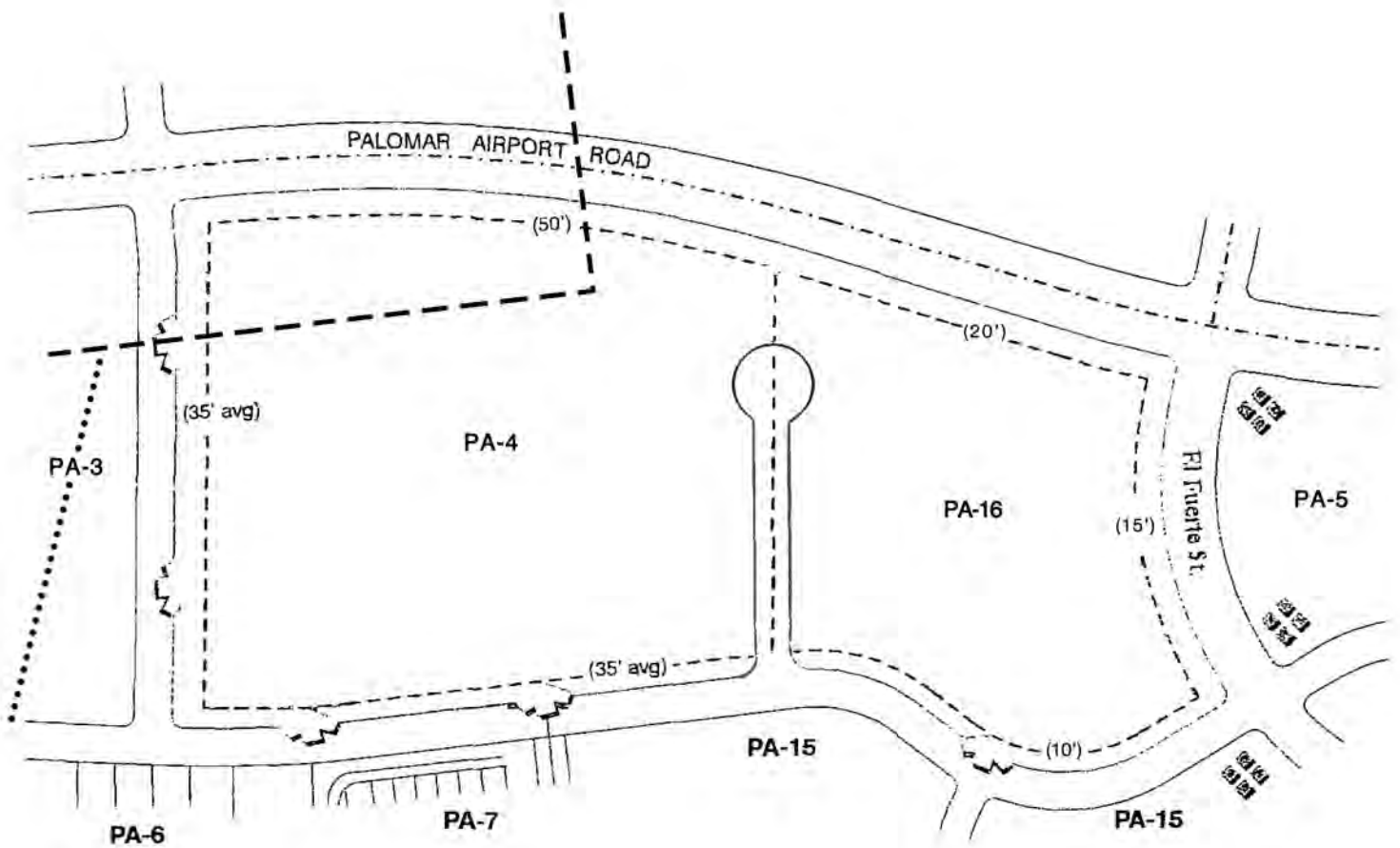
Zoning:                            Planned Industrial (P-M)

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

Uses Allowed

Office, Research & Development, Light Manufacturing and Warehousing, and all uses identified in Section 21.34 of the Carlsbad Municipal Code (P-M Zone) are allowed in Planning Area 4.



Concept Sketch  
(for illustrative purposes only)

Note: Industrial access points shall provide intersection spacing per Section VIIB.

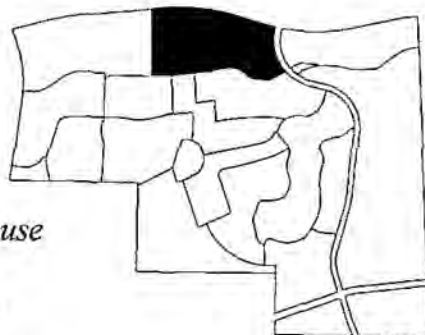
### Legend

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Wide Trail
-  Setbacks (Width in Feet)
-  Bus Stop/Bus Turnout
-  Inner Approach/Departure Zone
-  Inner Turning Zone

General Plan Land Use:  
PI

Zoning:  
PM

Product:  
Industrial/Office/Warehouse  
Per P-M Zone as further  
restricted by Master Plan



Bressi Ranch

Planning Area 4

VII-14



Use Restrictions

No facilities in the development shall store, handle, or use toxic or highly toxic gases as defined in the most recent Uniform Fire Code (UFC) at quantities that exceed exempt amounts as defined in the most recent UFC.

Facilities which store, handle, or use regulated substances as defined in the California Health and Safety Code §25532(g) in excess of threshold quantities shall prepare risk management plans for determination of risk to the community.

Facilities which store, handle, or use any quantity of a toxic or highly toxic gas as defined in the most recent Uniform Fire Code which are also regulated substances as defined in the California Health and Safety Code §25532(g) shall prepare an offsite consequence analysis (OCA). This analysis shall be performed in accordance with Title 19 of the California Code of Regulations §2750.2 through §2750.3. If the OCA shows the release could impact the residential community, the facility will not store, handle, or use the material in those quantities. If a decrease in the quantity of material reduces the distance to toxic endpoint to where the community is not impacted, the facility shall be able to utilize the material in that quantity.

Design Criteria

In addition to complying with the Design Criteria contained in Sections VII.A, B and C of this Master Plan, development in Planning Area 4 shall comply with the following Design Criteria:

- Most of Planning Area 4 is located within the boundaries of the airport's Flight Activity Zone. Development within this portion of Planning Area 4 should be restricted in accordance with the requirements of the Comprehensive Land Use Plan for McClellan-Palomar Airport.
- Special attention shall be given to the design of projects and the type of uses in the portions of Planning Area 4 that are adjacent to the boundaries of the residential planning areas to the east and south. Buildings shall be setback from the top of the slopes a minimum of 15 feet so they do not loom over the residential units to the east and south.
- Special attention shall be provided to the landscaping along the road located on the southern edge of Planning Area 4. The landscape treatment of this road along with its location along the southern portion of Planning Area 4 will create a buffer between the non-residential uses and the residences to the south.



- Strong pedestrian linkages shall be provided between Planning Area 4 and the mixed-use development in Planning Area 15.
- All loading docks, outdoor storage areas or refuse collection visible from El Camino Real, Palomar Airport Road, El Fuerte Street or any adjacent residential area shall be appropriately screened.

#### Development Standards

Planning Area 4 shall be developed in accordance with the Planned Industrial (P-M) Zone standards set forth in Chapter 21.34 of the Carlsbad Municipal Code, which are measured from the right-of-way line.

- Front or Side Yard facing El Camino Real, Palomar Airport Road or El Fuerte Street – Minimum setback of fifty (50) feet fully landscaped and irrigated shall be provided. Subject to approval of the City Planner the landscaped portion of the setback may be reduced to thirty-five (35) feet to accommodate a driveway along the portion of the setback farthest from the right-of-way.
- Front or Side Yard on a Collector, Local or Private Street – Average setback of thirty-five (35) feet, with a minimum of twenty-five (25) feet. This setback shall be fully landscaped.
- Side Yard Interior – Ten (10) feet fully landscaped and irrigated.
- Rear Yard – Twenty (20) feet, of which ten (10) feet adjacent to the rear property line shall be fully landscaped and irrigated.

5. Planning Area 5

Description

Planning Area 5 (see Exhibit VII-15 on page VII-35) is located in the northeast corner of the Master Plan, south of Palomar Airport Road and bordered on the west by El Fuerte Street. Open Space Area 4 borders the southern boundary of Planning Area 5. Planning Area 4 is located across El Fuerte Street to the west of Planning Area 5. Planning Area 5 has a gross area of 28.0 acres and a net developable area of 27.7 acres.

Implementation of Master Plan Vision and Goals

The purpose of this planning area, including its design standards and allowed uses, is to provide for office or industrial uses (Goal 5). Planning Area 5 is designated as Planned Industrial due to its location within the Airport Influence Zone and its location near the intersection of Palomar Airport Road and El Fuerte Street, which provides for safe, convenient vehicular access (Goal 6). Residents of both the Bressi Ranch Master Plan and the affordable housing portion of the Rancho Carrillo Master Plan may also conveniently access this area using the sidewalks along the public streets (Goal 2). Employees in Planning Area 5 will be able to take advantage of the proposed bus stop on Palomar Airport Road (Goal 3).

General Plan & Zoning

General Plan Land Use:           Planned Industrial (PI)

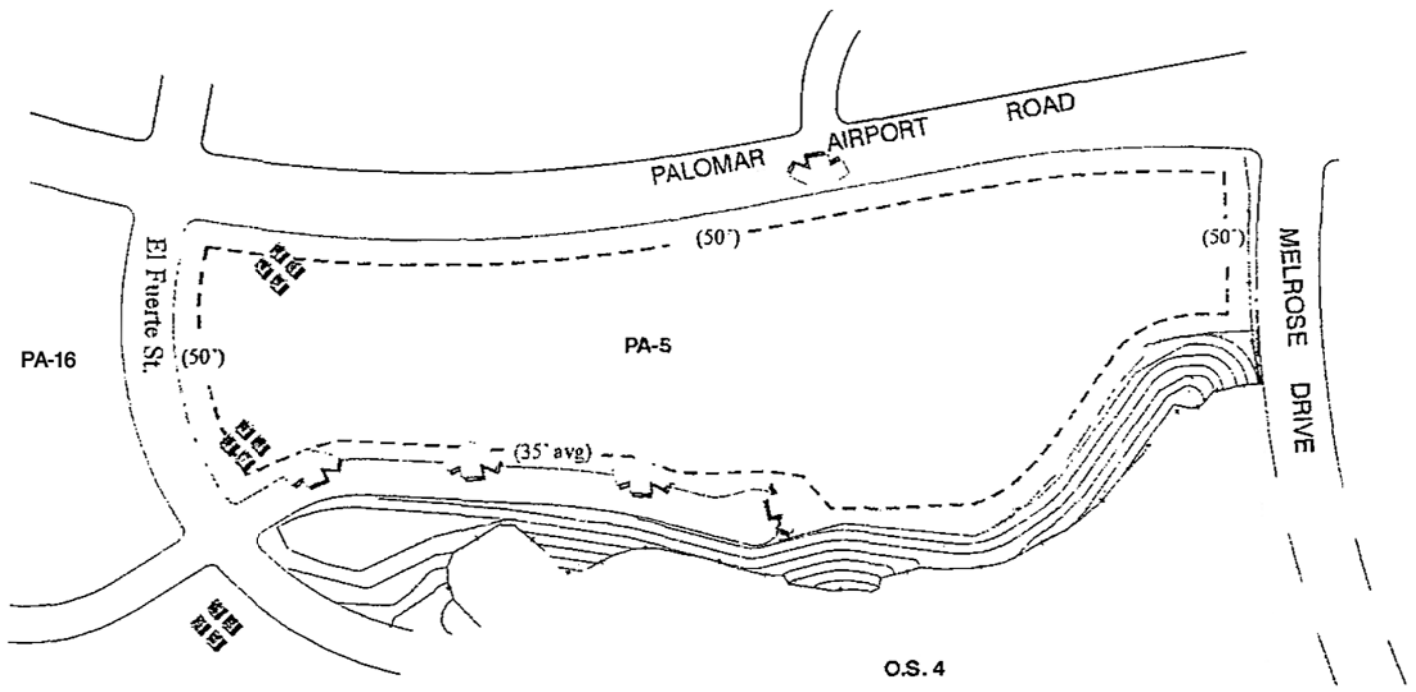
Zoning:                            Planned Industrial (P-M)

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan. No development shall occur on the proposed site at the southwest corner of Palomar Airport Road and Melrose Drive in Planning Area 5 unless first reviewed after notice and public hearing by the Planning Commission and approval by the City Council.

Uses Allowed








Office, Research & Development, Light Manufacturing and Warehousing, and all uses identified in Section 21.34 of the Carlsbad Municipal Code (P-M Zone) are allowed in Planning Area 5.



**Concept Sketch**  
*(for illustrative purposes only)*

Note: Industrial access points shall provide intersection spacing per Section VIIB.

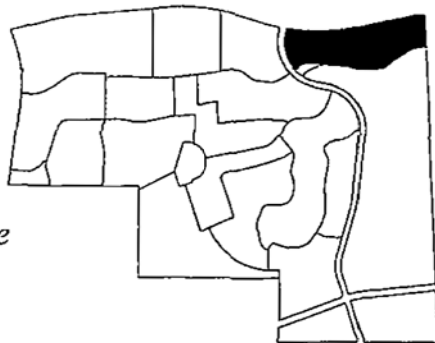
**Legend**

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Wide Trail
-  Setbacks(Width in Feet)
-  Bus Stop/ Bus Turnout

General Plan Land Use:  
*PI*

Zoning:  
*PM*

Product:  
*Industrial/Office/Warehouse*  
*Per P-M Zone as further*  
*restricted by Master Plan*



Use Restrictions

No facilities in the development shall store, handle, or use toxic or highly toxic gases as defined in the most recent Uniform Fire Code (UFC) at quantities that exceed exempt amounts as defined in the most recent UFC.

Facilities which store, handle, or use regulated substances as defined in the California Health and Safety Code §25532(g) in excess of threshold quantities shall prepare risk management plans for determination of risk to the community.

Facilities which store, handle, or use any quantity of a toxic or highly toxic gas as defined in the most recent Uniform Fire Code which are also regulated substances as defined in the California Health and Safety Code §25532(g) shall prepare an offsite consequence analysis (OCA). This analysis shall be performed in accordance with Title 19 of the California Code of Regulations §2750.2 through §2750.3. If the OCA shows the release could impact the residential community, the facility will not store, handle, or use the material in those quantities. If a decrease in the quantity of material reduces the distance to toxic endpoint to where the community is not impacted, the facility shall be able to utilize the material in that quantity.

Design Criteria

In addition to complying with the design criteria contained in Sections VII.A, B and C of this Master Plan, development in Planning Area 5 shall comply with the following design criteria:

- The northern portion of Planning Area 5 is located within the McClellan-Palomar Airport Flight Activity Zone. Development within this portion of Planning Area 5 will be restricted in accordance with the requirements of the Comprehensive Land Use Plan for McClellan-Palomar Airport.
- All buildings shall be constructed in accordance with the Design Criteria contained in Section VII.C to ensure they appear attractive from Palomar Airport Road and El Fuerte Street.
- Access to Citywide Trail Link No. 35 shall be provided along the southern and/or eastern edge of Planning Area 5. This area should be designed to encourage pedestrian access from the adjacent, affordable housing portion of the Rancho Carrillo Master Plan.
- All loading docks, outdoor storage areas or refuse collection visible from El Camino Real, Palomar Airport Road, El Fuerte Street or any adjacent residential area shall be appropriately screened.

•

Development Standards

Planning Area 5 shall be developed in accordance with the Planned Industrial (P-M) Zone standards set forth in Chapter 21.34 of the Carlsbad Municipal Code, which include the following setbacks that are measured from the right-of-way line:

- Front or Side Yard facing El Camino Real, Palomar Airport Road or El Fuerte Street – Minimum setback of fifty (50) feet fully landscaped and irrigated shall be provided. Subject to approval of the City Planner the landscaped portion of the setback may be reduced to thirty-five (35) feet to accommodate a driveway along the portion of the setback farthest from the right-of-way.
- Front or Side Yard on a Collector, Local or Private Street – Average setback of thirty-five (35) feet, with a minimum of twenty-five (25) feet. This setback shall be fully landscaped.
- Side Yard Interior – Ten (10) feet fully landscaped and irrigated.
- Rear Yard – Twenty (20) feet, of which ten (10) feet adjacent to the rear property line shall be fully landscaped and irrigated.

6. Planning Area 14

Description

Planning Area 14 (see Exhibit VII-16 on page VII-39) is located at the southeast corner of El Fuerte Street and Planning Area 5. Planning Area 14 has a gross area of 1.1 acres and a net developable area of 1.1 acres.

Implementation of Master Plan Vision and Goals

The purpose of this planning area, including its design standards and allowed uses, is to provide for a wide variety of job opportunities in the office or industrial/warehouse uses that will be developed in this area (Goal 5). Planning Area 14 is designated as Planned Industrial due to its location along El Fuerte directly to the south of Planning Area 5. El Fuerte Street provides convenient vehicular access to Planning Area 14 and offers a transit stop for workers in this area (Goal 3). Residents of the Master Plan may conveniently access this area using the sidewalks along the public streets (Goal 2). Due to its small size and location along the periphery of the industrial area, Planning Area 14 may be developed with a gas station or other facility that provides service to people working in the industrial area, subject to approval of a Conditional Use Permit.

General Plan & Zoning

General Plan Land Use:           Planned Industrial (PI)

Zoning:                               Planned Industrial (P-M)

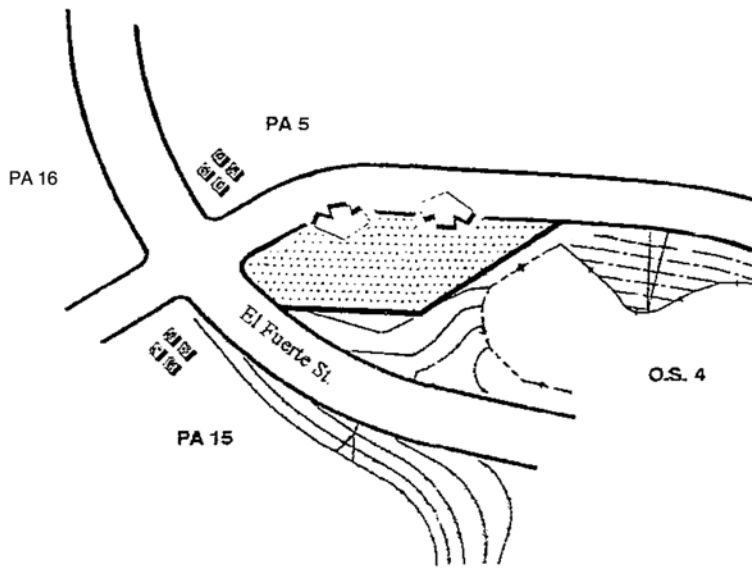
Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

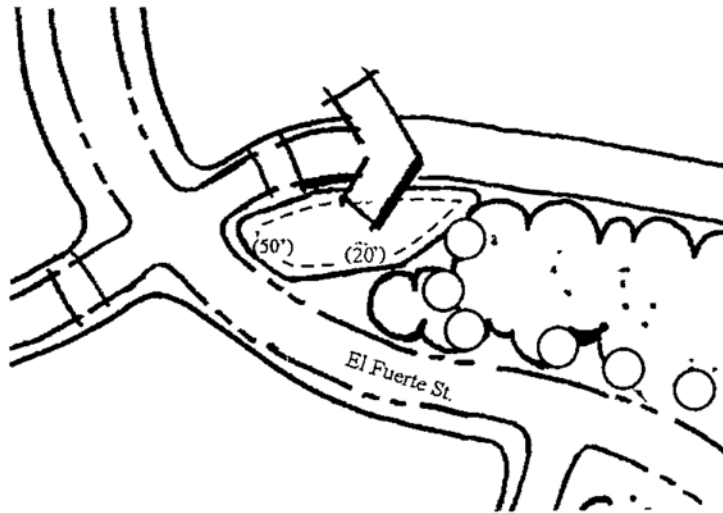
Uses Allowed

Office, Research & Development, Light Manufacturing and Warehousing, and all uses identified in Section 21.34 of the Carlsbad Municipal Code (P-M Zone) are allowed in Planning Area 14 except as further restricted by this Master Plan.












**Concept Sketch**  
*(for illustrative purposes only)*



**Legend**

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Trail
-  Setbacks(Width in Feet)
-  Bus Stop/ Bus Turnout

General Plan Land Use:  
PI

Zoning:  
PM

Product:  
*Service Station*  
*Per P-M Zone as further*  
*restricted by Master Plan*



Note: *Service Station*  
*subject to a C.U.P.*

*Bressi Ranch*

*Planning Area 14*

VII-16



Use Restrictions

If Planning Area 14 is developed with industrial uses it shall comply with the following use restrictions:

No facilities in the development shall store, handle, or use toxic or highly toxic gases as defined in the most recent Uniform Fire Code (UFC) at quantities that exceed exempt amounts as defined in the most recent UFC.

Facilities which store, handle, or use regulated substances as defined in the California Health and Safety Code §25532(g) in excess of threshold quantities shall prepare risk management plans for determination of risk to the community.

Facilities which store, handle, or use any quantity of a toxic or highly toxic gas as defined in the most recent Uniform Fire Code which are also regulated substances as defined in the California Health and Safety Code §25532(g) shall prepare an offsite consequence analysis (OCA). This analysis shall be performed in accordance with Title 19 of the California Code of Regulations §2750.2 through §2750.3. If the OCA shows the release could impact the residential community, the facility will not store, handle, or use the material in those quantities. If a decrease in the quantity of material reduces the distance to toxic endpoint to where the community is not impacted, the facility shall be able to utilize the material in that quantity.

Design Criteria

In addition to complying with the design criteria contained in Sections VII.A, B and C of this Master Plan, development in Planning Area 14 shall comply with the following design criteria:

- No access to Planning Area 14 shall be located along El Fuerte Street. Access to Planning Area 14 from the industrial street shall be located a minimum of 150 feet from the intersections of the industrial street with El Fuerte.
- Any development shall be compatible in appearance to development within Planning Area 5.

Development Standards

Planning Area 14 shall be developed in accordance with the Planned Industrial (P-M) Zone standards set forth in Section 21.34 of the Carlsbad Municipal Code, which include the following setbacks:

All development in Planning Area 14 shall comply with the setback requirements of Section 21.34.070 of the Planned Industrial Zone. All setbacks are measured from the right of way line. The following paragraphs summarize these setback requirements.

- Front or Side Yard facing El Camino Real, Palomar Airport Road or El Fuerte Street – Minimum setback of fifty (50) feet fully landscaped and irrigated shall be provided. Subject to approval of the City Planner the landscaped portion of the setback may be reduced to thirty-five (35) feet to accommodate a driveway along the portion of the setback farthest from the right-of-way.
- Front or Side Yard on a Collector, Local or Private Street – Average setback of thirty-five (35) feet, with a minimum of twenty-five (25) feet. This setback shall be fully landscaped.
- Side Yard Interior – Ten (10) feet fully landscaped and irrigated.
- Rear Yard – Twenty (20) feet, of which ten (10) feet adjacent to the rear property line shall be fully landscaped and irrigated.

## **VIII. COMMERCIAL/COMMUNITY FACILITIES DEVELOPMENT STANDARDS & DESIGN GUIDELINES**

Development in Planning Areas 13, 15 and 16 shall be compatible with and complimentary to the residential development of the Bressi Ranch Master Plan that it serves. Elements and design features from the residential design guidelines shall be incorporated into the design and development of these planning areas. All signage shall be compatible in appearance and scale with the adjacent buildings and designed in conformance with the requirements of Chapter 21.41 of the Carlsbad Municipal Code.

### **A. Individual Planning Area Standards**

#### **1. Planning Area 13**

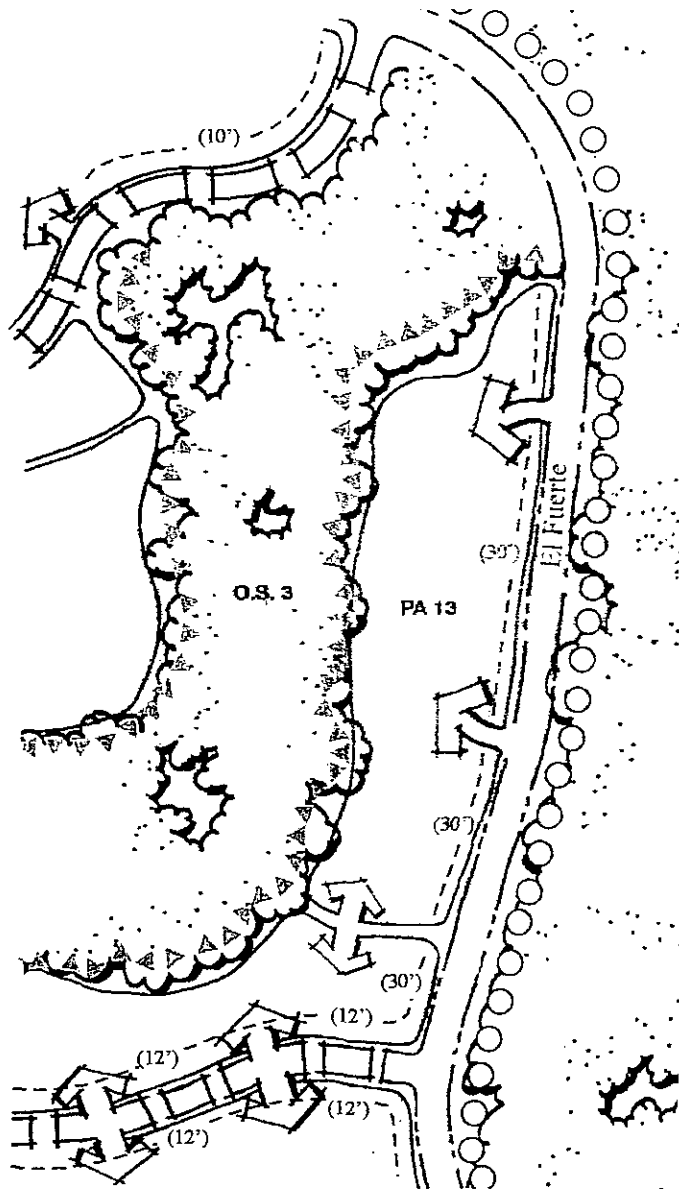
##### Description

Planning Area 13 is located north of Planning Area 12, east of Open Space Area 3, and to the west of El Fuerte Street. Planning Area 13 has a gross area of 13.7 acres and a net developable area of 13.6 acres (see Exhibit VIII-1 on page VIII-2).

##### Implementation of Master Plan Vision and Goals

Planning Area 13 will be developed with community facilities to serve the residents of the Bressi Ranch Master Plan in accordance with Goals 1 and 2 of the Master Plan. A recreational vehicle storage area shall be provided in the northern portion of Planning Area 13.

Subject to approval of a Conditional Use Permit a private school may be located in Planning Area 13. A minimum of 5.09 net acres of Planning Area 13 must be developed with community facility uses as provided for in Section 21.25.050 of the Carlsbad Municipal Code. A minimum of 2.0 acres of the 5.09 acres must be developed with a day care center. A private school may be located on the remaining 7.51 net acres of this site. If a school campus is not developed on this site the entire 13.6 net acres of this site shall be developed with community facilities as permitted by Section 21.25.050 of the Carlsbad Municipal Code.

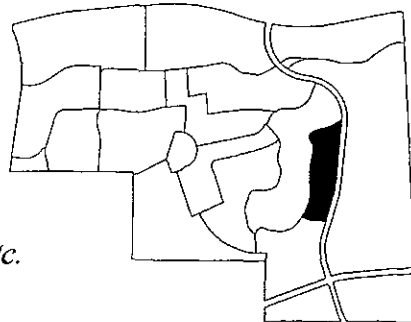


Concept Sketch  
(for illustrative purposes only)

General Plan Land Use:  
*CF/P*

Zoning:  
*CF*

Product:  
*Private School, RV, Church etc.*



### Legend

- Access
- Detention Basin
- Open Space
- Fire Protection Zone
- City Trail
- Setbacks(Width in Feet)
- Bus Stop/ Bus Turnout
- Runway Protection Zone
- Airport Influence Area
- Flight Activity Zone

*Bressi Ranch*

A planned community in the City of Carlsbad, California



3-26-02 Bressi PA13.cdr

VIII-2

*Planning Area 13*

VIII-1



General Plan & Zoning

General Plan Land Use: Community Facilities / Private School (CF/P)

Zoning: Community Facilities (C-F)

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

Uses Allowed

Day care facilities are permitted subject to approval of a Site Development Plan.

The following community facilities are permitted subject to approval of a Conditional Use Permit:

- Churches, synagogues, temples, and other places of worship
- Religious reading room (separate from church structure)
- Welfare and charitable services (private or semi-private) with no permanent residential uses (e.g. Goodwill, Red Cross, Travelers Aid)
- Social clubs (non-commercial)
- Fraternal associations and lodges (except college fraternities/sororities)
- Youth organizations (e.g., Boy Scouts, Girl Scouts, Boys' and Girls' Clubs, YMCA and YWCA facilities except lodgings)
- Civic associations (e.g., League of Women Voters, etc.)
- Veterans' organizations (including meeting facilities)
- Adult and/or senior day care and/or recreation facility (private or non-private)
- Other uses of a similar character as determined by the City Planner to be community facilities uses
- Private school temporary relocateable buildings may be allowed on an interim basis for the private school similar to those used for public schools subject to approval of a Conditional Use Permit.
- Recreational vehicle storage



Design Criteria

- All buildings shall be designed and located so as to provide an attractive appearance from El Fuerte Street.
- A strong pedestrian connection shall be provided from Planning Area 13 to the other portions of the Master Plan.
- The recreational vehicle storage area shall be screened from the adjacent community facilities by berming and/or landscaping. Adequate screening shall also be provided to screen the RV storage area from motorists on El Fuerte Street.

Development Standards

In addition to complying with all applicable requirements of the Community Facilities (C-F) Zone, development in Planning Area 13 shall comply with the following standards:

- Setbacks from El Fuerte Street - All structures shall maintain a thirty (30) foot setback from the El Fuerte Right-of-Way. Parking, drive aisles and walls over 42" in height shall maintain a fifteen (15) foot setback from El Fuerte Street.
- Interior Side & Rear Yards - All structures shall maintain a ten (10) foot setback from all interior lot lines. Parking and drive aisles shall maintain a minimum five (5) foot setback from all interior lot lines.
- Building Separations – A minimum ten (10) foot separation shall be maintained between all single story units, with a maximum plate height of fifteen (15) feet. All other structures shall maintain a minimum twenty (20) foot building separation.
- Parking – Parking shall be provided in accordance with the requirements of Chapter 21.44 of the Carlsbad Municipal Code.
- Lot Size – No lots less than one acre in size shall be permitted in Planning Area 13.
- The RV storage lot shall provide at least 10,460 square feet of storage area (exclusive of driveways and approaches). This equates to 20 square feet of RV storage for each market rate residential unit built in the Bressi Ranch Master Plan.

2. Planning Area 15

Description

Planning Area 15 is located in the northern portion of the Master Plan area, south of Planning Areas 4 and 16, east of Planning Area 6, and west of El Fuerte Street. The southern edge of Planning Area 15 borders Planning Area 7. Planning Area 15 has a gross area of 27.7 acres and a net developable area of 26.7 acres (see Exhibit VIII-2 on page VIII-6).

Implementation of Master Plan Vision and Goals

The purpose of this planning area is to allow for the creation of a unique mixed-use neighborhood that will create a sense of place within the Bressi Ranch Master Plan that is not dependent on the automobile. Planning Area 15 will function like a small traditional village, which may include residential and commercial uses surrounding a Village Square. Within this planning area there will be a mixture of commercial/retail, office, housing, and community facilities.

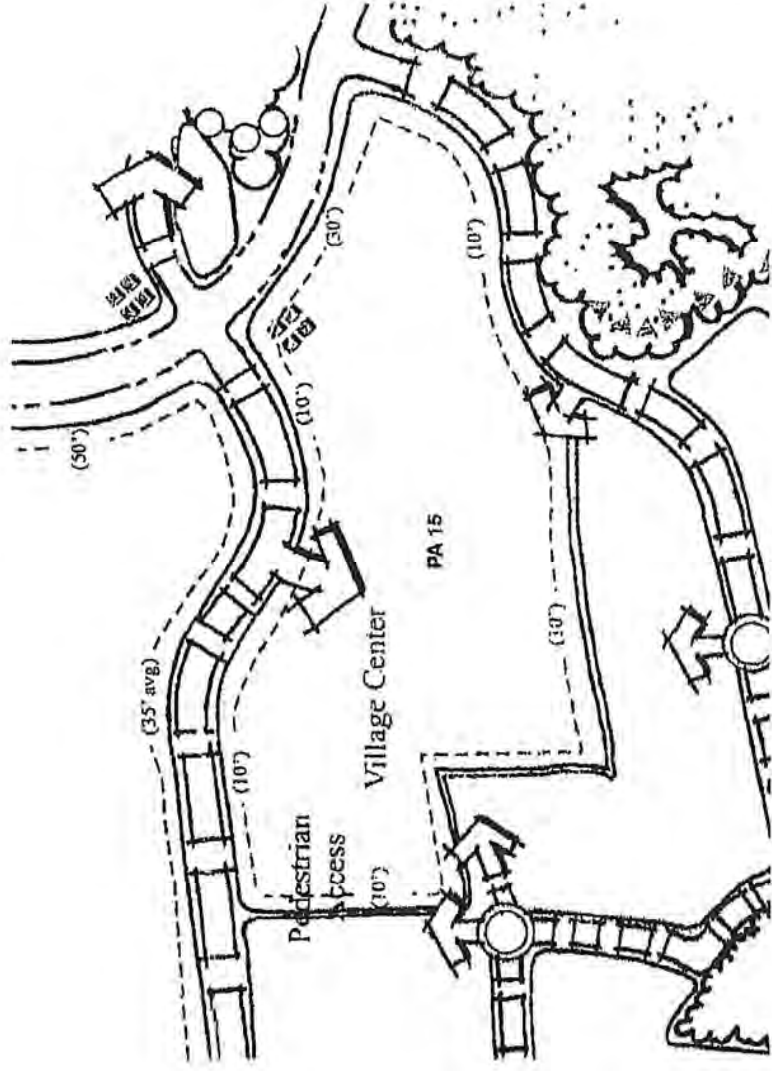
Many of the goals of the Bressi Ranch Master Plan will be incorporated into the design of this planning area. Housing, jobs, daily needs and other activities are all within easy walking (Goal 2). A variety of uses are also within easy walking distance of transit stops on El Fuerte (Goal 3). Planning Area 15 will contain housing that satisfies the City's Affordable Housing Criteria and possibly market rate units (Goal 4). There will be a variety of job opportunities in Planning Area 15 (Goal 5). Planning Area 15 will serve as the central activity focus for the Bressi Ranch Master Plan by combining residential, commercial, civic and recreational uses (Goal 7). A Village Square may be centrally located in Planning Area 15 to encourage frequent use by the public (Goals 8 and 9).

Exhibit VIII-3 on page VIII-7 shows a possible scenario for the development of Planning Area 15 and how the uses can tie into the adjacent commercial and residential areas of the Master Plan. The exact location and details of the various land uses will be determined as a part of the review of the Site Development Plans and/or Conditional Use Permits for the various land uses listed below.

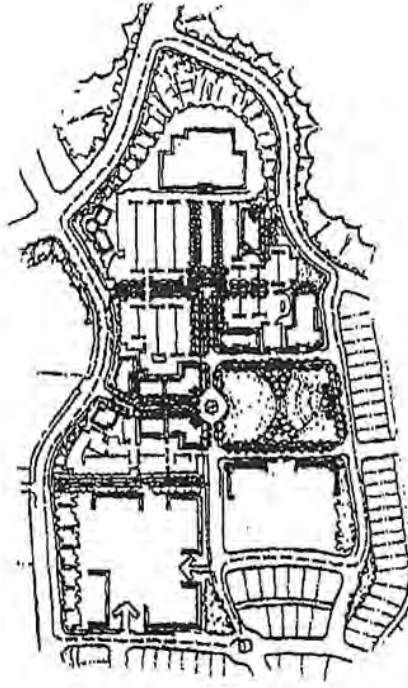
General Plan & Zoning

General Plan Land Use: R-23/L/CF

Zoning: RDM/C-2/C-F



Concept Sketch  
*(for illustrative purposes only)*










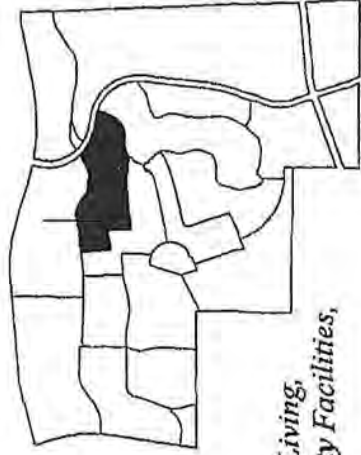
General Plan Land Use:  
R-23/L/CF

Zoning:  
RD-M/C-L/CF

Product:  
*Affordable Housing, Assisted Living,  
Commercial Office, Community Facilities,  
Town Green*

## Legend

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Trail
-  Setbacks (Width in Feet)
-  Bus Stop/ Bus Turnout



**Brewer Ranch**

A planned community in the City of Carlsbad, California

Planning Area 15

VIII-2

VIII-6









Discretionary Permits Required for Development

Prior to issuance of any grading or building permits for this planning area, discretionary applications shall be processed in accordance with the requirements of Chapter VI of this Master Plan.

Uses Allowed

All commercial uses shall be conducted wholly within a building except enterprises customarily conducted in the open (e.g., outdoor dining).

Products made incidental to a permitted commercial use and manufactured or processed on the premises shall be sold only at retail on the premises, and not more than five persons may be employed in such manufacturing, processing and treatment of products.

Storage for commercial enterprises shall be limited to accessory storage of commodities sold at retail on the premises.

The Master Plan anticipates the following uses will be developed in Planning Area 15:

- Up to 120 housing units, of which 100 units must be “affordable” per an approved housing agreement with the City of Carlsbad.
- Up to 200 assisted care living units
- A Village Square that serves as the major public gathering area for the Master Plan. The intention for the Village Square is to provide an area where office workers, shoppers and the surrounding neighborhoods can frequent this open space throughout the day and evening for strolling, lunch and/or recreation and provide an area for small-scale weekend events. A community facility may be included to serve as a focal point and backdrop for the Village Square.
- A temporary information center
- Up to 130,000 square feet of commercial/office/community facilities space

In addition to the listed anticipated uses, those uses identified in Chapters 21.24 (RD-M), 21.25 (CF), 21.27 (O) and 21.28 (C-2; retail and service business catering directly to the consumer) of the Carlsbad Municipal Code are permitted in Planning Area 15.

Uses and structures permitted subject to approval of a Conditional Use Permit include:

- Athletic Club
- Arcade
- Bar
- Bed and Breakfast
- Hotel or Motel
- Bowling Alley
- On and Off Sale Liquor
- Open Air Theater
- Pool hall
- Private Recreation Center
- Residential Care Facility (more than 6 persons)
- Veterinarian
- Incidental outdoor dining subject to the development standards set forth in Section 21.26.013(c) of the Carlsbad Municipal Code.

#### Prohibited Uses

The following uses, though allowed by right or by approval of a Conditional Use Permit in the RD-M (Chapter 21.24), CF (Chapter 21.25), O (Chapter 21.27) or C-2 (Chapter 21.28) zones of the Carlsbad Municipal Code, do not support the vision of a walkable mixed-use Village Center and are therefore prohibited in Planning Area 15:

- Single Family Detached Units
- Duplex Units
- Hospital
- Gas/Service Station
- Car Wash
- Auto Repair
- Upholstery Shop
- Commercial Printing
- Photo Engraving

#### Development Standards

Unless specified otherwise in this section, all development in Planning Area 15 will comply with the corresponding regulations and development standards of Chapters 21.28 (C-2) and 21.44 (Parking) of the Carlsbad Municipal Code as outlined below:

- No habitable building shall exceed thirty-five feet or three levels in height except that additional habitable building height may be permitted to a maximum of forty-five feet through a Minor Site Development Plan

processed in accordance with provisions of Chapter 21.06 of the Carlsbad Municipal Code.

- Allowed non-habitable architectural height protrusions as described in Section 21.46.020 of the Carlsbad Municipal Code shall not exceed forty-five feet.
- No setbacks are required except that residential buildings that are not an integrated component of a mixed-use building or group of attached buildings within the same block shall maintain setbacks consistent with Chapter 21.24 of the Carlsbad Municipal Code.
- Residential “Planned Developments” that are an integrated component of a mixed-use building or group of attached buildings within the same block shall include:
  - i. 200 square feet of centralized community recreational space per unit. The required recreational space may be part of a larger community facility.
  - ii. 480 cubic feet of enclosed storage area shall be provided for each unit.
  - iii. Either a 10' x 10' ground floor patio or 6' x 10' balcony.
- Residential “Planned Developments” that are not an integrated component of a mixed-use building or group of attached buildings within the same block shall comply with all of the applicable development standards of Chapter 21.45 of the Carlsbad Municipal Code.



General Design Criteria

Planning Area 15 will consist of a variety of land uses, which will be linked together by the following design guidelines that are applicable to all portions of Planning Area 15:

- Buildings should be designed with the purpose of preserving land area for pedestrian uses, which may include creating second floor space for less pedestrian intense uses like offices and residences.
- Adequate building separation should be provided to preserve a sense of place, open space, pedestrian scale and amenities to make the space inviting.
- All structures should be located in relation to each other to enhance the Village Center vision of creating safe, convenient, and interesting pedestrian places.
- All aspects of development within the Village Center should promote the quality of life for residents, shoppers, business owners and visitors.
- Buildings shall be oriented with windows, entries, balconies and/or porches oriented towards the street or active pedestrian area. This design will increase public safety by placing “eyes” and activity on the street.
- Street trees will also help to define and organize the Village Center, providing familiar connections throughout. Each type of street shall have one species of street tree for the sidewalk planting area, with an additional street tree type for any in-street parking space trees or linear parks or planted medians.
- Sidewalks and/or trails will be an integral part of project design to provide convenient pedestrian access to jobs in the industrial area to the north. Sidewalks should be located on both sides of every street throughout the Village Center, providing for pedestrian connections to adjacent uses.
- Wherever feasible, buildings should be oriented toward the Village Square and mixed-use street. This, along with the use of landscaping, will serve to create a buffer from the non-residential uses to the north.
- Where appropriate, streets should have parking on both sides of the street, helping to slow traffic while increasing pedestrian activity. Traffic calming techniques such as bulb-outs may be used at intersections to slow traffic and make street crossings safer for pedestrians in the Village Center.
- Appropriate lighting and site furnishings will help set the character of the Village Center. Pedestrian scale lighting similar in style and size to that used in Carlsbad’s Village Area should be used in this portion of the Master Plan. Cobra head style fixtures and poles are discouraged.
- Where appropriate, a combination of landscaping and/or short walls shall be provided at the perimeter of parking lots adjacent to residences. All

parking lot lighting shall be designed so that it does not spill over into adjacent residential areas.

The following design guidelines are applicable to the variety of components of Planning Area 15:

**Mixed-Use Street** – The following design guidelines will ensure small scale, pedestrian oriented retail, residential and office uses to create a lively village atmosphere.

*Building and Massing*

- Towers, integrated into the building massing, may be used to emphasize the center of a retail area or entry to a particular vehicular travel corridor. Smaller tower elements may be used on the buildings to create balance and identify entries.
- Shops and businesses may be sited close to sidewalks with a minimum setback. Outdoor seating areas for restaurants and cafes along the street and sidewalk are encouraged.
- Building entries and windows should line streets to create a pedestrian-friendly character.
- Building massing will be used to highlight the location of building entries. Greater height may be used to accentuate entries in the form of tower elements, tall openings, or a central mass at an entry plaza.
- Parking lots visible from and along street frontages should be minimized to encourage a stronger pedestrian connection between buildings.
- Convenient but inconspicuous pedestrian links should be provided to parking areas.
- Residential designated parking spaces should be located behind the buildings, away from public pedestrian areas.

*Landscape and Hardscape*

- A landscape buffer should be placed where parking lot entries abut streets. However, care should be taken to design and site the buffers such that they do not provide ambush points or obstruct views.
- Street trees should be planted on both sides of the street and spaced no more than required for the chosen species.
- Landscaped islands should be located between spaces in the parking aisles to provide for additional greenery. Deciduous shade trees and low growing flowering shrubs should be planted in the islands.
- Due to the tight dimensions of the mixed-use street (60' right-of-way with two story buildings), a smaller type of tree should be planted in the sidewalk area, with a larger street tree type for any in-street parking space trees.

- Generous tree grates should occur along sidewalks and in plazas where a continuous walking surface is needed.
- Tree guards should extend vertically from tree grates to protect trees in highly active areas.
- Raised planters and/or pots should be placed along retail frontages or where setback occurs to soften the character of the mixed-use street.

#### *Connection to Adjacent Uses*

- Pedestrian activity should be encouraged through the use of inviting pedestrian pathways and/or sidewalks along streets leading from the residential and industrial areas.
- Extra wide sidewalks that can accommodate street furniture and outside café seating should be provided.
- Sidewalks and plazas in high pedestrian use areas should be designed with a variety of textures and colors to create interest.
- A strong pedestrian connection from proposed bus stops should be provided with sidewalks and visual keys.

#### *Traffic Calming*

- Bulbouts should be used to narrow the effective crossing distance for pedestrians at crosswalks while still permitting on-street parking along the majority of the block.
- Raised or textured crosswalks should be used at intersections to force cars to slow down.
- On-street parking should be provided to help slow traffic while keeping the street narrow.
- Parking islands located in the parking lane will encourage slower speeds and give the appearance of a narrower street.
- All traffic calming measures shall be approved by the City's Traffic Engineer prior to construction.

#### *Street Lighting and Furniture*

- Lighting of streets and parking lots should correspond with the scale and style of the surrounding architecture and address pedestrian activity.
- Accent lighting of architectural features is encouraged to highlight building massing and enhance the pedestrian environment. In addition, building entries with high activity levels should be illuminated.
- Benches should be provided at regular intervals along the mixed-use street to encourage on-street activity. Their size should range from four feet to eight feet long.
- Bicycle racks should be provided to encourage alternative modes of transportation.
- Newspaper racks should be located near major pedestrian gathering areas. The design should consolidate all vending boxes into one rack.

- Trash receptacles should be located in retail areas at convenient locations.
- A variety of colors and textures are recommended for public sidewalks, subject to approval by the City Engineer and Public Works Director.

**Retail Center** – The following design guidelines will ensure that retail development maintains a strong pedestrian scale and link to surrounding development.

#### *Building and Massing*

- Retail buildings should be located along pedestrian links and provide pedestrian scale architecture.
- A tower element (focal point), possibly as part of a restaurant, should serve as an identifying feature.
- Pedestrian scale entry elements should be incorporated into all building entries.
- Wherever feasible, building entries should be located to take advantage of pedestrian connections.
- Buildings should be designed to present a “front” to adjacent streets.
- Parking lots to serve anchor tenants should be located conveniently near the buildings they serve.

#### *Landscaping*

- A landscape buffer should be placed where parking lots abut streets. However, care should be taken to design and site the buffers such that they do not provide ambush points or obstruct views of the retail businesses.
- Shade trees should be provided throughout parking lots to minimize the impact of hardscaping.
- Trees should be planted along connecting walkways to help identify the pedestrian connections and provide shade.

#### *Connection to Adjacent Uses*

- Where building entries do not abut a street, connecting walkways should be provided to link the building entrance to street sidewalks. This configuration is most likely for large anchor store tenants that require large parking lots.

#### *Traffic Calming*

- Parking lots should be organized with drive aisles running perpendicular to the anchor’s entry to minimize interaction with pedestrians.
- An adequate pedestrian drop-off zone should be provided in front of the anchor.

- Landscaped islands at the ends of parking lanes should be provided to help slow traffic through parking lots.
- All traffic calming measures must be approved by the City Traffic Engineer prior to construction.

*Street Lighting and Furniture*

- Parking lots and pedestrian walkways should be clearly lit.
- Decorative pedestrian scale streetlights as approved by the City should be provided.
- Bicycle parking should be visible from storefronts in order to improve the security for parked bicycles.
- Light from parking lots should not leak into residences or other sensitive areas. Landscaping may help reduce light glare and leakage.

**Housing** – The following design guidelines will ensure that there is a strong convenient pedestrian access to the industrial areas to the north, retail, recreation, and other residential areas.

- The primary entry of every building should face and be accessible from the street.
- Surface parking for multi-family residential buildings should be provided to the rear of the residence structure and away from any streets.
- There should be clearly defined landscaped paths linking the industrial, retail, and recreation areas to housing.
- Wherever possible, housing should be oriented either towards the Village Square or pedestrian corridors with visibility to public streets. Adequate landscaping should be provided to buffer the parking area from buildings.
- There should be a substantial separation between buildings fronting a street to allow for a landscaped drive into the interior parking area.

**Village Square** – A Village Square may serve as the major public gathering area for the Bressi Ranch Master Plan. Office workers, shoppers and the surrounding neighborhoods will frequent this open space throughout the day and evening for strolling, lunch and/or recreation. The following design guidelines will ensure that a Village Square could accommodate a community facility type of use while still providing sufficient open area to create a pedestrian friendly atmosphere.

- A Village Square should be large enough to accommodate small-scale weekend events.
- A large open field should be provided for the general public to enjoy.
- A community facility may be provided to serve as a focal point and backdrop.
- Deciduous trees or trellis-type structures should be located to provide adequate shading for comfortable mid-day use during the summer, and sunny areas for winter use.

- Landscape design should respect vistas created by streets. Views and linkages to streets and other public spaces and buildings must be respected and reinforced through design elements. For example, paths should align with important viewpoints; trees should not block views of significant public monuments or buildings, and perimeter landscaping should allow views into the park.
- There should be a clearly defined pedestrian connection from a Village Square to Open Space Area 2 (Village Green).



### 3. Planning Area 16

#### Description

Planning Area 16 is located in the northern portion of the Master Plan area, north of Planning Area 15, east of Planning Area 4, and west of El Fuerte Street. Planning Area 15 has a gross area of 17.7 acres and a net developable area of 17.7 acres (see Exhibit VIII-4 on page VIII-18).

This section only addresses the portion of Planning Area 16 proposed for commercial uses. For more information on the portion of the site that will be dedicated to residential uses, please see Section X.E.8. on page X-46.

#### Implementation of Master Plan Vision and Goals

The purpose of this planning area is to allow for the creation of a unique mixed-use neighborhood that will provide additional residential units and expand the commercial and restaurant uses available within walking distance to many Bressi Ranch residents. The commercial component of Planning Area 16 will extend Finnilla Place, and the main street feeling of the commercial center.

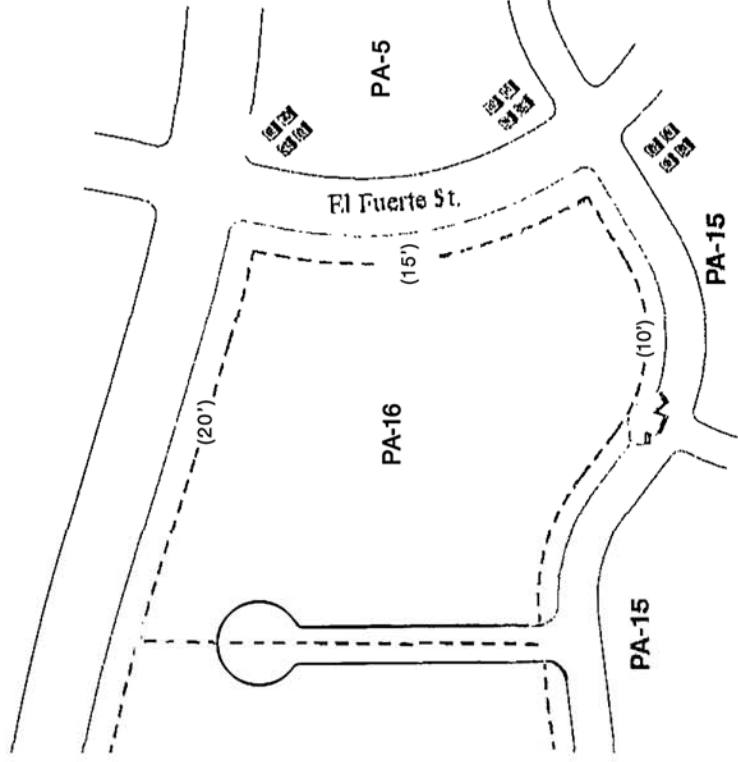
Many of the goals of the Bressi Ranch Master Plan will be incorporated into the design of this planning area. Housing, jobs, daily needs and other activities are all within easy walking (Goal 2). A variety of uses are also within easy walking distance of transit stops on El Fuerte (Goal 3). There will be a variety of job opportunities in Planning Area 16 (Goal 5). Planning Area 16 will expand the central activity focus for the Bressi Ranch Master Plan by extending the neighborhood main street (Goal 7).

Exhibit VIII-5 on page VIII-19 shows a possible scenario for the development of Planning Area 16 and how the uses can tie into the adjacent areas of the Master Plan. The exact location and details of the various land uses will be determined as a part of the review of the Site Development Plans and/or Conditional Use Permits for the various land uses listed below.

#### General Plan & Zoning

General Plan Land Use: R-23/L

Zoning: RD-M/C-L



## Legend

Concept Sketch  
*(for illustrative purposes only)*

*Note: Industrial access points shall provide a minimum intersection spacing of 300 feet.*



Access



Detention Basin



Open Space



Fire Protection Zone

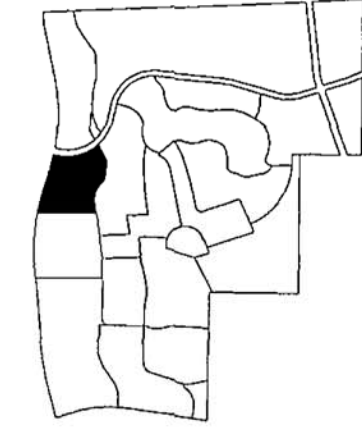


City Wide Trail

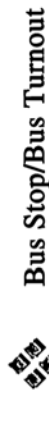
General Plan Land Use:  
**R-23/L**

Zoning:  
**RD-M/C-L**

Product:  
**Multi-Family Attached/  
Commercial**



--- Setbacks (Width in Feet)



Bus Stop/Bus Turnout

--- Inner Approach/Departure Zone

..... Inner Turning Zone

**Brewer Ranch**

**Planning Area 16**

VIII-4

A planned community in the City of Carlsbad, California



**UPTOWN BRESSI**

**PROJECT NAME:** UPTOWN BRESSI  
**EXISTING ZONE:** PLANNED COMMUNITY (P-C) - (ZONED PLANNED INDUSTRIAL (P-M) WITHIN THE BRESSI RANCH MASTER PLAN AND BRESSI RANCH MASTER PLAN (MP-178))  
**GENERAL PLAN:** PLANNED INDUSTRIAL (PI)  
**EXISTING USE:** VACANT LAND  
**PROPOSED USE:** R-2, U, S-2 (125 DWELLING UNITS AND COMMERCIAL (SEE COMMERCIAL TABULATION BY OTHERS))

UNIT TABULATION	PLAN				TOTAL DWELLING UNITS	NET ACRES	NET DENSITY
	PLAN 1	PLAN 2	PLAN 3	PLAN 4			
ROWHOMES	26	36	42	N/A	103		
STACKED FLATS	8	8	4	4	22		
AVERAGE NET DENSITY					125	7.67	16.50

**VISITABLE UNITS**  
 10% OF COVERED MULTI-FAMILY UNIT = 103, 1 to 11 UNITS

GROSS BUILDING COVERAGE			
BUILDING TYPE	BUILDINGS	AREA W/O DECK @ END	TOTAL
4 UNIT A (W/O END DECK)	1	4218 SF	8436 SF
4 UNIT B (W/O END DECK)	1	4332 SF	8664 SF
4 UNIT C (W/O END DECK)	0	4166 SF	0 SF
4 UNIT D (W/O END DECK)	4	4280 SF	17120 SF
5 UNIT A (W/O END DECK)	0	5211 SF	0 SF
5 UNIT B (W/O END DECK)	5	5325 SF	26625 SF
5 UNIT C (W/O END DECK)	2	5159 SF	10318 SF
5 UNIT D (W/O END DECK)	2	5273 SF	10546 SF
6 UNIT A (W/O END DECK)	0	6103 SF	0 SF
6 UNIT B (W/O END DECK)	1	6217 SF	6217 SF
6 UNIT C (W/O END DECK)	0	6039 SF	0 SF
6 UNIT D (W/O END DECK)	4	6153 SF	24612 SF
STACKED	2	7666 SF	15332 SF
POOL BUILDING	1	430 SF	430 SF
GROSS			124228 SF
NET ACRES			7.67 OR 33,4657 SF
COVERAGE ALLOWED			(0)
PERCENTAGE COVERAGE			38.2

**ROWHOME GROSS BUILDING AREA TABULATION**

UNIT AREAS (DOES NOT INCLUDE 2' AIR SPACE BETWEEN UNITS)			
UNIT AREA	AREA	NO. OF UNITS	UNIT TYPE
PLAN 1	1666 SF	26	3 BR / 3.5 BA
DECK 1A	116 SF	10	
DECK 1B	106 SF	16	
DECK 2	69 SF	26	
DECK 3A	69 SF	10	
DECK 3B	109 SF	16	
PATIO A	121 SF	26	
PATIO B	108 SF	10	
GARAGE	439 SF	16	
PLAN 2	1768 SF	36	3 BR / 3 BA
DECK 1	69 SF	36	
DECK 2	7 SF	36	
PATIO A	93 SF	15	
PATIO B	94 SF	20	
GARAGE	453 SF	36	
PLAN 3	2068 SF	42	3 BR / 3.5 BA
DECK 1A	124 SF	18	
DECK 1B	119 SF	24	
DECK 2A	123 SF	18	
DECK 2B	120 SF	24	
PATIO A	129 SF	18	
PATIO B	121 SF	24	
EXTENDED PATIO A	255 SF	18	
EXTENDED PATIO B	264 SF	24	
GARAGE	459 SF	42	
GROSS UNIT AREA			18226 SF
GROSS DECK/PATIO AREA			2184 SF
GROSS GARAGE AREA			42157 SF

**STACKED FLATS GROSS BUILDING AREA TABULATION**

UNIT AREAS (DOES NOT INCLUDE 2' AIR SPACE BETWEEN UNITS)			
UNIT AREA	AREA	NO. OF UNITS	UNIT TYPE
PLAN 1	735 SF	6	1 BR/1 BA
DECK	81 SF	6	
PLAN 2	963 SF	8	2 BR/2 BA
DECK	128 SF	8	
PLAN 3	1163 SF	4	2 BR/2 BA
DECK	78 SF	4	
PLAN 4	1340 SF	4	3 BR/2 BA
DECK	93 SF	4	
GROSS UNIT AREA			22126 SF
GROSS DECK AREA			2184 SF

BUILDING AREAS (INCLUDES 2' AIR SPACE BETWEEN UNITS)				
	GARAGE	RESIDENTIAL	CORE	TOTAL
ACTUAL 1ST FLOOR LEVEL	5273 SF	735 SF	1179 SF	7187 SF
ACTUAL 2ND FLOOR LEVEL	0 SF	5175 SF	1199 SF	6374 SF
ACTUAL 3RD FLOOR LEVEL	0 SF	5175 SF	1199 SF	6374 SF
TOTAL ACTUAL PER BLDG				19235 SF
GROSS (2 BLDGS)				38870 SF

PARKING TABULATION			
PARKING REQUIRED			
1.5 SPACES / UBR	1.5 COU		9
2 SPACES / 2 BR OR MORE	2 COU		238
GUEST	25 COU		32
TOTAL REQUIRED			279
PARKING PROVIDED			
RESIDENT TOWNHOMES			206
RESIDENT FLATS			40
RESIDENT SURFACE			2
GUEST			35
TOTAL PROVIDED			283
TOTAL EXCESS PARKING			4

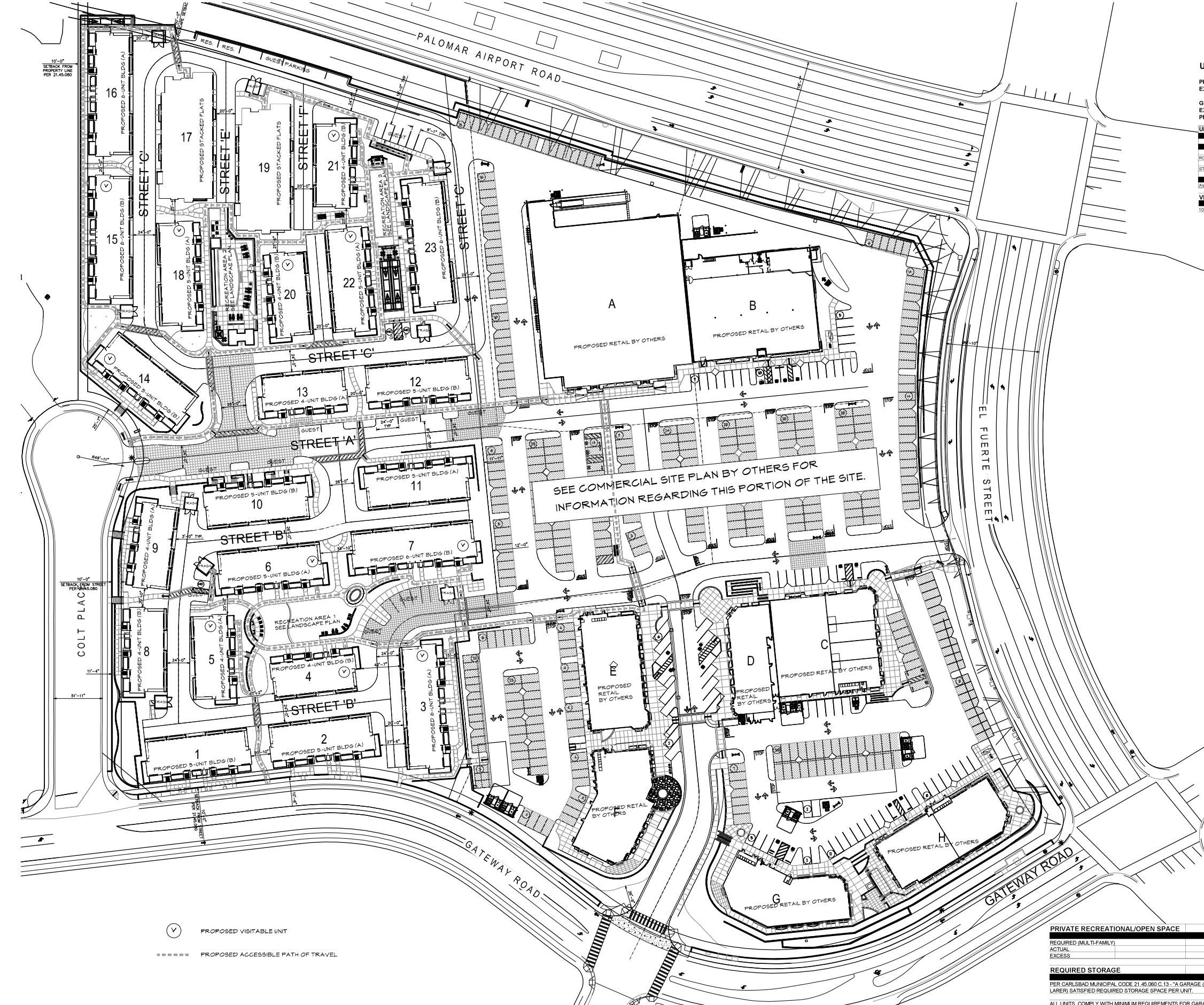
COMMUNITY RECREATIONAL/OPEN SPACE			
TOTAL REQUIRED			
RECREATION AREA 1	150 SF/OU		18750 SF
RECREATION AREA 2	9450 SF		9450 SF
RECREATION AREA 3	9647 SF		9647 SF
TOTAL PROVIDED			28655 SF
EXCESS			9935 SF
ACTIVE REQUIRED (75%)			14482.5 SF
ACTIVE PROVIDED			14073 SF
EXCESS			10.5 SF

PRIVATE RECREATIONAL/OPEN SPACE			
REQUIRED (MULTI-FAMILY)			
80 SF/DU			7500 SF
ACTUAL			25721 SF
EXCESS			18221 SF

REQUIRED STORAGE			
PER CARLSBAD MUNICIPAL CODE 21.45.060 C.13 - 1A GARAGE (12X20 ONE-CAR, 20X20 TWO-CAR, LARGER) SATISFIED REQUIRED STORAGE SPACE PER UNIT.			

ALL UNITS COMPLY WITH MINIMUM REQUIREMENTS FOR GARAGE SPACE INCLUDING STORAGE.

SEE COMMERCIAL SITE PLAN BY OTHERS FOR INFORMATION REGARDING THIS PORTION OF THE SITE.







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**Uptown Bressi Retail Center**  
Northwest Corner of Gateway Road & El Fuerte Street  
City of Carlsbad, California

**USE AND PARKING SUMMARY:**

ZONE	BUILDING AREA	PARKING REQUIRED	RATIO
SITE AREA: 437,080 SF 9.97 ACRES GROSS			
A MARKET	28,000 SF	140 SPACES	5.0 /1000
B PHARMACY WITH DRIVE THROUGH	16,454 SF	82 SPACES	5.0 /1000
C SHOPS	11,650 SF	58 SPACES	5.0 /1000
D SHOPS	5,000 SF	25 SPACES	5.0 /1000
E SHOPS	7,300 SF	37 SPACES	5.0 /1000
F SHOPS	7,160 SF	36 SPACES	5.0 /1000
G SHOPS	7,300 SF	37 SPACES	5.0 /1000
H SHOPS	8,300 SF	42 SPACES	5.0 /1000
OUTDOOR DINING	9,160 SF	46 SPACES	5.0 /1000
<b>TOTAL BUILDING AREA</b>	<b>91,164 SF</b>		
<b>TOTAL OUTDOOR DINING</b>	<b>9,160 SF</b>		
<b>TOTAL PARKING REQUIRED BY CITY</b>		<b>502 SPACES</b>	
<b>STANDARD PARKING PROVIDED</b>		<b>445 SPACES</b>	
<b>COMPACT PARKING PROVIDED (25% MAX)</b>		<b>46 SPACES</b>	
<b>TOTAL PARKING PROVIDED:</b>		<b>491 SPACES</b>	

**APPLICATIONS SUBMITTED:**

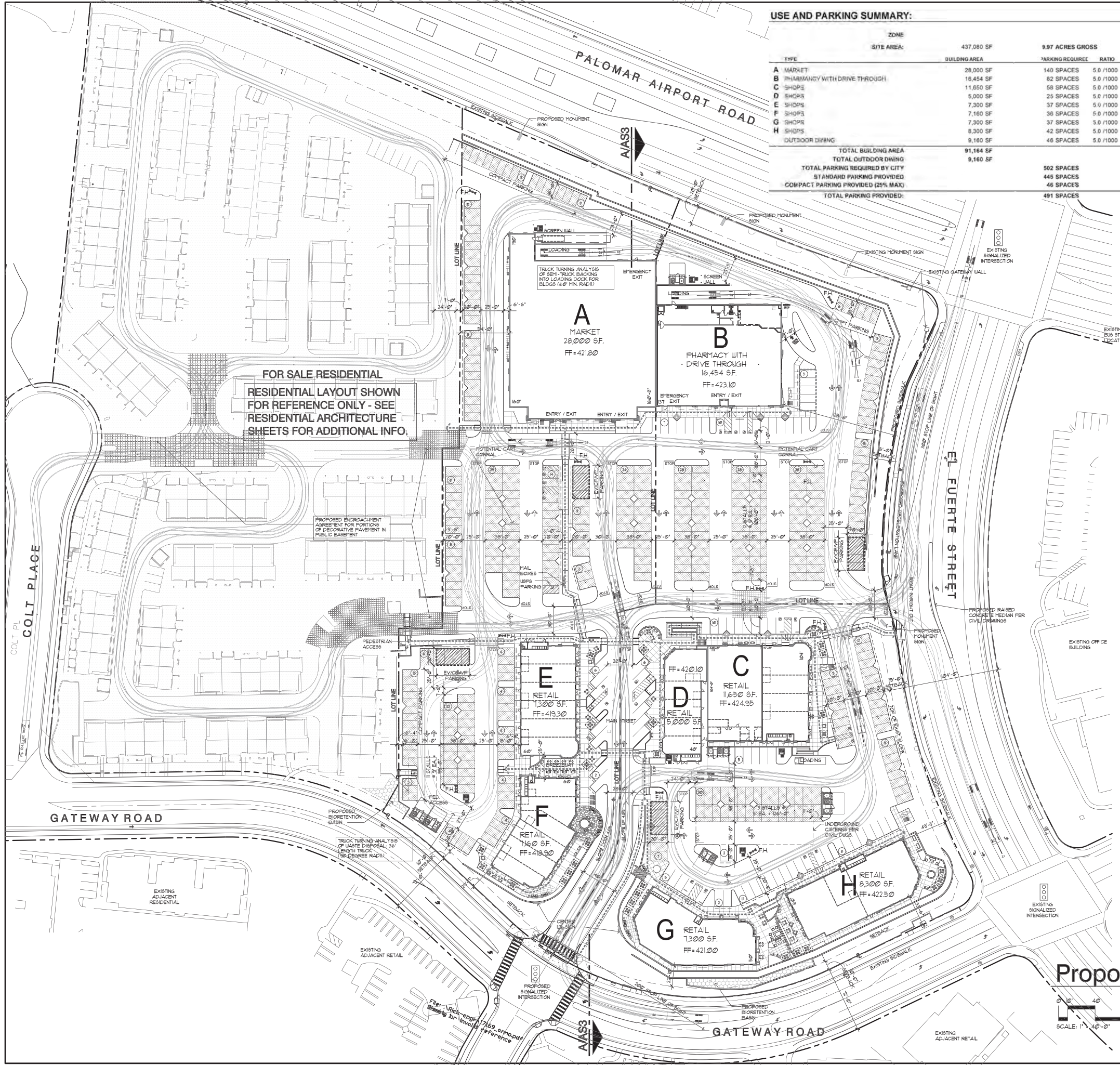
GENERAL PLAN AMENDMENT	GPA 14-04	CONDITIONAL USE PERMIT	CUP 14-03
MASTER PLAN AMENDMENT	MP 18(1)	CONDITIONAL USE PERMIT	CUP 16-01
TENTATIVE SUBDIVISION MAP	CT 14-03	ENVIRONMENTAL IMPACT REPORT	E-01
PLAN DEVELOPMENT PERMIT	PLD 14-10	LOCAL FACILITIES MANAGEMENT PLAN AMENDMENT	LFFP 01-11A
SITE DEVELOPMENT PLAN	SDP 14-13		
	SDP 16-06		

**NOTES:**

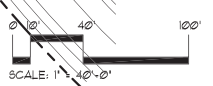
- ASSESSOR'S PARCEL NUMBERS: 213-262-06, 213-262-07, 213-262-08, 213-262-09
- LEGAL DESCRIPTION: LOTS 29 THROUGH 31 OF CARLSBAD TRACT CT 02-5 (SHESS) RANCH, IN THE CITY OF CARLSBAD, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF NO. 14360, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, FEBRUARY 4, 2005.
- SITE ACREAGE: 11.11 ACRES (SITE TOTAL) 931 ACRES (COMMERCIAL PORTION ONLY)
- SITE ZONING: P-C (PLANNED COMMUNITY)
- GENERAL PLAN LAND USE DESIGNATION: R-23 (RESIDENTIAL HIGH DENSITY) L (LOCAL COMMERCIAL)
- ALL PROPOSED COMMERCIAL BUILDINGS: ONE-STORY OCCUPANCY GROUP 11 (MERCANTILE) TYPE OF CONSTRUCTION 'V-B' NON-RATED FULLY SPRINKLERED
- PROPOSED ON-SITE BICYCLE RACKS/LOCKERS TOTAL: 463 PARKING STALLS x 5% (PER CALIF. GREEN CODE) = 24 REQUIRED (24 MIN PROVIDED)
- ALL OUTDOOR LIGHTING SHALL CONFORM TO SECTION 51020 ET. SEQ. OF THE SAN DIEGO COUNTY CODE WITH SECTION 6.322 ET. SEQ. OF THE COUNTY ZONING ORDINANCE.
- PROPOSED PROJECT COMPLIES WITH CARLSBAD BLDG. HEIGHT DEFINITION (ZONING ORDINANCE SECTION 2104.065)

**SITE PLAN LEGEND**

- PROPOSED PROPERTY LINE AND LOT LINE PER CIVIL DRAWINGS
- PROPOSED ACCESSIBLE PATH OF TRAVEL
- PROPOSED RETAINING WALL PER CIVIL DWGS. SEE LANDSCAPE DRAWINGS FOR COLOR AND FINISH
- PROPOSED FIRE HYDRANT PER CIVIL DWGS.
- PROPOSED SITE ELECTRICAL TRANSFORMER
- PROPOSED REUSE/RECYCLE ENCLOSURE W/ ROOF COVER, METAL SWING GATES & FRONT CONCRETE PAD (SEE SHEET A05)
- PROPOSED BICYCLE RACKS & LONG-TERM BICYCLE LOCKERS
- TRAFFIC DIRECTIONAL ARROWS
- VEHICLE STOP SIGN LOCATIONS
- PERMIABLE PAVERS PER CIVIL DRAWINGS
- POTENTIAL OUTDOOR DINING LOCATIONS & EMPLOYEE EATING AREAS (+/- 9,160 S.F.)
- PROPOSED PARKING LOT LIGHT STANDARDS - NOT SHOWN
- POTENTIAL DESIGNATED PARKING FOR LOW-EMITTING, FUEL EFFICIENT, AND CARPOOL/VAN POOL BY CHARGING STATION (21 STALLS TOTAL)
- PROPOSED TREE GRATE PER LANDSCAPE DWGS.



**Proposed Site Plan**



**Developer:**  
**SHEA PROPERTIES**  
9191 Towne Centre Dr.  
Suite 400  
San Diego, CA 92122  
(858) 526-6500

Issue Dates

SDP Submittal	11/24/2014
SDP Submittal	04/06/2015
SDP Submittal	03/10/2016
SDP Submittal	05/31/2016
Permit Set	-
Construction Set	-
Drawing Date	03/10/2016
Check By	NB
Drawn By	DFS
Scale	as noted
Job Number	13071.550
Sheet Number	

**AS1**

### Prohibited Uses

The following uses, though allowed by right or by approval of a Conditional Use Permit in the RD-M (Chapter 21.24), CF (Chapter 21.25), O (Chapter 21.27), or C-L (Chapter 21.31) zones of the Carlsbad Municipal Code, do not support the vision of a walkable, mixed-use Village Center and are therefore prohibited in Planning Area 16:

- Drive-through Restaurants
- Hospital
- Gas/Service Station
- Car Wash
- Auto Repair
- Upholstery Shop
- Commercial Printing
- Photo Engraving

### Development Standards

Unless specified otherwise in this section, all development in Planning Area 16 will comply with the corresponding regulations and development standards of Chapters 21.31 (C-L), 21.44 (Parking), and 21.41 (Signs) of the Carlsbad Municipal Code as outlined below:

- Height limitations for buildings shall be governed pursuant to Chapter 21.21 (C-L), Section 21.31.080.
- Residential buildings that are not an integrated component of a mixed-use building or group of attached buildings within the same block shall maintain development standards consistent with Section X.E.8. of this document, and Chapters 21.24 (RD-M) and 21.45 (PUD Standards) of the Carlsbad Municipal Code.
- Additional signage for Planning Area 16 will be detailed in a Sign Program that shall be submitted and approved pursuant to Section 21.41 of the Carlsbad Municipal Code.

### General Design Criteria

Planning Area 16 will consist of a mix of land uses, which will be linked together by the following design guidelines that are applicable to all portions of Planning Area 16:



- Adequate building separation should be provided to preserve a sense of place, open space, pedestrian scale and amenities to make the space inviting.
- All structures should be located in relation to each other to enhance the vision of creating safe, convenient, and interesting pedestrian places.
- All aspects of development within the commercial area should promote the quality of life for residents, shoppers, business owners and visitors.
- Buildings shall be oriented with windows, entries, balconies and/or porches oriented towards the street or active pedestrian area. This design will increase public safety by placing “eyes” and activity on the street.
- Street trees will also help to define and organize the commercial area, providing familiar connections throughout. Each type of street shall have one species of street tree for the sidewalk planting area, with an additional street tree type for any in-street parking space trees or linear parks or planted medians.
- Sidewalks and/or trails will be an integral part of project design to provide convenient pedestrian access to jobs in the industrial area to the north. Sidewalks should be located on both sides of every street throughout the commercial area, providing for pedestrian connections to adjacent uses.
- Wherever feasible, buildings should be oriented away from the residential use to the west. This, along with the use of landscaping, will serve to create a buffer from the residential areas.
- Where appropriate, streets or driveways/ drive aisles should have parking on both sides of the street, helping to slow traffic while increasing pedestrian activity. Traffic calming techniques such as bulb-outs may be used at intersections to slow traffic and make street crossings safer for pedestrians.
- Appropriate lighting and site furnishings will help set the character of the site. Pedestrian scale lighting similar in style and size to that used in the Bressi Village Center should be used in this portion of the Master Plan. Cobra head style fixtures and poles are discouraged.
- A mix of uses is desired in this Planning Area, however, some separation between uses may be appropriate. Wherever possible, the use of landscaping to provide visual separation should be used. The use of walls to separate uses is discouraged.
- All parking lot lighting shall be designed so that it does not spill over into adjacent residential areas.
- Outdoor dining and outdoor gathering spaces shall be encouraged to promote pedestrian activity and an active commercial space.

The following design guidelines are applicable to the variety of components of Planning Area 16:



**Main Street** – The following design guidelines will ensure small scale, pedestrian oriented retail, residential and office uses to create a lively village atmosphere.

*Building and Massing*

- Shops and businesses may be sited close to sidewalks with a minimum setback. Outdoor seating areas for restaurants and cafes along the street and sidewalk are encouraged.
- Building entries and windows should line streets to create a pedestrian-friendly character.
- Building massing will be used to highlight the location of building entries. Greater height may be used to accentuate entries in the form of tower elements, tall openings, or a central mass at an entry plaza.
- Parking lots visible from and along street frontages should be minimized to encourage a stronger pedestrian connection between buildings.
- Convenient but inconspicuous pedestrian links should be provided to parking areas.
- Residential designated parking spaces should be located behind the buildings, away from public pedestrian areas.

*Landscape and Hardscape*

- A landscape buffer should be placed where parking lot entries abut streets. However, care should be taken to design and site the buffers such that they do not provide ambush points or obstruct views.
- Street trees should be planted on both sides of the street and spaced no more than required for the chosen species.
- Landscaped islands should be located between spaces in the parking aisles to provide for additional greenery. Deciduous shade trees and low growing flowering shrubs should be planted in the islands.
- Due to the tight dimensions of the mixed-use street (60' right-of-way with two story buildings), a smaller type of tree should be planted in the sidewalk area, with a larger street tree type for any in-street parking space trees.
- Generous tree grates should occur along sidewalks and in plazas where a continuous walking surface is needed.
- Tree guards should extend vertically from tree grates to protect trees in highly active areas.
- Raised planters and/or pots should be placed along retail frontages or where setback occurs to soften the character of the mixed-use street.

*Connection to Adjacent Uses*

- Pedestrian activity should be encouraged through the use of inviting pedestrian pathways and/or sidewalks along streets leading from the residential and industrial areas.
- Extra wide sidewalks that can accommodate street furniture and outside café seating should be provided.
- Sidewalks and plazas in high pedestrian use areas should be designed with a variety of textures and colors to create interest.
- A strong pedestrian connection from proposed bus stops should be provided with sidewalks and visual keys.

*Traffic Calming*

- Raised or textured crosswalks should be used at intersections to force cars to slow down.
- On-street parking should be provided where feasible to help slow traffic while keeping the street narrow.
- Parking islands located in the parking lane will encourage slower speeds and give the appearance of a narrower street.
- All traffic calming measures shall be approved by the City's Traffic Engineer prior to construction.

*Street Lighting and Furniture*

- Lighting of streets and parking lots should correspond with the scale and style of the surrounding architecture and address pedestrian activity.
- Accent lighting of architectural features is encouraged to highlight building massing and enhance the pedestrian environment. In addition, building entries with high activity levels should be illuminated.
- Benches should be provided at regular intervals along the mixed-use street to encourage on-street activity. Their size should range from four feet to eight feet long.
- Bicycle racks are to be provided to encourage alternative modes of transportation.
- Newspaper racks should be located near major pedestrian gathering areas. The design should consolidate all vending boxes into one rack.
- Trash receptacles should be located in retail areas at convenient locations.

**Retail Center** – The following design guidelines will ensure that retail development maintains a strong pedestrian scale and link to surrounding development.

*Building and Massing*

- Retail buildings should be located along pedestrian links and provide pedestrian scale architecture.
- Pedestrian scale entry elements should be incorporated into all building entries.
- Wherever feasible, building entries should be located to take advantage of pedestrian connections.
- Buildings should be designed to present a “front” along Gateway Road.
- Parking lots to serve anchor tenants should be located conveniently near the buildings they serve.

*Landscaping*

- A landscape buffer should be placed where parking lots abut streets. However, care should be taken to design and site the buffers such that they do not provide ambush points or obstruct views of the retail businesses.
- Shade trees should be provided throughout parking lots to minimize the impact of hardscaping.
- Trees should be planted along connecting walkways to help identify the pedestrian connections and provide shade.

*Connection to Adjacent Uses*

- Where building entries do not abut a street, connecting walkways should be provided to link the building entrance to street sidewalks. This configuration is most likely for large anchor store tenants that require large parking lots.

*Traffic Calming*

- A traffic signal shall be installed at the Gateway Road/Finilla Place intersection.
- Parking lots should be organized with drive aisles running perpendicular to, or at 45 or 60 degree angles to, the anchor's entry to minimize interaction with pedestrians.
- An adequate pedestrian drop-off zone should be provided in front of the anchor.
- Landscaped islands at the ends of parking lanes should be provided to help slow traffic through parking lots.
- All traffic calming measures must be approved by the City Traffic Engineer prior to construction.

*Street Lighting and Furniture*

- Parking lots and pedestrian walkways should be clearly lit.

- Decorative pedestrian scale streetlights as approved by the City should be provided.
- Bicycle parking should be visible from storefronts in order to improve the security for parked bicycles.
- Light from parking lots should not leak into residences or other sensitive areas. Landscaping may help reduce light glare and leakage.

#### *Outdoor Dining*

- Outdoor dining areas have been identified on the Site Development Plan (SDP 14-13) for the site.
- Parking for outdoor dining areas is required pursuant to Section 21.44.090 of the Carlsbad Municipal Code.
- Tables and chairs should be placed so they do not disrupt pedestrian movement, including ADA compliant pathways.
- Tables should be placed a minimum of two feet from the curb to allow adequate space for the opening of vehicle doors.
- When tables are placed next to the building façade, a clear area of at least five (5) feet in width should be maintained between the building and the table.
- Tables must not block access to fire hydrants and other emergency facilities.

**Housing** – The following design guidelines will ensure that there is a strong convenient pedestrian access to the retail center to the east, and other areas in Bressi Ranch. Please see Section X.E.8. for more information on residential design standards for Planning Area 16.

- Guest parking for multi-family residential buildings should be scattered throughout the site. Parking on the commercial portion of the project cannot be used to satisfy guest parking requirements.
- There should be clearly defined landscaped paths linking the industrial, retail, and recreation areas to housing.
- Wherever possible, housing should be oriented either towards the pedestrian corridors with visibility to public streets. Adequate landscaping should be provided to buffer the parking area from buildings.
- There should be a substantial separation between buildings fronting a street to allow for a landscaped drive into the interior parking area.

## **IX. VILLAGE GREEN (OPEN SPACE AREA 2) DEVELOPMENT STANDARDS & DESIGN GUIDELINES**

### **A. Description**

The Village Green (Open Space Area 2) is located in the center of the Master Plan, surrounded by residential Planning Areas 7, 8, 10 and 11. Open Space Area 2 has a gross acreage of approximately 4.0 acres (see Exhibit IX-1 on page IX-2).

Open Space Area 2 will be developed with common recreation facilities to serve the residents of the Bressi Ranch Master Plan. The active and passive recreational facilities in Open Space Area 2 shall fulfill the requirements of Section 21.45.050 of the Carlsbad Municipal Code. The final design and types of facilities provided in Open Space Area 2 will be subject to approval of a Conditional Use Permit.

### **B. Use Allocation**

General Plan Land Use - Open Space (OS)

Zoning - Open Space (OS)

#### Discretionary Permits Required:

All development in Open Space Area 2 shall comply with the development standards of the Open Space O-S Zone except as provided below:

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan. Subject to approval of a Conditional Use Permit, the following uses may be permitted in the Village Green:

- Swimming pools
- Community clubhouse
- Community Association/HOA office
- Volleyball court(s)
- Basketball court(s)
- Ball fields

# Legend



Access



Detention Basin



Open Space



Fire Protection Zone



Setbacks(Width in Feet)



Bus Stop



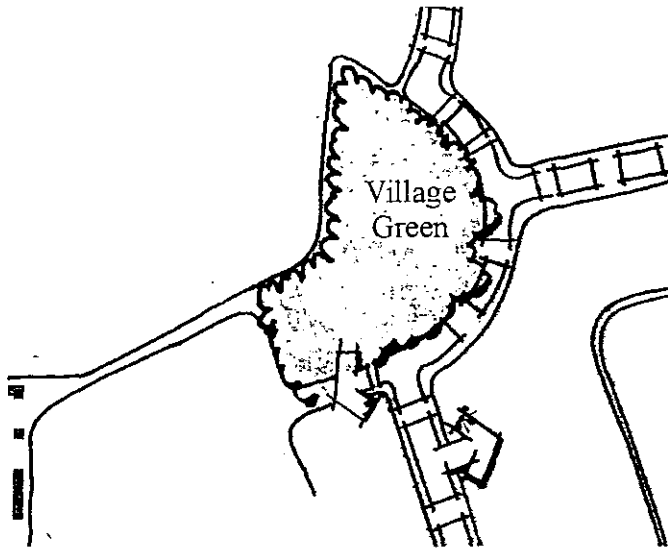
Runway Protection Zone



Airport Influence Area



Flight Activity Zone

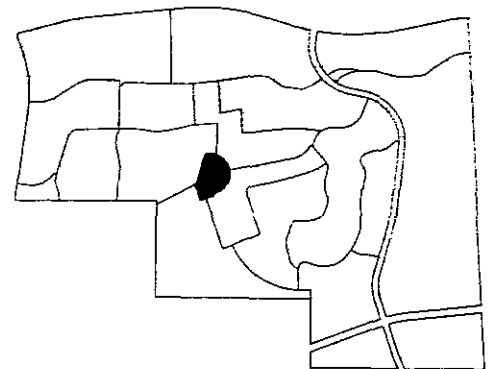


General Plan Land Use:

OS

Zoning:

OS





- Tennis courts
- Playgrounds
- Public gathering areas such as plazas, courtyards or turf areas
- Community classrooms
- Restrooms
- Parking areas
- Fencing and other similar accessory uses and structures required for a private Village Green as determined by the City Planner
- Nature Centers
- Community garden
- Temporary Information Center
- Small amphitheater with seating for up to 100 persons

If a community clubhouse is provided in this area a portion of it may be used as a Master Plan information office on a temporary basis during the construction of the Master Plan.

It is the intent of the developer of the Master Plan to make most of the facilities in Open Space Area 2 available concurrent with the occupancy of the first dwelling unit in the Master Plan. However, at the latest all recreational facilities in Open Space Area 2 shall be completed and available for the use of the residents prior to occupancy of the 150<sup>th</sup> residential unit.

### **C. Development Standards**

Open Space Area 2 will be one of the major focal points for the residential portion of the Bressi Ranch Master Plan. All development in Open Space Area 2 shall comply with the requirements of Chapter 21.33 (Open Space) of the Carlsbad Municipal Code except as noted below:

- To help create a focal point, buildings within this open space area may have a maximum height of 35 feet and additional non-habitable architectural features such as, but not limited to, towers, flagpoles and steeples (associated with a church) up to 55 feet in height by conditional use permit.

### **D. Special Design Criteria**

- Open Space Area 2 shall provide both active and passive recreational opportunities that cater to all age groups.
- No development shall occur on this site prior to approval of a Conditional Use Permit in accordance with Section 21.42.070 of the Carlsbad Municipal Code.
- Site planning shall minimize, to the extent possible, light and noise impacts to the adjacent residences.

## **X. RESIDENTIAL DEVELOPMENT STANDARDS & DESIGN GUIDELINES**

### **A. Introduction**

The major goal of the Bressi Ranch Residential Development Standards and Design Guidelines is to create a unique pedestrian oriented community. This is the first master plan to implement the Livable Streets Standards, Livable Neighborhood Policy and Neighborhood Architectural Design Guidelines that were approved by the City Council in 2001. These standards and guidelines will be used in combination with the landscape guidelines and special street cross sections to create a strong sense of community. Projects designed in accordance with these guidelines will ensure that the Bressi Ranch Master Plan becomes a community that harkens back to established neighborhoods and gets better over time. The following “Smart Growth” ideals have been incorporated into the development standards and design guidelines to encourage pedestrian activity and create a sense of community.

#### **1. Pedestrian Friendly Architecture**

Homes will be designed so that the garage is not the dominant element of the front elevation of every home by using architectural forward design, which puts the residential portion of the home forward of the garage. Many of the homes will have alley-loaded garages, while some homes will have garages located behind the residential portion of the homes. Most of the homes will have front porches and/or windows facing onto the adjacent street. This will result in “eyes on the street”, creating a feeling of safety which encourages pedestrian activity and neighborhood interaction.

#### **2. Short Front Yard Setbacks**

Many of the planning areas may be developed with short front yard setbacks to bring homes closer to the public realm and to encourage neighborhood pedestrian interaction.

#### **3. Garden District**

The Master Plan will be developed as a garden district with formal and informal landscape patterns and numerous focal points. This, along with the non-contiguous sidewalks, will encourage residents of the Master Plan to stroll from one neighborhood to another and stop at the various focal points that will be provided throughout the community. People are motivated to walk when they have a destination or focal point to walk to.

#### 4. Public Gathering Places

Public gathering places will be provided throughout the Master Plan to create focal points within the various planning areas. The public gathering places will range in size from the large Village Square in the mixed-use area to small neighborhood parks in the residential areas. These public gathering places will foster a strong sense of community within the Master Plan on two levels. On the neighborhood level, they will act as an extension of each unit's front yard to create local gathering places on a common green. On the Master Plan level, the neighborhood parks, with their focal points along with the Village Square and Village Green, will provide destination points to encourage pedestrian activity.

These public gathering places, along with the attractive tree lined streets and pedestrian friendly architecture of the homes, will encourage residents of the Master Plan to stroll through the neighborhoods and meet their neighbors, which contributes to the feeling of being part of a community. No matter where a resident lives in the Master Plan they will always be within two to three blocks of either the Village Square, Village Green or a neighborhood park. Many of the residents will be within two to three blocks of all three. The recreational opportunities provided by the neighborhood parks are in addition to those provided within the Village Square and Village Green. The active recreational facilities and acreage provided in the Village Square and Village Green alone substantially exceed all applicable City requirements.

In addition to complying with the requirements of Carlsbad's Municipal Code, Principals for the Development of Livable Neighborhoods, and Neighborhood Architectural Design Guidelines, all residential development within the Bressi Ranch Master Plan will comply with the following Development Standards and Design Guidelines unless modified in the individual planning area development standards or design guidelines. Unless stated otherwise in this Master Plan, when a percentage of units are described in the standards or guidelines, the intent is to have that percentage spread throughout the entire planning area. Where there is a conflict between the development standards of the Carlsbad Municipal Code or the design criteria of any City policy, the development standards and design criteria of this Master Plan shall prevail.

Throughout the following Design Guidelines, the words "shall" and "should" are used in various contexts to ensure the goal of a pedestrian oriented neighborhood is achieved by the Bressi Ranch Master Plan. Where "shall" is used, it is the intent of the Master Plan to provide a commitment to meet the spirit and intent of that guideline, and it is not a commitment of the implementation of that exact guideline for every condition throughout the project. "Shall" indicates a general commitment to the quality of design in the project. Where "should" is used, it is the intent of the Master Plan to have the applicant

examine the concept as part of the design process and apply it in those cases where it makes sense. "Should" is used in those cases where the project cannot commit to a particular guideline at this time in the design process.

## **B. Neighborhood Development Standards**

1. Entry Towers – Round or octagonal entry tower may encroach into the required side setbacks a maximum of 2'-0". The total length of the encroachment may not exceed 8 feet. However, the minimum setback may not be less than 5 feet.
2. Window Detailing – The design of windows shall include one or more of the following features:
  - Deeply recessed windows (12"-18")
  - Multiple pane windows
  - Decorative window ledges
  - Window lintels
  - Accent and varied shape windows appropriate to style
  - Window boxes and planters with architecturally evident supports
  - Exterior wood trim surrounds
  - Accent colors on shutters or other elements
  - Arched elements
  - Shutters
  - Raised stucco trim around windows
3. Architectural Projections – Architectural projections may encroach into the setback a maximum of 24 inches.
4. Garage Setbacks and Separations – Where two garages are adjacent to one another at a common property line there shall be a two-foot minimum difference in the setbacks where feasible.

5. Driveways Serving Single Family Garages – Garages that are set back 30 feet or more from the back of the sidewalk or property line, whichever is closest, may be served by a 12 foot wide driveway (see Exhibit X-1 on page X-5). Garages that are set back less than 30 feet from the back of sidewalk or property line, whichever is closest, shall provide a 19-foot wide driveway to the face of the garage (see Exhibit X-2 on page X-6).
6. Swing-in Garage – Swing in garages shall only be permitted on lots with a minimum width of 52 feet.
7. Modifications to the Hillside Development Ordinance Standards – The decision making body or official may approve a modification to the hillside development and design standards of Section 21.95.120 of the Carlsbad Municipal Code, if it finds that such modification will result in a grading design that better achieves the goals of this Master Plan as stated in Chapter II and the Ahwahnee Principles (Appendix D).

### C. Neighborhood Design Criteria

- Signature Elements – Throughout the Bressi Ranch community there will be specific ‘icons’ that identify various spaces within each planning area. These icons shall use the following features to create a strong sense of community:

Stone or other decorative masonry treatments entry pilasters and wall elements

Theme and retaining walls

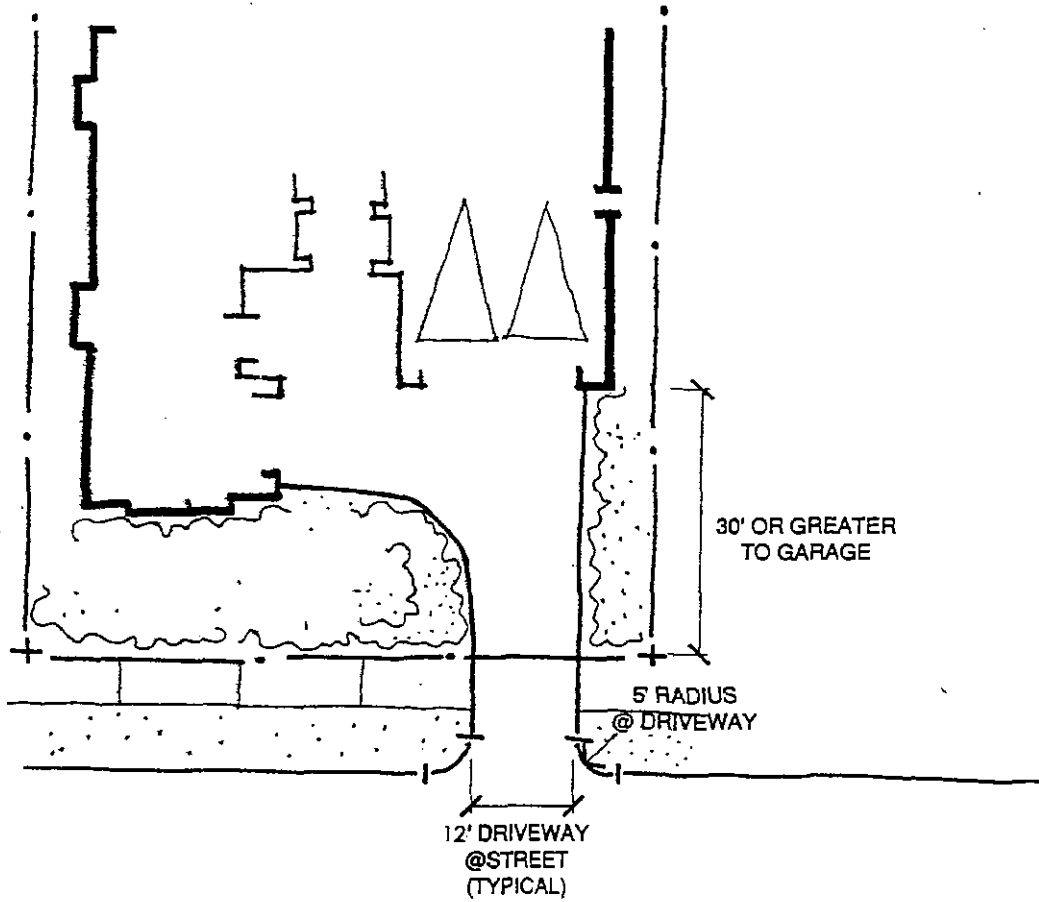
Arbors and trellises

Specific landscape and planter treatments

Small-scale pedestrian oriented streetlights, subject to the approval of the City Engineer and Public Works Director.

Exhibit X-3 on page X-7 provides some conceptual design ideas for these features. The exact design and details of these features will be determined as a part of the final design process.

Entry Statement – Distinctive entry statements shall be provided at the entry to the Master Plan. Exhibit X-4 on page X-8 provide examples of these entry statements.



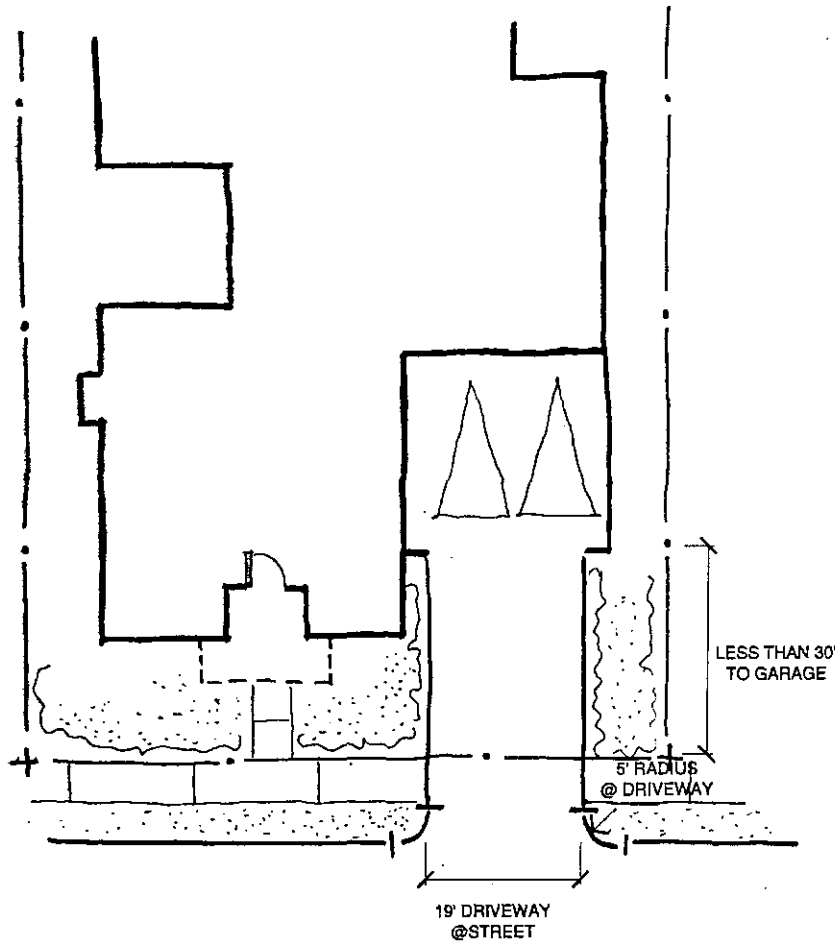
**SINGLE FAMILY DRIVEWAY DETAIL**

**12' WIDE DRIVEWAY**

CONCEPT SKETCH  
(for illustrative purposes only)





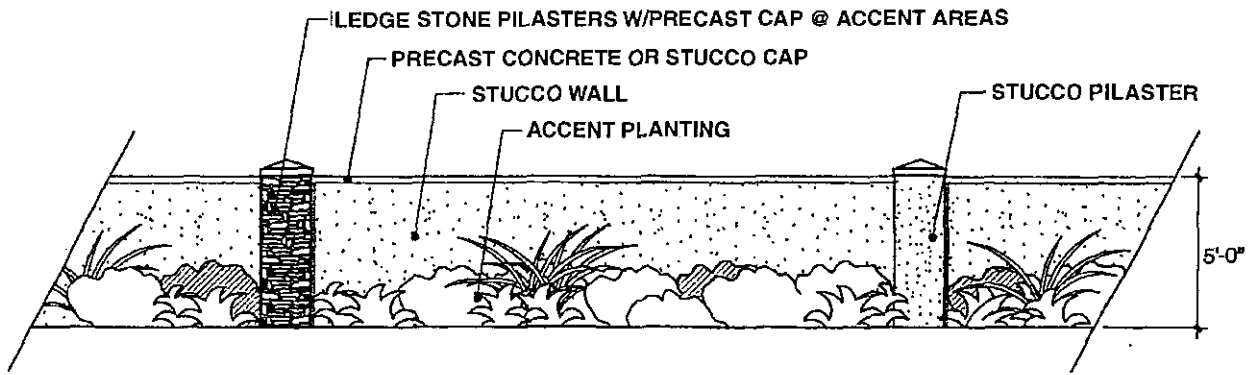


**SINGLE FAMILY DRIVEWAY DETAIL**

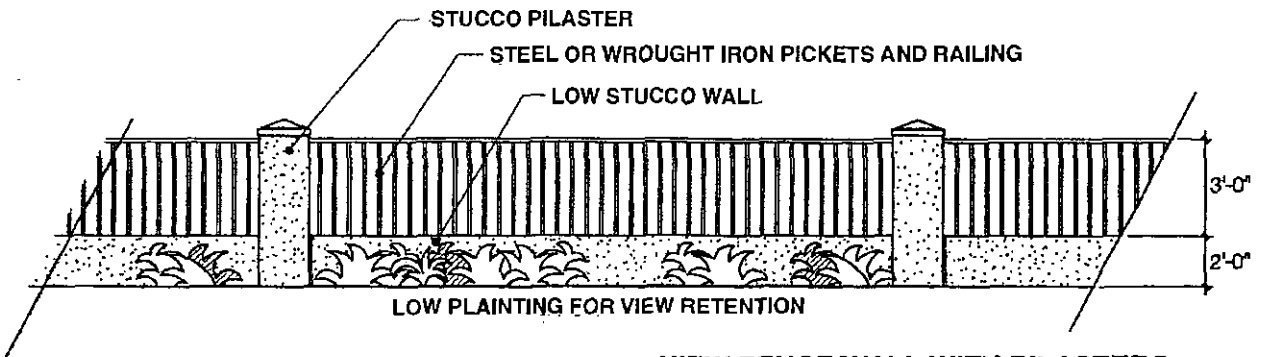
**19' WIDE DRIVEWAY**

CONCEPT SKETCH  
(for illustrative purposes only)

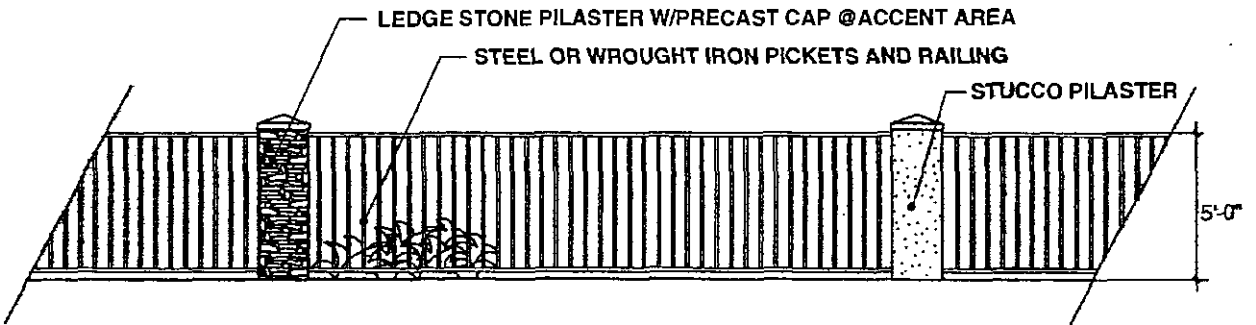




**SOLID WALL WITH PILASTERS**  
 NTS



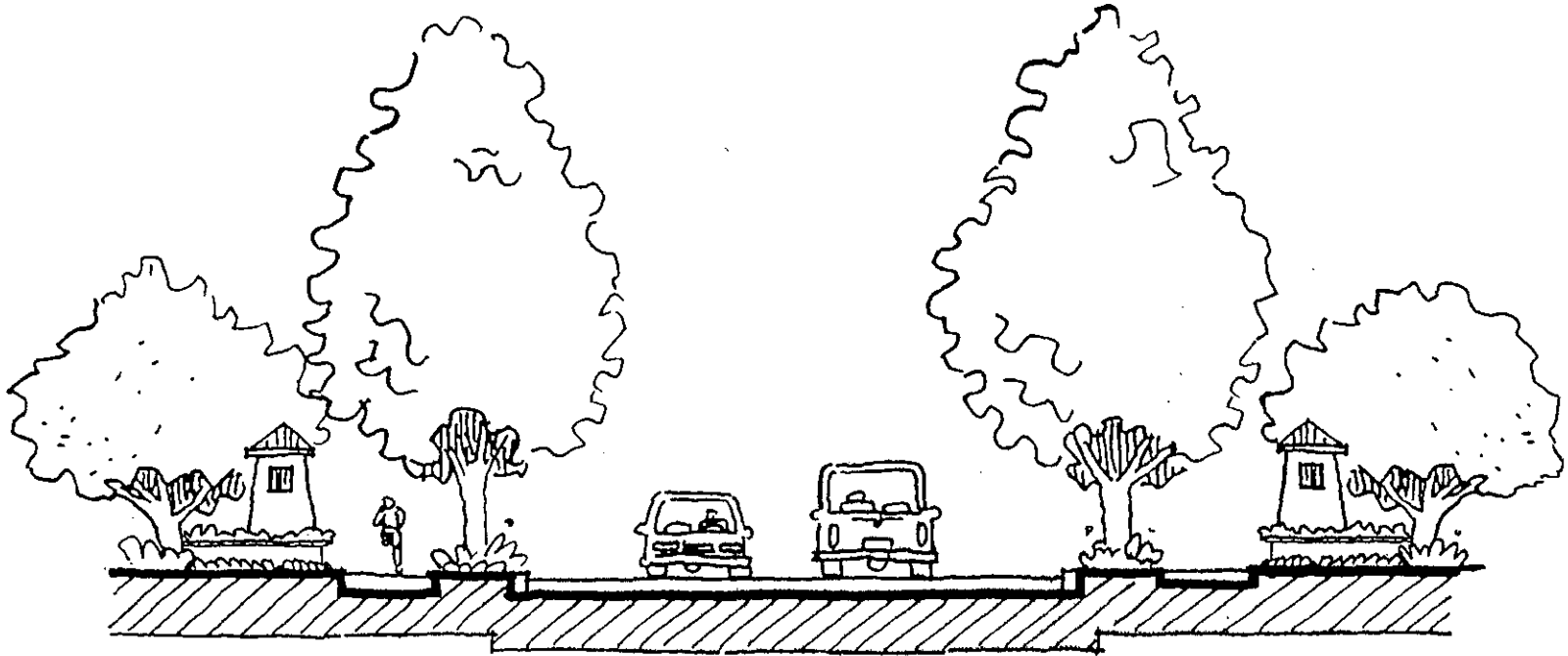
**VIEW FENCE/WALL WITH PILASTERS**  
 NTS



**OPEN FENCE WITH PILASTERS**  
 NTS

CONCEPTUAL SKETCHES  
 (Final construction may differ.)

8-X



CONCEPTUAL SKETCH  
(for illustrative purpose only)

Traffic Circles and Tapered Entries – Traffic Circles and tapered entries, where appropriate, will be used as a means of breaking up long, continuous street scenes and slowing traffic on the internal streets (see Exhibit IV-25 on page IV-38). These elements shall be carefully located so they do not adversely impact emergency vehicle response times. Planters shall be substantial enough in size for ample planting and conducive to smooth traffic patterns. The design and dimension of these features has been previously discussed in Section C of Chapter IV, which addressed traffic calming.

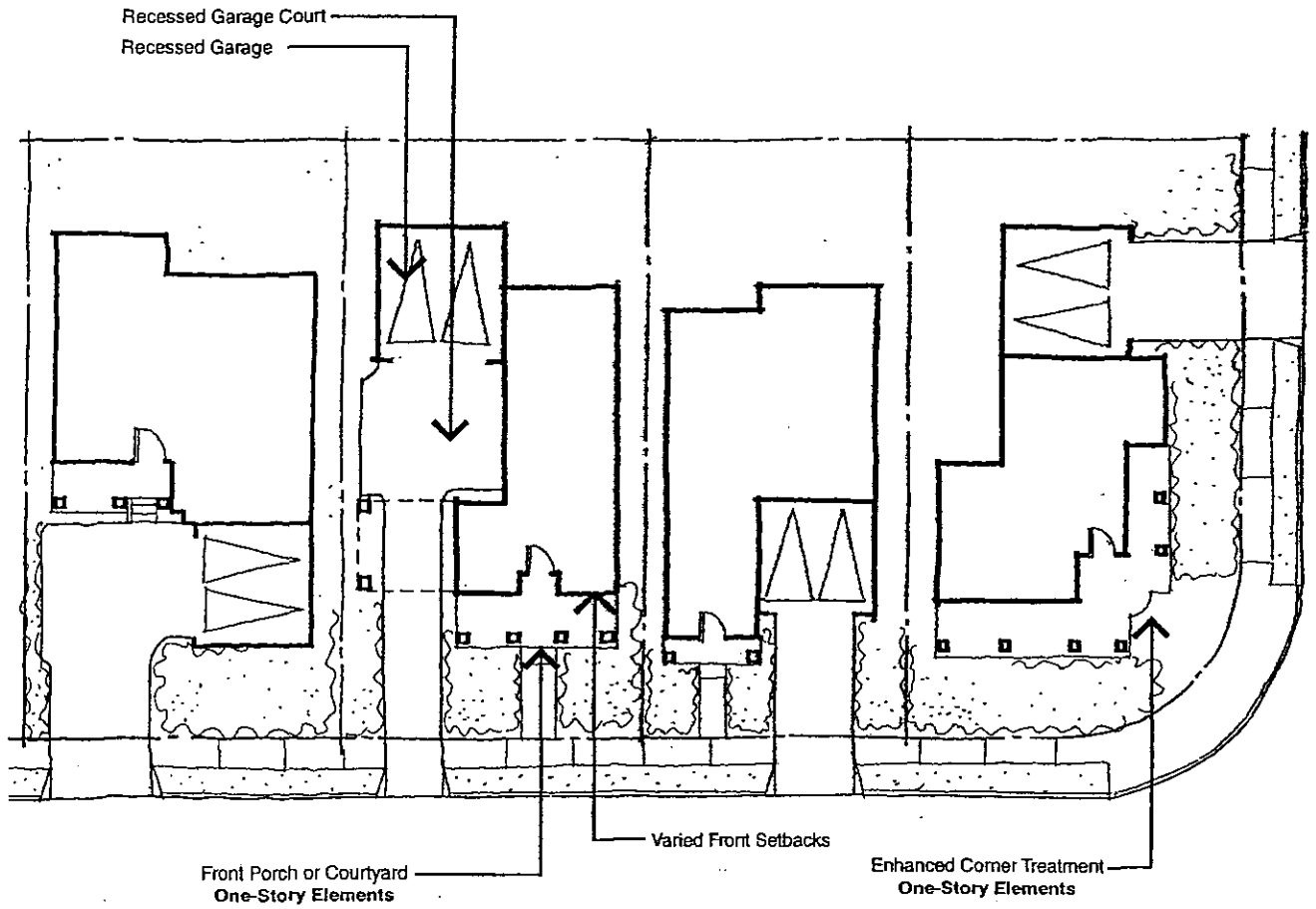
Variable Lot Configurations and Setbacks – Building setbacks for single family detached homes should be varied in order to break up continuous visual planes of building facades to create an attractive streetscape (see Exhibit X-5 on page X-10). Building setbacks for alley-loaded products may be more uniform to create a more traditional urban form and add richness and variety to the project.

When developing on sloping streets, the low side of the building shall be placed on the low side of the lot, where possible, to reduce the feeling of mass and minimize the dominance of the slope (i.e. follow natural topography).

Special attention shall also be paid to lot configurations that promote visual interest such as corner lots. These techniques create a greater sense of street character and promote variety along lengthy stretches of lots. Specifically, on corner lots the building should be set away from the corner to soften the appearance of the architecture while providing more sight distance for vehicles. For one-story corner lots, wrap around articulation may be used to achieve a softer architectural appearance. Combining corner lots with variable setbacks can animate an otherwise monotonous street scene.

Neighborhood Edge Treatments – Berming and planting should be used to soften community edges and minimize noise transmission into the property. In instances where walls are necessary, consideration should be given to community specific materials and finish palettes.

Parkway Elements – In accordance with the City's Liveable Streets Standards, tree-lined parkways with curb-separated sidewalks will be used throughout the residential portion of the community to create attractive, pedestrian oriented street scenes. Additionally, street sections should taper at intersections to serve as traffic-slowing devices (see the Exhibits in Section C of Chapter IV). Sidewalks should be continuous and connect to meaningful destinations such as neighborhood parks and greens. Amenities within Bressi Ranch should be pedestrian oriented. Benches may be provided in shaded areas throughout each planning area as places to stop and rest. For those streets lined with alley loaded product types, parking islands shall be staggered.



CONCEPT SKETCH  
 (for illustrative purposes only)

Open Space and Parks – Neighborhood parks will be provided throughout the Master Plan. These neighborhood parks will create focal points, connect to the internal pedestrian system, and contain improvements such as benches, picnic tables, playground equipment, and open turf areas. If compatible with the design of the product type, homes should be oriented to face onto the neighborhood parks. It is not the intent of these areas to attempt to duplicate the facilities and function of the Village Green or Village Square. Instead, these neighborhood pocket parks will provide neighborhood gathering places and locations for children to play in close proximity to their homes (see Exhibit X-6 on page X-12).

Cul-De-Sacs – In accordance with the goals of the City’s Liveable Neighborhood Policy, the use of cul-de-sacs will be minimized in the Bressi Ranch Master Plan. In order to create an open feeling and facilitate pedestrian and bicycle access to an area beyond the street, ‘open cul-de-sacs’ shall be used where appropriate.

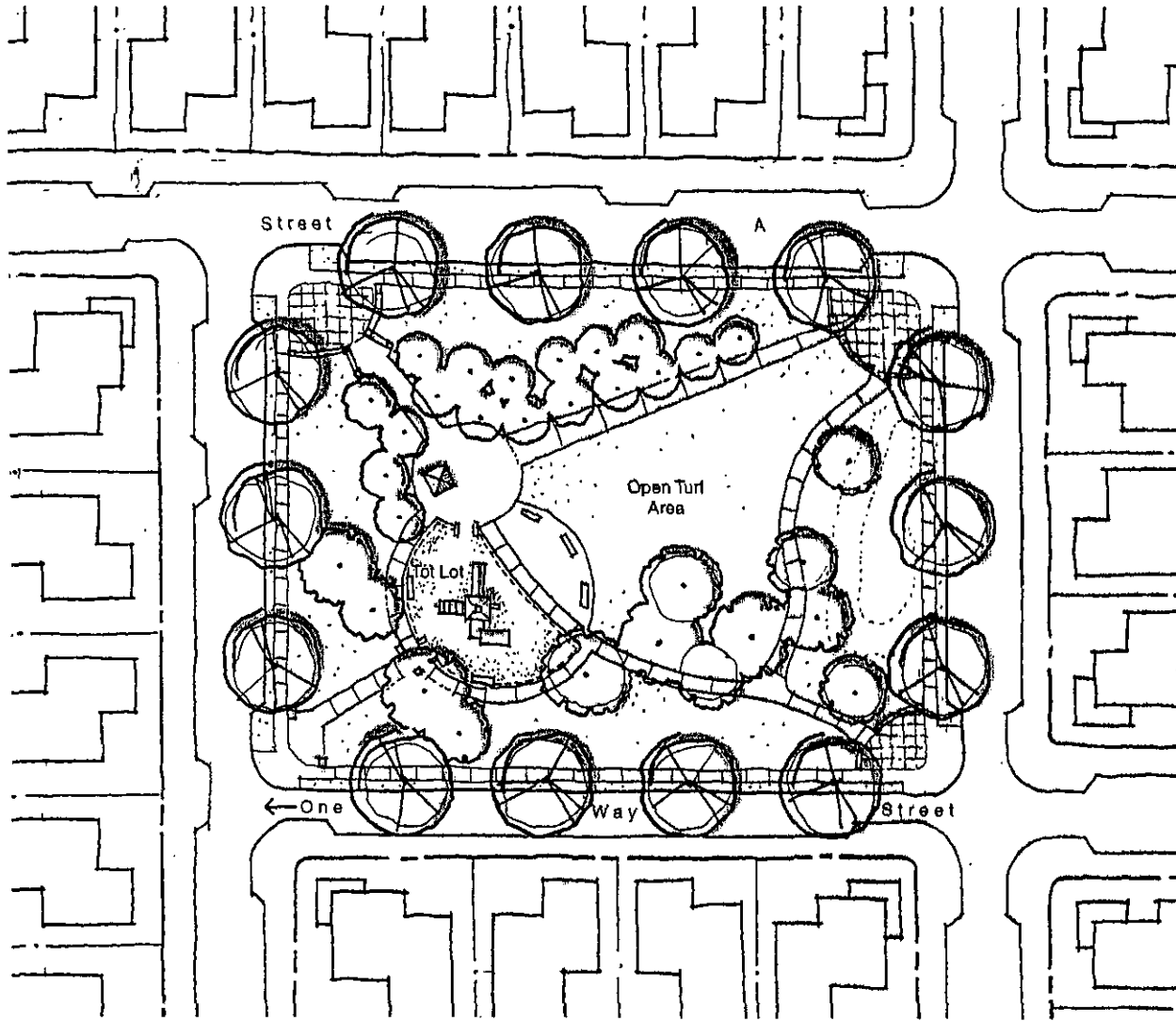
Open cul-de-sacs may be utilized where non-automobile circulation could be enhanced and at development edges where physical or visual access beyond the project is desirable. Sidewalk or trail connections shall be provided where appropriate from the cul-de-sac to the nearest sidewalk or trail beyond the open cul-de-sac. Not all cul-de-sacs need to be opened for these purposes.

#### **D. Architectural Design Guidelines**

1. Introduction – Homes within the Bressi Ranch Master Plan will be designed in accordance with Carlsbad’s Planned Development Ordinance & Neighborhood Architectural Guidelines and the Bressi Ranch Architectural Development Standards and Design Guidelines. The text and exhibits in this section show how these development standards and design guidelines will be implemented by the Master Plan.
2. Homes Adjacent to Local & Collector Roadways – Homes that are adjacent to local and collector roads shall provide special attention to detailing on the elevation fronting the roads. This will include window detailing equal to or better than that of the front elevation. Additional wall planes and balconies (where noise standards allow) shall be provided.



X-12

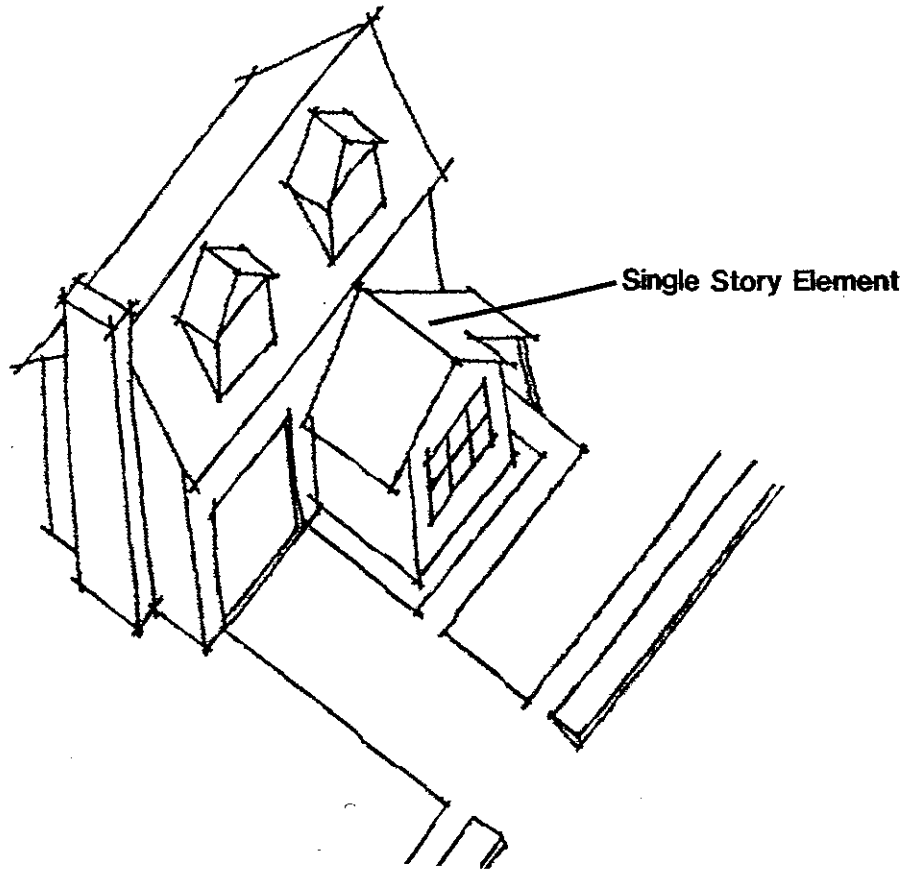


CONCEPTUAL SKETCH  
(for illustrative purpose only)

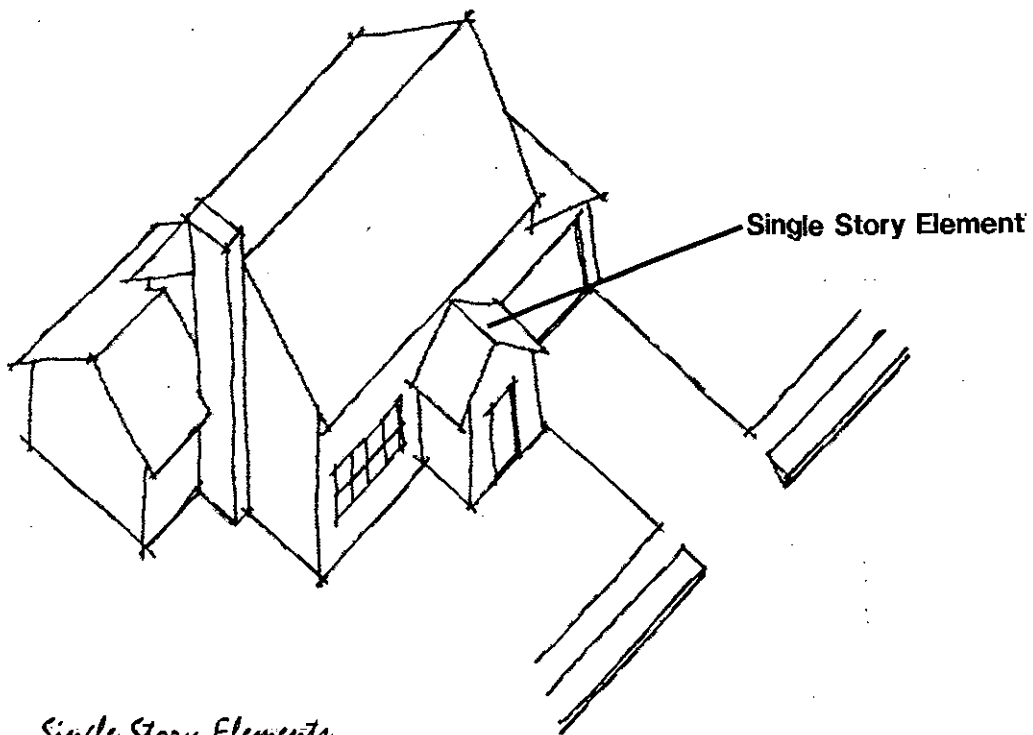
3. Varied Architectural Styles – A mixture of varied architectural styles should be used to provide visual interest and diversity. Separate planning areas can utilize similar building materials and architectural design elements to provide neighborhood cohesiveness. Adjacent homes with different architectural styles may provide architectural compatibility by utilizing both a complementary color palette and similar exterior accent materials on adjacent residential structures. This compatibility can be further enhanced through the use of common planning area walls and landscaping.
4. Massing – Massing is not strictly intended as an exterior focus, but should express relationships to interior spaces and specific plotting indicative of the architectural style. See Exhibit X-5 on page X-10 for an example of variable massing with variable garage setbacks.
5. Materials – A variety of building architectural accent features and materials shall be incorporated into the exterior elevations when compatible with the authenticity of the style.
6. Single Story Elements – Front porches, balconies and single story elements should be used to create variety within a street scene and at corner plotting conditions, when consistent with the chosen architectural style. In addition to varying the height and profile of residences, these elements encourage interaction with public spaces (see Exhibit X-7 on page X-14). When compatible with the architectural style, a combination of 1 and 2 story elements shall be provided. Homes designed in the American Colonial, Monterey or Italianate architectural style will not be required to include a single story element due to the incompatibility with these architectural styles.

The roof covering the single-story element shall be lower than the roof for the two-story element to the unit (this is not intended to preclude long shed-type roofs falling to a single-story element). Single story will be defined as a plate line maximum of 12 feet (10 feet is preferred).

7. Entries, Porches and Balconies – Front porches, balconies and entries shall be designed in accordance with the criteria of Carlsbad's Neighborhood Architectural Design Guidelines.
8. Roof Elements - Roof elements that enhance the architectural style of the home (gable, shed, etc.) shall be provided over porches. A balcony above the porch may also serve this purpose.
9. Front Articulation - Carlsbad's Architectural Guidelines require that for at least 66% of the homes in a project there shall be at least three separate building



*Simple Roof Forms*



*Single Story Elements*

Concept sketch for illustrative purposes only

*Bressi Ranch*

A planned community in the City of Carlsbad, California



planes on front elevations of lots with 45 feet of street frontage or less and four separate building planes on front elevations of lots with a street frontage greater than 45 feet. To encourage alternative designs up to 33% of the lots in a project (i.e. half of the 66% of the lots required to have either 3 or 4 separate building planes on the street side elevation) may limit the number of building planes to two, if a fully landscaped front courtyard is provided. The courtyard shall have a wall with a height between 36"-42" and must be related to the style of architecture.

10. Side Articulation – Because a large percentage of the lots within the Bressi Ranch development are less than 7,500 square feet and the setbacks between homes can be narrow, it is important that all building elevations have a certain level of articulation. Wherever possible, offsets should be provided on the sides of homes to avoid creating structures with a boxy appearance.

Each home should be designed with consideration for wrapping the design elements from the street elevation around and into the side elevations a minimum 12 feet. Except for the alley loaded units, where side yards are at a minimum, side elevations should include varied building planes to promote visual interest.

11. Rear Articulation – The rear elevations of homes will be reviewed from three perspectives: adjacent units and backyards, public areas such as pocket parks and collector streets, and as distant elevated silhouettes. As much as feasible, rear elevations shall be designed to be attractive when viewed from all three of those perspectives in terms of architectural detailing, roof form and ridge heights.

12. Roof Forms – Varied building roof heights and floor massing should be incorporated into unit designs for each planning area. Rooflines should be simple with variations in height and prominence to promote visual interest. Flat roofs are inappropriate, whereas towers, lofts and other features that break up the roofline are encouraged as they enhance the skyline and promote variety in each planning area. Additionally, a variety of building architectural accent features and materials shall be incorporated into the exterior elevations when compatible with the style.

13. Corner Lot Configurations – Corner lots offer a unique opportunity for side entry garages, three-sided architecture, and second story massing stepped back. In those planning areas where corner lots are available they should be developed with sensitivity to separating the entry from the garage and creating architecture that wraps the corner. For buildings on corner lots, an attempt should be made to have the front porch wrap around the side of the building (see Exhibit X-8 on page X-17).

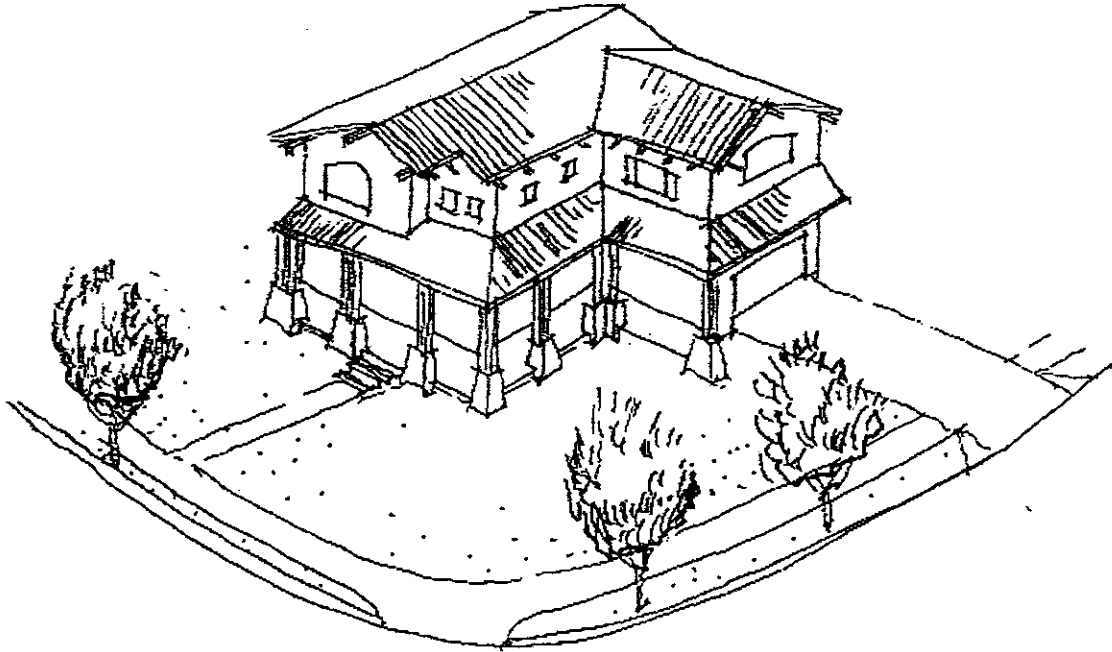
## GARAGES

1. Architecture Forward Garage Treatments – In accordance with the requirements of the Planned Development Ordinance, the land plan within the Bressi Ranch Master Plan will have many homes with architecture forward design, which may place the residential portion forward of the garage. This type of land plan will enhance the street scene and pedestrian quality of each planning area. Plans that have three car garages facing the street should be avoided unless they are split to either side of the home or one of the garages is turned 90 degrees to the street. Less than 50% of the home's frontage should be devoted to garages. It is the intent of these guidelines to create neighborhoods where the architecture of the homes is in front of the recessed garages throughout the Master Plan. To de-emphasize garages, the majority of the homes will have architecture forward of the garage.
2. Garage Configurations - A variety of garage configurations should be used within each neighborhood to improve the street scene. Examples of different siting configurations for garages include the following:

Corner Lots – Corner lots should be configured so that garage access is off of the side yard instead of the front to reduce the visual impact of garage doors (see Exhibit X-8 on page X-17).

Alley Loaded Garage – Garages in this configuration are located in an alley, which allows for a more traditional street scene. This scenario creates an intimate relationship between the pedestrian areas and the homes by eliminating garage facades at the street-facing elevation (see Exhibit X-9 on page X-18).

Swing-in Garage – A swing-in garage turns the garage 90 degrees to hide the doors from the street. Typically in this configuration a one-car and a two-car garage are split to provide a variation on the street-facing façade. Preferably, the single car garage is turned 90 degrees to the street. This configuration also allows for a reduced setback requirement of 10 feet to the side-loaded garage, but careful attention shall be paid to the articulation of the street-facing portion of the garage (see Exhibit X-10 on page X-19).



*Concept sketch for illustrative purposes only*

*Bressi Ranch*

*Corner Lot Configuration*

X-8

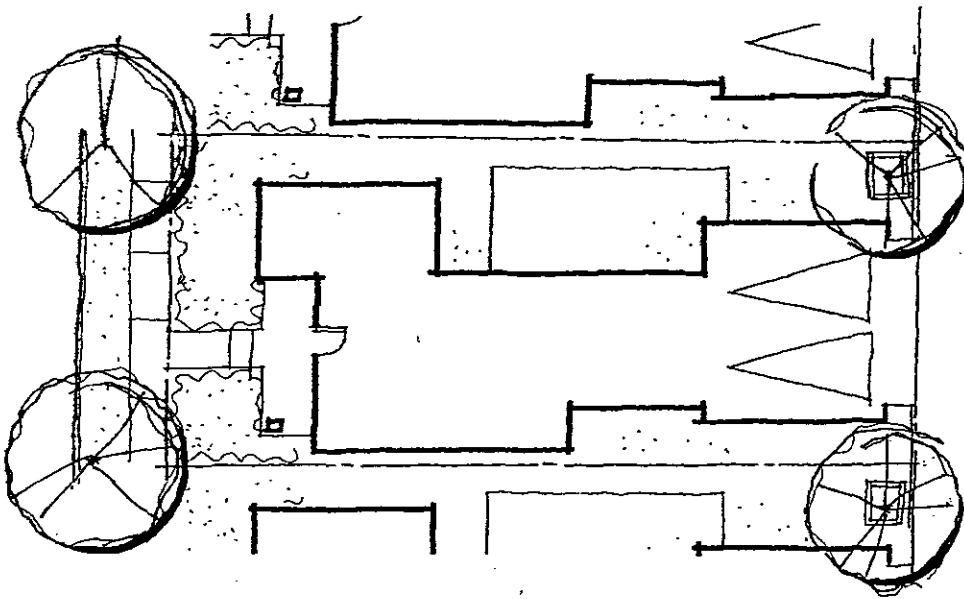
A planned community in the City of Carlsbad, California



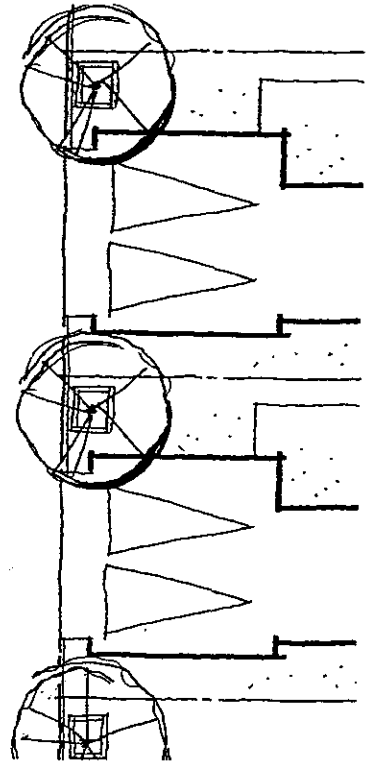
X-17



Public Street

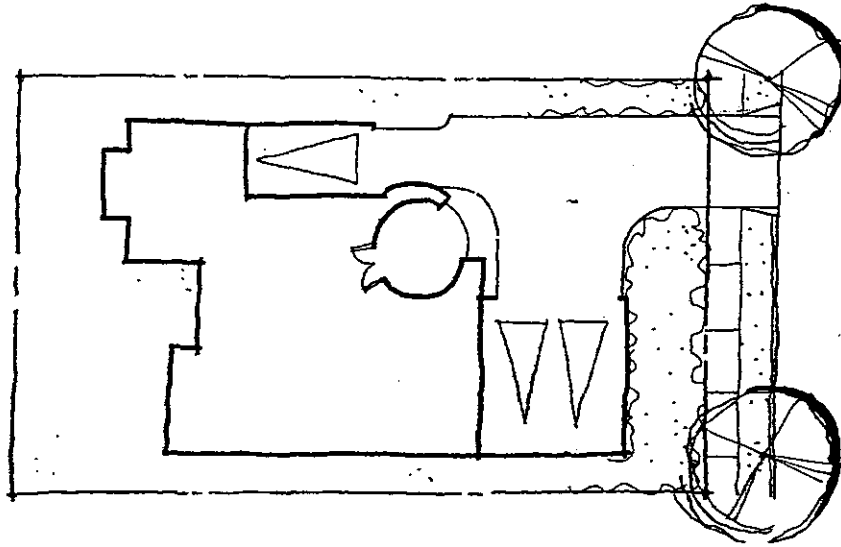


Alley

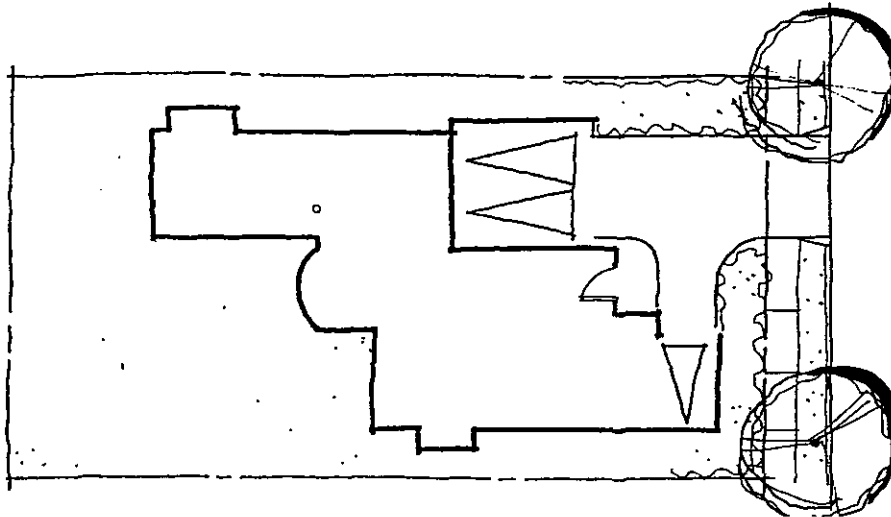


CONCEPT SKETCH  
(for illustrative purposes only)





*Three Car Split Swing-in Garage*



*Three Car Split Garage*

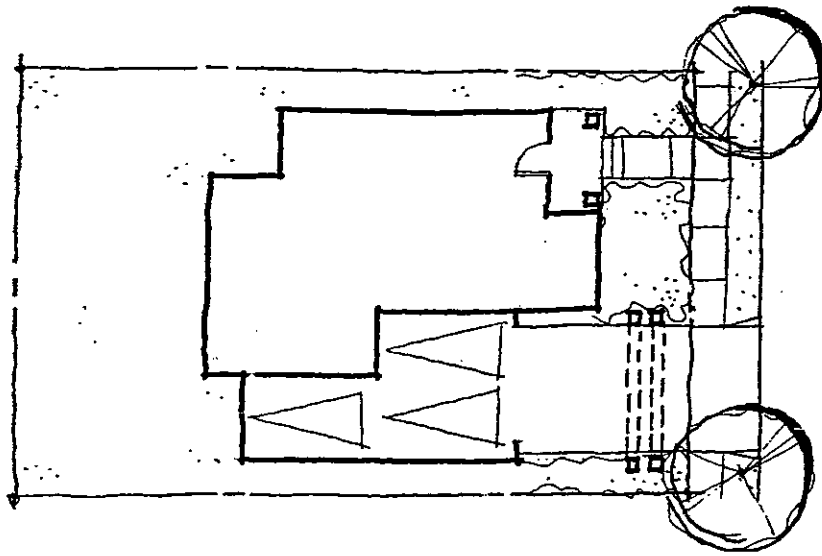
CONCEPT SKETCH  
(for illustrative purposes only)

Three Car Garage with Tandem Parking – This garage configuration minimizes door openings while parking two cars in line with one another. Additionally, the configurations can be shallow recessed or deep recessed depending upon the lot size (see Exhibits X-11 on page X-21 and X-12 on page X-22).

Three Car Front-Split Garage – Three garages facing the street are not considered three “in a row” (or side-by-side) garages if there is 10 feet or more of open area or living space (with a minimum front plane offset of 18 inches) between two of the garage doors and provided the combined garage area shall not exceed more the 50 percent of the building’s frontage.

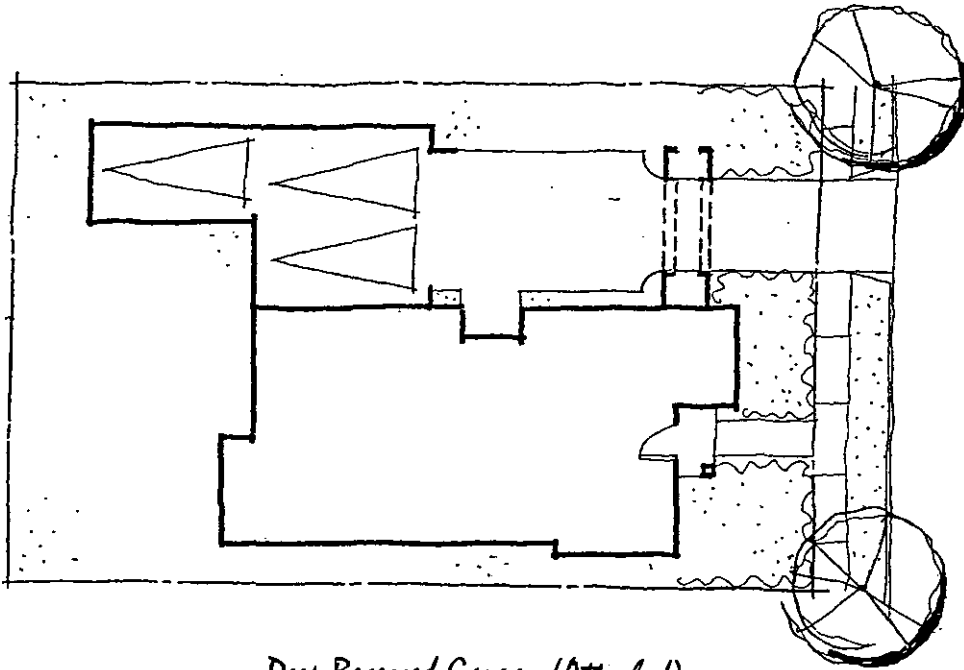
Attached, Deeply Recessed Garage – In this configuration, the garage is located behind the house, but is accessed from the street as seen in some older residential neighborhoods. To further de-emphasize the garage facade, a porte cochere can be provided to create a gateway into a courtyard shared by the garage (see Exhibit X-12 on page X-22). A “recessed garage” must be located a minimum of 5 feet behind habitable area of the front facade.

Detached Deeply Recessed Garage – The detached garage is deeply recessed at the rear of the lot, preferably with a porte cochere, and connected to the home via a breezeway. A small living space can be created above the garage to be used as a guest house or second unit which may be connected to the house via a balcony above the breezeway (see Exhibit X-12 on page X-22). A “recessed garage” must be located a minimum of 5 feet behind habitable area of the front facade.

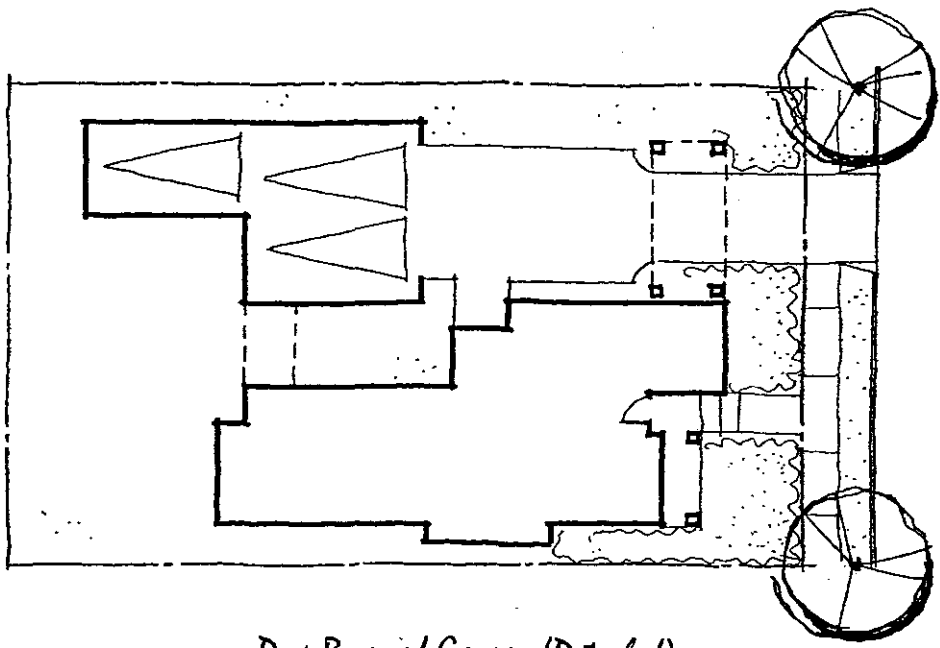


*Three Car Tandem Garage with Entry Area*

CONCEPT SKETCH  
(for illustrative purposes only)



*Deep Recessed Garage (Attached)*



*Deep Recessed Garage (Detached)*

CONCEPT SKETCH  
(for illustrative purposes only)

Sign Criteria

All signage shall be in conformance with Chapter 21.41 of the Carlsbad Municipal code. Prior to issuance of any sign permits in the Master Plan a detailed sign program shall be approved in accordance with the requirements of section 21.41.060 of Carlsbad Municipal Code. Signs utilized in the Master Plan include both temporary and permanent types.

1. Temporary Signs – Temporary signs which will be present until the completion of planning area construction may include:

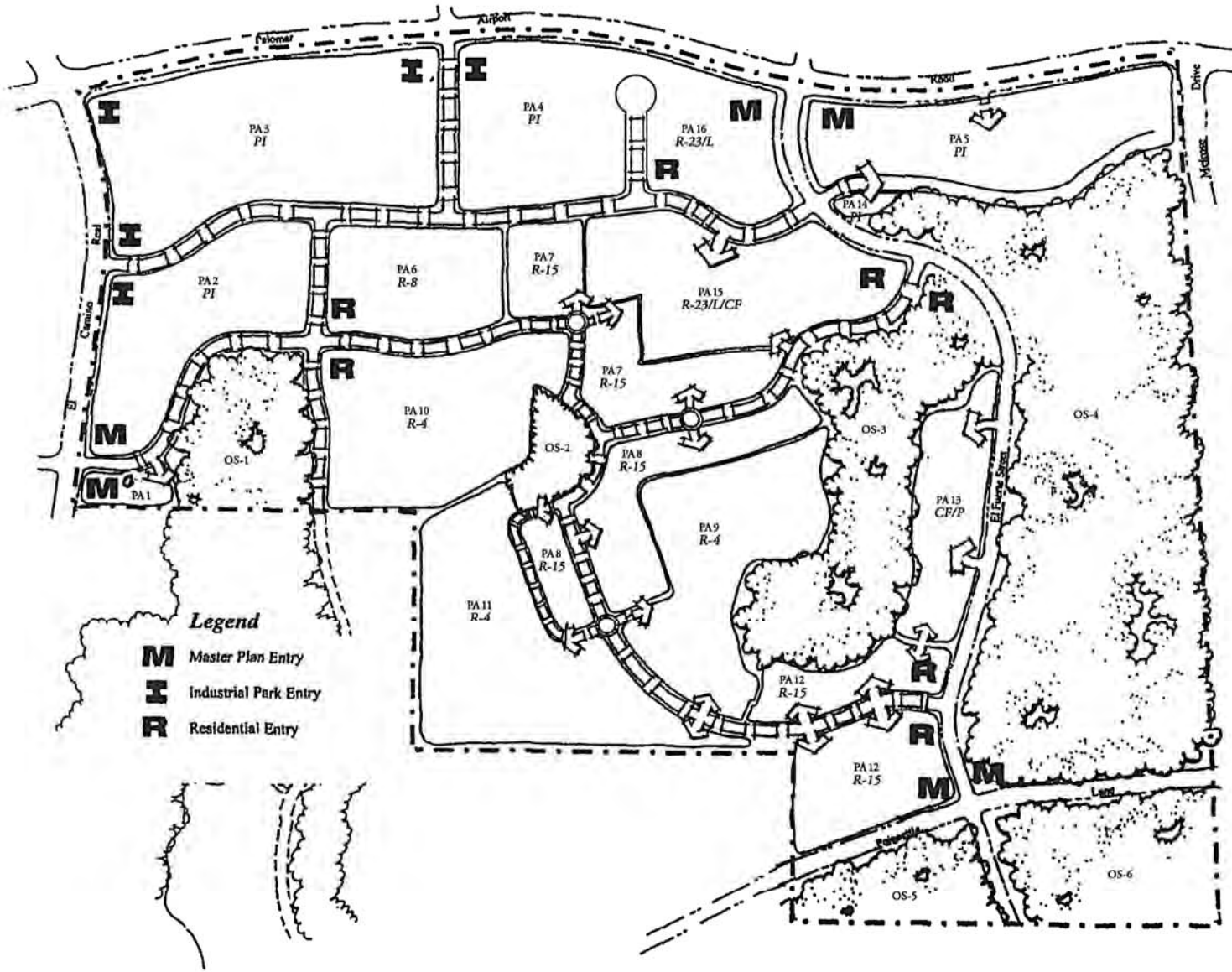
- Model home complex signs
- Temporary subdivision directional signs
- Future facility signs
- Information kiosk signs

All temporary community signs shall be consistent with Carlsbad Municipal Code Section 21.41.100. Additional temporary real estate signage may be allowed as described in Section 21.41.100 of the Carlsbad Municipal Code.

2. Permanent Signs – Permanent signs include:

- Vehicular directional signs
- Entry signs
- Neighborhood markers
- Recreation center signs
- Trail head markers

Permanent Master Plan, industrial park and residential community identity signs shall be subject to the requirements stated in Section 21.41.095 (Table B – Office/Industrial Projects) of the Carlsbad Municipal Code. Exhibit X-13 on page X-24 shows the approximate location of these signs.

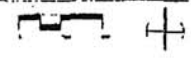


- Legend**
- M** Master Plan Entry
  - I** Industrial Park Entry
  - R** Residential Entry

*Bressi Ranch*

A planned community in the City of Carlsbad, California

*Bressi Ranch Conceptual Sign Locations* X-13



## **E. INDIVIDUAL PLANNING AREA STANDARDS**

### **1. Planning Area 6**

#### Description

Planning Area 6 is located south of Planning Areas 3 and 4, north of Planning Area 10, and west of Planning Area 7. Planning Area 6 has a gross area of 16.2 acres and a Growth Management net developable area of 11.5 acres (see Exhibit X-14 on page X-26). It is anticipated that Planning Area 6 will be developed with single-family detached homes gaining access to their garages directly from the street. However, the garages shall not be the dominant feature of the home.

#### Implementation of Master Plan Vision and Goals

The purpose of this planning area, including design criteria and allowed uses, is to provide housing within convenient walking distance of jobs in the industrial area, recreational opportunities in the Village Green, and both jobs and recreational opportunities in Planning Area 15 (Goal 2).

#### General Plan & Zoning

General Plan Land Use: R-8

Units: It is anticipated that Planning Area 6 will be developed with approximately 64 dwelling units.

Zoning: RD-M

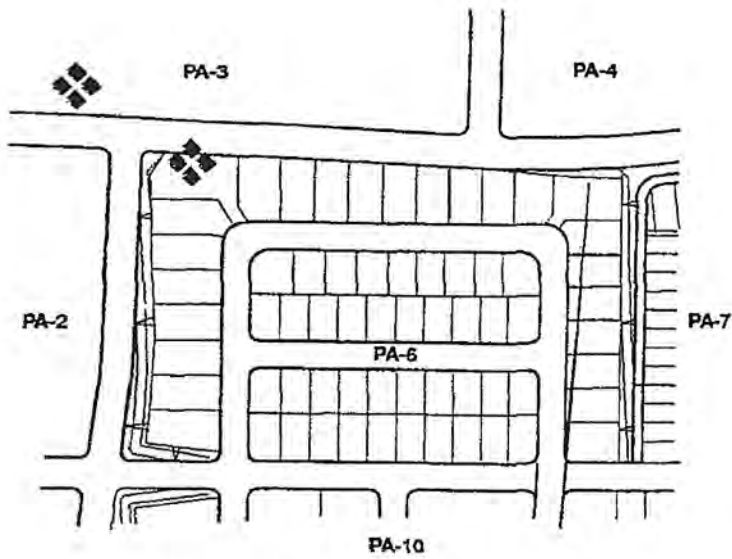
#### Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

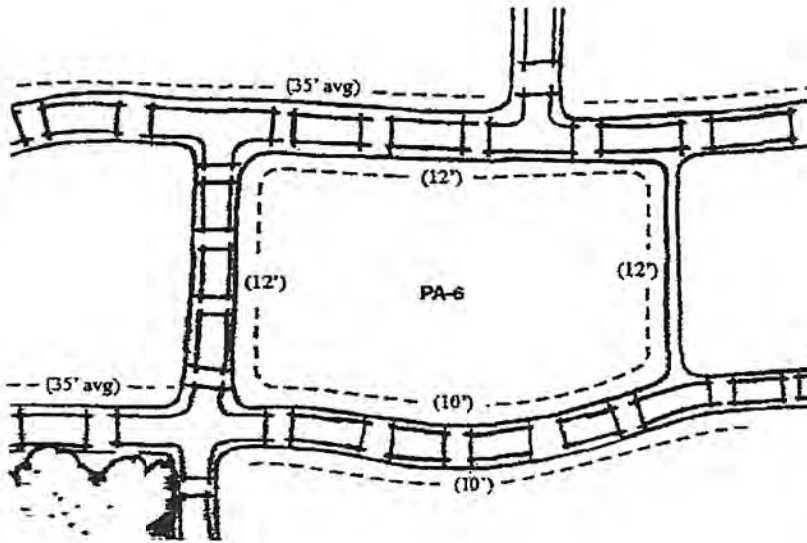
#### Uses Allowed

Residential – Single Family Detached or Multiple Family Attached





Concept Sketch  
(for illustrative purposes only)



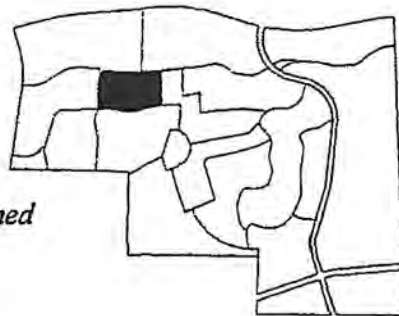
### Legend

- Access
- Detention Basin
- Open Space
- Fire Protection Zone
- City Trail
- Setbacks (Width in Feet)
- Bus Stop/ Bus Turnout

General Plan Land Use:  
R-8

Zoning:  
R-2

Product:  
Single Family Detached/Attached



*Bressi Ranch*

A planned community in the City of Carlsbad, California

*Planning Area 6*

X-14



Design Criteria

In addition to complying with the neighborhood design criteria, development in Planning Area 6 shall comply with the following design criteria:

- Residential development within Planning Area 6 shall be adequately buffered from industrial development to the north and west. Slope and landscape buffering shall be utilized to screen the industrial buildings and parking facilities from the residences. See Exhibits VII-2 and VII-3 on pages VII-5 and VII-6, which provide cross sections of this separation.
- A strong pedestrian link should be established between Planning Area 6 and the Village Center.

Development Standards

Development in Planning Area 6 shall comply with all applicable regulations and development standards of the Carlsbad Municipal Code, including the RD-M and PUD requirements, except as modified by Section X.B of this Master Plan.

## 2. Planning Area 7

### Description

Planning Area 7 is located south of Planning Area 15 and north of Planning Area 8. Planning Area 7 has a gross area of 20.0 acres and a net developable area of 18.3 acres (see Exhibit X-15 on page X-29). Planning Area 7 may be developed with alley-loaded homes. Several small neighborhood parks may be provided at convenient locations.

### Implementation of Master Plan Vision and Goals

The purpose of this planning area, including design criteria and allowed uses, is to provide housing within convenient walking distance of jobs in the industrial area, recreational opportunities in the Village Green, and both jobs and recreational opportunities in Planning Area 15 (Goal 2). Streets and paths within Planning Area 7 provide convenient pedestrian links between the Village Green and mixed-use area (Goal 11). Planning Area 7 contributes to the composition of Bressi Ranch, a community designed with a variety of housing, job types, commercial uses, unique street system, trails and pathways and open space areas to provide a pedestrian friendly atmosphere (Goals 4, 5, 7 and 8).

### General Plan & Zoning

General Plan Land Use: R-15

Units: It is anticipated that Planning Area 7 will be developed with approximately 99 dwelling units.

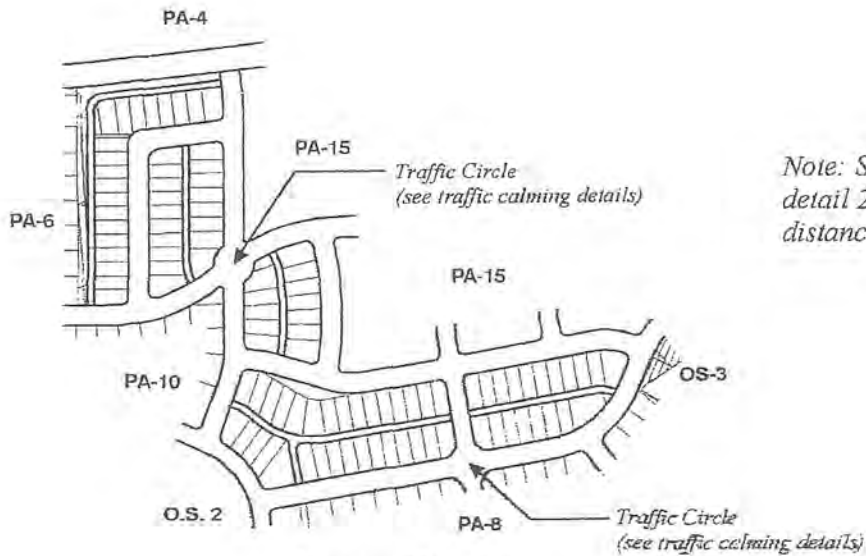
Zoning: RD-M

### Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

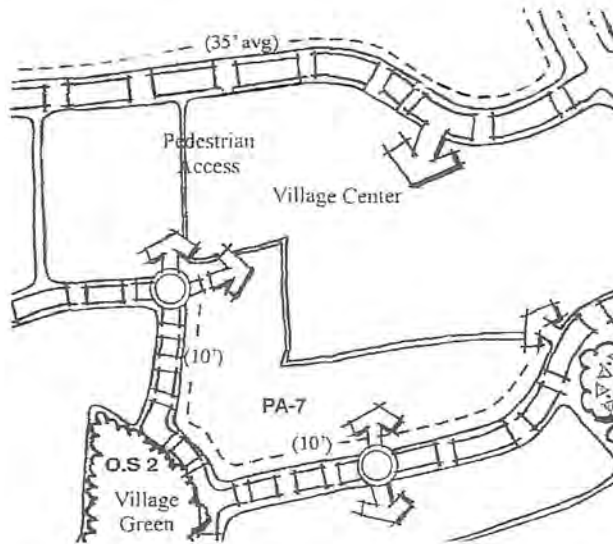
### Uses Allowed

Residential – Single Family Detached or Multiple Family Attached



Note: See traffic calming detail 2 for sight distance lines.

Concept Sketch  
(for illustrative purposes only)



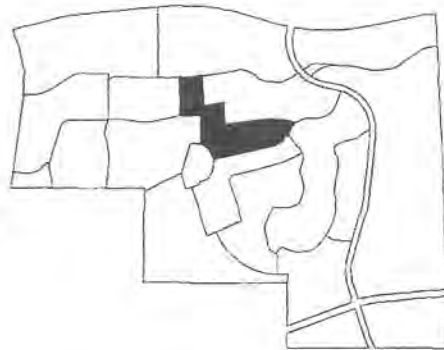
### Legend

- Access
- Detention Basin
- Open Space
- Fire Protection Zone
- City Trail
- Setbacks (Width in Feet)
- Bus Stop/ Bus Turnout
- Runway Protection Zone
- Airport Influence Area
- Flight Activity Zone

General Plan Land Use:  
R-15

Zoning:  
RD-M

Product:  
Single Family Detached



*Bressi Ranch*

A planned community in the City of Carlsbad, California

Planning Area 7

X-15



Design Criteria

In addition to complying with the neighborhood design criteria, Planning Area 7 shall comply with the following design criteria:

- A strong pedestrian linkage shall be provided through Planning Area 7, connecting the residents of this planning area with the commercial and community facilities in Planning Area 15 and the recreational opportunities in Open Space Area 2.
- Units adjacent to the Village Square or Village Green should be designed to front onto the Village Square or Village Green to take advantage of views into these areas.
- Special care should be taken to buffer the units adjacent to possible commercial use in Planning Area 15 from noise and light impacts.
- The vibrant architectural character and street orientation of the units in Planning Area 7 will create an active core adjacent to the Village Green, which will function as an interior park. The design of these units will reflect the San Diego suburban heritage found in communities such as Hillcrest and Mission Hills.
- These homes should be designed with raised or terraced front yards and rear-loaded garages. While adding a landscape buffer at the property edge, the terraced building pad creates a more usable and friendly front yard and porch area (see Exhibit X-5 on page X-10).
- Pedestrian walkways and landscaping in Planning Area 7 shall be designed to create a strong linkage between the Village Square and Village Green.
- To create a more intimate streetscape, uncovered front patios should be located as close to the front property line as feasible.

Development Standards

Development in Planning Area 7 shall comply with all applicable regulations and development standards of the Carlsbad Municipal Code, including the RD-M and PUD requirements, except as modified by Section X.B of this Master Plan.

### 3. Planning Area 8

#### Description

Planning Area 8 is located at the center of the residential core of the Master Plan area. The focal point of Planning Area 8 is the Village Green. Planning Area 8 has a gross area of 19.9 acres and a net developable area of 18.6 acres (see Exhibit X-16 on page X-32). Planning Area 8 may be developed with alley-loaded units, all of which will front onto a street.

#### Implementation of Master Plan Vision and Goals

The purpose of this planning area, including its design standards, is to allow for the development of residential units along a park like boulevard adjacent to the Village Green. Planning Area 8 contributes to the composition of Bressi Ranch, a community designed with a variety of housing, job types, commercial uses, unique street system, trails and pathways and open space areas to provide a pedestrian friendly atmosphere (Goals 4, 5, 7 and 8).

#### General Plan & Zoning

General Plan Land Use: R-15

Units: It is anticipated that Planning Area 8 will be developed with approximately 98 dwelling units.

Zoning: RD-M

#### Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

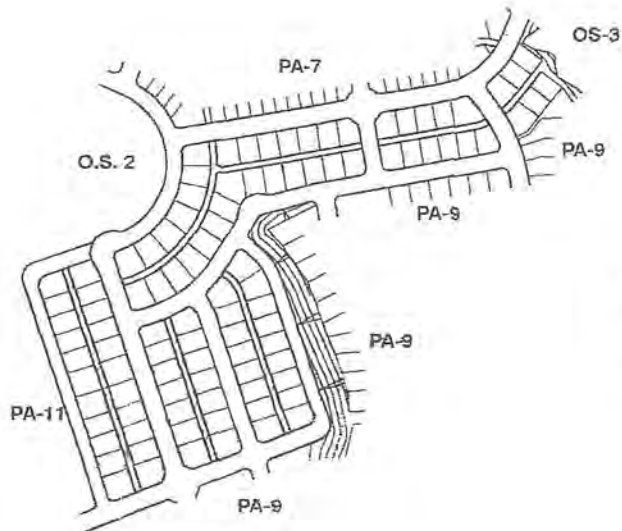
#### Uses Allowed

Residential - Single Family Detached or Multiple Family Attached

#### Design Criteria

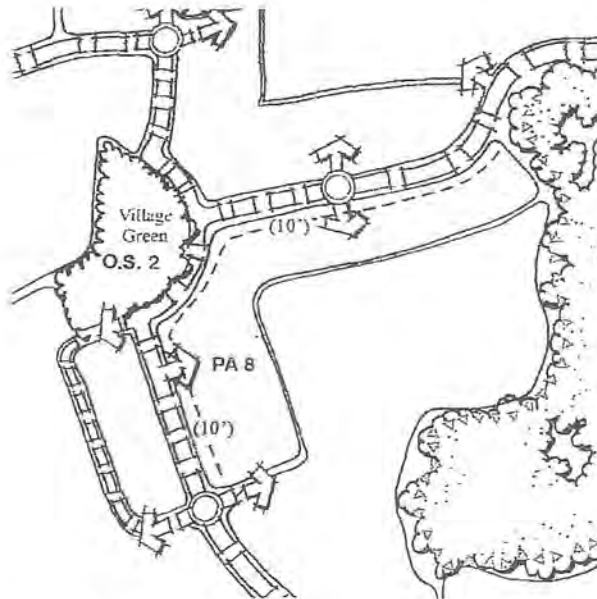
In addition to complying with the neighborhood design criteria, development in Planning Area 8 shall comply with the following design criteria:

- Planning Area 8 will be developed with pedestrian-friendly streets to promote interaction between pedestrians and homeowners. The vibrant



Note: See traffic calming detail 2 for sight distance lines.

Concept Sketch  
(for illustrative purposes only)



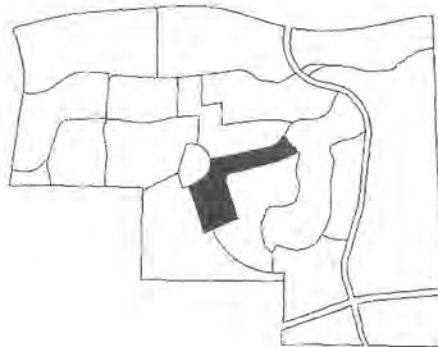
### Legend

- Access
- Detention Basin
- Open Space
- Fire Protection Zone
- City Trail
- Setbacks (Width in Feet)
- Bus Stop/ Bus Turnout
- Runway Protection Zone
- Airport Influence Area
- Flight Activity Zone

General Plan Land Use:  
R-15

Zoning:  
RD-M

Product:  
Single Family Detached/  
Multi-Family Attached



*Bressi Ranch*

A planned community in the City of Carlsbad, California

Planning Area 8

X-16





architectural character and street orientation of these units will create an active core surrounding the Village Green, which will function as an interior park. The design of homes in this planning area will reflect the San Diego suburban heritage found in communities such as Hillcrest and Mission Hills.

- These homes should be designed with raised or terraced front yards and rear-loaded garages. While adding a landscape buffer at the property edge, the terraced building pad creates a more usable and friendly front yard and porch area (see Exhibit X-5 on page X-10).
- Residential units within Planning Area 8 shall be alley loaded, fronting on the public streets. Units immediately adjacent to the Village Green shall be designed to take advantage of views into the recreation area.
- The central feature of Planning Area 8 is its proximity to the Village Green. The area between the access road and the Village Green shall be designed with architectural forward elements on a human scale to encourage pedestrian access.
- To create a more intimate streetscape, uncovered front patios should be located as close to the sidewalk as feasible.

#### Development Standards

Development in Planning Area 8 shall comply with all applicable regulations and development standards of the Carlsbad Municipal Code, including the RD-M and PUD requirements, except as modified by Section X.B of this Master Plan.

#### 4. Planning Area 9

##### Description

Planning Area 9 is located south and east of Planning Area 8, north of Planning Area 11 and west of Open Space Area 3. Planning Area 9 has a gross area of 26.8 acres and a net developable area of 24.1 acres (see Exhibit X-17 on page X-35). Planning Area 9 will be divided into two separate areas, both of which will be developed with residences gaining access to their garages directly from the street, however the garages shall not be the dominant feature of the home. A neighborhood park will create a focal point at the northern portion of Planning Area 9.

##### Implementation of Master Plan Vision and Goals

The purpose of this planning area is to allow for the development of single family detached housing within walking distance of the Village Green as well as the commercial and community facilities in Planning Area 15. Planning Area 9 contributes to the composition of Bressi Ranch, a community designed with a variety of housing, job types, commercial uses, unique street system, trails and pathways and open space areas to provide a pedestrian friendly atmosphere (Goals 4, 5, 7 and 8).

##### General Plan & Zoning

General Plan Land Use: R-4

Units: It is anticipated that Planning Area 9 will be developed with approximately 72 dwelling units.

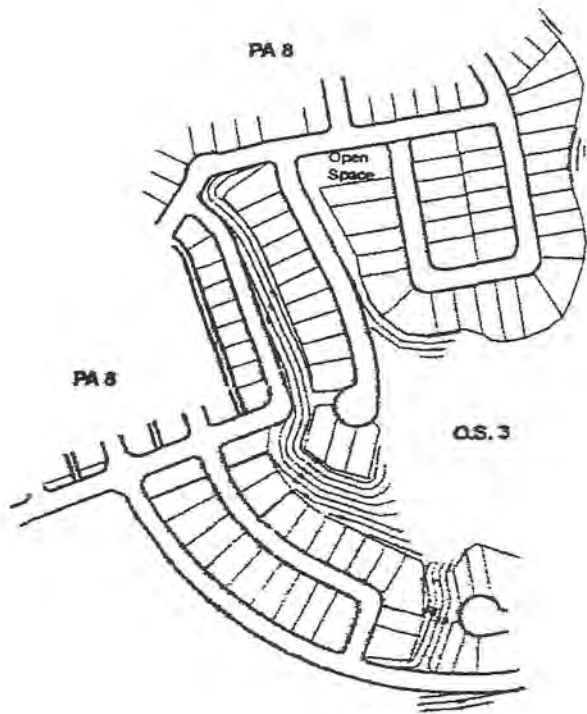
Zoning: R-1

##### Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

##### Uses Allowed

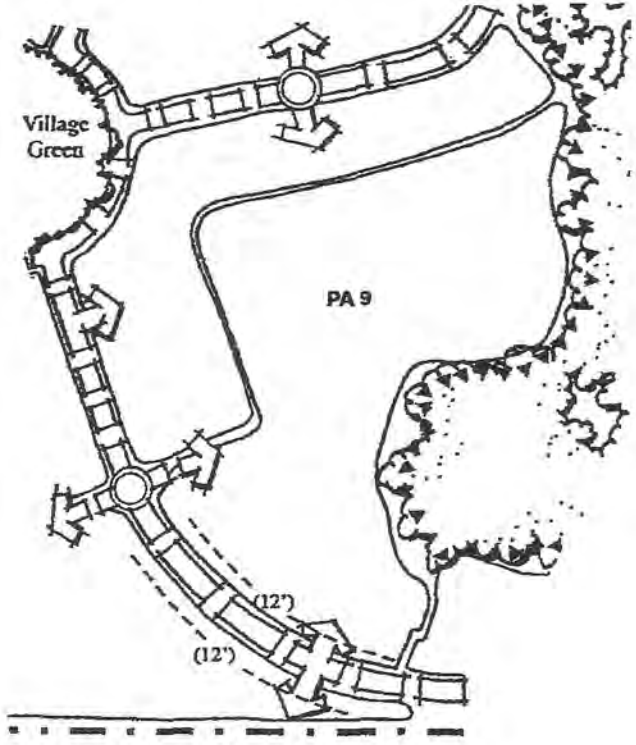
Residential - Single Family Detached



Concept Sketch  
(for illustrative purposes only)

### Legend

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Trail
-  Setbacks (Width in Feet)
-  Bus Stop/ Bus Turnout
-  Runway Protection Zone
-  Airport Influence Area
-  Flight Activity Zone



General Plan Land Use:  
R-4

Zoning:  
R-1

Product:  
Single Family Detached



Design Criteria

In addition to complying with the neighborhood design criteria, development in Planning Area 9 shall comply with the following design criteria:

- Residential units in Planning Area 9 shall be designed to take advantage of views into Open Space Area 3 to the east and the golf course to the south.
- Planning Area 9 shall be designed with pedestrian access to adjacent neighborhoods, the Village Green and commercial and community facility uses in Planning Area 15 through an integrated sidewalk system.
- The garages for all homes in Planning Area 9 shall be designed in accordance with the portion of Section VIII.C of this Master Plan which addresses the design of garages to ensure that they are not to become the dominant element of the portion of the home facing the street.

Development Standards

Development in Planning Area 9 shall comply with all applicable regulations and development standards of the Carlsbad Municipal Code, including the R-1 and PUD requirements, except as modified by Section X.B of this Master Plan.

5. Planning Area 10

Description

Planning Area 10 is located south of Planning Area 6, east of Planning Area 7, west of Open Space Area 1 and north of Planning Area 11. Planning Area 10 has a gross area of 31.5 acres and a net developable area of 26.9 acres (see Exhibit X-18 on page X-38). It is anticipated that Planning Area 10 will be developed with single-family detached homes gaining access to their garages directly from the street. However, the garages shall not be the dominant feature of the home. A neighborhood park will create a focal point for Planning Area 10.

Implementation of Master Plan Vision and Goals

The purpose of this planning area is to allow for the development of single-family detached housing within walking distance of both the Village Green and the Village Center. Planning Area 10 contributes to the composition of Bressi Ranch, a community designed with a variety of housing, job types, commercial uses, unique street system, trails and pathways and open space areas to provide a pedestrian friendly atmosphere (Goals 4, 5, 7 and 8).

General Plan & Zoning

General Plan Land Use: R-4

Units: It is anticipated that Planning Area 10 will be developed with approximately 86 dwelling units.

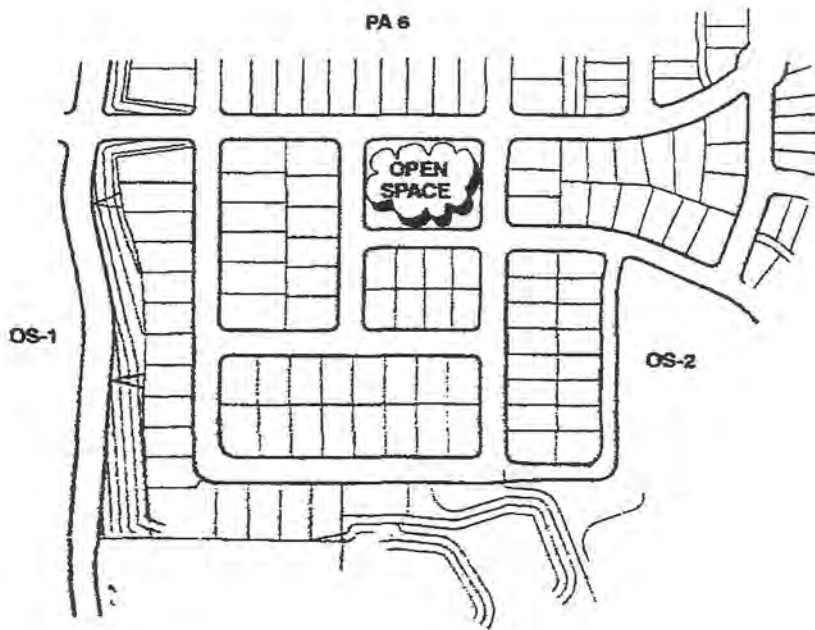
Zoning: R-1

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

Uses Allowed

Residential - Single Family Detached or multi-family



Concept Sketch  
(for illustrative purposes only)

## Legend

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Trail
-  Setbacks (Width in Feet)
-  Bus Stop/ Bus Turnout
-  Runway Protection Zone
-  Airport Influence Area
-  Flight Activity Zone

General Plan Land Use:

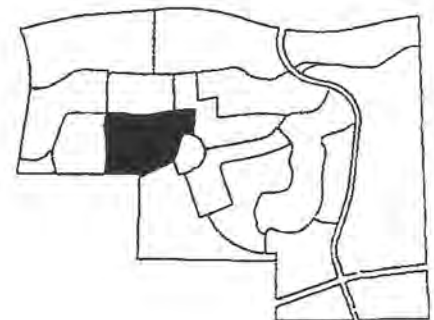
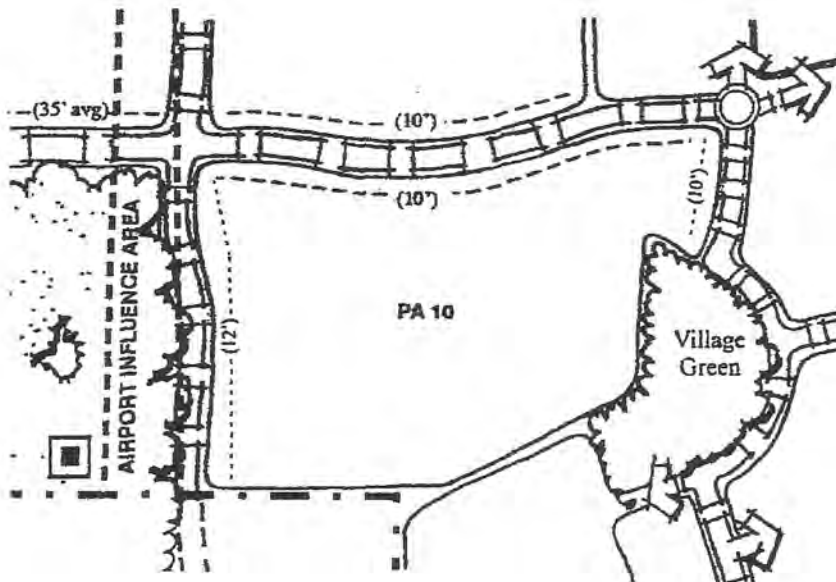
R-4

Zoning:

R-1

Product:

Single Family Detached



*Brenn Ranch*

A planned community in the City of Carlsbad, California

Planning Area 10

X-18



Design Criteria

- In addition to complying with the neighborhood, development in Planning Area 10 shall comply with the following design criteria:
- Planning Area 10 shall be designed with convenient pedestrian access to adjacent neighborhoods and the Village Green.

Development Standards

Development in Planning Area 10 shall comply with all applicable regulations and development standards of the Carlsbad Municipal Code, including the R-1 and PUD requirements, except as modified by Section X.B of this Master Plan.



6. Planning Area 11

Description

Planning Area 11 is located south of Planning Area 10 and to the west of Planning Areas 8 and 9. Planning Area 11 has a gross area of 36.4 acres and a net developable area of 23.2 acres (see Exhibit X-19 on page X-41).

Implementation of Master Plan Vision and Goals

The purpose of this planning area, including its design standards and allowed uses, is to provide for large lot semi-custom and custom homes within walking distance of the industrial/office area and the Village Green. Planning Area 11 contributes to the composition of Bressi Ranch, a community designed with a variety of housing, job types, commercial uses, unique street system, trails and pathways and open space areas to provide a pedestrian friendly atmosphere (Goals 4, 5, 7 and 8).

General Plan & Zoning

General Plan Land Use: R-4

Units: It is anticipated that Planning Area 11 will be developed with between 10 and 25 dwelling units.

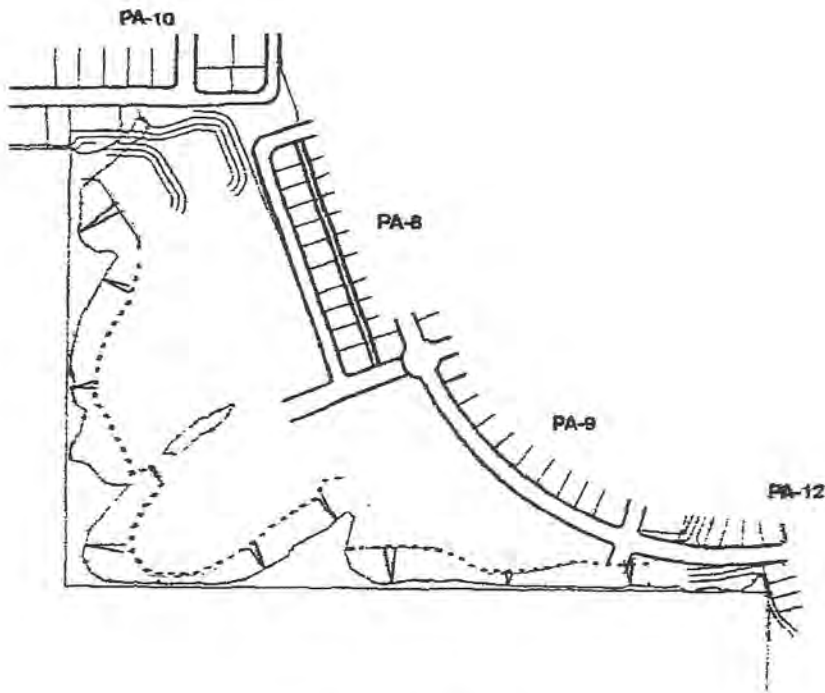
Zoning: R-1-15,000

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

Uses Allowed

Residential - Single Family Detached



Concept Sketch  
(for illustrative purposes only)

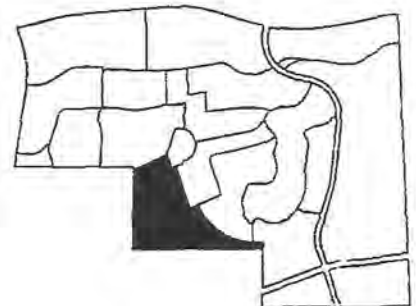
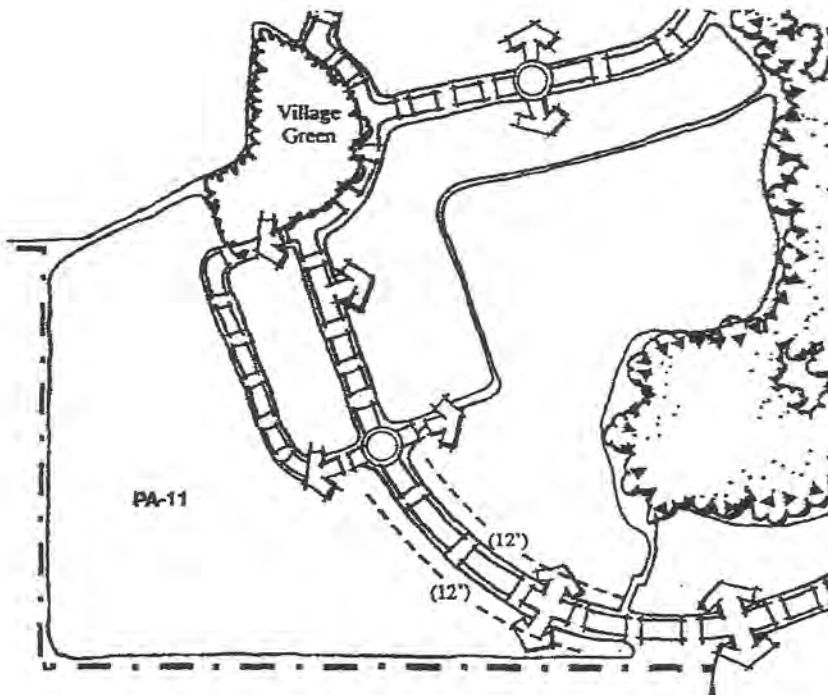
### Legend

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Trail
-  Setbacks (Width in Feet)
-  Bus Stop/Bus Turnout
-  Runway Protection Zone
-  Airport Influence Area
-  Flight Activity Zone

General Plan Land Use:  
R-4

Zoning:  
R-1

Product:  
Single Family Detached



*Bressi Ranch*

A planned community in the City of Carlsbad, California

Planning Area 11

X-19



Design Criteria

In addition to complying with the neighborhood design criteria, development in Planning Area 11 shall comply with the following design criteria:

- Planning Area 11 may be developed as a neighborhood served by a private street system through the approval of a Planned Unit Development application.
- Whenever possible homes shall be designed and located to take advantage of views to the south and west.
- Development in Planning Area 11 should be designed to provide convenient pedestrian links to the Village Green and other portions of the community.
- The proposed La Costa Master Plan shows homes being located adjacent to the southern and western boundaries of Planning Area 11. If this occurs, no brush management planting will be required in this area.

Development Standards

Development in Planning Area 11 shall comply with all applicable regulations and development standards of the Carlsbad Municipal Code, including the R-1 and PUD requirements, except as modified by Section X.B of this Master Plan.

7. Planning Area 12

Description

Planning Area 12 is located in the southeastern corner of the Master Plan area with Planning Area 13 to the north. El Fuerte Street is located to the east of this planning area and Poinsettia Lane is located to the south. Planning Area 12 has a gross area of 24.1 acres and a Growth Management net developable area of 21.4 acres (see Exhibit X-20 on page X-44). Greenhaven Road, the southerly entrance into the Master Plan, divides Planning Area 12 into two portions. A neighborhood park will be the focal point for the southern portion of Planning Area 12.

Implementation of Master Plan Vision and Goals

The purpose of this planning area is to allow for the development of single-family detached housing within walking distance of the Community Facilities in Planning Area 13.

General Plan & Zoning

General Plan Land Use: R-15

Units: It is anticipated that Planning Area 12 will be developed with approximately 94 dwelling units.

Zoning: RD-M

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

Uses Allowed

Residential - Single Family Detached or Multiple Family Attached

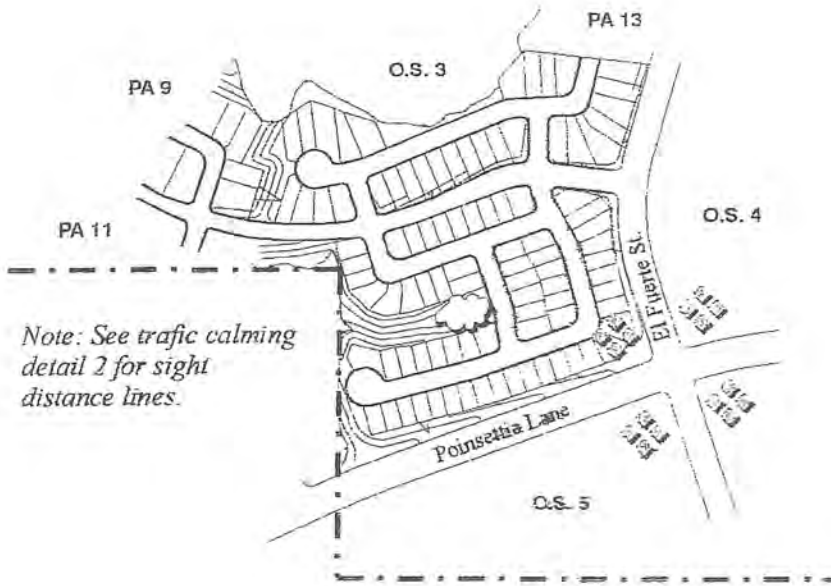
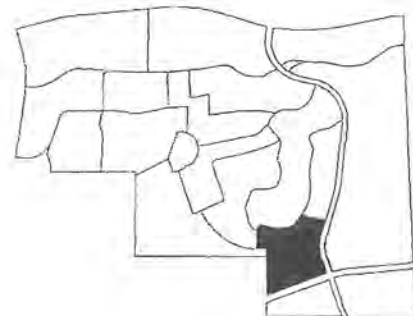
# Legend

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Trail
-  Setbacks(Width in Feet)
-  Bus Stop/ Bus Turnout
-  Runway Protection Zone
-  Airport Influence Area
-  Flight Activity Zone

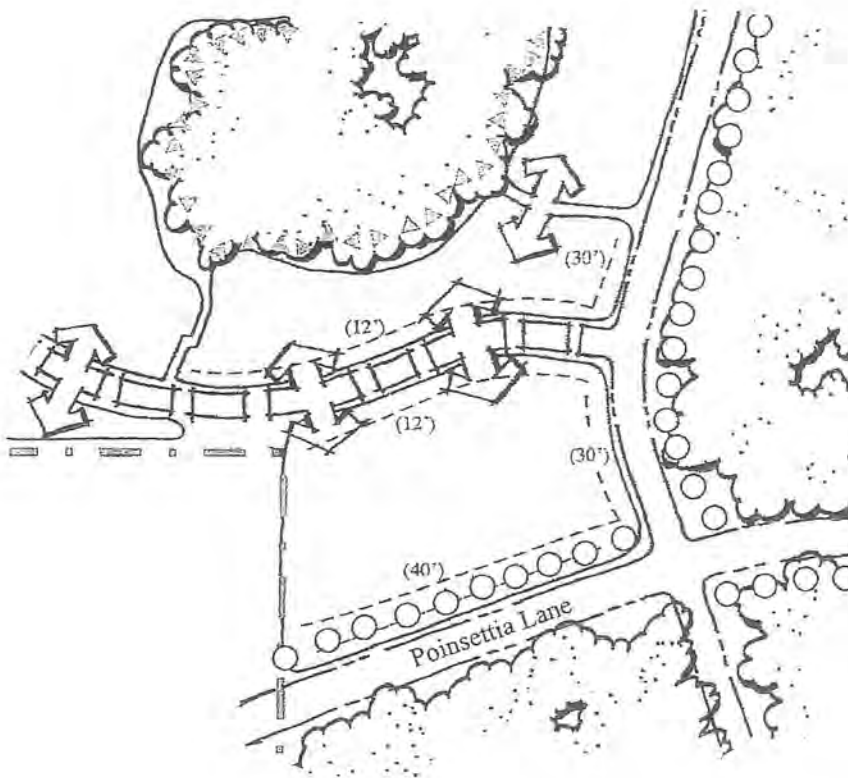
General Plan Land Use:  
*R-15*

Zoning:  
*RD-M*

Product:  
*Single Family Detached  
/Attached Multi-Family*



Concept Sketch  
*(for illustrative purposes only)*



*Bressi Ranch*

A planned community in the City of Carlsbad, California

Planning Area 12

X-20



Design Criteria

In addition to complying with the neighborhood design criteria, development in Planning Area 12 shall comply with the following design criteria:

- The proposed Villages of La Costa Master Plan shows homes being located adjacent to the western boundary of Planning Area 12. If this occurs, no brush management planting will be required in this area.
- If feasible, a third vehicular access into the northern portion of Planning Area 12 through Planning Area 13 may be provided. If vehicular access from the northern portion of Planning Area 12 into Planning Area 13 is not provided, a strong pedestrian connection between these two areas shall be provided.

Development Standards

Development in Planning Area 12 shall comply with all applicable regulations and development standards of the Carlsbad Municipal Code, including the RD-M and PUD requirements, except as modified below and by Section X.B of this Master Plan.

- Planning Area 12 may be developed with two story homes on all lots if the 15% single story units required by Carlsbad's Neighborhood Architectural Design Guidelines are constructed in Planning Areas 6, 9, 10 and 11.
- At least 20% of the homes in Planning Area 12 shall have a single story dominant floor plan.

8. Planning Area 16

Description

Planning Area 16 is located in the northern portion of the Master Plan area, north of Planning Area 15, east of Planning Area 4, and west of El Fuerte Street. Planning Area 15 has a gross area of 17.7 acres and a net developable area of 17.7 acres (see Exhibit VIII-4 on page VIII-18).

This section only addresses the portion of Planning Area 16 that will be developed with residential uses. For information on the portion of the site that will be developed with commercial uses, please see Section VIII.A.3. on Page VIII-17.

Implementation of Master Plan Vision and Goals

The purpose of this planning area is to allow for the creation of a unique mixed-use neighborhood that will provide additional residential units and expand the commercial and restaurant uses available within walking distance to many Bressi Ranch residents. The commercial component of Planning Area 16 will extend Finnilla Place, and the main street feeling of the commercial center.

Many of the goals of the Bressi Ranch Master Plan will be incorporated into the design of this planning area. Housing, jobs, daily needs and other activities are all within easy walking (Goal 2). A variety of uses are also within easy walking distance of transit stops on El Fuerte (Goal 3). There will be a variety of job opportunities in Planning Area 16 (Goal 5).

General Plan & Zoning

General Plan Land Use: R-23/L

Zoning: R-23/L

Units: It is anticipated that Planning Area 16 will be developed with 125 dwelling units.

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.



Uses Allowed

Residential - Multiple Family Attached

Design Criteria

In addition to complying with the neighborhood design criteria, development in Planning Area 16 shall comply with the following design criteria:

- Development in Planning Area 16 should be designed to provide convenient pedestrian links to the adjacent retail center and other portions of the community.

Development Standards

Residential development in Planning Area 16 shall comply with all applicable regulations and development standards of the Carlsbad Municipal Code, including the PUD requirements, except as modified by Section X.B of this Master Plan.

**XI. OPEN SPACE**

Approximately 191 gross acres or 33% of the Bressi Ranch Master Plan is Open Space and is located in Open Space planning areas as shown by Exhibit III-2 on page III-16. This Open Space will remain undeveloped. As the table below shows, the 191 gross acres includes 157.1 acres of performance standard (unconstrained) open space.

MASTER PLAN OPEN SPACE

Total Master Plan Area	585.1 acres
Total Unconstrained Master Plan Area	518.2 acres
Total Open Space in Master Plan	190.7 acres or 33%
Required Master Plan Open Space per Planned Community Zone (Section 21.38.060 (1)(c) of the Carlsbad Municipal Code) @ 15%	87.8 acres
Required Unconstrained Open Space per Growth Management (Section 21.90.080 of the Carlsbad Municipal Code) @ 15%	77.7 acres
Total Performance Standard Open Space	157.1 acres

The 191 gross acres of open space does not include interior slopes or any recreation areas within the individual planning areas, which will provide additional open space within the boundaries of the Master Plan. The exact quantity of this additional acreage will be determined when detailed tentative maps and Site Development Plans are processed for the planning areas.

**A. Master Plan Open Space**

In addition to the Growth Management requirement, Section 21.38.060 of the Carlsbad Municipal Code states that 15 percent of all Master Plans must be provided in an integrated open space program that addresses:

- Open space for the preservation of natural resources
- Open space for the managed production of resources
- Open space for outdoor recreation
- Open space for public health and safety.

Based on this standard, the Bressi Ranch Master Plan is required to provide a total of 87.8 acres of open space. The 122.4 acres of unconstrained open space

provided by the Bressi Ranch Master Plan is significantly more than is required by this section of the Municipal Code. Provided below is a discussion of the four categories of Master Plan Open Space.

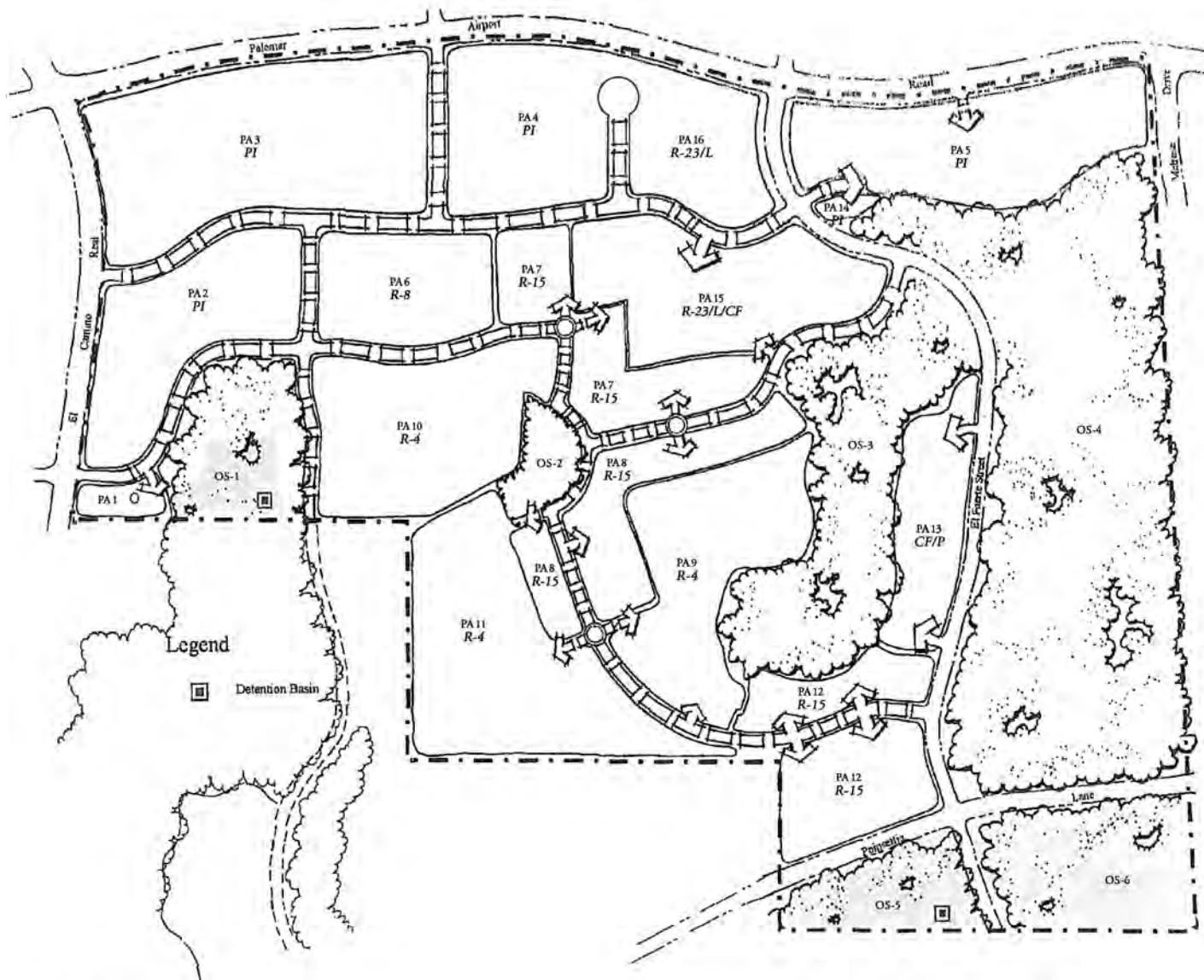
1. Open Space for the Preservation of Natural Resources

Most of Bressi Ranch has been graded and disked for agricultural purposes for the past 60 years. This has resulted in a highly fragmented pattern of native habitats. The majority of the remaining native habitat on Bressi Ranch has been confined to the steeper slopes and the low-lying wetlands area in the southeastern corner of the site. This fragmented pattern of native habitats was recognized when Carlsbad prepared the citywide Habitat Management Plan (HMP).

Open space within the Bressi Ranch Master Plan is being provided in accordance with the Conservation Goals for LFMP Zone 17 of Carlsbad's Habitat Management Plan (HMP). The goals state:

Minimize impacts to remaining natural vegetation communities, particularly in larger blocks in the eastern half and southeastern corner of the zone. Ensure a net loss of no more than 10% of coastal sage scrub habitat, with habitat conserved and restored in the eastern portion of the zone to improve habitat continuity and to widen the east-west habitat linkage immediately south of the zone (Linkage D and Core Area 6 in Zones 18 and 10).

Exhibit XI-1 on page XI-3 shows how the open space within the Master Plan has been configured in conformance with these goals. The majority of the natural open space being preserved within the Master Plan will be located in the eastern and southeastern portions of the site. Most of the native vegetation located on the slopes in Open Space Area 3 will be preserved. Open Space Area 4, in the eastern portion of the site, will provide the largest contiguous block of open space in the Master Plan.



*Bressi Ranch*

A planned community in the City of Carlsbad, California

*Biological Mitigation Plan*

XI-1



Open Space Areas 5 and 6 will preserve the existing and restored wetlands in the southeast corner of the site. A portion of Open Space Area 6 has been restored as off site mitigation for impacts to wetlands in the Rancho Carrillo Master Plan. Additional restoration and enhancement may occur in these open space areas to mitigate for impacts to wetlands by development in the Bressi Ranch Master Plan. The native vegetation in these open space areas as well as the vegetation in Open Space Area 3 will tie into the native open space being preserved in Link D of the HMP within the Rancho Carrillo Master Plan.

Open Space Area 1 will be preserved and enhanced to tie into the native habitat being preserved to the south as a part of the previously approved Habitat Conservation Plan for the Villages of La Costa Master Plan. The habitat being preserved and enhanced in this area as well as Open Space Area 5 will merge into the habitat being preserved in Core Area 6 of the HMP on the Villages of La Costa Master Plan.

As shown by the Biological Mitigation Plan, the habitat being preserved within the Master Plan will provide an important linkage between the habitat that has already been preserved to the east in the Rancho Carrillo Master Plan and west as a part of the Villages of La Costa Master Plan in conformance with the goals of Carlsbad's Habitat Management Plan.

The Biological Mitigation Plan also shows the limits of biological open space as well as the many on-site mitigation areas. This mitigation plan has been developed based on direct input from the City of Carlsbad, the U.S. Fish and Wildlife Service, U.S. Army Corps of Engineers and the California Department of Fish and Game.

## 2. Open Space for the Managed Production of Resources

Although most of the Master Plan area is currently being utilized for agricultural purposes, no portion of the Master Plan will be reserved for the production of agricultural resources.

## 3. Open Space for Outdoor Recreation

A Village Green will be located in Open Space Area 2 to serve the residents of the Master Plan. This facility will be one of the major focal points for the Bressi Ranch Master Plan. Open Space Area 2 will consist of a large open park-like area which may include the following: active recreation facilities, a multi-use building, swimming pool, deck, spa and children's play area. The exact details and location of the facilities to be provided in Open Space Area 2 will be determined as a part of the approval of the Site Development Plan for this site.

Sufficient passive and active recreational facilities shall be provided in this area to meet the recreational requirements of all residential projects within the Bressi Ranch Master Plan. Additional opportunities for passive recreational activities will be available in the Village Square in the mixed-use area. Pocket parks and neighborhood greens will be provided in most of the planning areas that are more than 1,000 feet from the Village Green or Village Square. The passive and active recreation opportunities provided in these areas will be above and beyond those required by any City ordinances or policies.

#### 4. Open Space for Public Health and Safety

All slope areas will be maintained in open space. Each planning area shall provide for dedication of open space easements for slope maintenance purposes at the time of final map approval for that planning area.

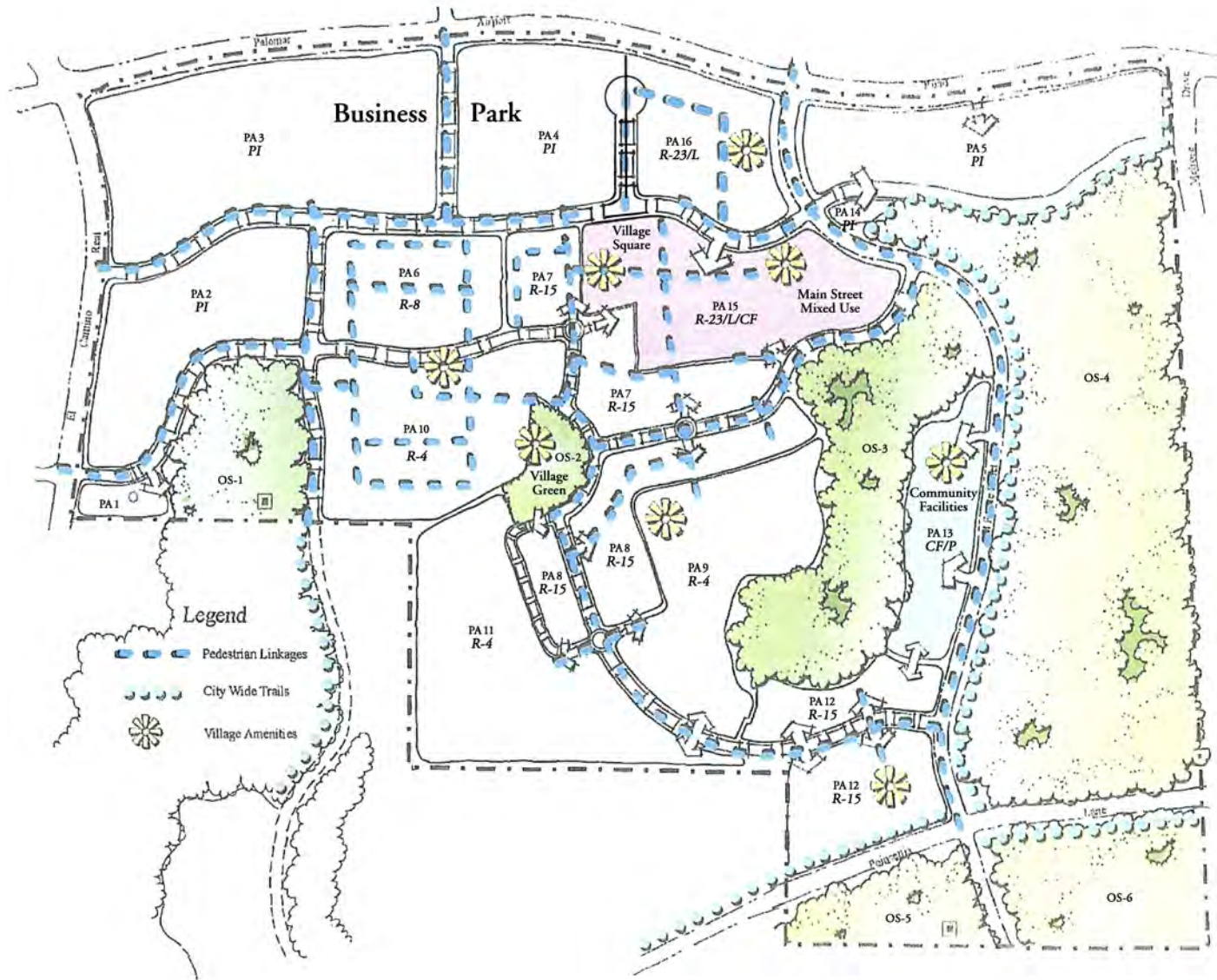
Where deemed necessary by the Fire Marshal, a fire protection plan shall be implemented as shown by Exhibits V-6 and V-7 on pages V-16 and V-17. Any irrigation system placed in the slope areas shall comply with the City of Carlsbad Landscape Manual.

### **B. Pedestrian System**

Due to the terrain of the Bressi Ranch Master Plan and the overall design of the site, there is relatively little opportunity for the creation of trails other than the segments of the Citywide Trail System in the southern and eastern portions of the Master Plan. However, special attention will be given to the sidewalks to encourage pedestrian activity. In conformance with the City's Livable Streets Policy, sidewalks along the streets will be non-contiguous with landscaping between the sidewalks and curb. Exhibit XI-2 on page XI-6 shows the location and types of trails that will be provided.

#### 1. Citywide Trails

Two segments of the Citywide Trail System will be aligned and developed in the Bressi Ranch Master Plan in accordance with the guidelines set forth in the City's Trails Feasibility Study dated June 20, 1990. This study shows a



*Bressi Ranch*

Approved Community in the City of Carlsbad, California

*Village Amenities w/ Pedestrian Linkages*

XI-2

XI-6





trail link (Link No. 35), extending from the intersection of Poinsettia Lane and El Fuerte Street northward to Palomar Airport Road and Melrose Avenue. This link parallels El Fuerte Street and then cuts across Open Space Area 4 to Palomar Airport Road. Trail Link 35 connects with the community trail system within the Master Plan area at El Fuerte Street.

City trail Link 34 is located in the southeastern section of the Master Plan area and serves as a connector to the Rancho Carrillo trail system to the east and to trail systems within the Villages of La Costa Master Plan to the west. The portion of this trail in Open Space Area 6 will be linked with the existing Class 2 trail in the Rancho Carrillo Master Plan on the south side of Poinsettia Lane. At the intersection of Poinsettia Lane and El Fuerte Street this trail will cross Poinsettia Lane and be relocated to the north side of Poinsettia Lane to the west of the El Fuerte Street intersection to avoid environmental constraints on the south side of Poinsettia Lane.

Both of these trail links will be designated as Class 2 trails, which are unpaved hiking and bike paths. The trails will be constructed of decomposed granite, compacted gravel, woodchips, bark, and/or coarse asphalt (where heavy use or erosion may be a problem). If decomposed granite is used, then a binder will be added. The minimum width of the trails will be 8 feet with a total right-of-way of 20 feet. The portion of Trail Link 34 west of El Fuerte Street will be constructed as a ten-foot wide meandering trail to connect into the segment of this trail that will be constructed by the development in the Villages of La Costa Master Plan.

### **C. Maintenance**

#### **1. Open Space**

The Community Association shall maintain open space located outside of the boundaries of the individual planning areas. If a City Maintenance District or other maintenance entity is approved as part of Carlsbad's Habitat Management Plan, these open space areas (OS 1, 3, 4, 5 and 6) shall be maintained by that entity instead of the Community Association.

#### **2. Citywide Trails**

Master Plan trails that are a part of the Citywide Trail Program shall be maintained by the Master Community Association until the City accepts responsibility for their maintenance. Furthermore, other trails of the Master Plan that are open to the public may also be maintained by the City as may be approved by the City of Carlsbad.

To facilitate maintenance of public open space parkways and trails by the City, these public open space areas and public trails may be created as legal lots, where possible, during the subdivision process.

If the City has not developed a maintenance program by the time of development of the trails, the Master Plan trails shall be maintained by the Master Community Association until the City forms such a program and takes over liability and maintenance for these trails. The Master Community Association will be funded in a formula to be established prior to the first final map within the Master Plan. Generally, the formula for cost allocation shall be based on residential units with an allocation factor also provided to non-residential uses based on a residential equivalency.

The Master Community Association may be in the form of a non-conventional community association, as may be approved by the City. It can be in the form of other entities acceptable to the City Planner including, but not limited to, a limited liability company.

For purposes of establishing residential assessments for the Master Community Association (or equivalent), secondary units (if any) will not be treated as separate residential units unless determined otherwise by the Department of Real Estate. In addition, a primary unit with a secondary unit shall not bear a higher assessment than a residential unit without a secondary unit.

The structure of any community association and responsibility thereof will be determined prior to approval of a final map or the issuance of the first residential building permit, whichever occurs first, as discussed in detail in Section III.A.13 of this Master Plan.










#### **D. Individual Open Space Areas**

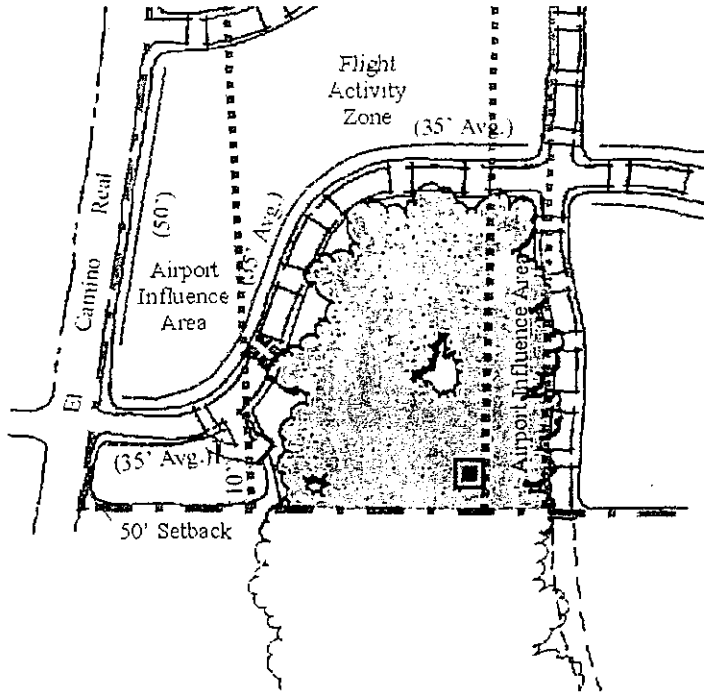
##### **1. Open Space Area 1**

###### Description

Open Space Area 1 is located near the El Camino Real entry to the Master Plan area on the south side of an internal entry road and to the west of Alicante Road. Open Space Area 1 has a gross acreage of 16.9 acres (see Exhibit XI-3 on page XI-9).

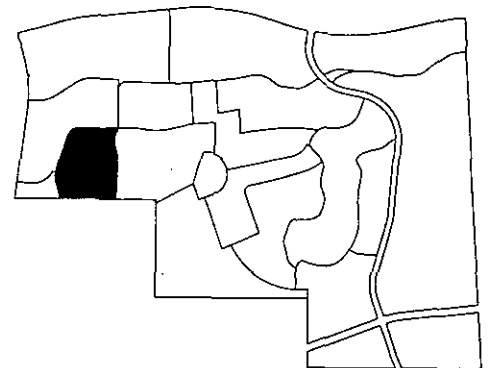
# Legend

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  Setbacks(Width in Feet)
-  Bus Stop
-  Runway Protection Zone
-  Airport Influence Area
-  Flight Activity Zone



General Plan Land Use:  
OS

Zoning:  
OS



Implementation of Master Plan Vision and Goals

This open space area buffers Planning Area 10 from industrial development in Planning Area 1 and traffic on El Camino Real. It provides an entry statement into the residential portion of the Master Plan area and will help to establish the ambiance for the Master Plan.

General Plan & Zoning

General Plan Land Use - Open Space (OS)

Zoning - Open Space (OS)

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

Uses Allowed

All development in Open Space Area 1 shall comply with the regulations and development standards of Open Space, O-S Zone. Other than the following uses, no development shall occur in Planning Area 1:

- Slope grading
- Biological Habitat Preserve
- Trails
- Installation and maintenance of detention basin and other erosion control measures
- Transportation right-of-ways
- Vista points
- Open Space and non-vehicular public access easements
- Public facilities, such as water and sewer lines
- Other similar uses as approved by the City Planner

Special Design Criteria

Due to the impacts to the existing native vegetation caused by the construction of Alicante Road, manufactured slopes shall be enhanced with sensitive plant species where appropriate.

Naturalized re-vegetation will be required in this area as this open space area will tie into the Villages of La Costa Master Plan HCP to the south. Special attention shall be given to the re-vegetation of this area to ensure compatibility with the open space to the south.

Wherever feasible the large manufactured slopes created by the construction of Alicante Road and Town Garden Road shall be re-sculpted to resemble the existing grades and planted with native vegetation.

Development Standards

All development in Open Space Area 1 shall comply with the regulations and development standards of Chapter 21.33 (Open Space) of the Carlsbad Municipal Code. Unless shown as a part of an approved tentative map or other discretionary approval, all uses allowed by Section 21.33 of the Open Space Zone shall be subject to approval of a Site Development Plan by the Planning Commission.

2. Open Space Area 2

Open Space Area 2 (the Village Green) is discussed in Chapter IX of the Master Plan. Although it is designated as an Open Space Area, Planning Area 2 is not discussed in this portion of the Master Plan because it will be developed differently than Open Space Areas 1 and 3-6. Open Space Area 2 will be developed as a Village Green with active and passive recreational uses for the residents of the Master Plan. The remaining Open Space Areas will remain in a natural state except for trails and limited public facilities.

3. Open Space Area 3

Description

Open Space Area 3 is located in the eastern portion of the Master Plan area and consists of large slopes covered with native habitat. Open Space Area 3 has 39.5 gross acres (see Exhibit XI-4 on page XI-14).

Implementation of Master Plan Vision and Goals

This open space area represents a large contiguous block of open space extending southward from El Fuerte Street into the central residential planning areas of the Master Plan Area. Most of the native vegetation located on the slopes in Open Space Area 3 will be preserved and, in conjunction with the vegetation in Open Space Areas 5 and 6, will tie into the native open space being preserved in Link D of Carlsbad's Habitat Management Plan.

General Plan & Zoning

General Plan Land Use - Open Space (OS)

Zoning - Open Space (OS)

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

Uses Allowed

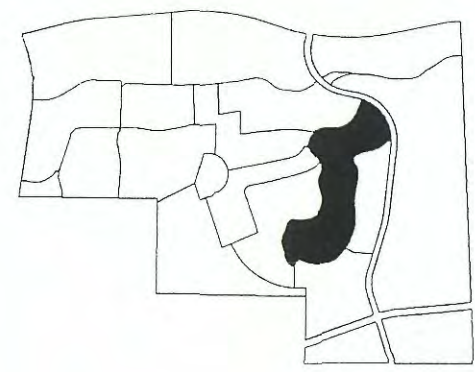
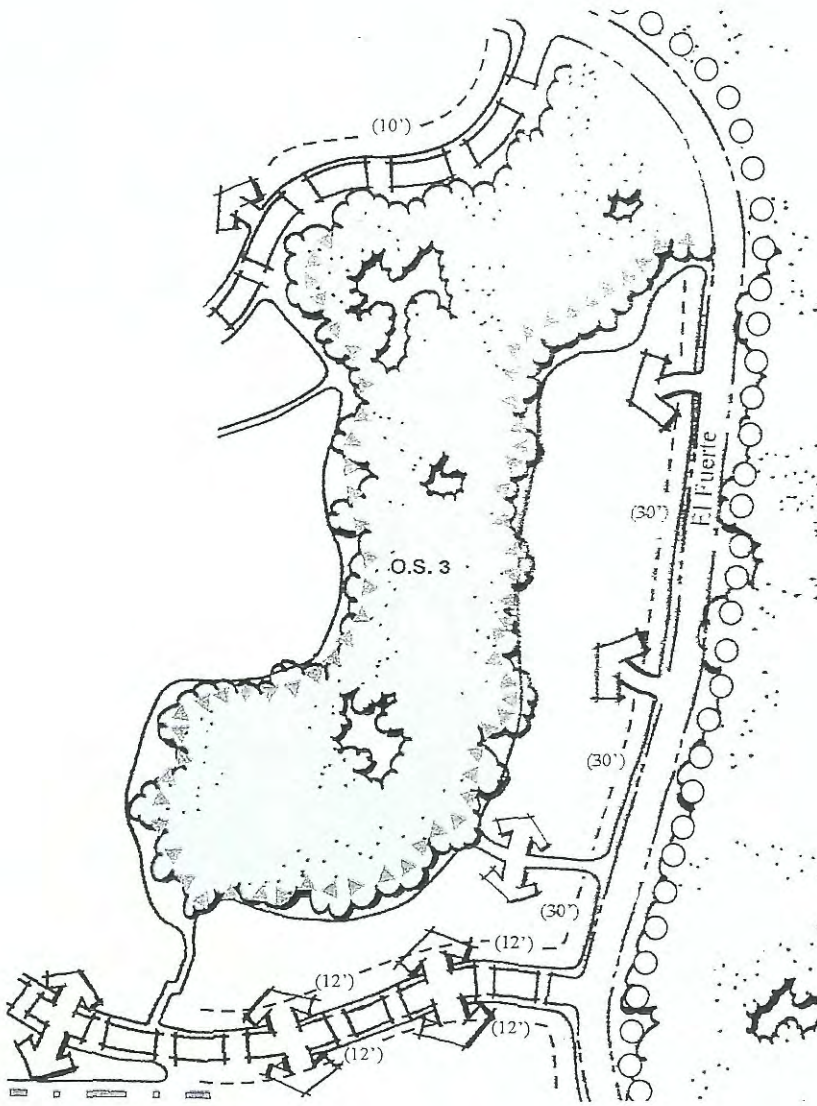
All development in Open Space Area 3 shall comply with the development standards of the Open Space, O-S Zone. Other than the following uses, no development shall occur in Planning Area 3:

- Slope grading
- Biological Habitat Preserve
- Trails
- Installation and maintenance of detention basin and other erosion control measures
- Transportation right-of-ways



# Legend


-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Wide Trail
-  Setbacks(Width in Feet)
-  Bus Stop/ Bus Turnout
-  Runway Protection Zone
-  Airport Influence Area
-  Flight Activity Zone



General Plan Land Use:  
OS

Zoning:  
OS

*Bressi Ranch*

A planned community in the City of Carlsbad, California  
  
 Date: 3/26/02 Bressi 02/14

Open Space 3 XI-4



- Vista points
- Open Space and non-vehicular public access easements
- Public facilities, such as water and sewer lines

Other similar uses as approved by the City Planner

Special Design Criteria

- This open space area shall remain in its natural state except where impacted by manufactured slopes due to the construction of El Fuerte Street, internal circulation roadways and the development of community facilities in Planning Area 13.
- Manufactured slopes shall be enhanced with sensitive plant species where appropriate.

Development Standards

- All development in Open Space Area 3 shall comply with the regulations and development standards of Chapter 21.33 (Open Space) of the Carlsbad Municipal Code. Unless shown as a part of an approved tentative map or other discretionary approval, all uses allowed by Section 21.33 of the Open Space Zone shall be subject to approval of a Site Development Plan by the Planning Commission.

4. Open Space Area 4

Description

Open Space Area 4 is located between El Fuerte Street and the easternmost extent of the Master Plan area. It extends to Poinsettia Lane to the south. Open Space Area 4 has a gross acreage of 98.9 acres (see Exhibit XI-5 on page XI-17).

Implementation of Master Plan Vision and Goals

Open Space Area 4 represents the largest contiguous block of open space within the Bressi Ranch Master Plan. This open space area will provide a link between the native vegetation areas of Open Space Areas 3, 5 and 6.

General Plan & Zoning

General Plan Land Use - Open Space (OS)

Zoning - Open Space (OS)

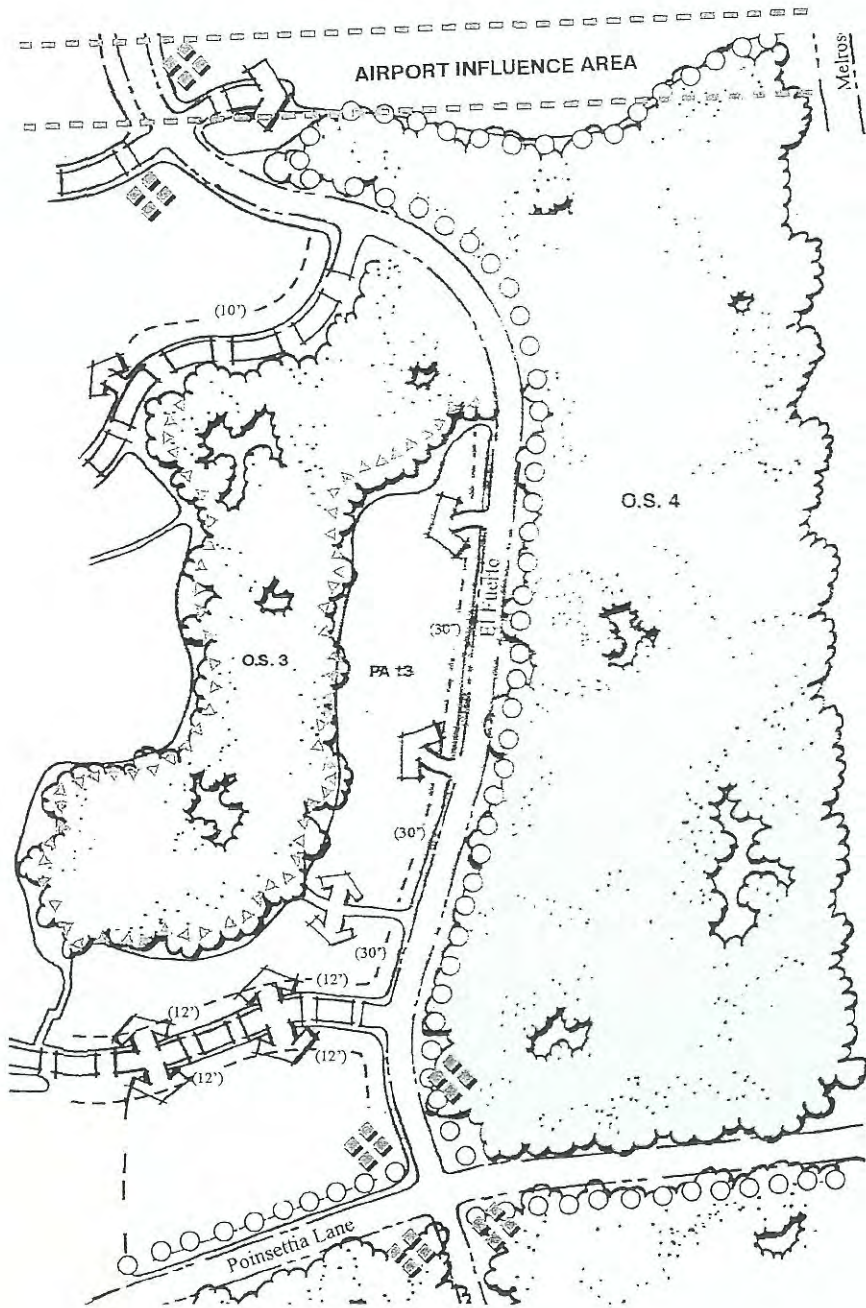
Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

Uses Allowed

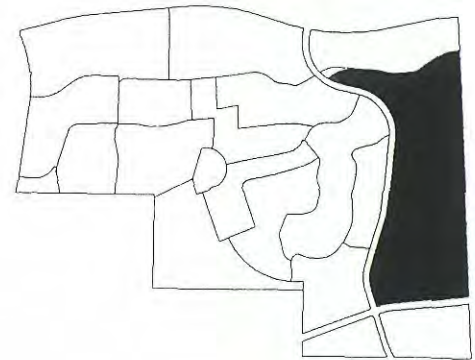
All development in Open Space Area 4 shall comply with the development standards of the Open Space, O-S Zone. Other than the following uses, no development shall occur in Planning Area 4:

- Slope grading
- Biological Habitat Preserve
- Trails
- Installation and maintenance of detention basin and other erosion control measures
- Transportation right-of-ways
- Vista points
- Open Space and non-vehicular public access easements



# Legend

- Access
- Detention Basin
- Open Space
- Fire Protection Zone
- City Wide Trail
- Setbacks(Width in Feet)
- Bus Stop/ Bus Turnout
- Runway Protection Zone
- Airport Influence Area
- Flight Activity Zone



General Plan Land Use:  
OS

Zoning:  
OS

*Bressi Ranch*

Planned community in the City of Carlsbad, California



Date: 1/20/07 Bressi 001.rvt

*Open Space 4*

XI-5





- Public facilities, such as water and sewer lines
- Other similar uses as approved by the City Planner

An entry monument sign may be permitted in this Open Space Area, across from the roadway providing access into the residential portion of the Master Plan.

#### Special Design Criteria

- This open space area shall remain in its natural state except where impacted by manufactured slopes of planning areas and manufactured slopes due to the construction of El Fuerte Street and the Citywide Trail System.
- Manufactured slopes shall be enhanced with sensitive plant species where appropriate and as shown on the Mitigation Plan in conformance with the goals of Carlsbad's Habitat Management Plan.

#### Development Standards

All development in Open Space Area 4 shall comply with the regulations and development standards of Chapter 21.33 (Open Space) of the Carlsbad Municipal Code. Unless shown as a part of an approved tentative map or other discretionary approval, all uses allowed by Section 21.33 of the Open Space Zone shall be subject to approval of a Site Development Plan by the Planning Commission.

#### Trails

This open space area provides an important link in the Citywide Trail System, which runs through the eastern portion of the Master Plan area. The trail link is designated as Trail Link No. 35 in the City of Carlsbad's Trail Feasibility Study dated June 20, 1990. This link runs from Palomar Airport Road through Open Space Area 4 then, parallels El Fuerte southward to the intersection of El Fuerte Street and Poinsettia Lane. The exact alignment of this trail link may be modified so that it does not conflict with the requirements of Carlsbad's Habitat Management Plan.

5. Open Space Area 5

Description

Open Space Area 5 is located south of Poinsettia Lane and west of El Fuerte in the southernmost portion of the Master Plan. Open Space Area 5 has a gross acreage of 11.0 acres (see Exhibit XI-6 on page XI-20).

Implementation of Master Plan Vision and Goals

Open Space Area 5 will preserve existing and restored wetlands. The native vegetation in Open Space area 5, in conjunction with Planning Areas 3 and 6, will tie into the native open space being preserved in Link D of Carlsbad's Habitat Management Plan.

General Plan & Zoning

General Plan Land Use - Open Space (OS)

Zoning - Open Space (OS)

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

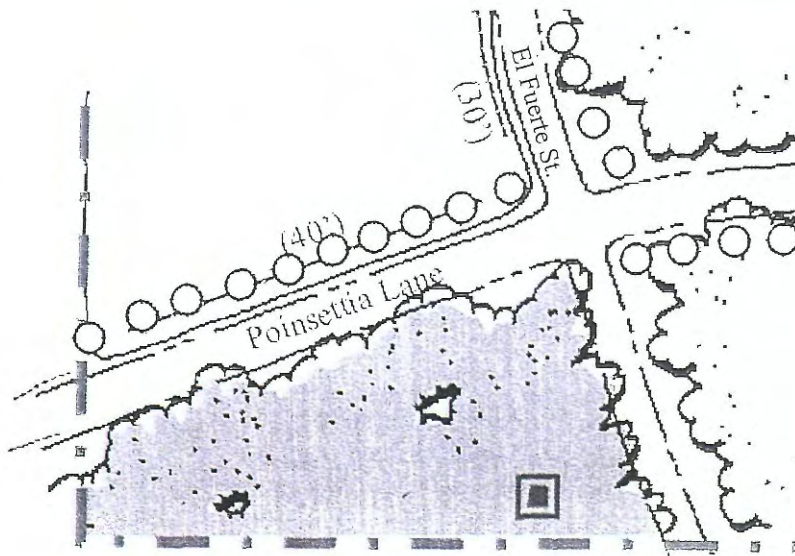
Uses Allowed

All development in Open Space Area 5 shall comply with the development standards of the Open Space, O-S Zone. Other than the following uses, no development shall occur in Planning Area 5:

- Slope grading
- Biological Habitat Preserve
- Trails
- Installation and maintenance of detention basin and other erosion control measures
- Transportation right-of-ways
- Vista points
- Open Space and non-vehicular public access easements

# Legend

-  Access
-  Detention Basin
-  Open Space
-  Fire Protection Zone
-  City Wide Trail
-  Setbacks (Width in Feet)
-  Bus Stop
-  Runway Protection Zone
-  Airport Influence Area
-  Flight Activity Zone

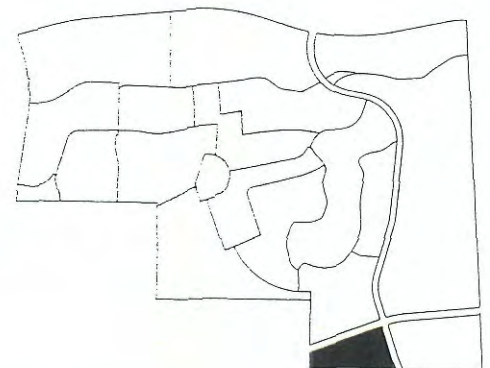


General Plan Land Use:

OS

Zoning:

OS





- Public facilities, such as water and sewer lines
- Other similar uses as approved by the City Planner

Special Design Criteria

- This open space area shall remain in its natural state except where impacted by manufactured slopes due to the construction of El Fuerte Street and Poinsettia Lane, the Citywide Trail segment and the detention basin.
- Manufactured slopes shall be enhanced with native plant species where appropriate and as shown on the Mitigation Plan.

Development Standards

- All development in Open Space Area 5 shall comply with the regulations and development standards of Chapter 21.33 (Open Space) of the Carlsbad Municipal Code. Unless shown as a part of an approved tentative map or other discretionary approval, all uses allowed by Section 21.33 of the Open Space Zone shall be subject to approval of a Site Development Plan by the Planning Commission.

Trails

This open space area provides an important link in the Citywide Trail System, which runs through the southern portion of the Bressi Ranch Master Plan. The trail link is designated as Trail Link 34 in the City's Trails Feasibility Study dated June 20, 1990. This link connects to the Rancho Carrillo Master Plan trail system to the east and to the Villages of La Costa Master Plan trail system to the west.

6. Open Space Area 6

Description

Open Space Area 6 is located south of Poinsettia Lane and east of El Fuerte in the southeastern portion of the Master Plan. Open Space Area 6 has a gross acreage of 20.4 acres (see Exhibit XI-7 on page XI-23).

Implementation

Open Space Area 6 will preserve existing and restored wetlands. The native vegetation in Open Space area 6, in conjunction with Planning Areas 3 and 5, will tie into the native open space being preserved in Link D of Carlsbad's Habitat Management Plan.

General Plan & Zoning

General Plan Land Use - Open Space (OS)

Zoning - Open Space (OS)

Discretionary Permits Required for Development

All development proposals shall be processed in conformance with the requirements of the Carlsbad Municipal Code as detailed in Chapter VI of this Master Plan.

Uses Allowed

All development in Open Space Area 6 shall comply with the development standards of the Open Space, O-S Zone. Other than the following uses, no development shall occur in Planning Area 6:

- Slope grading
- Biological Habitat Preserve
- Trails
- Installation and maintenance of detention basin and other erosion control measures
- Transportation right-of-ways
- Vista points
- Open Space and non-vehicular public access easements

# Legend



Access



Detention Basin



Open Space



Fire Protection Zone



City Wide Trail



Setbacks(Width in Feet)



Bus Stop



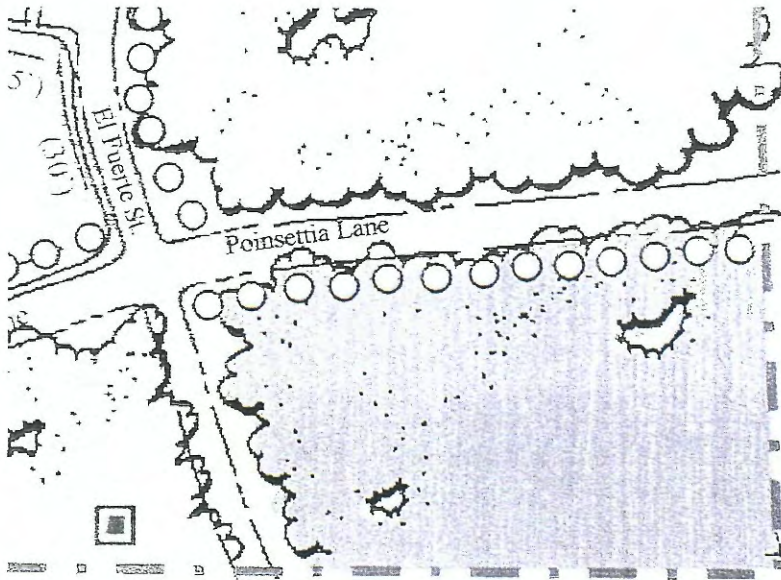
Runway Protection Zone



Airport Influence Area



Flight Activity Zone

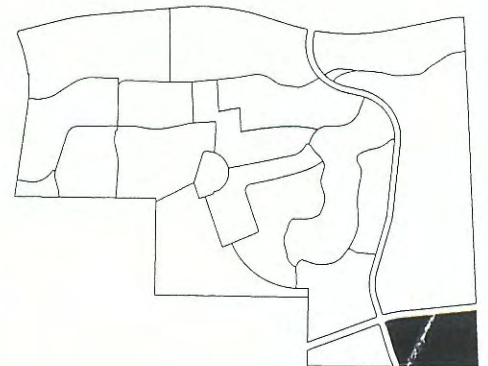


General Plan Land Use:

OS

Zoning:

OS



- Public facilities, such as water and sewer lines
- Other similar uses as approved by the City Planner

### Special Design Criteria

- This open space area shall remain in its natural state except where impacted by manufactured slopes due to the construction of El Fuerte Street, Poinsettia Lane, the Citywide Trail segment and wetland mitigation.
- Manufactured slopes shall be enhanced with native plant species where appropriate.

### Development Standards

All development in Open Space Area 6 shall comply with the regulations and development standards of Chapter 21.33 (Open Space) of the Carlsbad Municipal Code. Unless shown as a part of an approved tentative map or other discretionary approval, all uses allowed by Section 21.33 of the Open Space Zone shall be subject to approval of a Site Development Plan by the Planning Commission.

### Trails

This open space area provides an important link in the Citywide Trail System, which runs through the southern portion of the Bressi Ranch Master Plan. The trail link is designated as Trail Link 34 in the City's Trails Feasibility Study dated June 20, 1990. This link connects to the Rancho Carrillo Master Plan trail system to the east and to the Villages of La Costa Master Plan trail system to the west.

## **XII. GRADING**

### **A. Introduction**

The purpose of this chapter is to establish appropriate guidelines for the grading of the Bressi Ranch Master Plan. Since the intent of the Master Plan process is to review the area in question in a comprehensive manner, the proposed grading will be examined on an overall Master Plan basis rather than on an individual planning area basis. A Hillside Development Permit is being processed concurrently with the Bressi Ranch Master Plan to address the proposed grading of the entire site. The extensive public facility requirements, topography and soils of the Master Plan area require that a comprehensive grading plan be prepared.

There are a number of conditions that are addressed in the grading plan. A major concern is the construction of four circulation element roadways within and adjacent to the boundary of the Master Plan. One of the most significant impacts will be the construction of El Fuerte Street, a secondary arterial, running north/south within the Master Plan area. El Fuerte Street will require significant amounts of cut/fill in order to meet the City's engineering grade and alignment standards. In addition, substantial amounts of grading will be required for the construction for Poinsettia Lane (including off-site Poinsettia), Alicante and the collector roads that connect to these arterial roadways. In addition, the Village Center requires substantial grading to create the pedestrian oriented relationships.

Development in Planning Areas 1, 2, 3, 4, 5, 14, 15 and 16, the non-residential portion of the Master Plan, will require large pads to accommodate the large scale office-industrial uses and their required parking. Development in Planning Area 13, the Community Facilities site, may also require larger pads to accommodate the community facility type uses and their required parking. Grading in these areas will be in compliance with Section 21.95.140 of Carlsbad's Hillside Development Ordinance.

A significant topographic difference exists between the northern and southern portions of the Bressi Ranch Master Plan. In addition, several large steep sided ravines bisect the northern portion of the site. Approximately 33% of the Master Plan will be set aside as native open space in accordance with the goals of Carlsbad's Habitat Management Plan. A significant portion of the open space being preserved for natural habitat is relatively flat and has no significant native vegetation. However, this area is being preserved for habitat purposes to create large contiguous areas of native habitat and to create viable connections between native habitats to the east and west of the Bressi Ranch Master Plan.

Residential development has been located on the central and western portion of the site to accommodate the creation of these open space corridors. This portion of the site is split by several isolated steep ravines and other areas with large slopes. These areas have relatively little habitat value, and this was recognized by City staff and the Wildlife Agencies when the HMP boundaries were determined for the Bressi Ranch Master Plan. Special consideration was given in the creation of the grading plan for the Bressi Ranch Master Plan to ensure the developability of this area while still complying with the requirements of Carlsbad's Hillside Development Ordinance.

The grading plan will ensure that needed public facility improvements and remedial grading for geotechnical problems will be adequately addressed while minimizing potential impacts to the existing landforms. The proposed single phase of grading will reduce the overall impacts of the grading (i.e. no crossing of constructed circulation element roadways).

With the proposed mass grading the majority of grading within the Master Plan will be complete. The remaining grading will be limited to final grading within each planning area. This grading will be limited in scope and will not require hauling across previously constructed roadways.

Exhibit XII-1 on page XII-3 shows the approximate cut and fill required for development of the Bressi Ranch Master Plan.

Separate and individual grading permits will be required for final or site grading. Each subsequent grading permit shall be based upon the specific site development plan or planned industrial permit, or approved by discretionary action.

## **B. Conformance with City of Carlsbad Ordinances**

All grading proposed by a grading design intended to implement this plan shall also conform to the following City of Carlsbad ordinances, policies and/or guidelines unless otherwise permitted within this Master Plan:

1. Excavation & Grading Ordinance (Chapter 15.16, Carlsbad Municipal Code)
2. Hillside Development Regulations (Chapter 21.95, Carlsbad Municipal Code)
3. Zone 17 Local Facilities Management Plan
4. City of Carlsbad Design Guidelines





— CUT CONTOUR  
— FILL CONTOUR

Bressi Ranch

Grading Cut & Fill Areas XII-1  
XII-3



5. City of Carlsbad Landscape Guidelines
6. City of Carlsbad Master Drainage Plan
7. City of Carlsbad Planning Department Policies
8. City of Carlsbad Engineering Department Policies

**C. Advanced Grading**

Prior to Final Map, an advanced grading permit for the Master Plan may be requested for mass grading for the up-front construction of on-site circulation element roadways, public infrastructure (sewer, water, drainage) and community facilities.

The advanced grading permit will benefit the City by providing public facilities prior to development within the Master Plan.

The advanced grading permit will conform to the requirements of Chapter 11.06 of the Carlsbad Municipal Code.

### **XIII. PUBLIC FACILITIES**

#### **A. Introduction**

The boundaries of the Bressi Ranch Master Plan and the Zone 17 Local Facilities Management Plan (LFMP) are contiguous. Facility requirements for the Bressi Ranch Master Plan are identified in the Zone 17 Local Facilities Management Plan. The financing of these facilities is addressed in the Financing Section of the Zone 17 LFMP. The Zone 17 LFMP addresses the eleven facilities and services required by the City's Growth Management Program. The Bressi Ranch Master Plan implements but does not supercede the Zone 17 Local Facilities Management Plan.

#### **B. Facilities Based on Master Plan Phasing**

The Zone 17 Local Facilities Management Plan was prepared pursuant to Chapter 21.90 of the Carlsbad Municipal Code. The Zone 17 LFMP requires that a financing program guaranteeing the construction of required facilities must be approved prior to recordation of any map, issuance of a building or grading permit, whichever occurs first in the Master Plan area (Zone 17). The finance plan links specific facilities improvements to the planning area requiring those facilities for development.

The LFMP demonstrates how and when each required facility and improvement will be constructed to accommodate development within the Master Plan and provides a complete description of how each facility and improvement will be financed when mitigation is necessary. Any planning area or portion of the Master Plan may develop so long as adequate facilities are provided concurrently or ahead of development to serve that planning area. In addition, prior to recordation of a final map, issuance of a grading permit or building permit for each planning area, the applicant shall demonstrate the adequacy and availability of public facilities and services.

#### **C. Citywide Facilities**

The Zone 17 LFMP analyzed the adequacy and availability of City Administrative Facilities, Library Facilities and Wastewater Treatment Capacity at a citywide level. These are discussed below:

1. City Administrative Facilities

City Administrative Facilities currently meet and are anticipated to continue to meet the adopted performance standard through build out. Administrative facilities include maintenance, warehouse and administrative buildings. These facilities are funded by revenues generated through the City's Public Facilities Fees, Water and Sewer Replacement Fees, Community Facilities District No. 1. Per the Zone 17 LFMP, no special mitigation is required for the zone to meet the adopted performance standard. The proportional obligation by the Bressi Ranch property owners will be met through the payment of Public Facilities Fees and Water Replacement Fees and participation in Community Facilities District No. 1.

2. Library Facilities

Library Facilities currently meet and are anticipated to continue to meet the adopted performance standard through build out. Library facilities include the new library located in south Carlsbad and the expansion of the old library on Carlsbad Village Drive. These facilities are funded through Public Facilities Fees and Community Facilities District No. 1. No mitigation is required by the Zone 17 LFMP, however, the proportional obligation by the Bressi Ranch property owners will be met through the payment of Public Facilities Fees and participation in Community Facilities District No. 1.

3. Wastewater Facilities

Primary treatment capacity for the City of Carlsbad is provided through the Encina Wastewater Pollution Control Facility, which will ultimately be able to treat up to 45.0 million gallons per day (MGD). After being treated, wastewater is then discharged through the Encina Ocean Outfall into the Pacific Ocean. The Carlsbad Municipal Water District retains capacity rights of 9.24 MGD of Encina's treatment capacity, and shares outfall capacity on a percentage basis with five other water/sewer service districts. The water/sewer service districts will jointly monitor Encina treatment plant flows to ensure adequate capacity.

**D. Quadrant Facilities**

Zone 17 is located within the southeast quadrant of the City of Carlsbad. Park facilities are the only facility evaluated on a quadrant basis. Park Facilities are discussed below:

1. Park Facilities

Zone 17 is within the southeast quadrant park district. Park Facilities within this quadrant currently meet and are anticipated to continue to meet the adopted performance standard through build out. Zone 17 will contribute its fair share of park facilities through the payment of park-in-lieu fees at the time of issuance of building permits.

**E. Zone Facilities**

Public facilities evaluated on a zone-by-zone basis include the following: Drainage, Circulation, Fire, Open Space, Schools, Sewer Collection and Water Distribution. Each of these facilities within Zone 17 will meet the performance standard through build out.

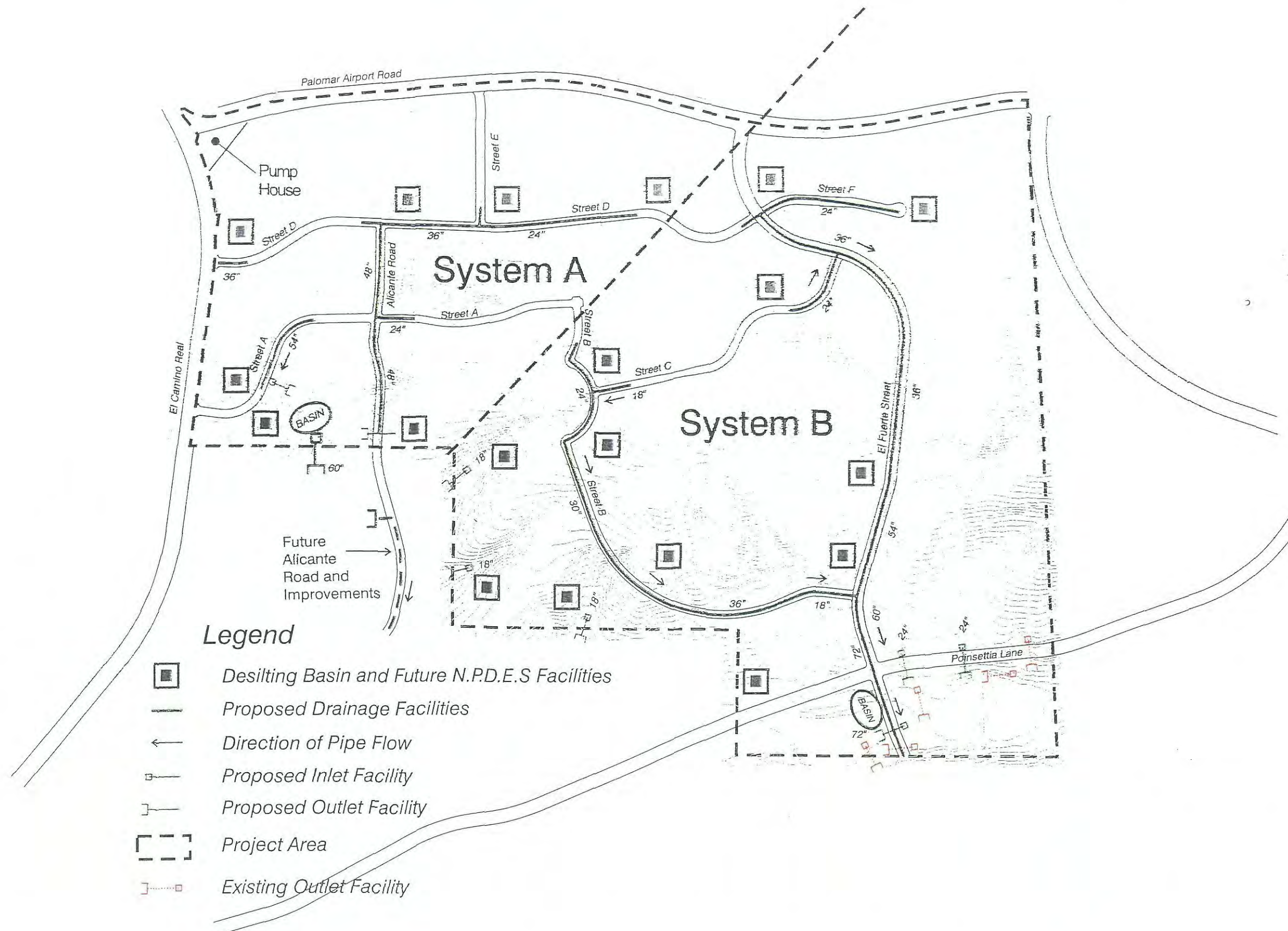
1. Drainage

Drainage facilities must be provided as required by the City concurrent with development. These facilities are more accurately assessed as specific development plans within each zone are finalized. The Bressi Ranch Master Plan is proposing construction of two major drainage systems within Zone 17: System A and System B (see Exhibit XIII-1 on page XIII-4). The Zone 17 LFMP addresses these facilities and establishes the special conditions for development within the zone.

The proposed project is located in the City of Carlsbad and is located in the San Diego Hydrologic Region (SDHR), which generally drains westerly toward the Pacific Ocean. The site contributes storm water runoff to two hydrologic areas, the majority draining into the San Marcos Creek Basin and the northwestern most portion of the project into the Encinas Creek Basin.

The majority of the project site has been used as an agricultural site, with some native terraces and canyons. Drainage improvements will be constructed as part of the Master Plan that will address reduction of peak flows, sedimentation and water quality. These improvements will meet the developed policies, rules and procedures as currently adopted by the San Diego Regional Water Quality Control Board.

The future storm drain facilities vary in size from 24 inch to 72 inch, with the major flow outletting into a detention basin located east of El Fuerte and south of Poinsettia Lane. The improvements will include storm drain facilities within the internal streets, desilting basins within each planning area (or as required by the grading), two detention/retention basins and grass-lined



**Legend**

- Desilting Basin and Future N.P.D.E.S Facilities
- Proposed Drainage Facilities
- Direction of Pipe Flow
- Proposed Inlet Facility
- Proposed Outlet Facility
- Project Area
- Existing Outlet Facility

*Bressi Ranch*

A planned community in the City of Carlsbad, California



Date: 1/29/02 or graphics/1123.cad/113MP-Bressi2010.dwg

*Proposed Major Drainage Facilities*

XIII-1

XIII-4



swales (where applicable). Standards associated with the National Pollutant Discharge Elimination System (NPDES) permit for the San Diego County area will also be incorporated into the proposed improvements once such standards are adopted, as well as implementing best management practices as shown in the California Storm Water Best Management Plan.

Temporary desiltation basins and future NPDES facilities shall be provided as shown in the Concept Water Quality Plan for the Bressi Ranch Master Plan prepared by Project Design Consultants, dated March 2002, which is consistent with the City of Carlsbad's Master Drainage and Storm Water Quality Management Plan. Depending on the topography of the site after grading, one or more of the temporary desiltation basins shown on Exhibit XIII-1 on page XIII-4 may be combined subject to approval of the City Engineer.

2. Fire

Per the Growth Management performance standard, no more than 1,500 dwelling units are allowed outside of a five-minute response time. Fire Station #2 and #5 will serve Zone 17 and provide adequate fire support to the zone.

3. Open Space

The LFMP open space performance standard requires that 15% of the total land area exclusive of environmentally constrained non-developable land must be set aside for permanent open space and must be available concurrent with development. Zone 17's build out performance standard requirement for open space is 77.7 acres of environmentally unconstrained developable land, and the Bressi Ranch Master Plan is proposing 157.1 net acres of open space. Therefore Zone 17 will have adequate open space through build out.

4. Schools

Zone 17 is within the jurisdiction of the Carlsbad Unified School District. Due to the proximity of the McClellan-Palomar Airport, no public school facility will be located within the zone. The Bressi Ranch Master Plan will annex into a Community Facilities District to pay for impacts to school facilities from residents within Zone 17.

5. Sewer Facilities

Sewer service in the City of Carlsbad is provided by three independent sewer agencies: City of Carlsbad Municipal Water District (CMWD), Leucadia County Water District and Vallecitos Water District. Zone 17 is located entirely within the CMWD service area. According to the LFMP, any sewer facility needed to serve development within the zone must be in place prior to or commensurate with development.

The western portion of Zone 17 (Planning Area 1, 2, 3, 6 and a part of 10) will sewer south on future Alicante Road offsite through Zone 10. However, if at the time Planning Areas 1, 2, 3, 6, and 10 are developed and the offsite Alicante Road sewer extension is not feasible to serve this portion of Zone 17, the developer may request an interim sewer lift station as an amendment to the city's Master Plan of Sewerage, to pump sewage east to the Carrillo Lift Station from Greenhaven Drive, El Fuerte Street and Poinsettia Lane. The temporary lift station shall be located on private property, maintained by private funds and be removed when a permanent gravity systems becomes feasible.

The eastern portion Zone 17 (Planning Areas 4, 5, 7, 8, 9, part of 10, 11, 12, 13, 14 and 16) will sewer south via 8" lines within Greenhaven Drive and El Fuerte Street to the existing 12" line serving the Rancho Carrillo properties. Exhibit XIII-2 on page XIII-8 shows the proposed sewer facilities for Bressi Ranch.

6. Water Facilities

Water service within Zone 17 is provided entirely by the Carlsbad Municipal Water District. Since the water district requires development to install domestic water and fire flow needs as conditions of approval to future development, conformance with the adopted standards will be maintained to ultimate development of the project. Exhibit XIII-3 on page XIII-9 shows the proposed water facilities for Bressi Ranch.

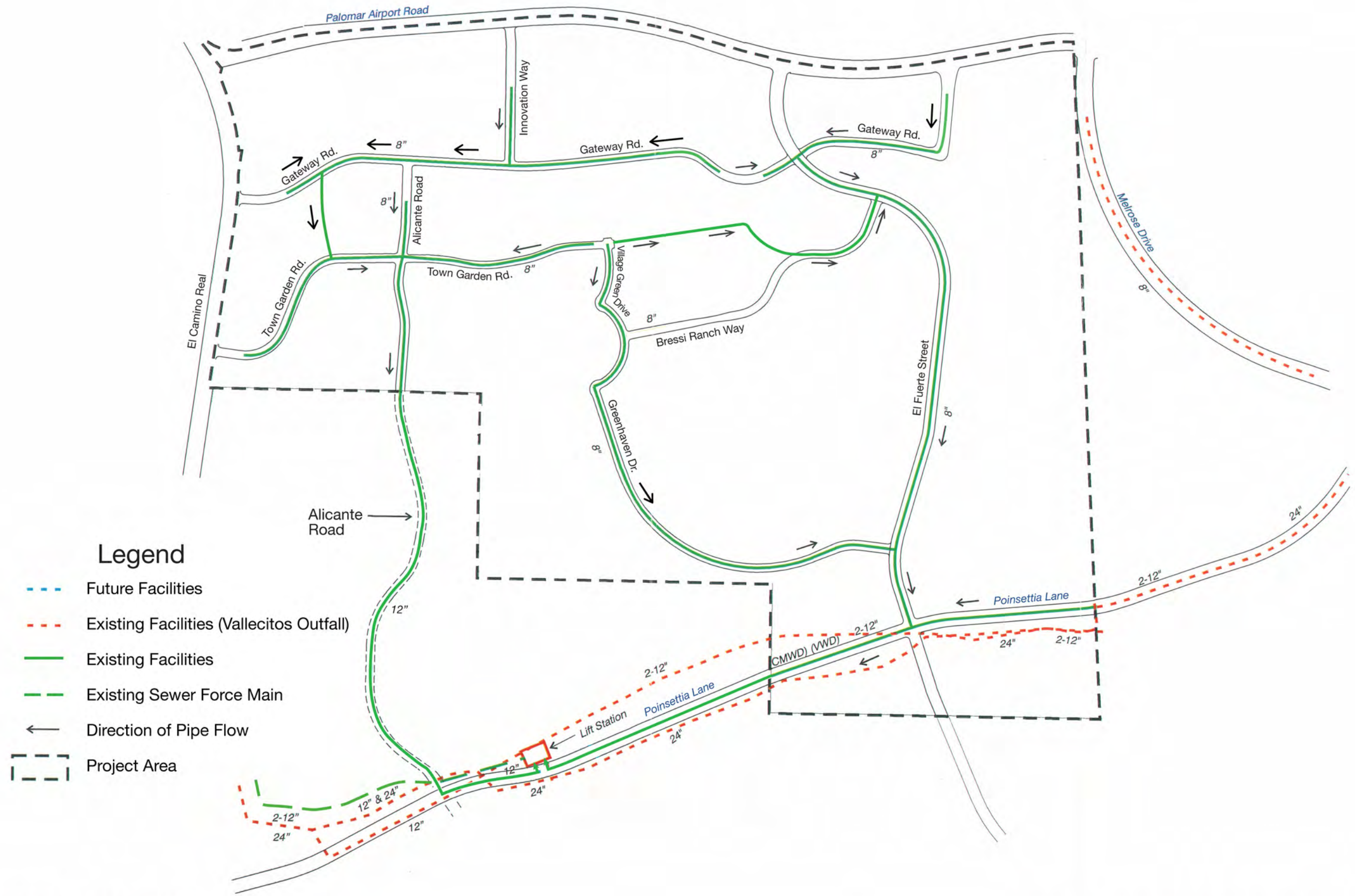
7. Recycled Water Facilities

When a source of recycled water becomes available it will serve some of the irrigation needs in the Bressi Ranch Master Plan. Development of the Master Plan will help provide portions of the system that will provide useable reclaimed water. Exhibit XIII-4 on page XIII-10 shows the proposed recycled water facilities for Bressi Ranch.



8. Reclaimed Water Pump House Screening

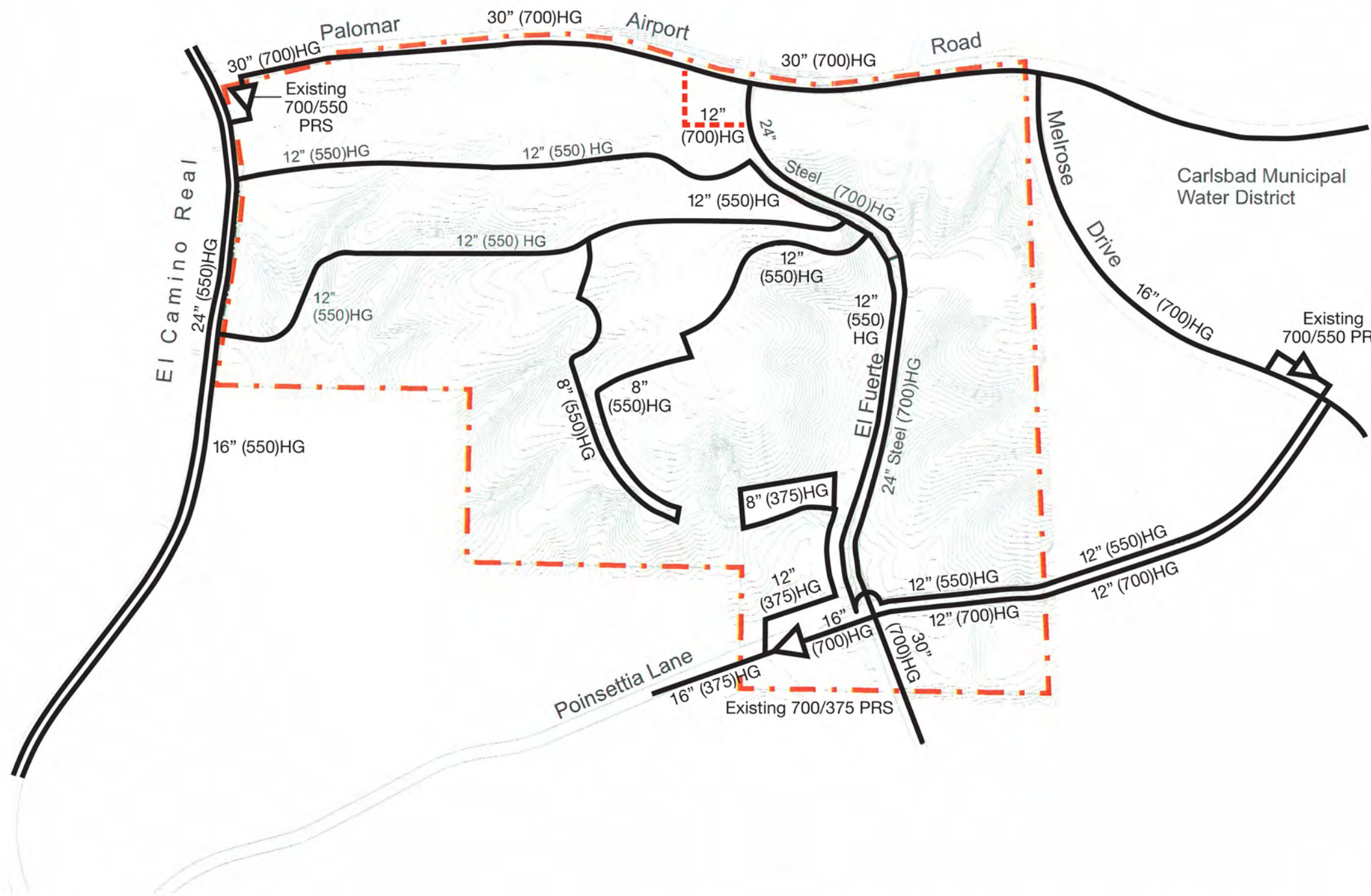
A large reclaimed water pump will be located near the intersection of Palomar Airport Road and El Camino Real in Planning Area 3 of the Bressi Ranch Master Plan. The pump station and the screening structure shall be located so that they do not do not impact line of site near the intersection.



**Legend**

- - - Future Facilities
- - - Existing Facilities (Vallecitos Outfall)
- Existing Facilities
- - - Existing Sewer Force Main
- ← Direction of Pipe Flow
- Project Area





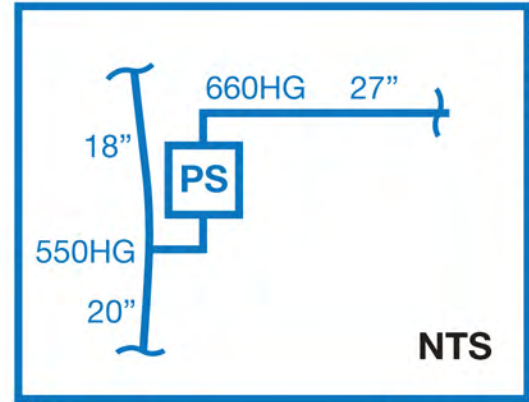
- ### Legend
-  Existing Major Water Lines
  -  Proposed Major Water Lines
  -  Pressure-Regulating Station
  -  Project Limits

## Management Zone 17

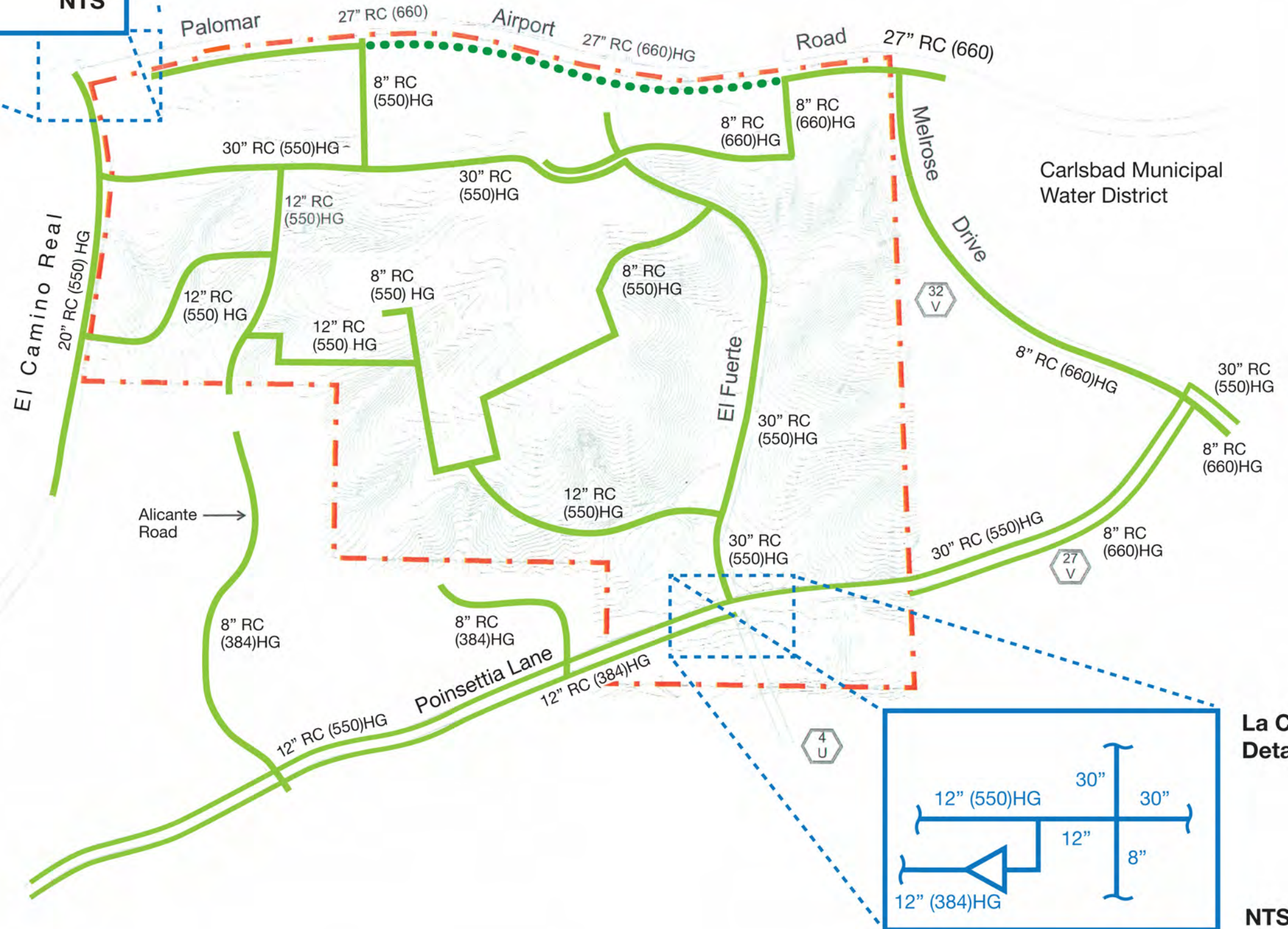
GROWTH MANAGEMENT PROGRAM - CITY OF CARLSBAD

## Existing and Proposed Major Potable Water Facilities

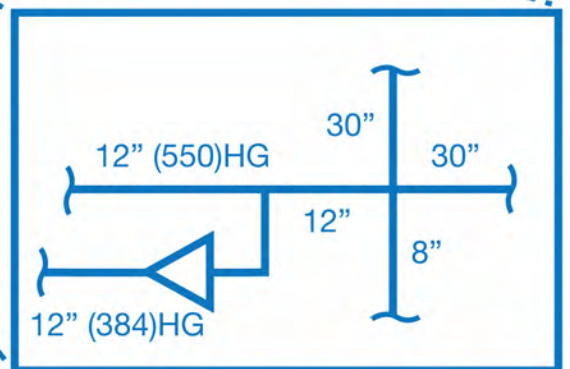




**BRESSI 550/660 PS  
DETAIL**



- Legend**
- Existing Recycled Water Line
  - ⋯ Proposed Recycled Water Line
  - Recycled Water System (RC)
  - · - · Project Limits



**La Costa 550/384 PRS  
Detail**

NTS

**XIV. MASTER PLAN AMENDMENT LOG**

**A. Planning Area Unit Count**

As discussed in Section VI.G of this Master Plan, the Master Plan provides an estimate of the approximate number of units that will be developed within each Planning Area. The exact number of units that will be developed within each Planning Area will be determined during the subsequent review of the tentative maps that divide each Planning Area into individual residential lots. A Master Plan Amendment is required with each tentative map requesting a change in the anticipated number of dwelling units stated in Exhibit XIV-1 (below) for the Planning Areas. This amendment will revise Exhibit XIV-1 to indicate exactly how many units have been approved in each Planning Area and how many remaining units exist in the Master Plan.

<b>Exhibit XIV-1 Master Plan Amendment Log</b>			
<b>PLANNING AREA</b>	<b>UNITS ESTIMATED BY MASTER PLAN</b>	<b>UNITS APPROVED BY TENTATIVE MAP</b>	<b>UNITS TO TRANSFER TO/FROM ANOTHER PLANNING AREA</b>
<b>6</b>	64	<b>63</b>	
<b>7</b>	99	<b>96</b>	
<b>8</b>	98	<b>95</b>	
<b>9</b>	72	<b>70</b>	
<b>10</b>	86	<b>84</b>	
<b>11</b>	10-25	<b>25</b>	
<b>12</b>	94	<b>91</b>	
<b>15</b>	100	<b>100</b>	
<b>16</b>	125	<b>125</b>	
<b>TOTAL*:</b>	<b>748 - 763</b>	<b>749</b>	

\* The maximum cumulative number of dwelling units permitted in Planning Areas 6, 7 8, 9, 10, 11, 12,15 and 16 is 749. In no case shall the number of dwelling units approved in an individual Planning Area exceed that allowed by its General Plan designation (see Exhibit III-1 on page III-11), nor shall the total number of residential units in the Master Plan exceed 749.

**B. Master Plan Amendments**

- MP 178 (A): Planning Commission Resolution No. 5460  
September 17, 2003
- MP 178 (B): Planning Commission Resolution No. 5501  
November 5, 2003
- MP 178 (C): Planning Commission Resolution No. 5923  
June 15, 2005  
City Council Ordinance No. NS-768  
August 2, 2005
- MP 178 (D): Planning Commission Resolution No. 6221  
January 3, 2007
- MP 178 (E): Planning Commission Resolution No. 6310  
June 20, 2007
- MP 178 (F): Planning Commission Resolution No. 6361  
December 5, 2007  
City Council Ordinance No. NS 874  
March 12, 2008
- MP 178 (G): Planning Commission Resolution No. 6531  
February 4, 2009
- MP 178 (H): Withdrawn
- MP 178 (I): Planning Commission Resolution No. 7175  
December 7, 2016  
City Council Ordinance CS-315  
February 14, 2017
- MP 178 (K): Planning Commission Resolution No. 7197  
October 5, 2016

**C. Clarifications/Interpretations**

As discussed in Section VI.H, clarifications and/or interpretations of the Bressi Ranch development standards is within the purview of the City Planner. This section provides a record of clarifications and/or interpretations by the City Planner: